

Portsmouth
Parking & Traffic Safety Committee
8:00 A.M. – June 2, 2016
City Hall – Eileen Dondero Foley City Council Chambers

ON-SITE COMMITTEE: There will be no site visit this month.

AGENDA

- I. CALL TO ORDER**
- II. ROLL CALL**
- III. ACCEPTANCE OF THE MINUTES**
- IV. FINANCIAL REPORT**
- V. PUBLIC COMMENT (15 MINUTES)**
- VI. PRESENTATIONS**
 - A. Chestnut Street Streetscape Improvement project by The Music Hall
 - B. Peirce Island Road bridge deck repair project
- VII. NEW BUSINESS:**
 - A. Valet parking license renewals, Atlantic Parking Services – 3 locations. **Sample motion – move to approve requests to renew valet parking licenses.**
 - B. Valet parking license renewals, The One Hundred Club. **Sample motion – move to approve request to renew valet parking license.**
- VIII. OLD BUSINESS/ACTION ITEMS:**
 - A. Moped, motorcycle and scooter parking. Report back on request from Marc Stettner to revise rules regarding enforcement. **Sample motion – move to approve recommended ordinance to allow moped parking on sidewalks.**
- IX. PUBLIC COMMENT**
- X. INFORMATIONAL**
 - A. 2016 Parking Utilization study data collection begins June 2nd
 - B. PS21 “Islington Street Lab” implementation update
 - C. Proposed Neighborhood Traffic Calming program

ADJOURNMENT

Unaudited

Percentage of Fiscal Year Complete 83.33%
--

Totals Thru
April 30, 2016

FY 16

Parking Meter Fees
 Parking Meter Space Rentals
 Meter In Vehicle
 Parking Garage Revenue
 Garage Passes
 Parking Validation
 Pass Reinstatement
 Vaughan St Parking Facility

 Parking Violations
 Immobilization Administration Fee
 Summons Admin Fee

 Total FY 16 Parking

FY 16 to Date:		
TOTALS	BUDGETED	% of Budget
1,902,833	1,765,500	108%
94,905	50,000	190%
80,477	50,000	161%
1,715,469	1,850,000	93%
885,875	1,000,000	89%
11,150	10,000	112%
2,565	2,000	128%
13,750	15,000	92%
637,892	700,000	91%
19,435	12,000	162%
200	5,000	4%
5,364,550	5,459,500	98%

FY 16 BUDGETED

	(3,047,195.00)	Transfer to Parking Fund
	2,412,305.00	Funds Remaining in Gen Fund
	5,459,500.00	Total Revenue

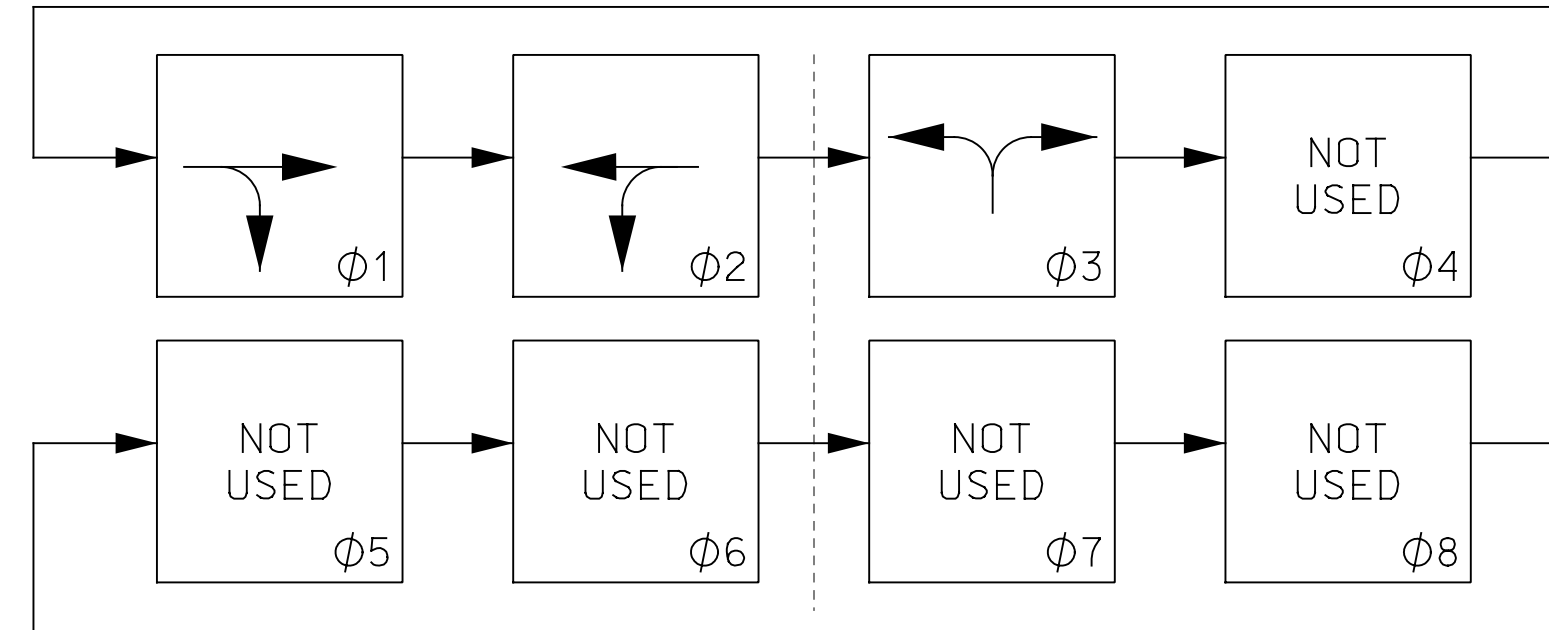
VI.A. Chestnut Street Streetscape Improvement project by The Music Hall



TRAFFIC CONTROL NOTES

1. TRAFFIC CONTROL DEVICES SHALL CONFORM TO SECTION 619 OF THE NHDOT STANDARD SPECIFICATIONS, AND THE 2009 MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), AS PUBLISHED BY THE U.S. DEPARTMENT OF TRANSPORTATION (USDOT) AND ADOPTED BY THE COMMISSIONER OF THE NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION. SIGNS SHALL ALSO CONFORM TO USDOT STANDARD HIGHWAY SIGNS AND NHDOT CONSTRUCTION SIGN STANDARDS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUPPLYING, ERECTING AND MAINTAINING PERMANENT CONSTRUCTION SIGNS AND WARNING DEVICES AS LISTED ON THE PLANS, AND SHALL ALSO BE RESPONSIBLE FOR SUPPLYING, ERECTING AND MAINTAINING ALL OPERATIONAL SIGNS AND WARNING DEVICES FOR THE PLANNED METHODS OF OPERATION IN CONFORMANCE WITH THE MUTCD.
3. THE CONTRACTOR SHALL MARK ALL HAZARDS WITHIN THE LIMITS OF THE PROJECT AND CONNECTING ROADS WITH WELL MAINTAINED SIGNS AND WARNING DEVICES. ALL SIGNS AND WARNING DEVICES SHALL BE MOVED, SUPPLEMENTED, CHANGED, OR REMOVED DURING THE PROGRESS OF THE CONSTRUCTION AS NEEDED.
4. TRAFFIC CONTROL DEVICES SHALL BE REMOVED, AND SIGNS SHALL BE COVERED OR REMOVED, WHEN THEY NO LONGER APPLY TO THE EXISTING CONDITIONS.
5. PLYWOOD SUBSTRATE FOR CONSTRUCTION SIGNS SHALL CONFORM TO SECTION 619. AND FLAT ALUMINUM SHEETS SHALL CONFORM TO SECTION 615 OF THE NHDOT STANDARD SPECIFICATIONS.
6. DETOURS INVOLVING THE ROUTING OF TRAFFIC OVER ROADS OUTSIDE THE LIMITS OF THE PROJECT SHALL BE MARKED AND MAINTAINED BY THE CONTRACTOR (UNLESS OTHERWISE NOTED). THE CONTRACTOR SHALL BE REQUIRED TO ERECT AND MAINTAIN ANY REQUIRED SIGNS AND WARNING DEVICES AT THE BEGINNING AND END OF THE WORK AND AT INTERSECTING ROADWAYS. THE LOCATION AND POSITION OF THESE SIGNS AND WARNING DEVICES SHALL BE AS APPROVED BY THE ENGINEER. THE CONTRACTOR MAY ALSO BE REQUIRED TO UNCOVER, COVER AND OTHERWISE MAINTAIN DETOUR SIGNS SUPPLIED BY OTHERS.
7. WORK ON THE PROJECT, OR ANY SEPARATE ACTIVITY THEREIN, SHALL NOT START UNTIL ALL THE REQUIRED SIGNS AND WARNING DEVICES ARE INSTALLED AND APPROVED BY THE ENGINEER.
8. SIGN LOCATIONS SHOWN ON THESE STANDARDS ARE RECOMMENDED AND MAY BE ADJUSTED AS DETERMINED BY THE ENGINEER. TYPICAL LAYOUTS SHOWN ARE NOT TO SCALE.
9. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE THE ENGINEER WITH CERTIFICATION THAT ALL THE SIGNS AND WARNING DEVICES USED ON THE PROJECT MEET THE SPECIFICATIONS.
10. THE USE OF CONSTRUCTION SIGNS AND WARNING DEVICES NOT SHOWN ON THESE STANDARDS OR MUTCD, UNLESS APPROVED BY THE ENGINEER, SHALL BE PROHIBITED.
11. ALL COSTS FOR TRAFFIC CONTROL DEVICES, INCLUDING PLACEMENT, RELOCATION AND REMOVAL OF SIGNS SHALL BE INCLUDED IN ITEM 619.1, MAINTENANCE OF TRAFFIC.
12. THE CONTRACTOR SHALL MAINTAIN SAFE, CONTINUOUS ACCESS TO ALL PROPERTIES ADJACENT TO THE PROJECT LOCATION.

NEMA STD 8Φ CONTROLLER

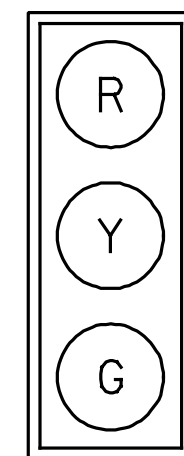


SIGNAL PHASING			
	Φ1	Φ2	Φ3
TIMING IN SECONDS	↖	↗	↔
INITIAL INTERVAL	8	8	5
VEHICLE EXTENSION	3	3	3
MAX. I *	20	10	5
MAX. II *	10	45	5
YELLOW	4	4	4
ALL RED	22	22	22
RECALL	OFF	OFF	OFF
DETECTOR MEMORY	L	L	L
FLASH	RED	RED	RED

* MAX. I = 0600 TO 1200
 * MAX. II = 1200 TO 0600

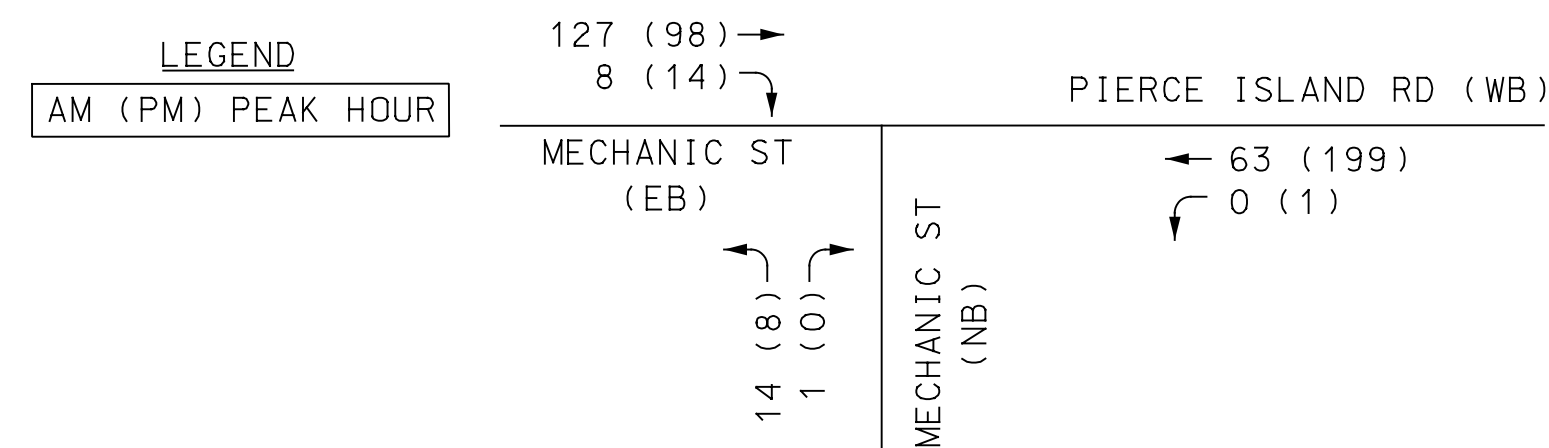
SIGNAL HEADS

ALL LENSES ARE 12" LED WITH 5" LOUVERED BACK PLATE



1, 2, 3

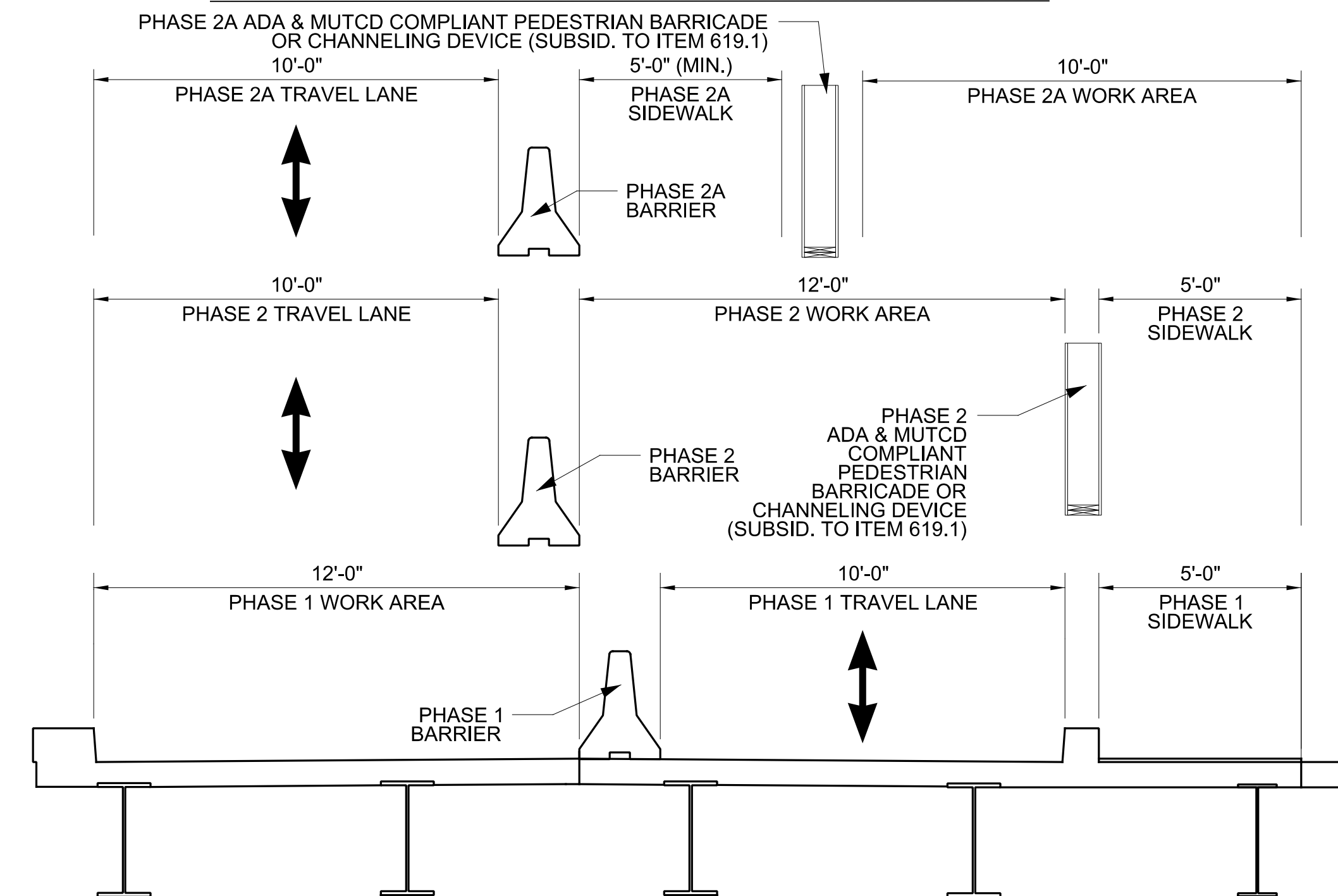
AM, PM PEAK TRAFFIC VOLUMES



CONSTRUCTION SIGNS AND WARNING DEVICES (ITEM 619.1)

TYPE	DESCRIPTION	SIZE WxH	SO. FT.	NO REQ.	TOTAL AREA	POST	COLOR
G20-2	END ROAD WORK	36" x 18"	4.5	2	9	1 POST PER SIGN	B/O
R10-6	STOP HERE ON RED	24" x 36"	6	3	18	1 POST PER SIGN	B/W
R10-7a	DO NOT BLOCK DRIVEWAY	24" x 30"	5	1	5	MOUNT WITH W3-3 ON EAST SIDE OF BRIDGE	B/W
R10-11a	NO TURN ON RED	30" x 36"	7.5	2	15	MOUNT ON TEMPORARY MAST ARM & WITH R10-6	B/W
W3-3	(Traffic Light Symbol)	36" x 36"	9	3	27	1 POST PER SIGN	B/O
W20-1a	ROAD WORK AHEAD	36" x 36"	9	3	27	1 POST PER SIGN	B/O
W20-4	ONE LANE ROAD AHEAD	36" x 36"	9	3	27	1 POST PER SIGN	B/O

TYPICAL SECTION- FACING WEST



REV	DESCRIPTION	DATE
APRIL 2016	DESIGN BY: AGB	
	DRAWN BY: JFMS	
	CHKD. BY: SBH	
	SCALE: AS SHOWN	

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 100 International Drive - Suite 360
 Portsmouth, NH 03801
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 www.hoyletanner.com

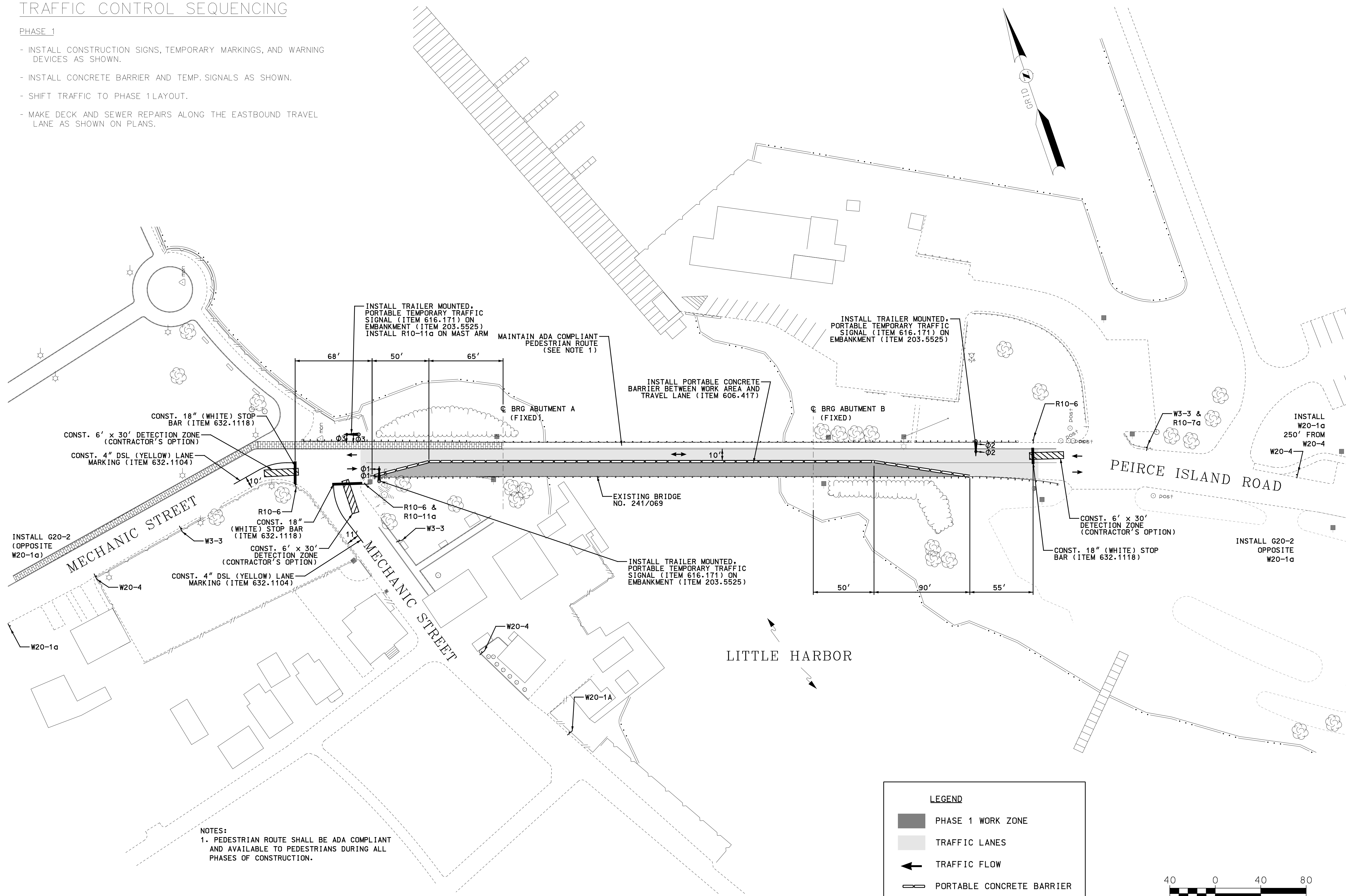
CITY OF PORTSMOUTH
 PORTSMOUTH, NEW HAMPSHIRE
 PEIRCE ISLAND BRIDGE OVER LITTLE HARBOR
 CONSTRUCTION PHASING AND SIGNALIZATION NOTES

PROJECT NO.:	905105
FILE NAME:	905105trfplans
MODEL NAME:	TCP Notes
SHEET NO.	8
SHEET 8 OF 11	

TRAFFIC CONTROL SEQUENCING

PHASE 1

- INSTALL CONSTRUCTION SIGNS, TEMPORARY MARKINGS, AND WARNING DEVICES AS SHOWN.
- INSTALL CONCRETE BARRIER AND TEMP. SIGNALS AS SHOWN.
- SHIFT TRAFFIC TO PHASE 1 LAYOUT.
- MAKE DECK AND SEWER REPAIRS ALONG THE EASTBOUND TRAVEL LANE AS SHOWN ON PLANS.



NOTES:
 1. PEDESTRIAN ROUTE SHALL BE ADA COMPLIANT AND AVAILABLE TO PEDESTRIANS DURING ALL PHASES OF CONSTRUCTION.

LEGEND

- PHASE 1 WORK ZONE
- TRAFFIC LANES
- TRAFFIC FLOW
- PORTABLE CONCRETE BARRIER



REV	DESCRIPTION	DATE

APRIL 2016	DESIGN BY: AGB	DATE
	DRAWN BY: JFMS	
	CHKD. BY: SBH	
	SCALE: AS SHOWN	

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 PORTSMOUTH, NEW HAMPSHIRE
 PEIRCE ISLAND BRIDGE OVER LITTLE HARBOR
 PHASE 1 TRAFFIC CONTROL PLAN

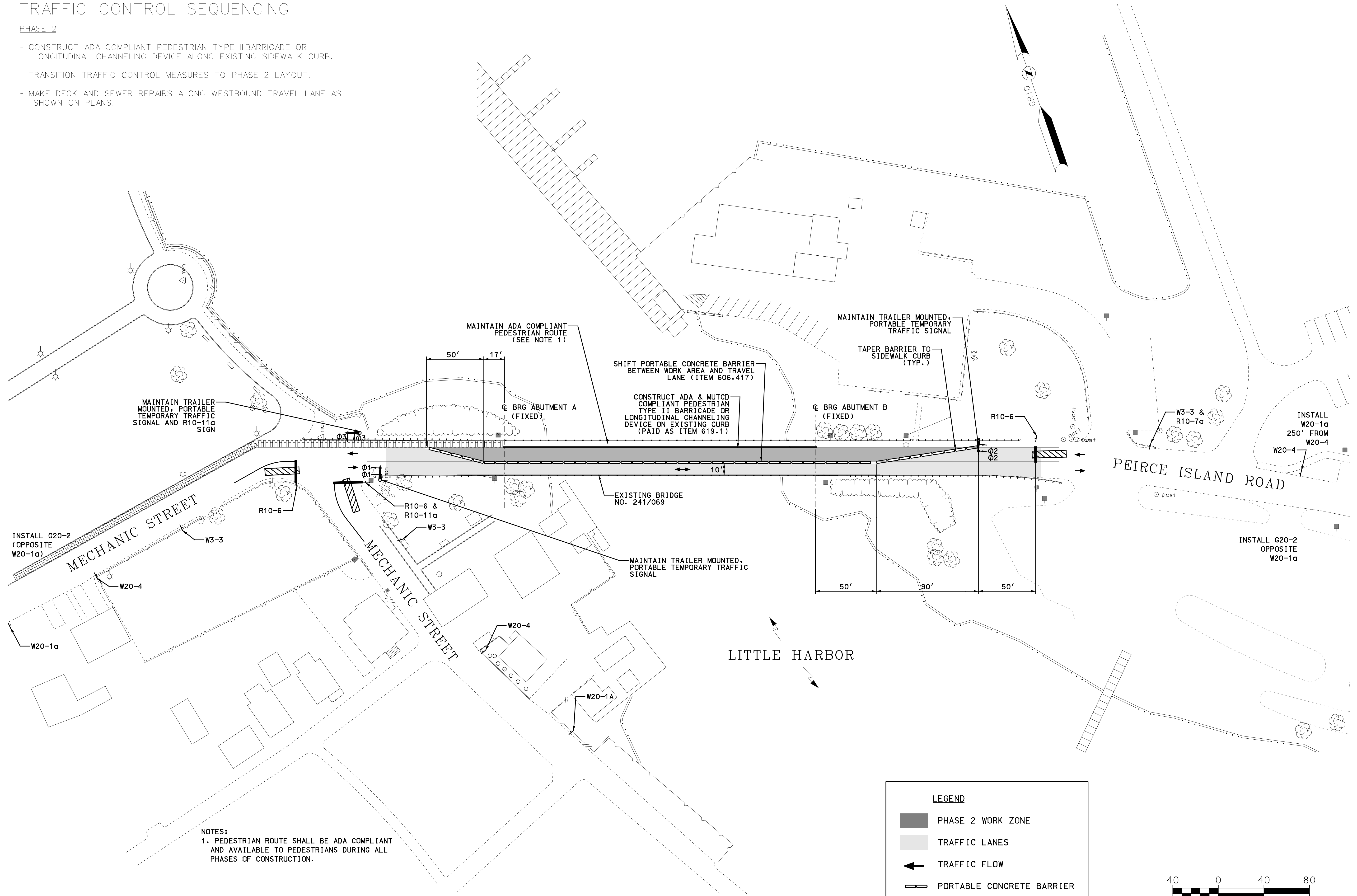
PROJECT NO.: 905105
 FILE NAME: 905105trfplans
 MODEL NAME: TCP-Phase 1
 SHEET NO.
9
 SHEET 9 OF 11

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TRAFFIC CONTROL SEQUENCING

PHASE 2

- CONSTRUCT ADA COMPLIANT PEDESTRIAN TYPE II BARRICADE OR LONGITUDINAL CHANNELING DEVICE ALONG EXISTING SIDEWALK CURB.
- TRANSITION TRAFFIC CONTROL MEASURES TO PHASE 2 LAYOUT.
- MAKE DECK AND SEWER REPAIRS ALONG WESTBOUND TRAVEL LANE AS SHOWN ON PLANS.



NOTES:
1. PEDESTRIAN ROUTE SHALL BE ADA COMPLIANT AND AVAILABLE TO PEDESTRIANS DURING ALL PHASES OF CONSTRUCTION.

LEGEND

- PHASE 2 WORK ZONE
- TRAFFIC LANES
- TRAFFIC FLOW
- PORTABLE CONCRETE BARRIER



REV	DESCRIPTION	DATE
APRIL 2016	DESIGN BY: AGB	
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	CHKD. BY: SBH	
	SCALE: AS SHOWN	

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 PEIRCE ISLAND BRIDGE OVER LITTLE HARBOR
 PHASE 2 TRAFFIC CONTROL PLAN

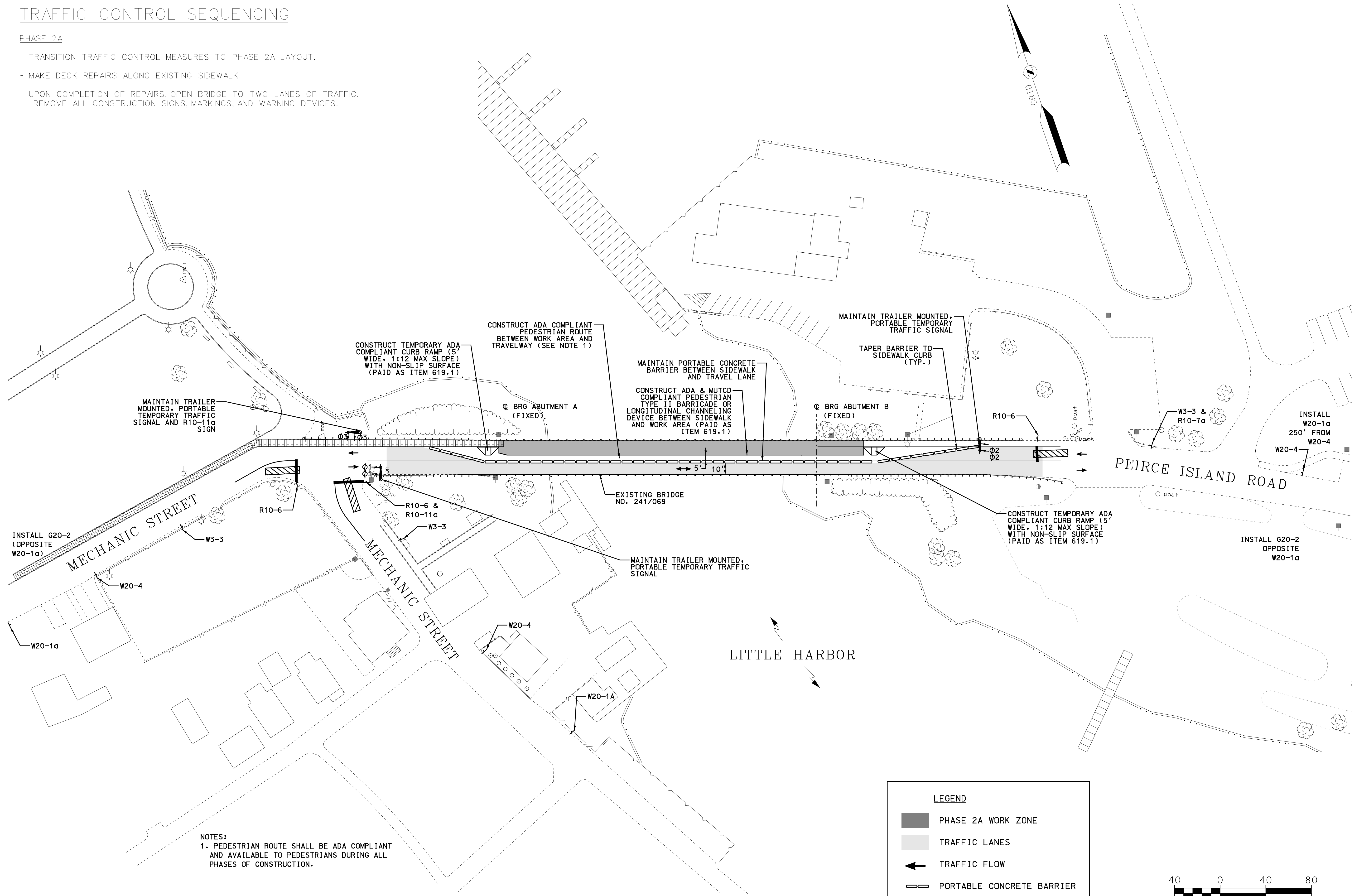
PROJECT NO.: 905105
 FILE NAME: 905105trfplans
 MODEL NAME: TCP-Phase 2
 SHEET NO.
10
 SHEET 10 OF 11

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TRAFFIC CONTROL SEQUENCING

PHASE 2A

- TRANSITION TRAFFIC CONTROL MEASURES TO PHASE 2A LAYOUT.
- MAKE DECK REPAIRS ALONG EXISTING SIDEWALK.
- UPON COMPLETION OF REPAIRS, OPEN BRIDGE TO TWO LANES OF TRAFFIC.
REMOVE ALL CONSTRUCTION SIGNS, MARKINGS, AND WARNING DEVICES.



REV	DESCRIPTION	DATE
APRIL 2016	DESIGN BY: AGB	
	DRAWN BY: JFMS	
	CHKD. BY: SBH	
	SCALE: AS SHOWN	

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CITY OF PORTSMOUTH
 PORTSMOUTH, NEW HAMPSHIRE
 PEIRCE ISLAND BRIDGE OVER LITTLE HARBOR
 PHASE 2A TRAFFIC CONTROL PLAN

PROJECT NO.:	905105
FILE NAME:	905105trfplans
MODEL NAME:	TCP-Phase 2A
SHEET NO.	11
SHEET 11 OF 11	

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**Proposal for Valet Operations
Thursday, June 2, 2016**

City Council Members,

Atlantic Parking Services are requesting the permission to continue operating a Valet Parking Service in the parking spaces provided on Bow Street.

Operation Overview

Atlantic Parking Services will continue to manage the valet operations. The loading zone will be used as the Valet Loading Zone where motorists will drop off and pick up their cars from the valets only – vehicles will not be parked in these spaces. The Valet Service will be active year round from 5:00PM until 12:00AM any given evening.

Patrons of the Wellington Room and Surf Restaurant will be directed to the Valet Loading Zone by three signs placed within the loading zone, which will direct patrons to the loading zone. On arrival to the loading zone, motorists will be given a Valet ticket in exchange for their car. The Valet will then drive the vehicle to the One Harbour Garage where the vehicles will be stored and monitored by the valet staff.

Once the customers have finished their visit, they will come back to the Valet Loading Zone, where the patron will receive their vehicle.

Please see diagram attached.

Valet Routes

When cars are dropped off, the valet will pull out of the loading zone onto Bow Street, continue up the hill on Chapel Street. Valets will continue on Chapel Street until turning left on to State Street and onto the parking garage which is located at 1 Harbour Place. When bringing the vehicles back from the garage, vehicles will continue on to Daniel Street, continue to intersection of Penhallow and Daniel street. Vehicles will take a right turn and continue on Penhallow until intersection with Bow street and turn right and pull into valet parking loading zone on Bow Street. These routes are intended to maintain optimal traffic flow and safety in or around the Bow Street loading zone.

Permissions Needed

- Permission to set up Three A- Frame Signs in or next to loading zone during valet operating hours.
- Permission to use the three parking spaces that comprise the valet loading zone on Bow Street nightly from 5:00PM through 12:00AM.
- Permission to use a 8x8 pop up tent during inclement weather
- Permission to use a Key Storage Podium in the loading zone



Orange Line = Vehicle return route
Blue Line = Vehicle route to garage
Green Pentagon = 1 Harbour Garage
Yellow Parking Spaces = Loading zone

Sincerely,

A handwritten signature in cursive script, appearing to read "Brian Slovski".

Brian Slovski
Atlantic Parking Services, LLC



Proposal for Valet Operations

Dear Parking Council-

Atlantic Parking Services is requesting the permission to continue operating a Valet Parking Service in the Loading Zone on Pleasant Street at the intersection of Pleasant and State Streets. See Figure 1.

Operation Overview

Atlantic Parking Services will manage the valet operations. The current loading zone will be used as the Valet Loading Zone where motorists will drop off and pick up their cars from the valets only – vehicles will not be parked in these spaces. The Valet Service will be active year round from 5:00PM until 12:00AM any given evening. Primarily the service will be active from Thursday through Saturday evenings from 5:00PM through 11:00PM

Patrons will be directed to the Valet Loading Zone by three signs placed within or next to the loading zone, which will direct patrons to the loading zone. On arrival to the loading zone, motorists will be given a Valet ticket in exchange for their car. The Valet will then drive the vehicle to the Citizens Bank Parking Lot where the vehicles will be stored and monitored by the valet staff. See figure 1.

Once the customers have finished their visit, they will come back to the Valet Loading Zone, where the patron will receive their vehicle.

Valet Routes

When cars are dropped off, the valet will pull out of the loading zone on Pleasant Street and drive directly to the Citizens bank parking lot. When bringing the vehicles back from the Citizens Bank lot, cars will be turn left on Court street, turn right on Court Place, turn right onto State Street and then right onto Pleasant street. These routes are intended to maintain optimal traffic flow in or around the Pleasant Street loading zone. See figure 1.

Permissions Needed

Permission to set up Three A- Frame Signs in or next to loading zone during valet operating hours.
Permission to use the loading zone on Pleasant Street from 5:00PM through 12:00AM. See figure 1.
Permission to use a 8x8 pop up tent during inclement weather
Permission to use a Key Storage Podium in the loading zone

Below is a Map of Downtown Portsmouth with the Proposed Loading Zone, Parking Lot and Vehicle Route



Figure 1

Pink Highlighter = Valet Route

Red Dot Highlighter = Valet Loading Zone (Pleasant St. Loading zone)

Yellow Dot Highlighter = Valet Parking Lot (Citizens Bank)

Sincerely,
Brian Slovenski
APS Representative



Proposal for Valet Operations

Dear Parking Council-

Atlantic Parking Services is requesting the renewal of permission to operate a Valet Parking Service in the Loading Zone located at 96 State Street.

Operation Overview

Atlantic Parking Services will manage the valet operations. The loading zone will be used as the valet loading zone where motorists will drop off and pick up their cars from the valets only – vehicles will not be parked in this pick up and drop off zone. The valet service will be active year round from 5:00PM until 12:00AM any given evening.

Patrons will be directed to the valet loading zone by two signs placed within the loading zone, which will direct patrons to the loading zone. On arrival to the loading zone, motorists will be given a valet ticket in exchange for their car. The Valet will then drive the vehicle to the One Harbour Parking Garage where the vehicles will be stored and monitored by the valet staff.

Once the customers have finished their visit, they will come back to the Valet Loading Zone, where the patron will receive their vehicle.

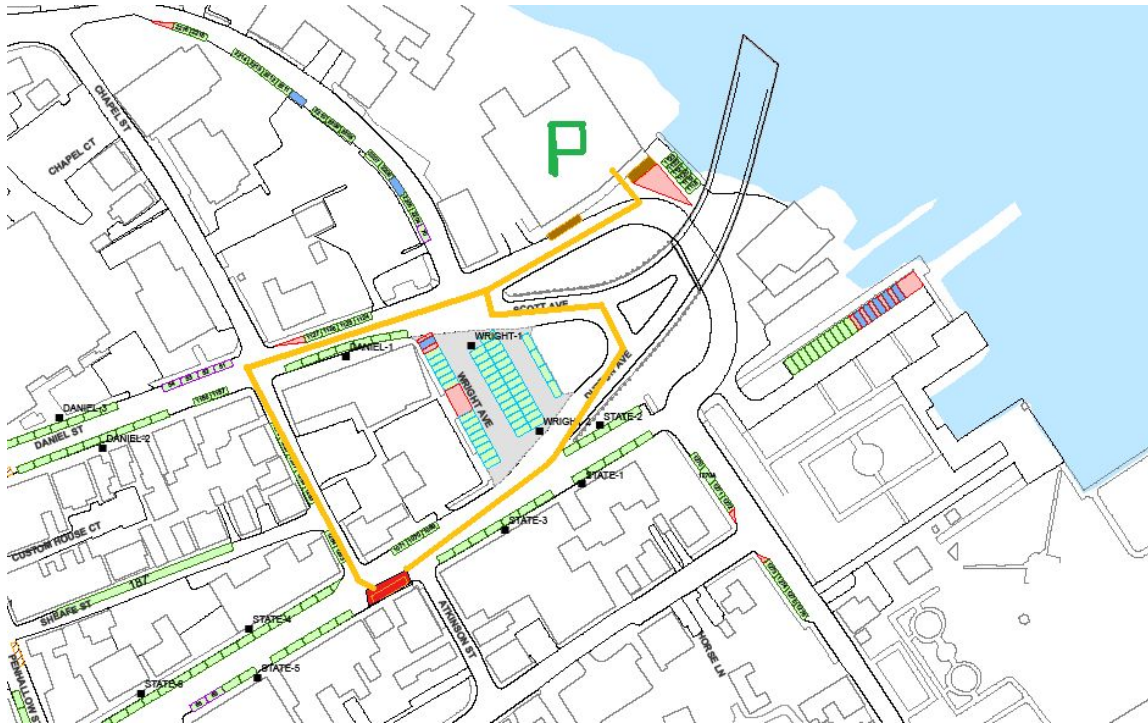
Valet Routes

When cars are dropped off, the valet will pull out of the loading zone onto State Street, continue onto Dutton Ave, then onto Scott Ave. The valet will then turn right onto Harbour Place Street and then enter the parking garage at the bottom of the hill. When bringing the vehicles back from the One Harbour Garage, cars will exit the garage onto Harbour Place Street and continue up the hill onto Daniel Street. Vehicles will then take a left hand turn on to Chapel Street, continue and turn left on to State Street and pull into the valet loading zone in front of 96 State Street. These routes are intended to maintain optimal traffic flow and safety in or around the State Street loading zone. Please see exhibit A.

Permissions Needed

- Permission to set up Two A- Frame Signs in or next to loading zone during valet operating hours.
- Permission to use the loading zone on State Street, seven evenings per week from 5:00PM through 12:00AM.
- Permission to use a 6x6 pop up tent during inclement weather
- Permission to use a Key Storage Podium in the loading zone

Exhibit A



= One Harbour Parking Garage



= State Street Loading Zone



= Valet vehicle route

Sincerely,
 Brian Slovenski
 Atlantic Parking Services, LLC

THE ONEHUNDRED CLUB

100 Market Street Suite 500, Portsmouth NH
603-766-4100

April 27th, 2016

To whom it may concern,

Please find this letter as The One Hundred Club Concepts formal request for renewal of the valet license through the City of Portsmouth, NH.

Best regards,

A handwritten signature in black ink, appearing to read "Dana Wergen". The signature is fluid and cursive, with the first name "Dana" being more prominent than the last name "Wergen".

Dana Wergen

Managing Director

One Hundred Club Concepts

dana@onehundredclub.com

City of
Portsmouth
Department of Public Works



MEMORANDUM

TO: John P. Bohenko, City Manager

FROM: Eric Eby, P.E., Parking and Transportation Engineer

DATE: May 25, 2016

SUBJECT: Recommendation – Moped and motorcycle parking

In response to concerns from a resident regarding the ability to park mopeds and similarly sized motorcycles on City sidewalks, staff has researched the legal requirements, including the current City ordinances and policies, as well as current State RSAs relating to the issue. Currently, as a matter of policy, the City allows mopeds to park for free on sidewalks and in the Hanover garage, as long as they do not impede pedestrian access. Motorcycles, regardless of their size, are not allowed to park on sidewalks, but are allowed to park multiple units side by side in on-street parking spaces, with only one of the motorcycles required to purchase a pay and display sticker.

A review of the State RSAs indicates that motor vehicles are not allowed to park on sidewalks, and mopeds and motorcycles are considered motor vehicles. However, State RSA 265:70 permits individual cities to create local ordinances allowing exceptions to this statute in the compact areas of the City. The downtown area of Portsmouth qualifies as a compact area eligible for a local ordinance on parking. There is currently no City ordinance allowing mopeds to park on sidewalks, or for just one motorcycle to purchase a pay and display sticker when multiple bikes are parked in a single on-street parking space. Only parking enforcement policies allow these practices.

Mopeds registered in New Hampshire receive a license plate with the word Moped on it. Likewise, motorcycles registered in New Hampshire receive a license plate with the word Motorcycle on it. However, mopeds and motorcycles registered in Maine receive license plates that do not distinguish between the types of vehicle.

It is desirable to encourage the use of smaller, more efficient vehicles such as mopeds and motorcycles, as they take up less space for parking and help to cut down on congestion on the downtown streets. **It is therefore recommended that an ordinance be adopted that will allow mopeds to park on sidewalks, provided they do not impede the normal and reasonable movement of pedestrian or other traffic, if they display a sticker obtained from the City certifying that they comply with the state's legal description of a moped.** Legally, mopeds cannot exceed 30 mph and their engine size cannot exceed 50 cubic centimeters. This would allow owners of mopeds registered in Maine to be able to park on sidewalks in Portsmouth. The sticker could be obtained from the Parking Clerk's office for a nominal fee to cover the cost of administering the program.

To formalize the policy of requiring only one motorcycle to purchase a pay and display sticker when parking in a group, staff recommends that a separate ordinance be adopted to codify this practice.

If the PTS Committee desires to look at other solutions for moped and motorcycle parking, another option to consider is to designate specific existing on-street parking spaces for the exclusive use of mopeds and motorcycles. These spaces could be restriped to allow 4 or 5 mopeds to use a single vehicle space. Under this option, there would be no need to issue stickers to mopeds. Rather, a discounted parking fee could be applied to mopeds and motorcycles. Other cities, such as Boston, are providing this amenity for motorcycles and mopeds in an effort to encourage and accommodate their use. Typically, one or two on-street spaces on each block are designated as moped and motorcycle spaces. If this option is desired, it is recommended that staff report back with suggested locations for the on-street moped and motorcycle parking spaces. This option would allow more room on sidewalks for pedestrians and other streetscape items. An example of an on-street parking space converted to motorcycle and moped parking use is shown below:



Marc Stettner
91 Fairview Ave
Portsmouth NH 03801

5/24/16



For consideration at the June 2, 2016 Meeting

Subject: Dedicated Motorcycle, Moped and Scooter Parking

Dear Parking and Traffic Committee,

I am requesting that a pilot program be instituted for this year to convert 4-6 parking spots in the middle of the downtown (north end of Pleasant Street) area into dedicated motorcycle, moped, scooter and motorcycle parking. This is a solution to some of the parking problems we face in Portsmouth NH. We should follow the lead of Boston and other cities with parking and traffic congestion by and encouraging this type of high density parking. **Please pass a motion at this meeting to direct the transportation department to make the changes by August.** I am also requesting that while the pilot program is in place that this parking is free. It can be seasonal parking from March 1st thru November 30th which corresponds to the majority of seasonal use it would most likely see. After a year of data is gathered the Committee can evaluate this pilot parking and determine what should be done (expand, cancel fees, etc.). There does not need to be any pavement/blacktop changes made since motorcycles have parked there for years with no effects on the pavement/blacktop.

I will also be submitting this letter to the City Council for their input.

Please see the attached article

Sincerely,

A handwritten signature in black ink, appearing to read "Marc Stettner", with a long horizontal flourish extending to the right.

Marc Stettner



Sep 7, 2010

Boston Launches Program With Dedicated Motorcycle And Scooter On-Street Parking Spaces

© 2016, Roadracing World Publishing, Inc. From a [press release](#) issued



MOTOR SCOOTER COMMUTERS EARN EXCLUSIVE PARKING SPACES IN BOSTON

Vespa Congratulates the City of Boston for Sustainability Effort

NEW YORK (Sept. 7, 2010) Metropolitan traffic congestion is an ever-growing concern that perplexes most U.S. cities, frustrating urban planners and residents alike. Increasing the use of two-wheel transportation is a simple alternative to reducing traffic congestion that urban commuters face daily across America.

For the growing number of residents living in metro Boston looking for the same solution to their in-town transportation challenges, switching to motor scooters is a very appealing option. However, with the switch, urban commuters sometimes find it difficult to locate designated parking areas, which prompted Boston to implement a motor scooter parking program modeled after successful efforts in other major metropolitan cities.

On August 26, the city of Boston began executing a motor scooter and motorcycle on-street parking pilot program to better accommodate two-wheel commuting in the city. Scooter enthusiasts will now find 40 spaces designated for scooter and motorcycle parking throughout the Back Bay area. The spaces are metered and pro-rated with 25 cents buying one hour instead of the standard 15 minutes. For commuters, the meters will be on from 8 a.m. to 8 p.m., and off at nights. Additionally, the parking spots do not have a time limit, eliminating the need to feed the meter.

"I'd like to congratulate Mayor Thomas Menino on his decision to execute a two-wheel parking program for the city of Boston," said Paolo Timoni, president and CEO of Piaggio Group Americas, importers of Vespa motor scooters. "With better fuel economy than hybrid cars, scooters are not only a viable solution when it comes to reducing traffic congestion, but equally effective at reducing America's oil consumption and protecting our environment. With this new initiative, therefore, Mayor Thomas Menino has also taken a concrete step to help Bostonians to reduce their oil consumption and carbon footprint. I can only hope that more cities will follow Boston's example and offer similar programs for two-wheel commuters."

In 2007 Piaggio Group Americas conducted a traffic model study in New York City in response to the growing concerns about traffic congestion. The results showed by shifting the daytime vehicle mix to 80 percent cars / 20 percent scooters, the following would result annually:

* A total decrease in delay of more than 4.6 million hours per year which translates to time savings of nearly 100 working hours per person;

* Reduction in carbon dioxide (CO₂) emissions by more than 26,000 tons (52,000,000-pounds) per year;

* A decrease in fuel consumption by more than 2.5 million gallons per year

* A total savings for New York City of more than \$122 million per year in fuel and labor productivity.

In addition to Boston, many other US metropolitan cities already have programs and incentives for two-wheel commuters. San Francisco has more than 1,000 designated parking spaces and areas for motor scooters, and several other cities have orchestrated motor scooter commuter days, including Atlanta and Philadelphia. On a national level, the annual Ride to Work Day (set for June 20, 2011; see more at www.ridetowork.org) continues to raise the visibility of two-wheel commuting and demonstrates how motor scooters and motorcycles make parking easier and help traffic flow.

For more information about the benefits of two-wheel transportation, visit www.vespau.com and click on the Vespanomics section.

About The Piaggio Group

Established in 1884 by Rinaldo Piaggio, and based out of Pontedera in Pisa, Italy; the Piaggio Group is one of the world's top manufacturers of two-wheel motor vehicles. With more than 7,000 employees, an annual production of more than 610,000 vehicles in 2009, five R&D centers, seven production facilities in Europe and Asia and operations in more than 50 countries - the Piaggio Group has a consolidated leadership in the European two-wheeler market. The company produces scooters, motorcycles and mopeds in the 50cc to 1,200cc displacement range, marketed under the Piaggio, Vespa, Gilera, Derbi, Aprilia, Scarabeo and Moto Guzzi brands. The Group also manufactures three- and four-wheel light transportation vehicles for the Ape, Porter and Quargo ranges.