

Ross Engineering, LLC
Civil / Structural Engineering

650 Islington Street, 2nd Floor
Portsmouth, NH 03801

603-433-7560
alexross@comcast.net

238 Austin St
Subdivision Application
Project Description

June 22, 2026

This subdivision application is for dividing a single “S” shaped parcel with double frontage on Austin Street and Coffins Court into two separate lots, one on Austin St and one on Coffins Ct. Tax Map 135, Lot 61 is a fully developed 6,384 SF parcel that includes an existing house and shed on Austin St and a large abandoned and dilapidated barn on Coffins Ct. A new lot line is proposed at a point between Austin St and Coffins Ct where the property necks down to create two lots.

The existing house and shed on the proposed Austin St lot are proposed to be demolished and a new house with attached garage is proposed in their place. A paver driveway is proposed next to the garage to allow for 2 parking spaces. Existing overhead utility and underground sewer and water lines are connected to the existing house. These existing connections will be utilized to the extent possible for the new house. Contractor to coordinate with DPW and Utility Companies to ensure all proper connections are made.

The barn on the proposed Coffins Ct lot is proposed to be demolished and a new house with attached garage is proposed in its place. An asphalt driveway is proposed next to the garage to allow for 2 parking spaces. There is a 6” PVC sewer line in Coffins Ct that has a cleanout on the parcel. A new sewer line is proposed to connect the house to this cleanout. A new water line is proposed to connect to the 2” Copper Water Main in Coffins Ct. New overhead utilities are proposed to connect to the Utility Pole on the north side of Coffins Ct. Contractor to coordinate with DPW and Utility Companies to ensure all proper connections are made.

Variances for each lot were necessary and obtained from the Portsmouth Zoning Board of Adjustment at its meeting on February 18, 2026. Variances obtained are listed below.

Proposed Austin St Lot

- A) *Allow a frontage of 49.75’ where 70’ is required.*
- B) *Allow an 8.5’ right side yard where 10’ is required.*

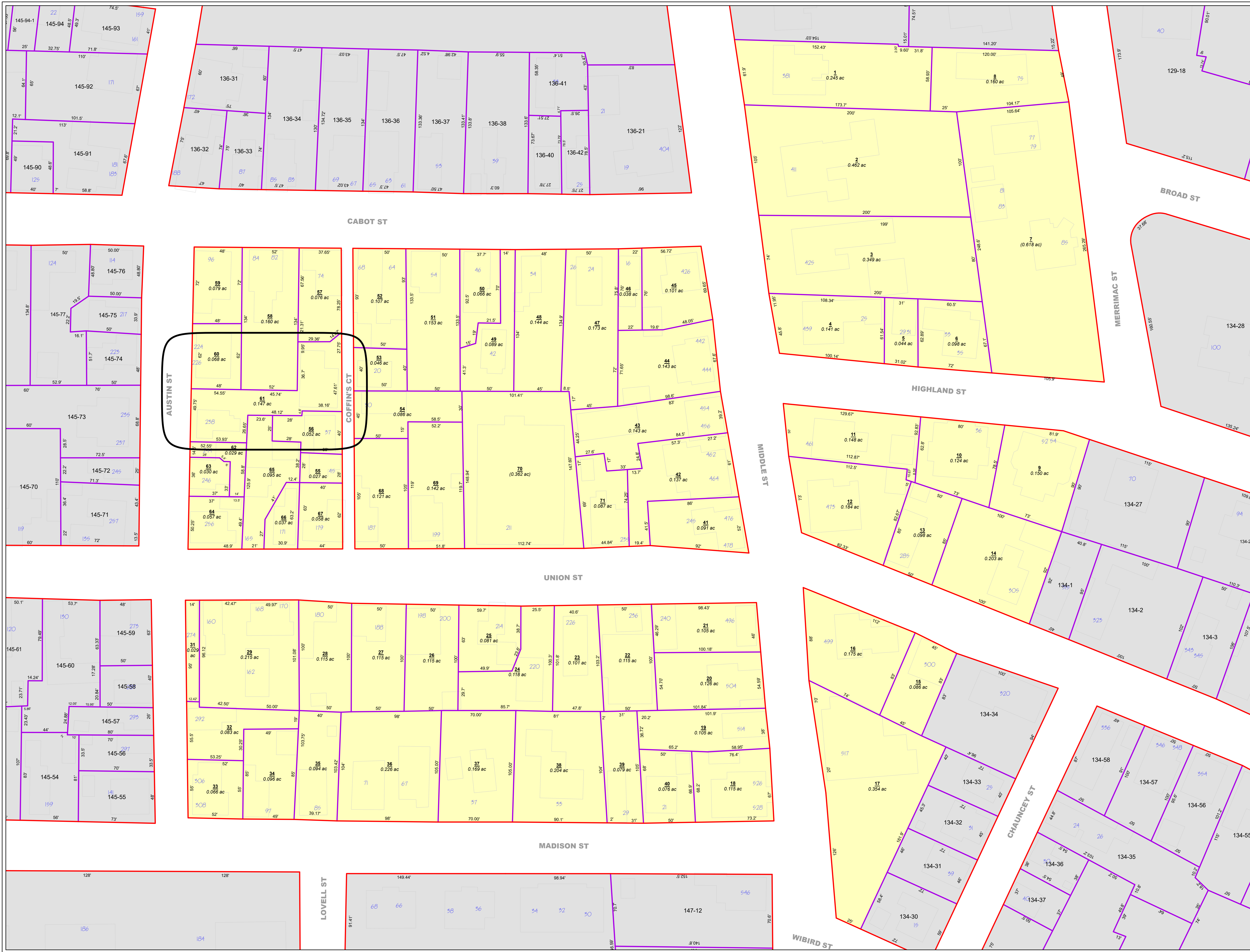
Proposed Coffins Ct Lot

- A) *Allow a 2,884 SF Lot Area where 3,500 SF is required.*
- B) *Allow a 2,884 SF Lot Area per dwelling unit where 3,500 SF is required.*
- C) *Allow a 5.5’ side yard where 10’ is required.*
- D) *Allow an 18’ rear yard where 20’ is required.*

The existing site is fully developed, and the end result of all the improvements will be two new lots without two frontages and two new structures which offer an improvement over the existing.

Sincerely,

Alex Ross, P.E.

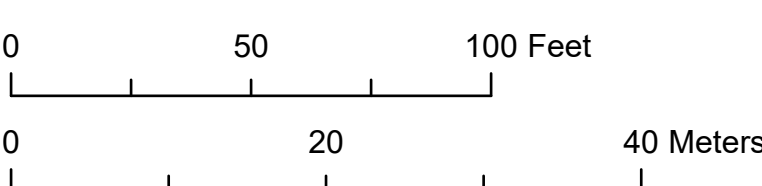


Partial Legend
 See the cover sheet for the complete legend.

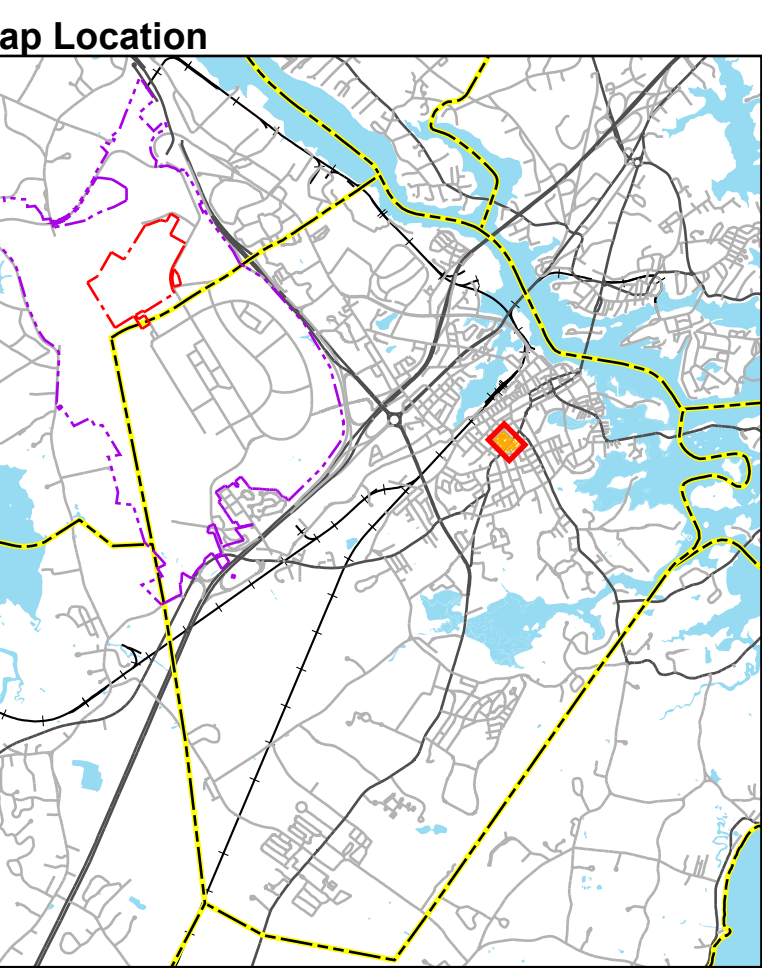
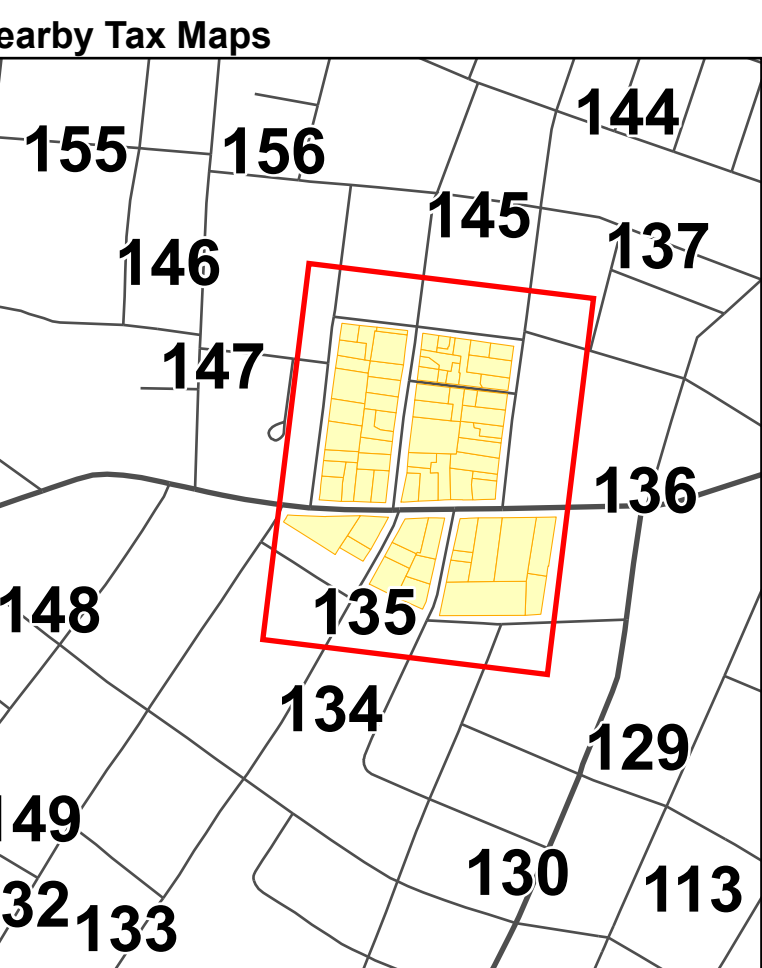
- 7-5A** Lot or lot-unit number
- 2.56 ac** Parcel area in acres (ac) or square feet (sf)
- Address number**
- 233-137** Parcel number from a neighboring map
- 68'** Parcel line dimension
- Street name**

SIMS AVE
 Street name

- Parcel/Parcel boundary
- Parcel/ROW boundary
- Water boundary
- Structure (1994 data)
- Parcel covered by this map
- Parcel from a neighboring map (see other map for current status)



This map is for assessment purposes only. It is not intended for legal description or conveyance. Parcels are mapped as of April 1. Building footprints are 2006 data and may not represent current structures. Streets appearing on this map may be paper (unbuilt) streets. Lot numbers take precedence over address numbers. Address numbers shown on this map may not represent posted or legal addresses.



Portsmouth, New Hampshire
 2025
Tax Map 135

Ross Engineering, LLC
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Photo 1 – View of the front of the existing house on Austin St looking to the south. (9/3/2025)



Photo 2 – View of rear of the existing house on Austin St looking to the north. (9/18/2025)

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**Photo 3 – View of existing shed on Austin St property looking to the south
(9/3/2025)**



**Photo 4 – View of existing barn on Coffins Ct looking to the northeast.
(9/3/2025)**

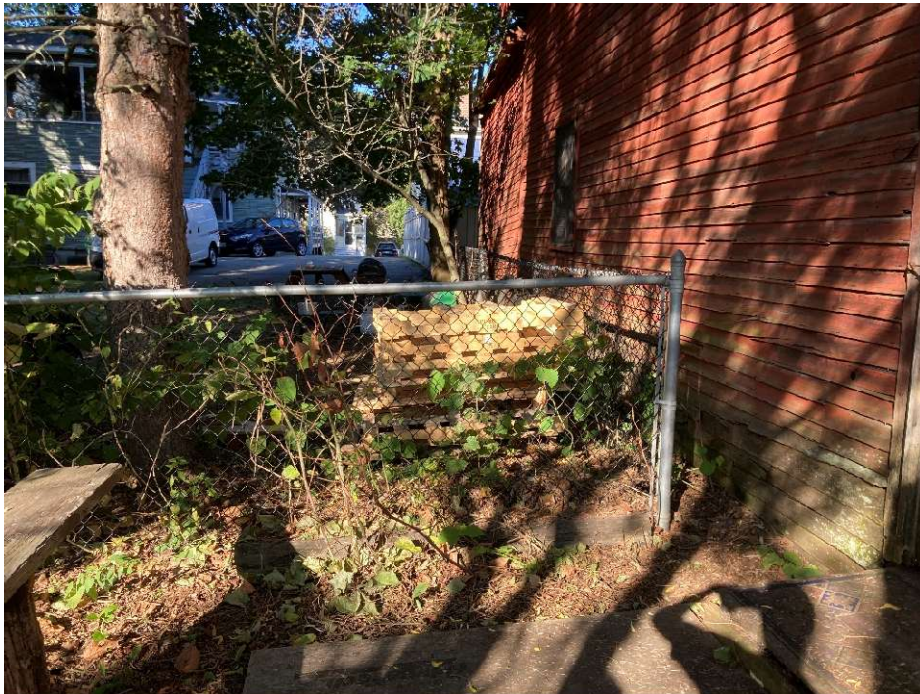
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**Photo 5 – View of left side of existing barn on Coffins Ct looking to the north.
(9/3/2025)**



**Photo 6 – View of the rear of existing barn on Coffins Ct looking to the east.
(9/3/2025)**



City of Portsmouth, New Hampshire

Subdivision Application Checklist

This subdivision application checklist is a tool designed to assist the applicant in the planning process and for preparing the application for Planning Board review. A pre-application conference with a member of the planning department is strongly encouraged as additional project information may be required depending on the size and scope. The applicant is cautioned that this checklist is only a guide and is not intended to be a complete list of all subdivision review requirements. Please refer to the Subdivision review regulations for full details.

Applicant Responsibilities (Section III.C): Applicable fees are due upon application submittal along with required number of copies of the Preliminary or final plat and supporting documents and studies. Please consult with Planning staff for submittal requirements.

Owner: Alea LLC Date Submitted: 6/22/2026

Applicant: Ross Engineering, LLC

Phone Number: 603-433-7560 E-mail: alexross@comcast.net

Site Address 1: 238 Austin St Map: 135 Lot: 61

Site Address 2: _____ Map: _____ Lot: 61

Application Requirements			
<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page or Plan Sheet/Note #)	Waiver Requested
<input checked="" type="checkbox"/>	Completed Application form. (III.C.2-3)	LU-25-177	N/A
<input checked="" type="checkbox"/>	All application documents, plans, supporting documentation and other materials provided in digital Portable Document Format (PDF) on compact disc, DVD or flash drive. (III.C.4)	Uploaded to Online Application in Viewpoint	N/A

Requirements for Preliminary/Final Plat				
<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page/line or Plan Sheet/Note #)	Required for Preliminary / Final Plat	Waiver Requested
<input checked="" type="checkbox"/>	Name and address of record owner, any option holders, descriptive name of subdivision, engineer and/or surveyor or name of person who prepared the plat. (Section IV.1/V.1)	Cover Sheet	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	N/A

Requirements for Preliminary/Final Plat				
<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page/line or Plan Sheet/Note #)	Required for Preliminary / Final Plat	Waiver Requested
<input checked="" type="checkbox"/>	<p>Preliminary Plat Names and addresses of all adjoining property owners. (Section IV.2)</p> <p>Final Plat Names and addresses of all abutting property owners, locations of buildings within one hundred (100) feet of the parcel, and any new house numbers within the subdivision. (Section V.2)</p>	Drawings 1 & 2 "Existing Conditions Plan" & "Subdivision Plan"	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	N/A
<input checked="" type="checkbox"/>	North point, date, and bar scale. (Section IV.3/V3)	Required on all Plan Sheets	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	N/A
<input checked="" type="checkbox"/>	Zoning classification and minimum yard dimensions required. (Section IV.4/V.4)	Drawings 1 & 2 "Existing Conditions Plan" & "Subdivision Plan"	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	N/A
<input checked="" type="checkbox"/>	<p>Preliminary Plat Scale (not to be smaller than one hundred (100) feet = 1 inch) and location map (at a scale of 1" = 1000'). (Section IV.5)</p> <p>Final Plat Scale (not to be smaller than 1"=100'), Location map (at a scale of 1"=1,000') showing the property being subdivided and its relation to the surrounding area within a radius of 2,000 feet. Said location map shall delineate all streets and other major physical features that may either affect or be affected by the proposed development. (Section V.5)</p>	Drawings 1 & 2 "Existing Conditions Plan" & "Subdivision Plan"	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	N/A
<input checked="" type="checkbox"/>	Location and approximate dimensions of all existing and proposed property lines including the entire area proposed to be subdivided, the areas of proposed lots, and any adjacent parcels in the same ownership. (Section IV.6)	Drawings 1 & 2 "Existing Conditions Plan" & "Subdivision Plan"	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	
<input checked="" type="checkbox"/>	Dimensions and areas of all lots and any and all property to be dedicated or reserved for schools, parks, playgrounds, or other public purpose. Dimensions shall include radii and length of all arcs and calculated bearing for all straight lines. (Section V.6/ IV.7)	Drawings 1 & 2 "Existing Conditions Plan" & "Subdivision Plan"	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	N/A
<input checked="" type="checkbox"/>	Location, names, and present widths of all adjacent streets, with a designation as to whether public or private and approximate location of existing utilities to be used. Curbs and sidewalks shall be shown. (Section IV.8/V.7)	Drawings 1 & 2 "Existing Conditions Plan" & "Subdivision Plan"	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	

Requirements for Preliminary/Final Plat				
<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page/line or Plan Sheet/Note #)	Required for Preliminary / Final Plat	Waiver Requested
<input checked="" type="checkbox"/>	Location of significant physical features, including bodies of water, watercourses, wetlands, railroads, important vegetation, stone walls and soils types that may influence the design of the subdivision. (Section IV.9/V.8)	No significant physical features. Site is fully developed.	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	
<input checked="" type="checkbox"/>	Preliminary Plat Proposed locations, widths and other dimensions of all new streets and utilities, including water mains, storm and sanitary sewer mains, catch basins and culverts, street lights, fire hydrants, sewerage pump stations, etc. (Section IV.10) Final Plat Proposed locations and profiles of all proposed streets and utilities, including water mains, storm and sanitary sewer mains, catchbasins and culverts, together with typical cross sections. Profiles shall be drawn to a horizontal scale of 1"=50' and a vertical scale of 1"=5', showing existing centerline grade, existing left and right sideline grades, and proposed centerline grade. (Section V.9)	Drawing 2 "Subdivision Plan"	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	
<input checked="" type="checkbox"/>	When required by the Board, the plat shall be accompanied by profiles of proposed street grades, including extensions for a reasonable distance beyond the subject land; also grades and sizes of proposed utilities. (Section IV.10)	No proposed streets	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	
<input checked="" type="checkbox"/>	Base flood elevation (BFE) for subdivisions involving greater than five (5) acres or fifty (50) lots. (Section IV.11)	<5 Acres, <50 Lots, Not in a FEMA SFHA	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	
<input checked="" type="checkbox"/>	For subdivisions of five (5) lots or more, or at the discretion of the Board otherwise, the preliminary plat shall show contours at intervals no greater than two (2) feet. Contours shall be shown in dotted lines for existing natural surface and in solid lines for proposed final grade, together with the final grade elevations shown in figures at all lot corners. If existing grades are not to be changed, then the contours in these areas shall be solid lines. (Section IV.12/ V.12)	<5 Lots	<input checked="" type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	

Requirements for Preliminary/Final Plat				
<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page/line or Plan Sheet/Note #)	Required for Preliminary / Final Plat	Waiver Requested
<input checked="" type="checkbox"/>	Dates and permit numbers of all necessary permits from governmental agencies from which approval is required by Federal or State law. (Section V.10)	No State or Federal Permits required	<input type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	
<input checked="" type="checkbox"/>	For subdivisions involving greater than five (5) acres or fifty (50) lots, the final plat shall show hazard zones and shall include elevation data for flood hazard zones. (Section V.11)	<5 Acres & <50 Lots	<input type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	
<input checked="" type="checkbox"/>	Location of all permanent monuments. (Section V.12)	Drawing 1 "Existing Conditions Plan"	<input type="checkbox"/> Preliminary Plat <input checked="" type="checkbox"/> Final Plat	

<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page/line or Plan Sheet/Note #)	Waiver Requested
<input checked="" type="checkbox"/>	15. Easements (VI.15)	N/A	
<input checked="" type="checkbox"/>	a. Utilities		
<input checked="" type="checkbox"/>	b. Drainage		
<input checked="" type="checkbox"/>	16. Monuments: (VI.16)	Drawing 2 "Subdivision Plan"	
<input checked="" type="checkbox"/>	17. Benchmarks: (VI.17)	N/A - No proposed streets	
<input checked="" type="checkbox"/>	18. House Numbers (VI.18)	House Numbers to be provided by City Assessing	

Design Standards			
	Required Items for Submittal	Indicate compliance and/or provide explanation as to alternative design	Waiver Requested
<input checked="" type="checkbox"/>	1. Streets have been designed according to the design standards required under Section (VII.1). a. Clearing b. Excavation c. Rough Grade and Preparation of Sub-Grade d. Base Course e. Street Paving f. Side Slopes g. Approval Specifications h. Curbing i. Sidewalks j. Inspection and Methods	N/A - No proposed streets	
<input checked="" type="checkbox"/>	2. Storm water Sewers and Other Drainage Appurtenances have been designed according to the design standards required under Section (VII.2). a. Design b. Standards of Construction	N/A	
<input checked="" type="checkbox"/>	3. Sanitary Sewers have been designed according to the design standards required under Section (VII.3). a. Design b. Lift Stations c. Materials d. Construction Standards	N/A	
<input checked="" type="checkbox"/>	4. Water Mains and Fire Hydrants have been designed according to the design standards required under Section (VII.4). a. Connections to Lots b. Design and Construction c. Materials d. Notification Prior to Construction	N/A	

Applicant's/Representative's Signature: _____



Date: 6/22/2026

¹ See City of Portsmouth, NH Subdivision Rules and Regulations for details.
Subdivision Application Checklist/January 2018

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Portsmouth, NH 03801

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June 22, 2026

City of Portsmouth Planning Department
1 Jenkins Avenue
Portsmouth, NH 03801

Waiver Request Letter

Re: 238 Austin St
Tax Map 135, Lot 61
Portsmouth, NH 03801

Planning Board Members, we are requesting waivers to the regulations listed below.

1) VI.6 Drainage Improvements:

The parcels are developed, and have been for many years. The lot coverage on site decreases resulting in less runoff.

Thank you for your consideration.

Sincerely,

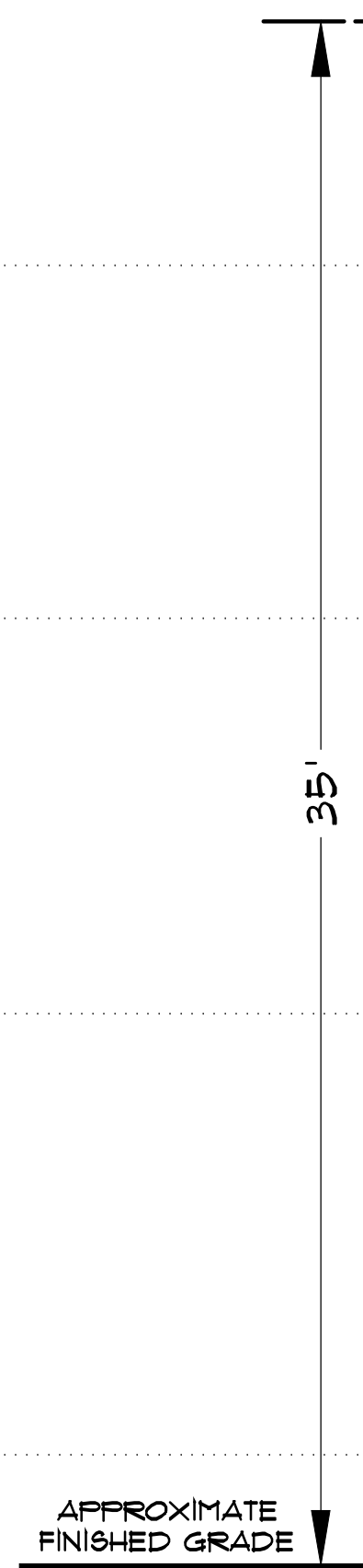
Alex Ross, P.E., LLS



RIGHT SIDE ELEVATION
 $\frac{1}{4}'' = 1'-0''$



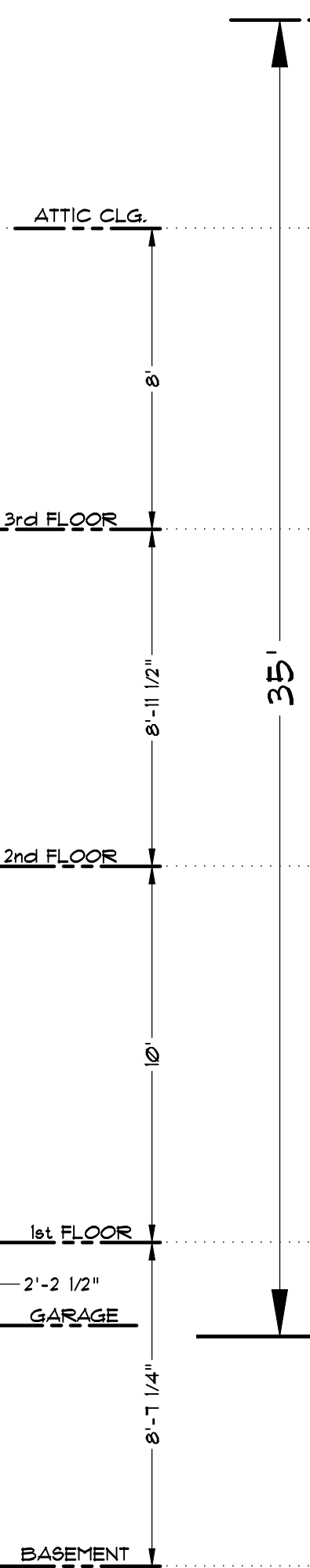
REAR ELEVATION
 $\frac{1}{4}'' = 1'-0''$



LEFT SIDE ELEVATION
 $\frac{1}{4}'' = 1'-0''$



FRONT ELEVATION
 $\frac{1}{4}'' = 1'-0''$

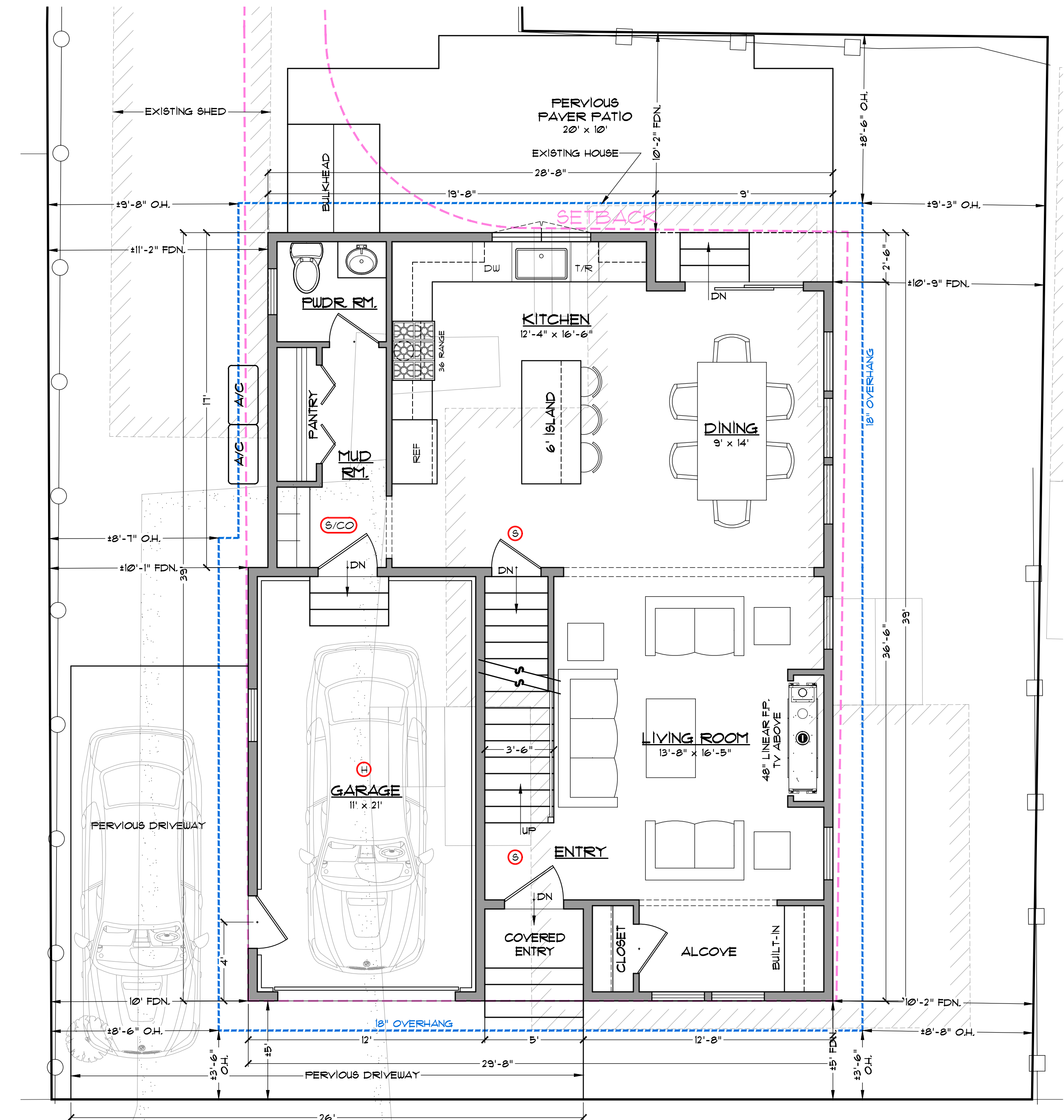
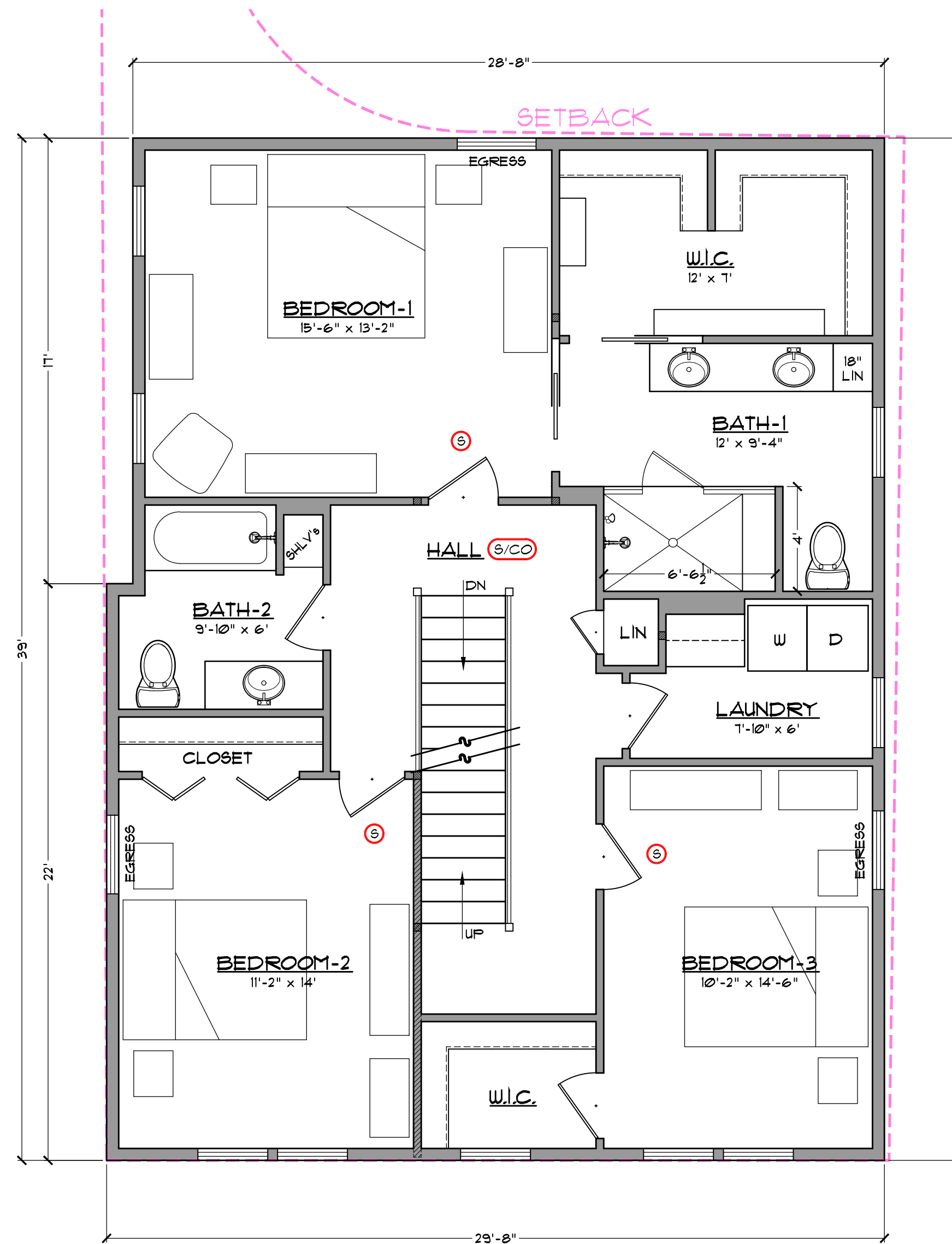
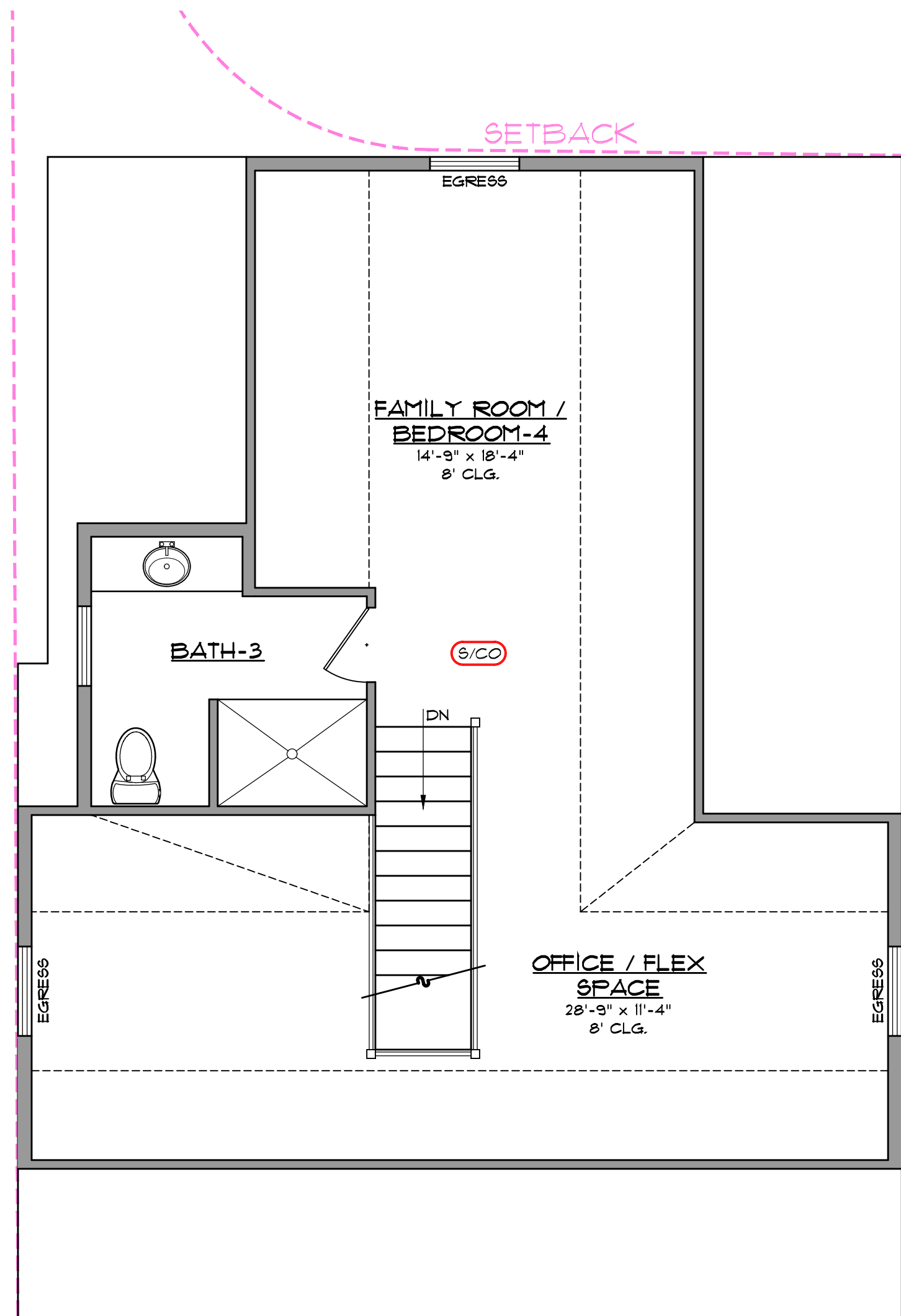


1. THESE PLANS ARE BUILT TO FOLLOW THE IRC 2021 BUILDING CODE AND STATE FIRE CODE AS ADMINISTERED BY THE TOWN OF PORTSMOUTH, NH.
2. ALL DIMENSIONS ARE FRAMING DIMENSIONS, CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO EXCAVATION.
3. DO NOT SCALE DRAWING, WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS.

C:\T\INC\2025\Austin Street\BD

AUSTIN STREET

PROJECT: 238 Austin Street (Coffins Ct.) ZBA Portsmouth, NH 03801		
E-mail: tech-ill2@comcast.net	Phone: 603-964-1300 Fax: 603-960-1414	DATE: 6-17-26
Technical Illustrations		REVISED:
ARCHITECTURAL DRAFTING SERVICE		DWG. NO. 1
196 Burker Hill Ave. Stratham, NH 03885		



S.F. TOTALS
 FIRST FLOOR = 843 SF.
 SECOND FLOOR = 1,100 SF.
 THIRD FLOOR = 712 SF.
 TOTAL = 2,655 SF.

EXISTING IMPERVIOUS COVERAGE = 1,432 SF.
 PROPOSED IMPERVIOUS COVERAGE 1,314 SF.

FIRE PREVENTION PLAN

- Ⓢ SMOKE DETECTOR
- Ⓢ/CO SMOKE / CARBON MONOXIDE DETECTOR
- Ⓢ HEAT DETECTOR

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2. ALL DIMENSIONS ARE FRAMING DIMENSIONS, CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO EXCAVATION.
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PROJECT: 238 Austin Street (Coffins Ct.) ZBA
 Portsmouth, NH 03801

E-mail: tech-112@comcast.net Phone: 603-964-1304 DATE: 6-17-26
 Fax: 603-960-1414

Technical Illustrations
 ARCHITECTURAL DRAFTING SERVICE

196 Burker Hill Ave. Stratham, NH 03885 DWG. NO. 3



RIGHT SIDE ELEVATION
1/4" = 1'-0"



REAR ELEVATION
1/4" = 1'-0"



LEFT SIDE ELEVATION
1/4" = 1'-0"



FRONT ELEVATION
1/4" = 1'-0"

COFFINS COURT

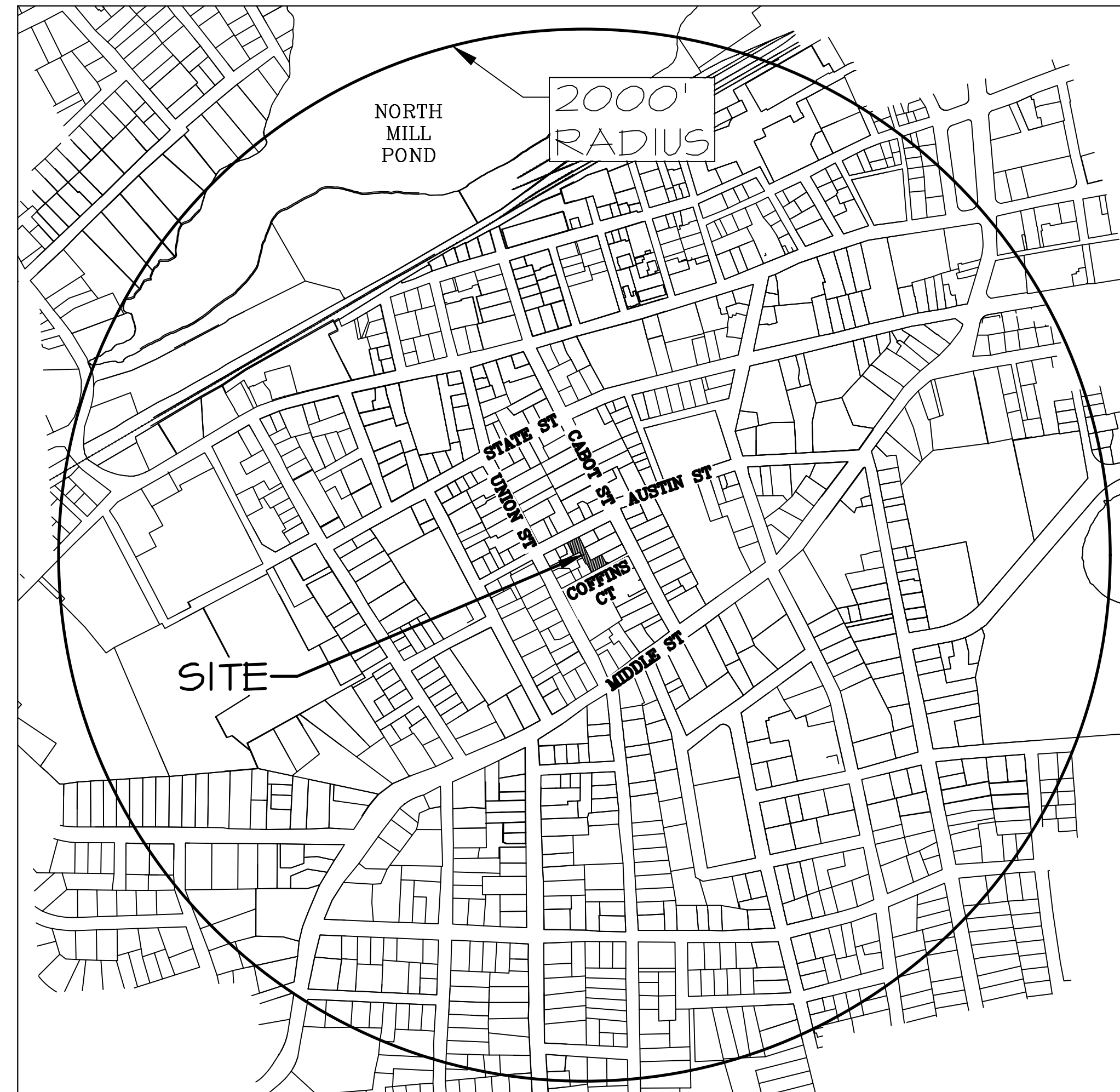
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PROJECT: 238 Austin Street (Coffins Ct.) ZBA Portsmouth, NH 03801		
E-mail: tech-112@comcast.net	Phone: 603-964-1300 Fax: 603-960-1414	DATE: 6-17-26
Technical Illustrations		REVISED:
ARCHITECTURAL DRAFTING SERVICE		DWG. NO. 1
186 Bunker Hill Ave. Stratham, NH 03885		

SITE PLAN REVIEW

238 AUSTIN ST

Portsmouth, NH 03801



LIST OF PROJECT PLANS AND DOCUMENTS:

CIVIL SET

- 1- Existing Conditions
- 2- Subdivision
- 3- Site Plan - Austin St
- 4- Site Plan - Coffins Ct
- 5- Elevations
- 6- TAC Street Plan
- 7- Details

PREPARED BY:
ROSS ENGINEERING, LLC

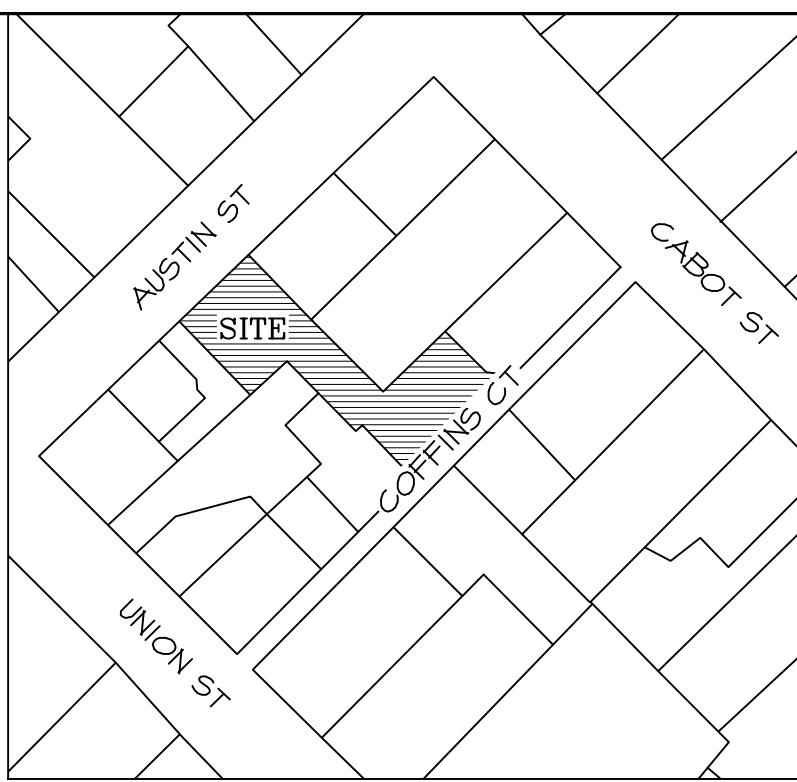
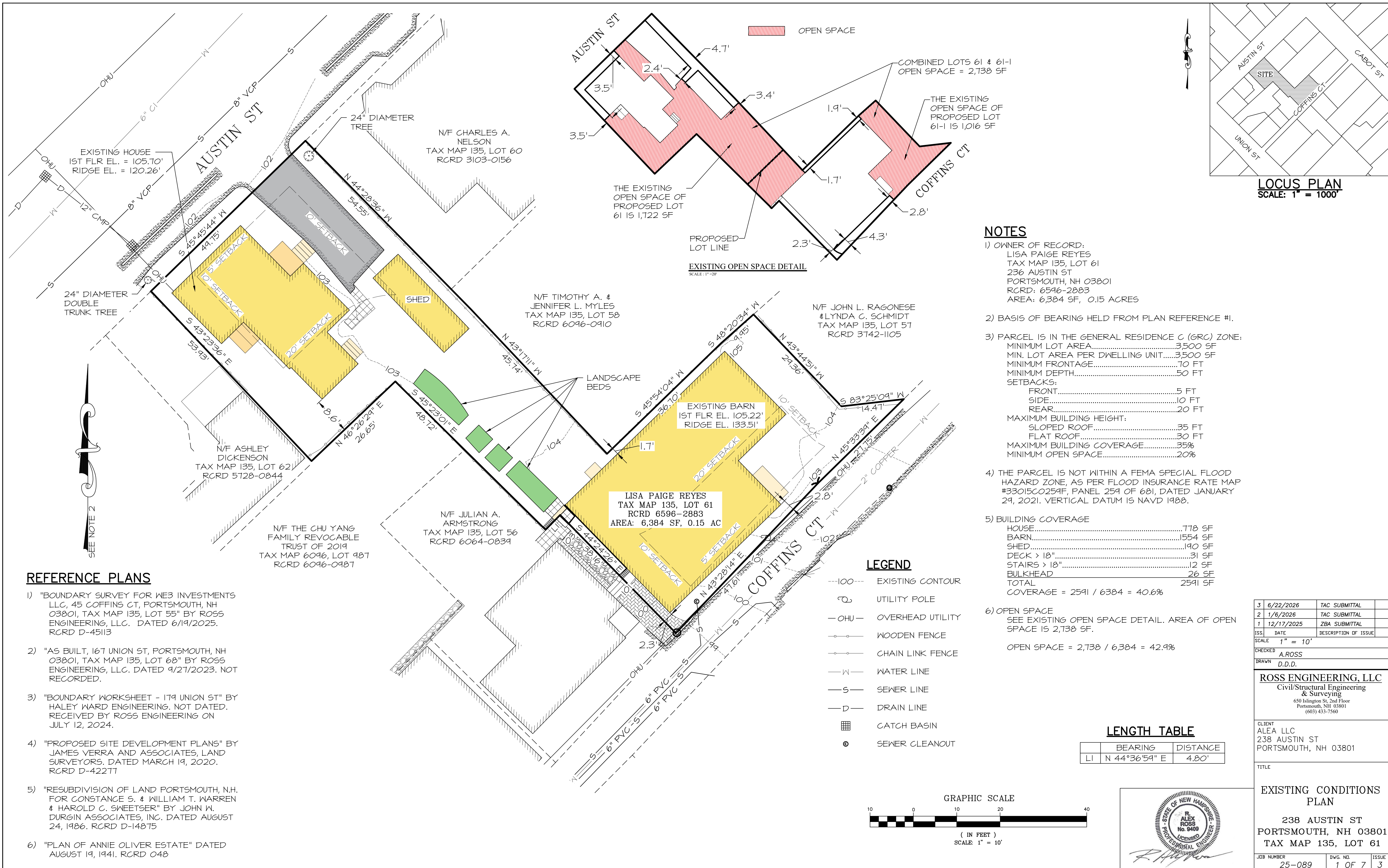
Civil/Structural Engineering
& Surveying
650 Islington St, 2nd Floor
Portsmouth, NH 03801
(603) 433-7560

PREPARED FOR:
ALEA LLC
238 AUSTIN ST
PORTSMOUTH, NH 03801

June 22, 2026

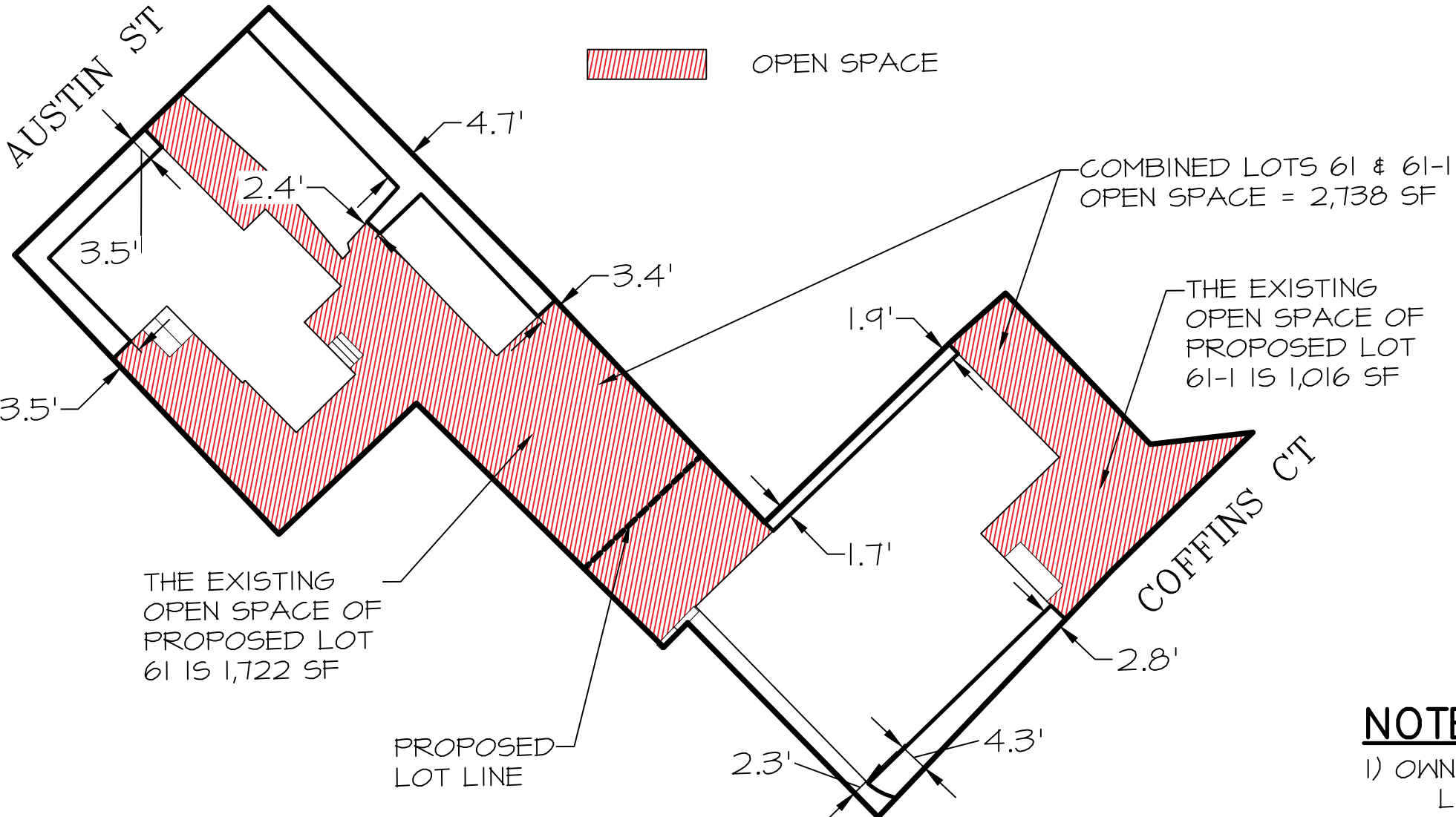
ARCHITECTURAL PLANS

Technical Illustrations
196 Bunker Hill Ave
Stratham, NH 03885



LOCUS PLAN
SCALE: 1" = 1000'

EXISTING OPEN SPACE DETAIL
SCALE: 1" = 50'



NOTES

- OWNER OF RECORD:
LISA PAIGE REYES
TAX MAP 135, LOT 61
236 AUSTIN ST
PORTSMOUTH, NH 03801
RCRD: 6596-2883
AREA: 6,384 SF, 0.15 ACRES
- BASIS OF BEARING HELD FROM PLAN REFERENCE #1.
- PARCEL IS IN THE GENERAL RESIDENCE C (GRC) ZONE:
MINIMUM LOT AREA.....3,500 SF
MIN. LOT AREA PER DWELLING UNIT.....3,500 SF
MINIMUM FRONTAGE.....70 FT
MINIMUM DEPTH.....50 FT
SETBACKS:
FRONT.....5 FT
SIDE.....10 FT
REAR.....20 FT
MAXIMUM BUILDING HEIGHT:
SLOPED ROOF.....35 FT
FLAT ROOF.....30 FT
MAXIMUM BUILDING COVERAGE.....35%
MINIMUM OPEN SPACE.....20%
- THE PARCEL IS NOT WITHIN A FEMA SPECIAL FLOOD HAZARD ZONE, AS PER FLOOD INSURANCE RATE MAP #33015C0259F, PANEL 259 OF 681, DATED JANUARY 29, 2021. VERTICAL DATUM IS NAVD 1988.
- BUILDING COVERAGE
HOUSE.....778 SF
BARN.....1554 SF
SHED.....190 SF
DECK > 18".....31 SF
STAIRS > 18".....12 SF
BULKHEAD.....26 SF
TOTAL.....2591 SF
COVERAGE = 2591 / 6384 = 40.6%
- OPEN SPACE
SEE EXISTING OPEN SPACE DETAIL. AREA OF OPEN SPACE IS 2,738 SF.
OPEN SPACE = 2,738 / 6,384 = 42.9%

REFERENCE PLANS

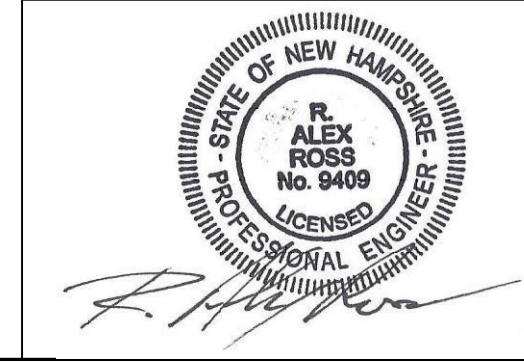
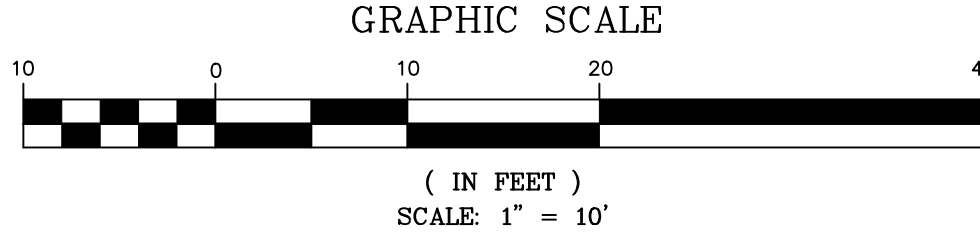
- "BOUNDARY SURVEY FOR WE3 INVESTMENTS LLC, 45 COFFINS CT, PORTSMOUTH, NH 03801, TAX MAP 135, LOT 55" BY ROSS ENGINEERING, LLC. DATED 6/14/2025. RCRD D-45113
- "AS BUILT, 167 UNION ST, PORTSMOUTH, NH 03801, TAX MAP 135, LOT 68" BY ROSS ENGINEERING, LLC. DATED 9/27/2023. NOT RECORDED.
- "BOUNDARY WORKSHEET - 179 UNION ST" BY HALEY WARD ENGINEERING. NOT DATED. RECEIVED BY ROSS ENGINEERING ON JULY 12, 2024.
- "PROPOSED SITE DEVELOPMENT PLANS" BY JAMES VERRA AND ASSOCIATES, LAND SURVEYORS. DATED MARCH 19, 2020. RCRD D-42277
- "RESUBDIVISION OF LAND PORTSMOUTH, N.H. FOR CONSTANCE S. & WILLIAM T. WARREN & HAROLD C. SWEETSER" BY JOHN W. DURGIN ASSOCIATES, INC. DATED AUGUST 24, 1986. RCRD D-14875
- "PLAN OF ANNIE OLIVER ESTATE" DATED AUGUST 19, 1941. RCRD 048

LEGEND

- 100--- EXISTING CONTOUR
- ⊙ UTILITY POLE
- OHU- OVERHEAD UTILITY
- WOODEN FENCE
- CHAIN LINK FENCE
- W --- WATER LINE
- S --- SEWER LINE
- D --- DRAIN LINE
- ⊞ CATCH BASIN
- ⊙ SEWER CLEANOUT

LENGTH TABLE

	BEARING	DISTANCE
LI	N 44°36'59" E	4.80'

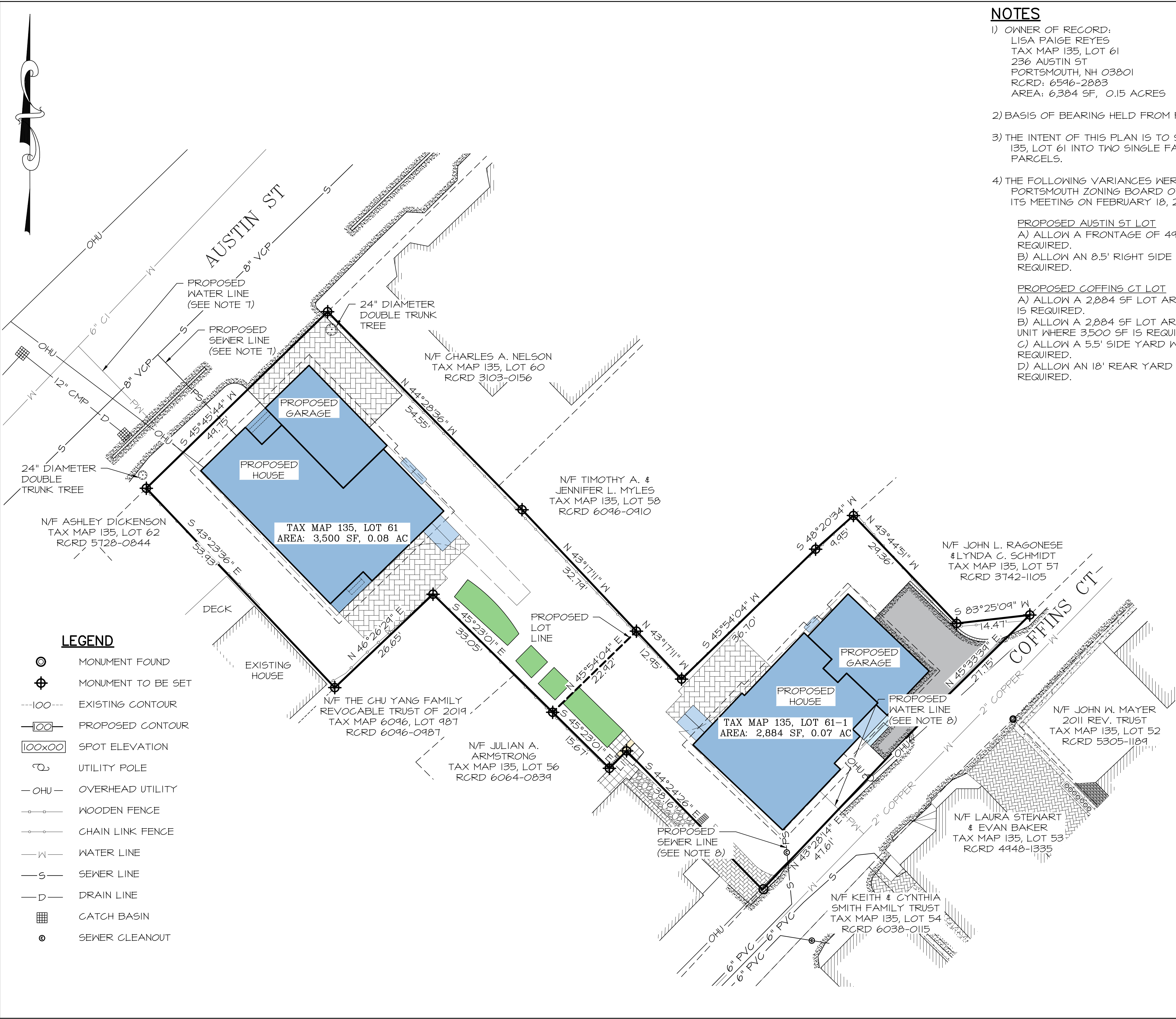


3	6/22/2026	TAC SUBMITTAL
2	1/6/2026	TAC SUBMITTAL
1	12/17/2025	ZBA SUBMITTAL
ISS.	DATE	DESCRIPTION OF ISSUE
SCALE 1" = 10'		
CHECKED	A.ROSS	
DRAWN	D.D.D.	

ROSS ENGINEERING, LLC
Civil/Structural Engineering
& Surveying
650 Islington St., 2nd Floor
Portsmouth, NH 03801
(603) 433-7560

CLIENT
ALEA LLC
238 AUSTIN ST
PORTSMOUTH, NH 03801

TITLE		
EXISTING CONDITIONS PLAN		
238 AUSTIN ST PORTSMOUTH, NH 03801 TAX MAP 135, LOT 61		
JOB NUMBER	DWG. NO.	ISSUE
25-089	1 OF 7	3



NOTES

- 1) OWNER OF RECORD:
LISA PAIGE REYES
TAX MAP 135, LOT 61
236 AUSTIN ST
PORTSMOUTH, NH 03801
RCRD: 6596-2883
AREA: 6,384 SF, 0.15 ACRES
- 2) BASIS OF BEARING HELD FROM PLAN REFERENCE #1.
- 3) THE INTENT OF THIS PLAN IS TO SUBDIVIDE TAX MAP 135, LOT 61 INTO TWO SINGLE FAMILY RESIDENTIAL PARCELS.
- 4) THE FOLLOWING VARIANCES WERE GRANTED BY THE PORTSMOUTH ZONING BOARD OF ADJUSTMENT AT ITS MEETING ON FEBRUARY 18, 2026.

PROPOSED AUSTIN ST LOT

 - A) ALLOW A FRONTAGE OF 49.75' WHERE 70' IS REQUIRED.
 - B) ALLOW AN 8.5' RIGHT SIDE YARD WHERE 10' IS REQUIRED.

PROPOSED COFFINS CT LOT

 - A) ALLOW A 2,884 SF LOT AREA WHERE 3,500 SF IS REQUIRED.
 - B) ALLOW A 2,884 SF LOT AREA PER DWELLING UNIT WHERE 3,500 SF IS REQUIRED.
 - C) ALLOW A 5.5' SIDE YARD WHERE 10' IS REQUIRED.
 - D) ALLOW AN 18' REAR YARD WHERE 20' IS REQUIRED.
- 5) PARCEL IS IN THE GENERAL RESIDENCE C (GRC) ZONE:
MINIMUM LOT AREA.....3,500 SF
MIN. LOT AREA PER DWELLING UNIT.....3,500 SF
MINIMUM FRONTAGE.....70 FT
MINIMUM DEPTH.....50 FT
SETBACKS:
FRONT.....5 FT
SIDE.....10 FT
REAR.....20 FT
MAXIMUM BUILDING HEIGHT:
SLOPED ROOF.....35 FT
FLAT ROOF.....30 FT
MAXIMUM BUILDING COVERAGE.....35%
MINIMUM OPEN SPACE.....20%
- 6) THE PARCEL IS NOT WITHIN A FEMA SPECIAL FLOOD HAZARD ZONE, AS PER FLOOD INSURANCE RATE MAP #33015C0254F, PANEL 259 OF 681, DATED JANUARY 29, 2021. VERTICAL DATUM IS NAVD 1988.
- 7) TAX MAP 135, LOT 61 WILL BE SERVICED BY CITY WATER AND SEWER. PROPOSED SEWER WILL CONNECT TO AN 8" VCP SEWER MAIN IN AUSTIN ST. WATER LINE WILL CONNECT TO A 8" CAST IRON MAIN IN AUSTIN ST.

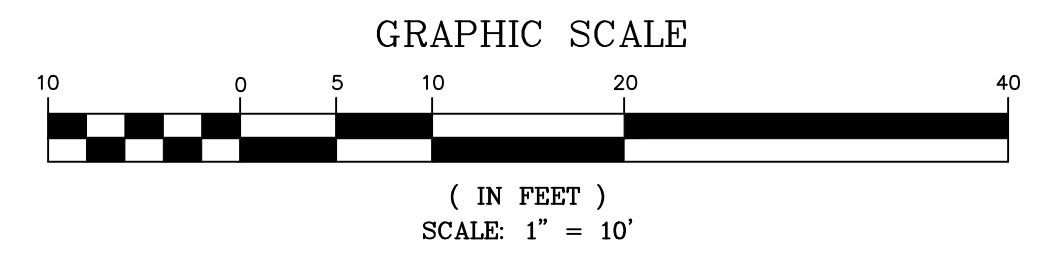
EXISTING CONNECTIONS POSSIBLY EXIST AND THE PROPOSED LINES SHOWN ON THE PLAN DO NOT REFLECT ACTUAL LOCATIONS. CONTRACTOR TO COORDINATE WITH DPW FOR LOCATION, SIZING AND MATERIAL OF PIPES.
- 8) TAX MAP 135, LOT 61-I WILL BE SERVICED BY CITY WATER AND SEWER. PROPOSED SEWER WILL CONNECT TO AN EXISTING SEWER CLEANOUT ON THE NORTH SIDE OF COFFINS CT WHICH IS OFF OF A 6" PVC SEWER CONNECTION. PROPOSED WATER LINE WILL BE CONNECTED TO 2" COPPER WATER MAIN IN COFFINS CT. CONTRACTOR TO COORDINATE WITH DPW FOR LOCATION, SIZING AND MATERIAL OF PIPES.
- 9) BOTH LOTS WILL BE SERVICED BY EXISTING OVERHEAD ELECTRIC LINES.
- 10) ALL NECESSARY CITY PERMITS MUST BE OBTAINED INCLUDING BUT NOT LIMITED TO BUILDING PERMITS AND DRIVEWAY PERMITS.

LEGEND

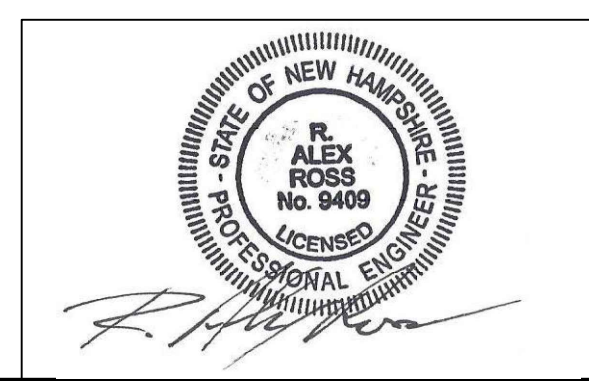
- ⊙ MONUMENT FOUND
- ⊕ MONUMENT TO BE SET
- - - - - EXISTING CONTOUR
- - - - - PROPOSED CONTOUR
- 100x00 SPOT ELEVATION
- ⊕ UTILITY POLE
- OHU - OVERHEAD UTILITY
- W - WOODEN FENCE
- C - CHAIN LINK FENCE
- W - WATER LINE
- S - SEWER LINE
- D - DRAIN LINE
- ☒ CATCH BASIN
- ⊙ SEWER CLEANOUT

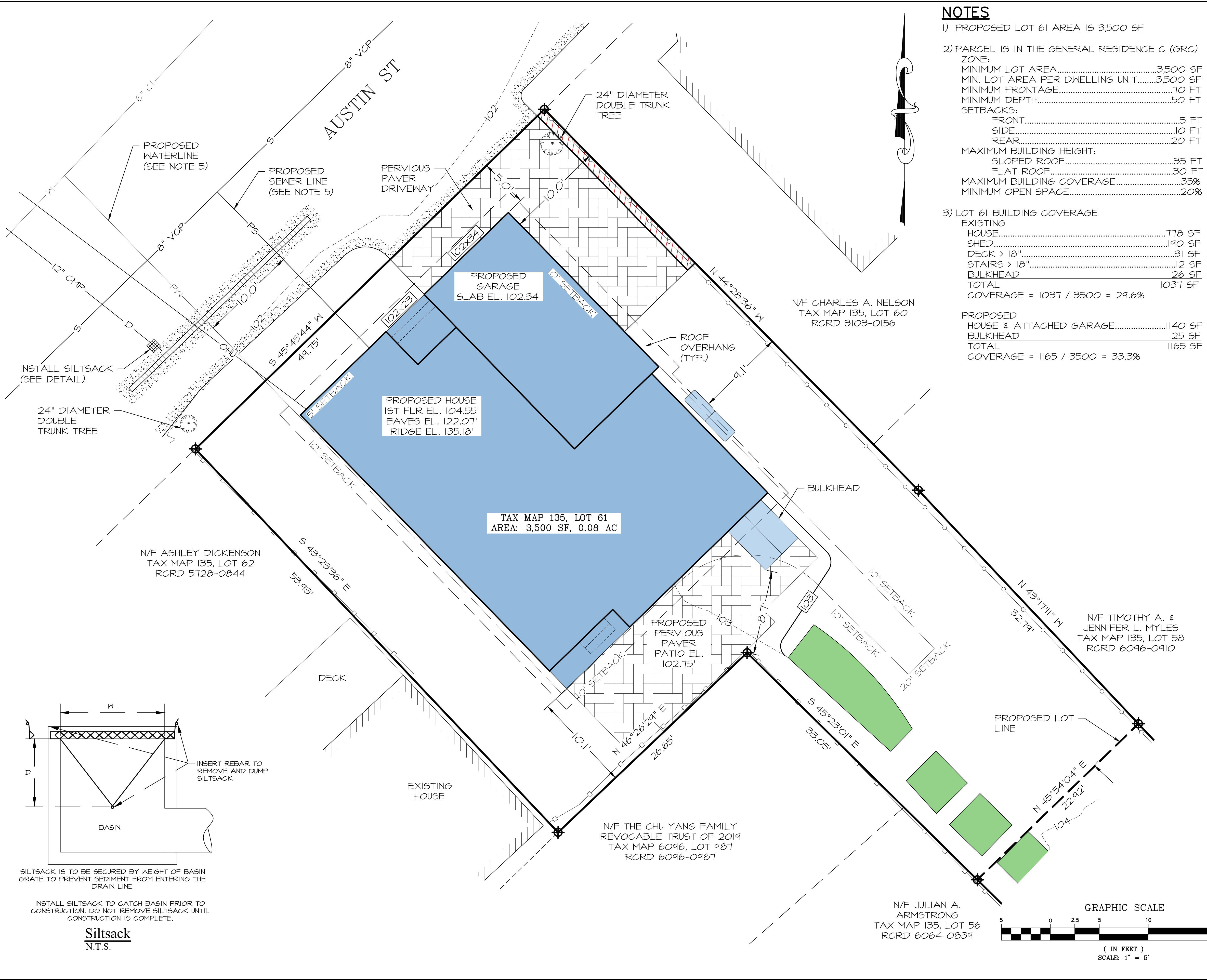
LENGTH TABLE

	BEARING	DISTANCE
LI	N 44°36'59" E	4.80'



3	6/22/2026	TAC SUBMITTAL
2	1/6/2026	TAC SUBMITTAL
1	12/17/2025	ZBA SUBMITTAL
ISS	DATE	DESCRIPTION OF ISSUE
SCALE	1" = 10'	
CHECKED	A. ROSS	
DRAWN	D.D.D.	
ROSS ENGINEERING, LLC Civil/Structural Engineering & Surveying 650 Islington St., 2nd Floor Portsmouth, NH 03801 (603) 433-7560		
CLIENT ALEA LLC 238 AUSTIN ST PORTSMOUTH, NH 03801		
TITLE SUBDIVISION PLAN 238 AUSTIN ST PORTSMOUTH, NH 03801 TAX MAP 135, LOT 61		
JOB NUMBER	DWG. NO.	ISSUE
25-089	2 OF 7	3





NOTES

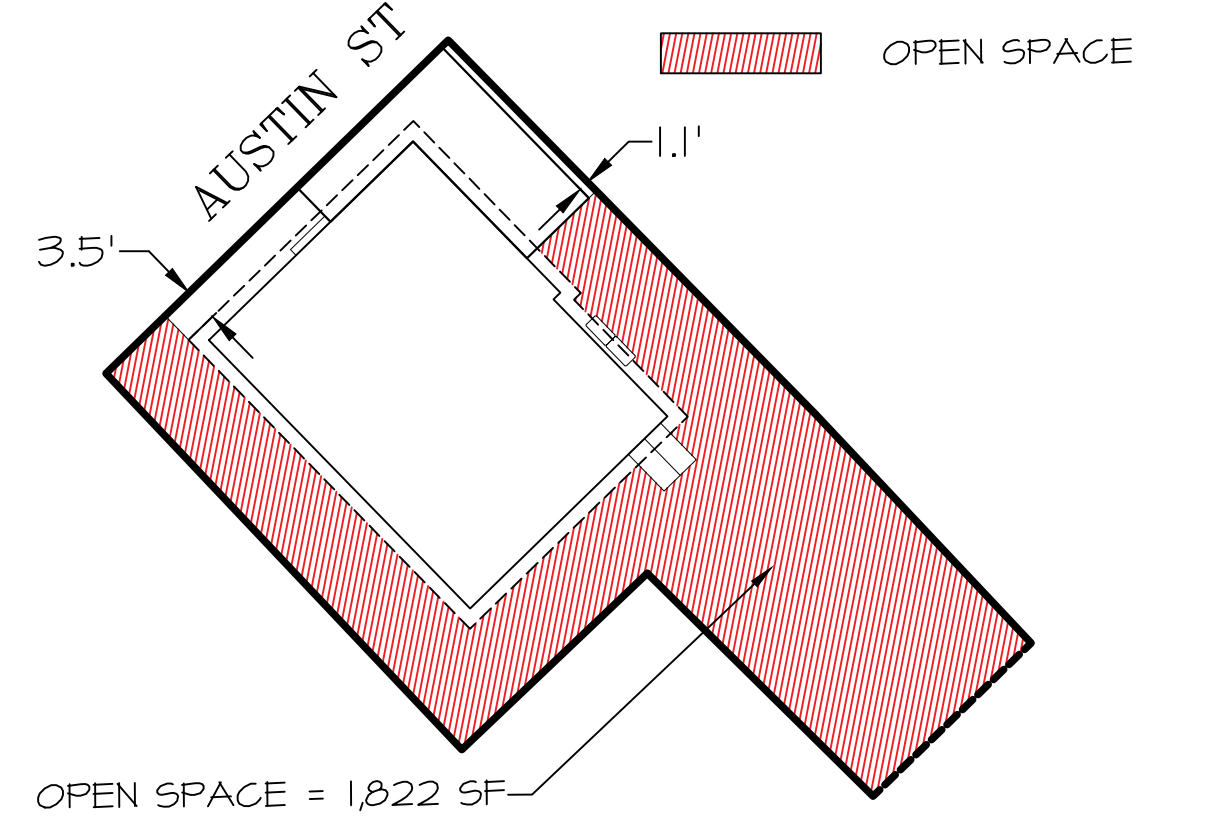
- 1) PROPOSED LOT 61 AREA IS 3,500 SF
- 2) PARCEL IS IN THE GENERAL RESIDENCE C (GRC) ZONE:
 - MINIMUM LOT AREA.....3,500 SF
 - MIN. LOT AREA PER DWELLING UNIT.....3,500 SF
 - MINIMUM FRONTAGE.....70 FT
 - MINIMUM DEPTH.....50 FT
 - SETBACKS:
 - FRONT.....5 FT
 - SIDE.....10 FT
 - REAR.....20 FT
 - MAXIMUM BUILDING HEIGHT:
 - SLOPED ROOF.....35 FT
 - FLAT ROOF.....30 FT
 - MAXIMUM BUILDING COVERAGE.....35%
 - MINIMUM OPEN SPACE.....20%
- 3) LOT 61 BUILDING COVERAGE

EXISTING	
HOUSE.....	778 SF
SHED.....	190 SF
DECK > 18".....	31 SF
STAIRS > 18".....	12 SF
BULKHEAD.....	26 SF
TOTAL.....	1037 SF
COVERAGE = 1037 / 3500 =	29.6%
PROPOSED	
HOUSE & ATTACHED GARAGE.....	1140 SF
BULKHEAD.....	25 SF
TOTAL.....	1165 SF
COVERAGE = 1165 / 3500 =	33.3%

- 4) LOT 61 OPEN SPACE

EXISTING	SEE EXISTING OPEN SPACE DETAIL. AREA OF OPEN SPACE = 1,722 SF
OPEN SPACE = 1,722 / 3500 =	49.2%
PROPOSED	SEE PROPOSED LOT 61 OPEN SPACE DETAIL. AREA OF OPEN SPACE = 1,822 SF
OPEN SPACE = 1822 / 3500 =	52.1%
- 5) TAX MAP 135, LOT 61 WILL BE SERVICED BY CITY WATER AND SEWER. PROPOSED SEWER WILL CONNECT TO AN 8" VCP SEWER MAIN IN AUSTIN ST. WATER LINE WILL CONNECT TO A 8" CAST IRON MAIN IN AUSTIN ST.

EXISTING CONNECTIONS POSSIBLY EXIST AND THE PROPOSED LINES SHOWN ON THE PLAN DO NOT REFLECT ACTUAL LOCATIONS. CONTRACTOR TO COORDINATE WITH DPW FOR LOCATION, SIZING AND MATERIAL OF PIPES.



PROPOSED LOT 61 OPEN SPACE DETAIL
SCALE: 1" = 20'

LEGEND

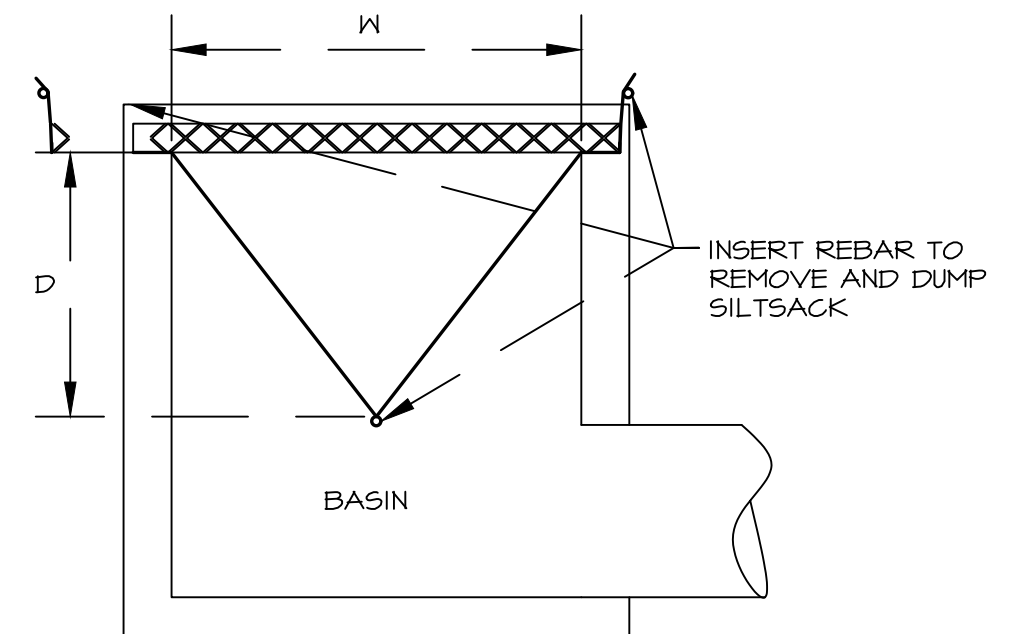
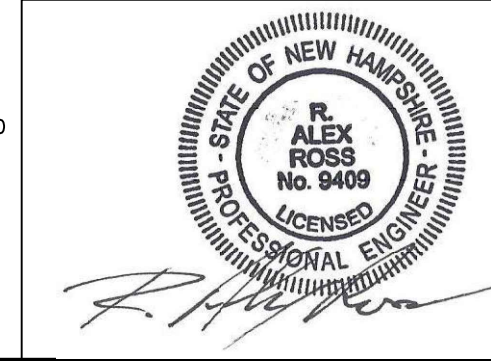
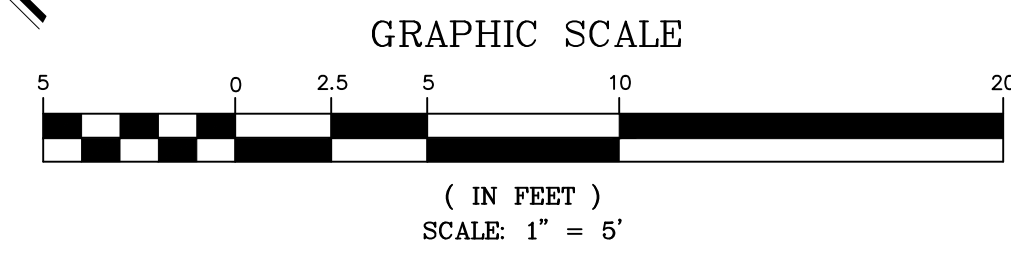
- ⊙ MONUMENT FOUND
- ⊕ MONUMENT TO BE SET
- - - - - EXISTING CONTOUR
- ▭ PROPOSED CONTOUR
- 100x00 SPOT ELEVATION
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2	1/6/2026	TAC SUBMITTAL	
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ISS.	DATE	DESCRIPTION OF ISSUE	
SCALE	1" = 5'		
CHECKED	A.ROSS		
DRAWN	D.D.D.		

ROSS ENGINEERING, LLC
Civil/Structural Engineering & Surveying
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CLIENT
ALEA LLC
238 AUSTIN ST
PORTSMOUTH, NH 03801

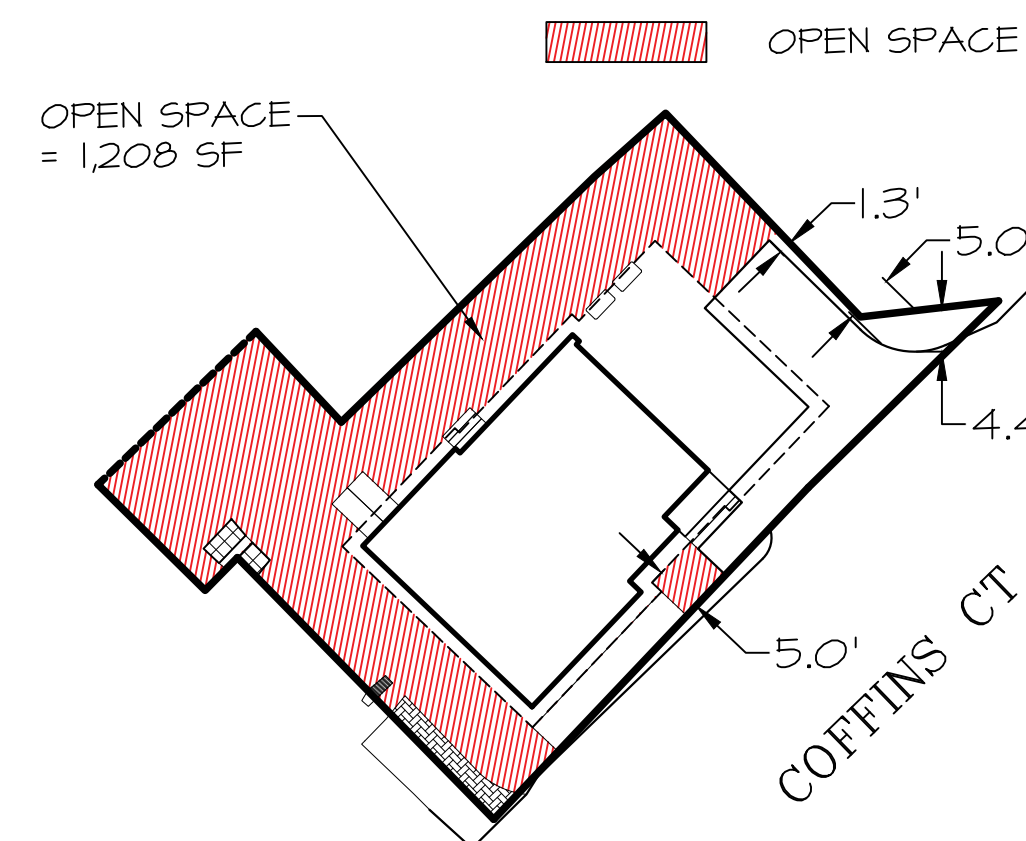
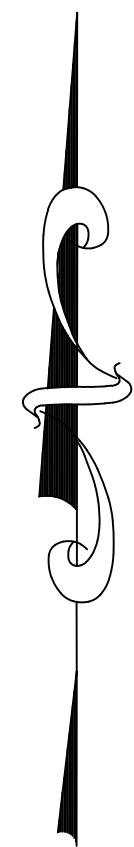
TITLE		
SITE PLAN		
238 AUSTIN ST PORTSMOUTH, NH 03801 TAX MAP 135, LOT 61		
JOB NUMBER	DWG. NO.	ISSUE
25-089	3 OF 7	3



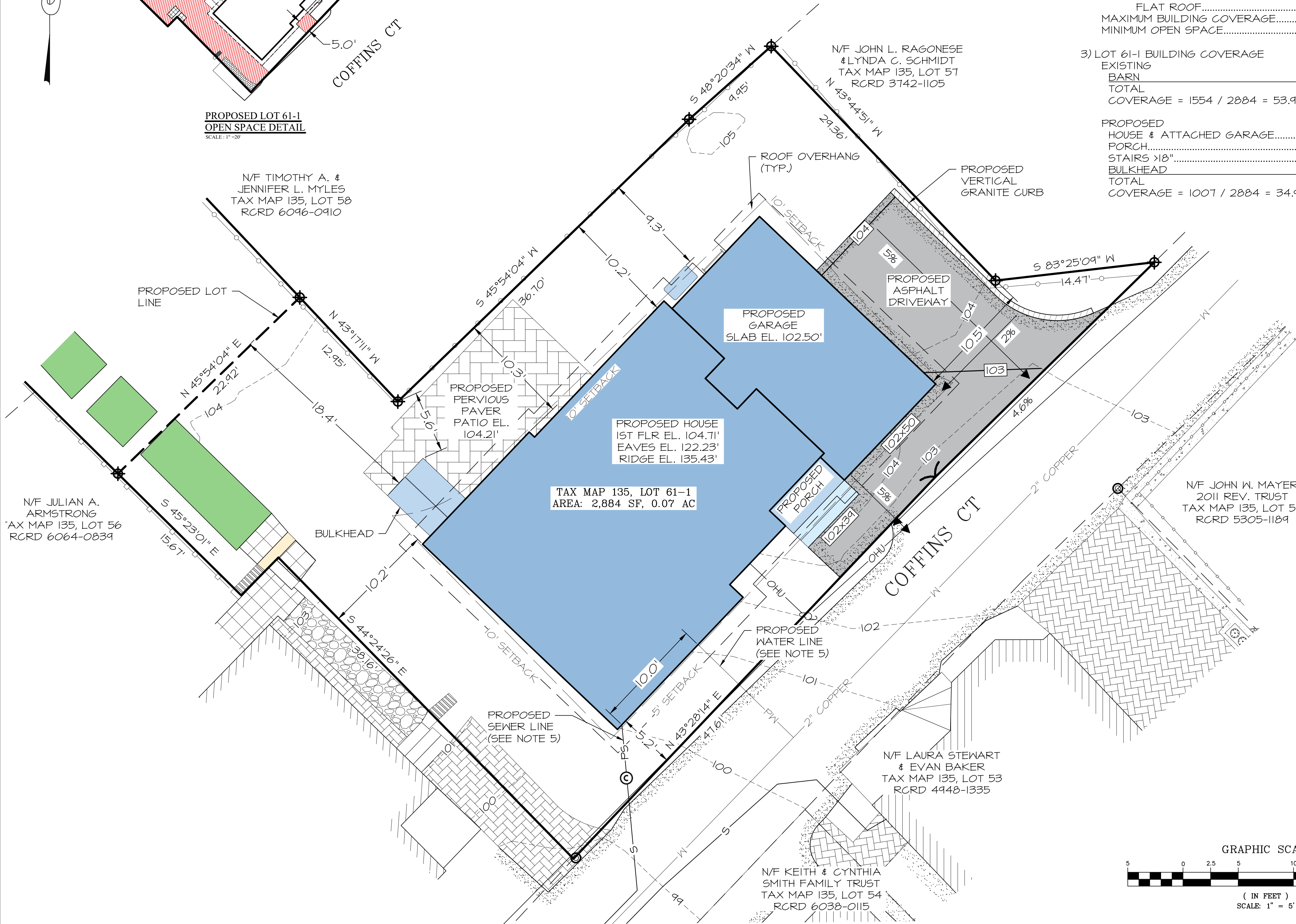
SILTSACK IS TO BE SECURED BY WEIGHT OF BASIN GRATE TO PREVENT SEDIMENT FROM ENTERING THE DRAIN LINE

INSTALL SILTSACK TO CATCH BASIN PRIOR TO CONSTRUCTION. DO NOT REMOVE SILTSACK UNTIL CONSTRUCTION IS COMPLETE.

Siltsack
N.T.S.



**PROPOSED LOT 61-1
OPEN SPACE DETAIL**
SCALE: 1" = 5'



NOTES

- 1) PROPOSED LOT 61-1 AREA IS 2,884 SF
- 2) PARCEL IS IN THE GENERAL RESIDENCE C (GRC) ZONE:
 MINIMUM LOT AREA.....3,500 SF
 MIN. LOT AREA PER DWELLING UNIT.....3,500 SF
 MINIMUM FRONTAGE.....70 FT
 MINIMUM DEPTH.....50 FT
 SETBACKS:
 FRONT.....5 FT
 SIDE.....10 FT
 REAR.....20 FT
 MAXIMUM BUILDING HEIGHT:
 SLOPED ROOF.....35 FT
 FLAT ROOF.....30 FT
 MAXIMUM BUILDING COVERAGE.....35%
 MINIMUM OPEN SPACE.....20%
- 3) LOT 61-1 BUILDING COVERAGE
 EXISTING
 BARN.....1554 SF
 TOTAL.....1554 SF
 COVERAGE = 1554 / 2884 = 53.9%

 PROPOSED
 HOUSE & ATTACHED GARAGE.....959 SF
 PORCH.....20 SF
 STAIRS >18".....3 SF
 BULKHEAD.....25 SF
 TOTAL.....1007 SF
 COVERAGE = 1007 / 2884 = 34.9%
- 4) LOT 61-1 OPEN SPACE
 EXISTING
 SEE EXISTING OPEN SPACE DETAIL. AREA OF OPEN SPACE = 1,016 SF

 OPEN SPACE = 1,016 / 2884 = 35.2%

 PROPOSED
 SEE PROPOSED LOT 61 OPEN SPACE DETAIL. AREA OF OPEN SPACE = 1208 SF

 OPEN SPACE = 1208 / 2884 = 41.9%
- 5) TAX MAP 135, LOT 61-1 WILL BE SERVICED BY CITY WATER AND SEWER. PROPOSED SEWER WILL CONNECT TO AN EXISTING SEWER CLEANOUT ON THE NORTH SIDE OF COFFINS CT WHICH IS OFF OF A 6" PVC SEWER CONNECTION. PROPOSED WATER LINE WILL BE CONNECTED TO 2" COPPER WATER MAIN IN COFFINS CT. CONTRACTOR TO COORDINATE WITH DPW FOR LOCATION, SIZING AND MATERIAL OF PIPES.

LEGEND

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- ⊕ MONUMENT TO BE SET
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LENGTH TABLE

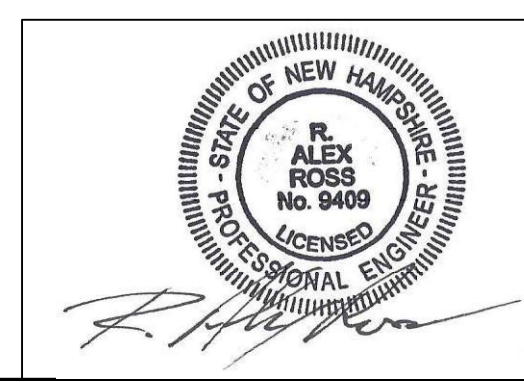
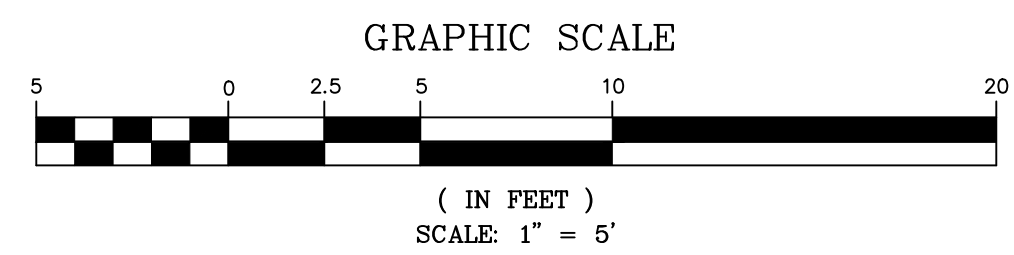
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CLIENT
 ALEA LLC
 238 AUSTIN ST
 PORTSMOUTH, NH 03801

TITLE		
SITE PLAN		
238 AUSTIN ST PORTSMOUTH, NH 03801 TAX MAP 135, LOT 61		
JOB NUMBER	DWG. NO.	ISSUE
25-089	4 OF 7	3





FRONT ELEVATION - AUSTIN ST

SCALE : 1" = 5'

NOTES – AUSTIN ST

1) AVERAGE FINISHED GRADE:
AS PER PORTSMOUTH ZONING ORDINANCE THE AVERAGE EXISTING GRADE SHALL BE THE AVERAGE GROUND LEVELS ADJOINING THE BUILDING AT ALL EXTERIOR WALLS MEASURED EVERY FIVE FEET AROUND THE PERIMETER OF THE BUILDING.

THE AVERAGE FINISHED GRADE WAS CALCULATED AS 102.63'.

2) BUILDING HEIGHT:
AS PER PORTSMOUTH ZONING ORDINANCE THE BUILDING HEIGHT SHALL BE THE GREATEST VERTICAL MEASUREMENT BETWEEN THE LOWER AND UPPER REFERENCE POINTS. THE LOWER REFERENCE POINT SHALL BE THE AVERAGE EXISTING GRADE OR AVERAGE FINISHED GRADE, WHICHEVER IS LOWER. THE UPPER REFERENCE POINT FOR GAMBREL, HIP, HIP-TOPPED MANSARD ROOF, OR PENTHOUSE SHALL BE THE ELEVATION MIDWAY BETWEEN THE LEVEL OF EAVES AND THE HIGHEST POINT OF THE ROOF.

AVERAGE FINISHED GRADE = 102.63'
EAVES EL. = 122.07'
RIDGE EL. = 135.18'
MIDPOINT EL. = $122.07 + 135.18 / 2 = 128.63'$

BUILDING HEIGHT = $128.63' - 102.63' = 26.00'$



FRONT ELEVATION - COFFINS CT

SCALE : 1" = 5'

NOTES – COFFINS CT

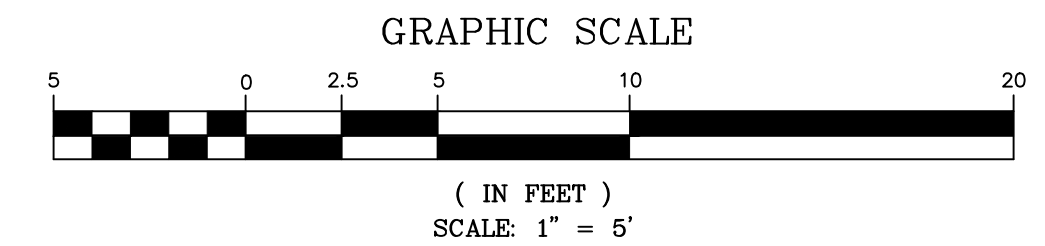
1) AVERAGE FINISHED GRADE:
AS PER PORTSMOUTH ZONING ORDINANCE THE AVERAGE EXISTING GRADE SHALL BE THE AVERAGE GROUND LEVELS ADJOINING THE BUILDING AT ALL EXTERIOR WALLS MEASURED EVERY FIVE FEET AROUND THE PERIMETER OF THE BUILDING.

THE AVERAGE FINISHED GRADE WAS CALCULATED AS 103.02'.

2) BUILDING HEIGHT:
AS PER PORTSMOUTH ZONING ORDINANCE THE BUILDING HEIGHT SHALL BE THE GREATEST VERTICAL MEASUREMENT BETWEEN THE LOWER AND UPPER REFERENCE POINTS. THE LOWER REFERENCE POINT SHALL BE THE AVERAGE EXISTING GRADE OR AVERAGE FINISHED GRADE, WHICHEVER IS LOWER. THE UPPER REFERENCE POINT FOR GAMBREL, HIP, HIP-TOPPED MANSARD ROOF, OR PENTHOUSE SHALL BE THE ELEVATION MIDWAY BETWEEN THE LEVEL OF EAVES AND THE HIGHEST POINT OF THE ROOF.

AVERAGE FINISHED GRADE = 103.02'
EAVES EL. = 122.23'
RIDGE EL. = 135.43'
MIDPOINT EL. = $122.23 + 135.43 / 2 = 128.83'$

BUILDING HEIGHT = $128.83' - 103.02' = 25.81'$



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SCALE	1" = 5'		
CHECKED	A.ROSS		
DRAWN	D.D.D.		

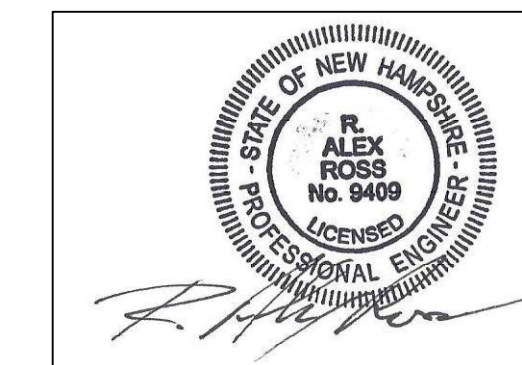
ROSS ENGINEERING, LLC
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& Surveying
650 Islington St., 2nd Floor
Portsmouth, NH 03801
(603) 433-7560

CLIENT
ALEA LLC
238 AUSTIN ST
PORTSMOUTH, NH 03801

TITLE
ELEVATIONS

238 AUSTIN ST
PORTSMOUTH, NH 03801
TAX MAP 135, LOT 61

JOB NUMBER	DWG. NO.	ISSUE
25-089	5 OF 7	3





N/F UP
MAINE LLC
TAX MAP
135, LOT 59
RCRD
4581-0991

AUSTIN ST

CABOT
ST

N/F CHARLES A.
NELSON
TAX MAP
135, LOT 60
RCRD
3103-0156

TAX MAP 135, LOT 61
AREA: 3,500 SF, 0.08 AC

N/F TIMOTHY
A. &
JENNIFER L.
MYLES
TAX MAP
135, LOT 58
RCRD
6096-0910

N/F JOHN L. RAGONESE
& LYNDY C. SCHMIDT
TAX MAP 135, LOT 57
RCRD 3742-1105

N/F ASHLEY DICKENSON
TAX MAP 135, LOT 62
RCRD 5728-0944

N/F ZACHARY TIERNEY
TAX MAP 135, LOT 63
RCRD 6534-1950

TAX MAP 135, LOT 61-I
AREA: 2,884 SF, 0.07 AC

N/F JOHN W. MAYER
2011 REV. TRUST
TAX MAP 135, LOT 52
RCRD 5305-1189

N/F TIMOTHY A. &
JENNIFER L. MYLES
TAX MAP 135, LOT 64
RCRD 6512-0364

N/F THE CHU YANG FAMILY
REVOCABLE TRUST OF 2019
TAX MAP 6096, LOT 987
RCRD 6096-0987

N/F JULIAN
A.
ARMSTRONG
TAX MAP
135, LOT 56
RCRD
6064-0839

N/F LAURA STEWART
& EVAN BAKER
TAX MAP 135, LOT 53
RCRD 4448-1335

N/F JOEL JOHNSON
TAX MAP 135, LOT 66
RCRD 5679-1054

N/F WES
INVESTMENTS
LLC
TAX MAP
135, LOT 55
RCRD
6397-0228

N/F KEITH & CYNTHIA
SMITH FAMILY TRUST
TAX MAP 135, LOT 54
RCRD 6038-0115

N/F JAY WARREN
ARMSTRONG & THERESE
A. LAGAMMA
TAX MAP 135, LOT 51
RCRD 4470-2246

COFFINS COURT DESCRIBED AS
"12 FOOT WIDE PASSAGEWAY" IN
RCRD 496-143 DATED OCT. 8, 1884

UNION ST

N/F JEFFREY P.
MCMAHON & LISA
MCMAHON HAGERTY
TAX MAP 135, LOT 67
RCRD 6259-1484

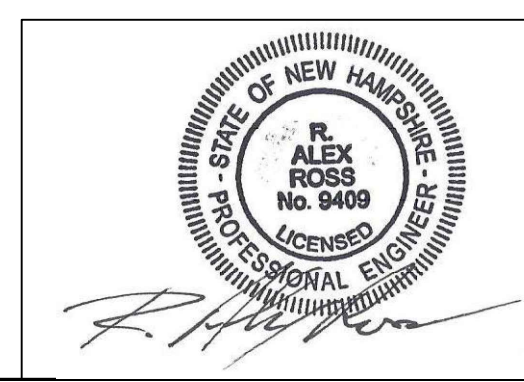
JOSEPH J. LEWINSKI III
& PAULA LEWINSKI
TAX MAP 135, LOT 68
RCRD 6443-2760

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CLIENT
ALEA LLC
238 AUSTIN ST
PORTSMOUTH, NH 03801

TITLE
**STREET
PLAN**
238 AUSTIN ST
PORTSMOUTH, NH 03801
TAX MAP 135, LOT 61



JOB NUMBER	DWG. NO.	ISSUE
25-089	6 OF 7	3

DATA COLLECTION

1. DETERMINE THE SIZE, SHAPE AND INTENDED USE OF FINISHED AREAS.
2. CLASSIFY SUB-GRADE SOILS.
3. DOCUMENT ALL EXISTING CONDITIONS. (FIXED POINTS, EXISTING GRADES, SITE CONTOURS, ETC.)
4. DOCUMENT SOIL TYPE, LOCATION, AND ELEVATION OF BELOW GRADE AND OVERHEAD UTILITIES BOTH PUBLIC AND PRIVATE.
5. ENSURE PUBLIC UTILITIES ARE MARKED THROUGH THE USE OF LOCATING SERVICE.
6. DETERMINE THE CROSS SECTION DESIGN OF THE SYSTEM BASED ON SOIL TYPE AND APPLICATION, SHOWING PROPOSED SUB-GRADE AND FINISHED GRADE ELEVATIONS AND ALL GEOTEXTILES AND DRAINAGE DRAINAGE PIPES NEEDED FOR CONSTRUCTION.
7. ESTABLISH THE TYPE, LOCATION, AND ELEVATION OF RELIEF STRUCTURES IF REQUIRED (OVERFLOW PIPE DISCHARGING TO RAIN GARDEN, ETC.).
8. DETERMINE CURB OR EDGE RESTRAINT TYPE, ELEVATION, AND LOCATION.
9. CHOOSE PATTERN APPROPRIATE TO THE APPLICATION (TRAFFIC TYPE AND LOAD).

EXCAVATION

1. BEFORE EXCAVATING, CALL ALL LOCAL UTILITY COMPANIES (E.G., PHONE, GAS, ELECTRICAL) TO ENSURE THAT THE AREA IN WHICH YOU PLAN TO DIG IS CLEAR OF UNDERGROUND CABLES OR WIRES. IF ANY ARE FOUND, PLEASE NOTIFY THE APPROPRIATE COMPANIES BEFORE YOU BEGIN.
2. EXCAVATION DEPTH IS DETERMINE FROM THE FOUNDATION THICKNESS ACCORDING TO THE PROJECT SPECIFICATIONS (FOUNDATION THICKNESS IS DETERMINED BY QUALIFIED ENGINEER BASED ON STRUCTURAL AND HYDROLOGIC ANALYSIS.)
3. THE SLOPE OF THE SUB-GRADE WILL DEPEND ON DRAINAGE DESIGN AND INFILTRATION TYPE, A MINIMUM SLOPE OF .5% (1/2" PER FOOT) IS REQUIRED.
4. THE DISTANCE THAT THE EXCAVATED AREA SHOULD EXTEND BEYOND THE AREA TO BE PAVED SHALL BE ONE TO 1.5 TIMES THE THICKNESS OF THE FOUNDATION. EXTRA SPACE ENSURE STABILITY OF PAVERS NEAR EDGE AND EDGE RESTRAINTS.
5. LEVEL THE BOTTOM OF THE EXCAVATED AREA WITH A RAKE.
6. COMPACTION WILL REDUCE THE PERMEABILITY OF THE SUB-GRADE. CARE SHOULD BE TAKEN TO MAINTAIN UNDISTURBED SOIL INFILTRATION DURING EXCAVATION AND CONSTRUCTION. STABILIZATION OF SUB-GRADE MAY BE REQUIRED WITH WEAK, OR CONTINUOUSLY SATURATED SOILS. REDUCED INFILTRATION MAY REQUIRE DRAINAGE PIPES WITHIN THE SUB-BASE TO CONFORM TO STORMWATER DRAINAGE REQUIREMENTS.

GEOTEXTILES, IMPERMEABLE LINERS, AND DRAIN PIPES

1. USE A WOVEN GEOTEXTILE WITH HIGH BI-AXEL STRENGTH.
2. PLACE THE GEOTEXTILE ON THE BOTTOM AND SIDES OF THE SOIL SUB-GRADE. ELIMINATE WRINKLES IN THE GEOTEXTILE AND ENSURE IT IS NOT DAMAGED DURING CONSTRUCTION.
3. OVERLAP OF GEOTEXTILE SHALL BE A MINIMUM 2'-0" IN THE DIRECTION OF DRAINAGE. OVERLAPPING SHOULD BE "SHINGLE" STYLE WITH RESPECT TO ANY SLOPE DIRECTION AND BASE STONE DISTRIBUTION DIRECTION. KEEP PROPERLY TENSIONED, ELIMINATE WRINKLES, AND AVOID DAMAGING FABRIC (NO SPIKES).

SUB-BASE

1. USE SUB-BASE ASTM NO. 2 OR NO. 3 MEETING THE FOLLOWING REQUIREMENTS:
 - A. 40% FRACTURED SYMMETRICAL PARTICALS
 - B. LESS THAN 5% PASSING 200 SIEVE
 - C. INDUSTRY HARDNESS TESTED
2. MOISTEN SPREAD AND COMPACT ASTM NO. 2 AGGREGATE SUB-BASE IN MINIMUM 6" LIFTS (WITHOUT DAMAGING OR DISTORTING THE GEOTEXTILE).
3. MAKE AT LEAST TWO PASSES IN VIBRATORY MODE FOLLOWED BY AT LEAST TWO PASSES IN STATIC MODE WITH A MINIMUM 10 TON VIBRATORY ROLLER, UNTIL THERE IS NOT VISIBLE MOVEMENT OF THE AGGREGATE.
4. DO NOT ALLOW COMPACTOR TO CRUSH AGGREGATE.
5. SURFACE TOLERANCE OF THE ASTM NO. 2 SUB-BASE SHOULD BE ±2 1/2" OVER 10'.

EDGE RESTRAINT

1. INSTALL AVIGNON, BELGIK, PIETRA, TUNDRA, OR UNIVERSAL EDGE CUT UNITS. CAST-IN-PLACE CONCRETE OR PRECAST CONCRETE CURBS SHALL BE UTILIZED IN VEHICULAR APPLICATIONS.
2. EDGE RESTRAINT MAY REST ON AN OPEN-GRADED OR DENSE-GRADED AGGREGATE BASE.

BASE

1. MOISTEN, SPREAD AND COMPACT THE ASTM NO. 57 AGGREGATE BASE LAYER IN ONE 4" THICK LIFT.
2. MAKE A MINIMUM OF TWO PASSES IN VIBRATORY MODE FOLLOWED BY AT LEAST TWO STATIC MODE WITH A MINIMUM 10 TON ROLLER, UNTIL NO VISIBLE MOVEMENT OF THE AGGREGATE. ALTERNATIVELY, A 13500 LB PLATE COMPACTOR CAN BE USED TO COMPACTER ASTM NO. 57 AGGREGATE BASE.
3. DO NOT ALLOW COMPACTOR TO CRUSH AGGREGATE.
4. SURFACE TOLERANCE OF THE ASTM NO. 57 BASE SHOULD BE ±1" OVER 10'.

BEDDING COURSE

1. MOISTEN, SPREAD AND SCREED ASTM NO 8. AGGREGATE BEDDING LAYER IN ONE 2" THICK LIFT
2. SURFACE TOLERANCE OF THE ASTM NO 8. BEDDING COURSE ±3/8" OVER 10'
3. CONSTRUCTION EQUIPMENT AND PEDESTRIAN TRAVEL ON SCREEDDED BEDDING COARSE IS PROHIBITED.

PAVER

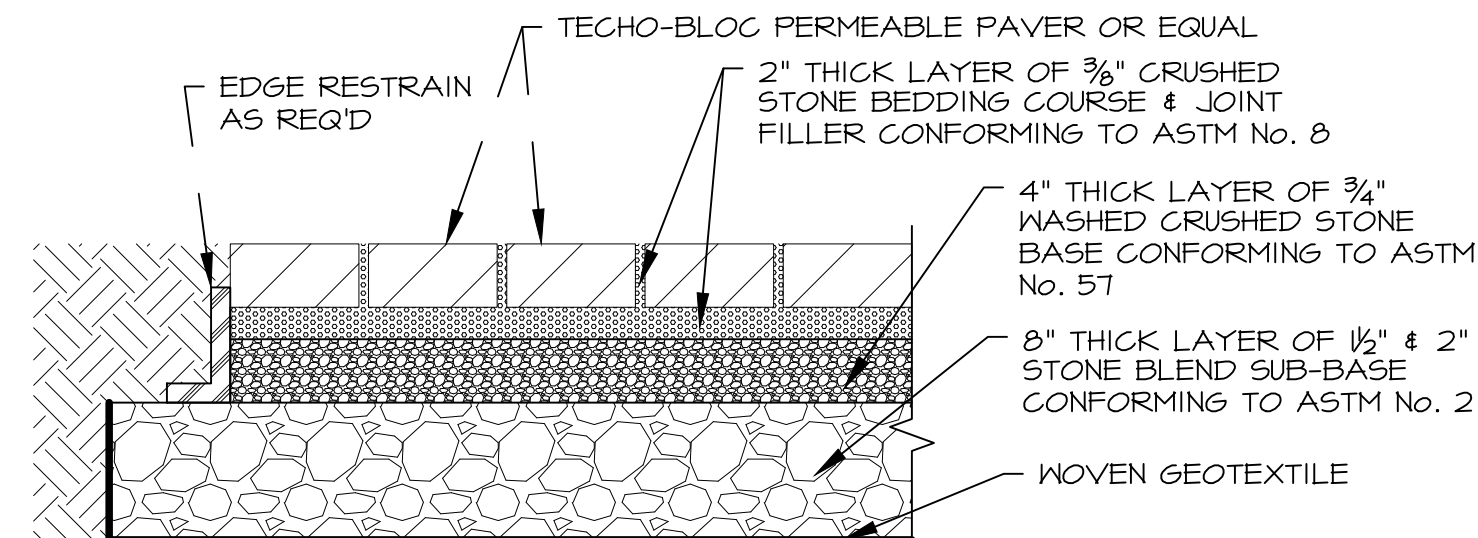
1. PAVERS SHOULD BE PLACED IN PATTERN SHOWN ON DRAWINGS. LAY UNITS TIGHT TO DESIGNATED LAYING PATTERNS. UNITS HAVE LUGS TO MAINTAIN CONSISTENT JOINT WIDTH.
2. IN SLOPED CONDITIONS START LAYING FROM THE BOTTOM IN AN UPHILL DIRECTION.
3. THE MINIMUM SLOPE FOR PERMEABLE PAVEMENT SURFACE IS 1%.
4. INFLO PAVERS CAN BE INSTALLED WITH TBIOOSI (TECHO-BLOC MECHANICAL TOOL) TO EXPEDITE INSTALLATION.
5. WHEN SUBJECT IT VEHICULAR TRAFFIC, CUT UNITS SHOULD NOT BE SMALLER THEN 1/3 THE WHOLE PAVER. WHEN USING CUT PAVERS MAINTAIN JOINT.
6. IN VEHICULAR APPLICATION LAY PATTERN PERPENDICULAR TO TRAFFIC FLOW.

JOINT FILL

1. FILL PAVER JOINT OPENINGS WITH ASTM NO. 8 AGGREGATE. SWEEP STONE TO FILL JOINTS. SURFACE MUST BE SWEEP CLEAN PRIOR TO COMPACTION
2. COMPACT WITH 5,000 LB PLATE COMPACTOR (TWO PASSES MINIMUM). INSTALL OF NEOPRENE PAD TO PROTECT THE TEXTURE OF THE PAVING UNITS.
3. DO NOT COMPACT WITHIN 6' OF UNRESTRAINED EDGES OF PAVERS.
4. APPLY ADDITIONAL AGGREGATE TO FILL THE JOINT OPENINGS IF NEEDED AND COMPACT.
5. SURFACE TOLERANCE OF COMPACTED PAVERS SHOULD BE ±3/8" OVER 10'.

POST INSTALLATION PROTECTION

1. MAINTAIN EROSION AND SEDIMENT MEASURES AT PERIMETER TO PREVENT CONTAMINATION OF POROUS PAVEMENT SYSTEM.



PERVIOUS PAVERS DETAIL

N.T.S.

3	6/22/2026	TAC SUBMITTAL	
2	1/6/2026	TAC SUBMITTAL	
1	12/17/2025	ZBA SUBMITTAL	
ISS.	DATE	DESCRIPTION OF ISSUE	
SCALE	NTS		
CHECKED	A.ROSS		
DRAWN	D.D.D.		

ROSS ENGINEERING, LLC
 Civil/Structural Engineering
 & Surveying
 650 Islington St, 2nd Floor
 Portsmouth, NH 03801
 (603) 433-7560

CLIENT
 ALEA LLC
 238 AUSTIN ST
 PORTSMOUTH, NH 03801

TITLE

DETAILS

238 AUSTIN ST
 PORTSMOUTH, NH 03801
 TAX MAP 135, LOT 61

JOB NUMBER	DWG. NO.	ISSUE
25-089	7 OF 7	3

