

AMBIT ENGINEERING, INC.

CIVIL ENGINEERS AND LAND SURVEYORS

200 Griffin Road, Unit 3, Portsmouth, NH 03801
Phone (603) 430-9282 Fax 436-2315

18 October 2021

Peter Britz, Acting Planning Director
City of Portsmouth
1 Junkins Avenue
Portsmouth, NH 03801

RE: Application for Site Plan Approval, Tax Map 125, Lot 3, 238 Deer Street

Dear Peter and TAC Members:

On behalf of 238 Deer Street, LLC we submit herewith the attached for Technical Advisory Committee review for the Site Plan approval. The project consist of the replacement of the existing structure (VFW – Statey's) at 238 Deer Street. The owners, 238 Deer Street LLC, are committed to providing much needed micro housing units to the Portsmouth downtown. This proposed replacement building will have first floor Retail Space and 21 Micro Housing Units, all under 500 square feet in size, on the second through fourth floors. The applicant obtained a Conditional Use Permit from the Planning Board on February 18, 2021, subject to the Conditions listed on the Site Plan, to the requirements for on-site parking. Relief from the Zoning Board was also obtained on September 28, 2021, subject to the Conditions listed on the Site Plan, for the following:

- 2.5 % Open Space
- 3.5 foot rear yard
- 8 foot penthouse setback with 60% floor area

Please find the following plans:

- Cover Sheet – This shows the Development Team, Legend, Site Location, and Site Zoning.
- Existing Conditions Plan C1 – This plan shows the property boundary, current improvements, and recently constructed features on the adjacent property.
- Demolition Plan C2 – This plan shows the removal of the existing structure.
- Site Plan C3 – This plan shows layout of the proposed features and contains Zoning Development and Impervious Surface charts.
- Utility Plan C4 – This plan shows the location of proposed utility connections to the proposed building.
- Grading Plan C5 – This plan shows site grading.
- Detail Sheets D1 and D2 – These plans show construction details.
- Architectural Plans A1 to A7 - These plans show the proposed building Architectural Footprint, Elevations, Details, and Unit Layout.

Also please find attached additional information regarding the application to assist in your review.

We ask that we be placed on the Agenda for the **November 2 Technical Advisory Committee Meeting**. We look forward to your review of this submission.

Sincerely,

John Chagnon

John R. Chagnon, PE
238 Deer Street Team

October 18, 2021

City of Portsmouth Planning Board

GREEN BUILDING STATEMENT

Re: Proposed 21 Micro Unit - Mixed Use Building at 238 Deer Street, Portsmouth, NH

- Foundation system to be cast in place concrete with continuous rigid insulation installed to depths required by the energy code. Continuous insulation to be provided under the concrete slab on grade for 2 feet along the exterior wall.
- Exterior Envelope: Designed to meet or exceed the prescriptive method of the 2015 International Energy Code requirements. Walls to have cavity filled with closed cell spray foam insulation and a continuous air barrier. Composite siding materials to utilize post-consumer materials.
- Exterior Windows to be aluminum clad wood windows, high-performance glazing to provide enhanced thermal performance and solar control. Residential unit windows will be operable for natural ventilation.
- Roofing system: Lighter colored membrane roofing system over sloped ridged insulation for cool roof performance.
- HVAC systems to consist of high-efficiency heat pumps. Meet ASHRAE ventilation code in all occupied spaces.
- Plumbing: All fixtures to be low flow.
- Lighting: Exterior lighting to be LED cutoff fixtures for energy efficiency and to minimize light pollution. All interior lighting to be LED throughout using less than 1 watt / sf and perimeter daylight sensors. Occupancy sensors to be utilized as required by code.
- Materials & Resources: Minimize waste during construction and operations. Also incorporate the use of regional, renewable, and low carbon footprint materials.

Sincerely,

Mark Gianniny, AIA
Senior Associate

INSPECTION & MAINTENANCE PLAN

FOR

238 Deer Street

Portsmouth, NH

Introduction

The intent of this plan is to provide 238 Deer Street (herein referred to as “owner”) with a list of procedures that document the inspection and maintenance requirements of the drainage structures for this development.

The following inspection and maintenance program is necessary to keep the stormwater management system functioning properly. These measures will also help minimize potential environmental impacts. By following the enclosed procedures, the owner will be able to maintain the functionality of the drainage structures and maximize their ability to drain the site effectively from stormwater runoff.

Annual Report

The owner shall prepare an annual Inspection & Maintenance Report. The report shall include a summary of the system’s maintenance and repair by transmission of the Inspection & Maintenance Log and other information as required. A copy of the report shall be delivered annually to the City of Portsmouth Code Enforcement Officer.

Inspection & Maintenance Checklist/Log

The following pages contain a Stormwater Management System Inspection & Maintenance Checklist and a blank copy of the Stormwater Management System Inspection & Maintenance Log. These forms are provided to the owner as a guideline for performing the inspection and maintenance. This is a guideline and should be periodically reviewed for conformance with current practice and standards.

DRAINAGE STRUCTURE COMPONENTS

Non-Structural BMP’s

Non-Structural best management practices (BMP’s) include temporary and permanent measures that typically require less labor and capital inputs and are intended to provide protection against erosion of soils. Examples of non-structural BMP’s on this project include but are not limited to: temporary and permanent mulching, temporary and permanent grass cover, trees, shrubs and ground covers, miscellaneous landscape plantings, dust control, tree protection, topsoiling, sediment barriers, and a stabilized construction entrance.

Structural BMP's

Structural BMP's are more labor and capital-intensive structures or installations that require more specialized personnel to install. Examples on this project include but are not limited to: storm drain catch basins, slot drains and pipes.

Inspection and Maintenance Requirements

The following summarizes the inspection and maintenance requirements for the various BMP's that may be found on this project.

- 1. Landscaped areas:** After each rain event of 0.5" or more during a 24-hour period, inspect landscaped areas for signs of disturbance, such as erosion. If damaged areas are discovered, immediately repair the damage. Repairs may include adding new topsoil, lime, seed, fertilizer and mulch.
- 2. Plantings:** Planting and landscaping (trees, shrubs) shall be monitored bi-monthly during the first year to insure viability and vigorous growth. Replace dead or dying vegetation with new stock and adjust the conditions that caused the dead or dying vegetation. During dryer times of the year, provide weekly watering or irrigation during the establishment period of the first year. Make the necessary adjustments to ensure long-term health of the vegetated covers, i.e. provide more permanent mulch or compost or other means of protection. Clean up dead leaves yearly to avoid drainage issues.
- 3. Storm Drain Catch Basins and Pipes:** Monitor drain inlets and outlets during construction. Monitor sediment levels in catch basin sumps and remove as necessary.

Stormwater Management System

Inspection & Maintenance Checklist for Post Construction Condition—for 238 Deer Street, Portsmouth, NH

BMP/System Component	Minimum Inspection Frequency	Minimum Inspection Requirements	Maintenance/Cleanout Threshold
Closed Drainage System			
Drainage Pipes	Yearly	<i>Check for sediment clogging, or soiled runoff.</i>	Clean entire drainage system and remove all sediments if discovered in piping.
Catch Basins	Bi-Annually	<i>Check for excessive accumulation of sediment in sump</i>	Remove sediment as necessary
Annual Report	Yearly	<i>Prepare Annual Report, including all Inspection & Maintenance Logs. Provide to City (if required).</i>	N/A

Stormwater Management System Maintenance Summary

Inspection & Maintenance Log—for 238 Deer Street, Portsmouth, NH

BMP/System Component	Date Inspected	Inspector	Problems Noted, Required Maintenance (List Items/Comments)	Date of Maintenance	Performed By

Data Sheets

REVISED - PARKING ASSESSMENT FOR 238 DEER STREET MIXED-USE BUILDING PORTSMOUTH, NH

Date: January 12, 2021

Subject: Revised - Parking Assessment
238 Deer Street Mixed-Use Building – Portsmouth, NH

To: Buz Couillard

From: Randy Dunton PE, PTOE, Gorrill Palmer (GP)

Copied: John Chagnon, Ambit Engineering;
Jeremiah Johnson, McHenry Architecture

Introduction:

Gorrill Palmer (GP) has prepared the following parking assessment for the proposed mixed-use building to be located at 238 Deer Street. On the first floor, the building will include approximately 2,629 square feet of retail space with additional space on the first floor dedicated for a lobby area and accesses to upper floors. The second through fourth floors include 7 micro (less than 500 square feet) apartments per floor for a total of 21 micro apartments. The proposed mixed-use building does not have enough area on-site to accommodate vehicular parking, and this assessment is to review what the impacts may be on the adjacent neighborhood from a parking perspective.

This parking assessment will review the parking requirements based on the ordinance, actual anticipated parking demand, and availability of parking spaces within the immediate area.

Parking Requirement per Ordinance:

The location of the proposed building is within an Overlay District. As such, it has specific parking requirements as identified in the “City of Portsmouth, New Hampshire – Zoning Ordinance” amended through December 16, 2019. The parking requirements for this use at this location are as follows (see attached for sections of the ordinance):

- **Retail Space: Section 10.1115.21** – Identifies that nonresidential uses within the Downtown Overlay District have no Off-Street parking requirement. Therefore, the first-floor retail space does not require any parking spaces.
- **Residential Units: Section 10.1112.311** – Requires that for dwelling units in a mixed-use development, the number of off-street parking spaces required is 0.5 spaces per unit for dwelling units less than 500 square feet. This would yield the need for 11 off-street parking spaces (rounded up from 10.5).



- **Section 10.1112.312** – Requires that any group of dwelling units on a lot containing more than 4 dwelling units provide one visitor parking space for every 5 dwelling units or portion thereof. This would yield the need for 5 off-street parking spaces.
- **Section 10.1115.23** – Because the site is located within the Overlay District, the number of required off-street parking spaces can be reduced by 4 spaces.

The following summarizes the number of required off-street parking spaces for the proposed mixed-use building (calculations attached):

Off-Street Parking Requirements	
Section	Spaces Required
Section 10.1115.21 (Retail)	0
Section 10.1112.311 (Residential – Occupant)	11
Section 10.1112.312 (Residential – Visitor)	5
Section 10.1115.23 (Overlay Dist. Reduction)	-4
Total Required Spaces	12

As the table summarizes, the proposed mixed-use building by strict interpretation of the ordinance would require 12 off-street parking spaces. However, it is our opinion this requirement is too high, and the following sections identify why it is too high.

Other Modes of Transportation:

The previous section identified what the off-street parking requirement would be based on the City Zoning Ordinance. The downtown location of this site makes it ideal to take advantage of other modes of transportation, thus reducing the need for a car and therefore reducing parking demand. The following identifies numerous advantages to the site's location and supporting amenities:

➤ **Pedestrian Accommodations:**

- Proximity to downtown – the location of the site is ideal in that residents of the units can easily walk from the apartment to the downtown on the existing sidewalk network without needing a car. They can walk to a place of employment, entertainment, food, or retail; all without needing a car. This ties well with the expected young professional that is expected to be attracted by this type of accommodations.
- Sidewalks – A robust sidewalk network is provided in the area that allows for easy walking from the site to the downtown for the purpose of business or personal.

➤ **Bicycle Accommodations:**

Per Section 10.1116.11 of the Zoning Ordinance (see attached ordinance section), 1 bicycle space for each 5 dwelling units or portion thereof is required for a multifamily dwelling. The developers will provide individual internal storage areas that will be sized to accommodate bicycles. These units will be accessible



from street level and will have easy access. A bicycle rack with space for a minimum of 5 bicycles will be provided outside. This will encourage the use of bicycles and reduce the need for a car, and therefore a parking space.

➤ Transit:

Within 650 feet of the site (easy walking distance), there are three Coast bus stops, providing direct access to Routes 40, 41, 42, & 43. Route 40 provides transportation primarily to the southwest and area like the Portsmouth Transportation Center, Route 41 provides transportation to the south and through the center of downtown, Route 42 provides transportation toward the west over to the Portsmouth International Airport at Pease, and Route 43 provides transportation toward the northwest and the Fox Run Mall. Through these routes, riders can access major points of destination for employment, entertainment, retail, transportation, and food.

➤ Ride-Share:

A waiting area will be provided within the building that includes a bulletin board in which to share postings of those offering or needing a ride. This area could also serve for those waiting for deliveries from somewhere else or Uber / taxi pick-up etc. The bulletin board can also serve as a location to post bus schedules and other transportation information to inform tenants of their options. This will provide additional alternatives for tenants who will not own a vehicle, and therefore not require parking within the area.

Forecast Parking Demand:

The previous “Parking Requirement per Ordinance” section identified parking requirements based on a strict interpretation of the ordinance. As identified in the previous “Other Modes of Transportation” section, the ordinance is most likely an overestimate of the actual parking demand for the proposed 21 micro units. The retail space does not require any on-street parking and therefore is not discussed in this section. The purpose of this section is to compare the parking demand based on supplemental sources to the parking requirements based on the City Ordinance. The following provides a summary of the methodology and findings of the parking demand assessment:

Micro Units:

The proposed mixed-use development is to include 21 micro units (less than 500 square feet) on floors 2-4. These units will be market rate units, but due to their small size the rent would remain lower than larger apartments. Typically, locations with lower rents attract those wishing to minimize their expenses, such as not having a car and the associated costs of having a car such as parking, maintenance, and insurance. To calculate the parking demand for the micro-units, GP reviewed the ITE Parking Generation Manual, 5th Edition.

Based on a review of that document, the most appropriate use appeared to be Land Use Code (LUC) – 223, Affordable Housing. The other option would be multi-family apartments, but at less than 500 square feet, this size unit is not intended for a family. Upon further review, there are multiple “setting/location” that could be used. For the purposes of this assessment, we averaged the rates of the different setting/locations. Those setting/location are listed as follows with their associated average parking rate (see attached).



Weekday Parking Demand – Summary Table

Peak Weekday Parking Demand		
Setting/Location	Peak Parking Demand – Average Rate per unit	Parking Demand (Spaces)
General Urban/Suburban (10 PM – 5 AM)	0.99	21
Dense Multi-Use Urban (10 PM – 5 AM)	0.53	12
Center City Core (10 PM – 5 AM)	0.16	4
Dense Multi-Use Urban – Single Room Only (10 PM - 5AM)	0.26	6
Average Peak Parking Demand	0.49	11

As can be seen from the summary table, the average peak parking demand rate is 0.49 spaces per unit with a respective parking demand of 11 spaces. Based upon the City Ordinance:

*10.1112.311 The required minimum number of **off-street** parking spaces for **uses 1.10 through 1.90, including dwelling units** in mixed-use developments, shall be based on the gross floor area of each **dwelling unit**, as follows:*

Dwelling Unit Floor Area	Required Parking Spaces
<i>Less than 500 sq. ft.</i>	<i>0.5 spaces per unit</i>
<i>500-750 sq. ft.</i>	<i>1.0 space per unit</i>
<i>Over 750 sq. ft.</i>	<i>1.3 spaces per unit</i>

As can be seen from the above Weekday Parking Demand table, the parking demand rate for this size apartment is 0.50 spaces per unit, with a parking demand of 11 spaces. The City Ordinance, when considering the tenant, potential visitors, and allowable deductions, resulted in a peak parking demand of 12 parking spaces. This indicates that the City Ordinance is relatively the same as the average rate from the ITE Parking Generation Manual, and that the LUC was the appropriate use.

However, the calculations based on the City Ordinance do not appear to factor in the downtown location and the use of other modes of transportation; walking, bicycling, transit, and ride-share as described previously herein.

It is our opinion that this rate (0.49 ITE or 0.50 City) is high and should be lower due to the downtown location of the proposed housing, the availability of public transportation and the small size of the units. Our opinion for a lower rate is supported by the lower ITE parking generation rates when the size and location of the apartments are considered. **If the “General Urban/Suburban” rate is removed from the Weekday Parking Demand summary table calculations, and the three rates that do consider the downtown location of the site as well as the small size, the average peak parking demand is reduced to 0.32 spaces per unit with a respective peak parking demand of 7 spaces.**

It should also be noted that the ITE peak parking demand rates are based on a time between 10:00 PM and 5:00 AM, the peak parking demand for residential uses. The peak parking demand for retail and office space is typically between 8:00 AM and 5:00 PM. Therefore, since the proposed residential housing is among offices and retail (some residential), this use compliments the parking demand well and is at its peak when the uses around it have



minimal demand, and has minimal demand when the uses around it are at their peak. **Therefore, the daytime demand for parking could be as few as 3 or 4 spaces during the peak time of the day.**

Locations to Park:

To evaluate parking in the area that could potentially accommodate the minimal need (7 spaces or less) of the proposed apartments, GP reviewed both the City's Park Portsmouth web site as well as completing field reviews of the immediate area around the site (approximately 650 feet). The field reviews were completed at approximately 9:30 AM on Tuesday, November 17, 2020 and again that evening at approximately 9:30 PM. The daytime field review was intended to review parking while businesses were open, and the evening field review was intended to review with most businesses closed, but with most of the residents home for the evening. We recognize this is a snapshot in time, but helps to establish some form of benchmark for discussion. Included in the field reviews were a walk through the Foundry Place and Hanover Street Parking Garages to get an approximation of the percent occupied. We also field reviewed the Portwalk Garage but the number of spaces that would be available to residents was limited and was full during our reviews. Included with the on-street and parking garage field review, we also reviewed the Bridge Street and Worth surface parking lots. The following summarizes our field observations of parking in the immediate area with the locations shown on the attached location plan:

Observed Percent Occupied (approximate)		
Location	Day Time	Evening
Foundry Place Parking Garage (600 spaces)	20%	10%
Hanover Street Parking Garage (900 spaces)	40-50%	20%
Bridge Street Surface Lot (62 spaces)	10-15%	Less than 10%
Worth Surface Lot (79 spaces)	70-80%	40-50%

As noted, this was a snapshot in time and based on general observation only. However, it provides a clear picture that there is considerable availability of parking in the immediate area for both residents / visitors of the proposed building to park their vehicles. The above does not include on-street metered parking which is available for short term guests. On-street parking is available on Deer Street, Bridge Street and Portwalk Place. Visitors could also use the parking garages and surface lots identified above.

It should be noted that the pandemic could be a factor in the low parking demand; however, even with a significant increase in demand, there still appears to be sufficient parking availability.

We also understand that currently, the Foundry Garage is not experiencing the demand that is committed for the garage from uses that are not currently built or occupied. We also recognize that the Foundry Garage is slighted to accommodate some parking when the nearby Hanover Street garage is updated.

However, with all these considerations, it would seem reasonable that the surrounding area, including a 600 space Foundry Garage, could accommodate 3 to 4 parking spaces during the day to support local residents and economic growth of the area.



Conclusions and Findings:

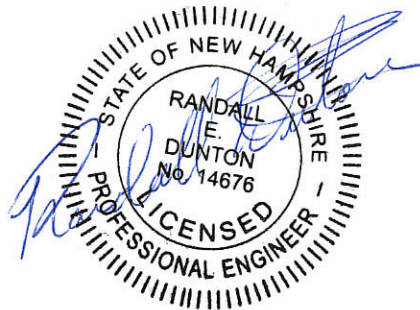
The following is a summary of the revised parking assessment's conclusions and findings:

1. **Parking Requirement per Ordinance:** By ordinance, the proposed mixed-use building requires 12 off-street parking spaces. Due to the limited on-site area, the required 12 off-street parking spaces cannot be accommodated on-site, and therefore need to be accommodated via satellite locations such as parking garages and surface lots.
2. **Forecast Parking Demand:** In further evaluating the peak parking demand for this type of use in a downtown area, the overall parking demand is forecast to be approximately 7 spaces. This peak parking demand occurs between 10:00 PM and 5:00 AM. The daytime parking demand could be as few as 3 to 4 vehicles.
3. **Other Modes of Transportation:** Given the close proximity to downtown and available other modes of transportation such as pedestrian accommodations, abilities for bicycles and access to transit, it could be expected that some residents will not need to own a vehicle and therefore not require a parking space. In addition, a bulletin board will be provided for those offering and needing ride share. The proposed building will require a minimum of 5 bicycle parking spaces. This requirement will be satisfied by both an internal storage area for bicycles as well as a proposed on-site bicycle rack.
4. **Locations to Park:** There are a considerable number of choices and availability for parking in the immediate area of the site, both during the day and at night. This includes two parking garages, two surface lots, metered parking spaces, and the potential to rent space privately in the area.
5. **Based on this assessment, it is our opinion that the peak parking demand for this use is relatively minimal and that the parking can be accommodated within the immediate area.**

Prepared by:

A handwritten signature in blue ink that reads "Randy Dunton".

Randy Dunton, PE, PTOE
Gorrill Palmer Consulting Engineers
rdunton@gorrillpalmer.com



JN 3768

Computed By: RED

11/23/2020

Parking Requirements for:
238 Mixed Use Building
Portsmouth, NH

Task: Calculate the required number of parking spaces

Reference: City of Portsmouth, New Hampshire
Zoning Ordinance (Amended Through December 16, 2019)
Section 10.1115.21 - Identifies Retail Space in Downtown Overlay District has no requirement for off-street parking
Section 10.1112.311 - Residential Units
Section 10.1112.312 - Visitor Parking Requirement
Section 10.1115.23 - Reduction in spaces due to Overlay District
Section 10.1116.11 - Bicycle Space Requirements

Given: 21 Micro Units (less than 500 sf)

Calculations :

Vehicle Off-Street Parking Requirements:

Section 10.1112.311
(21 units less than 500 sf) X (0.5 spaces per unit) = 10.5 spaces (**round up to 11 spaces**)

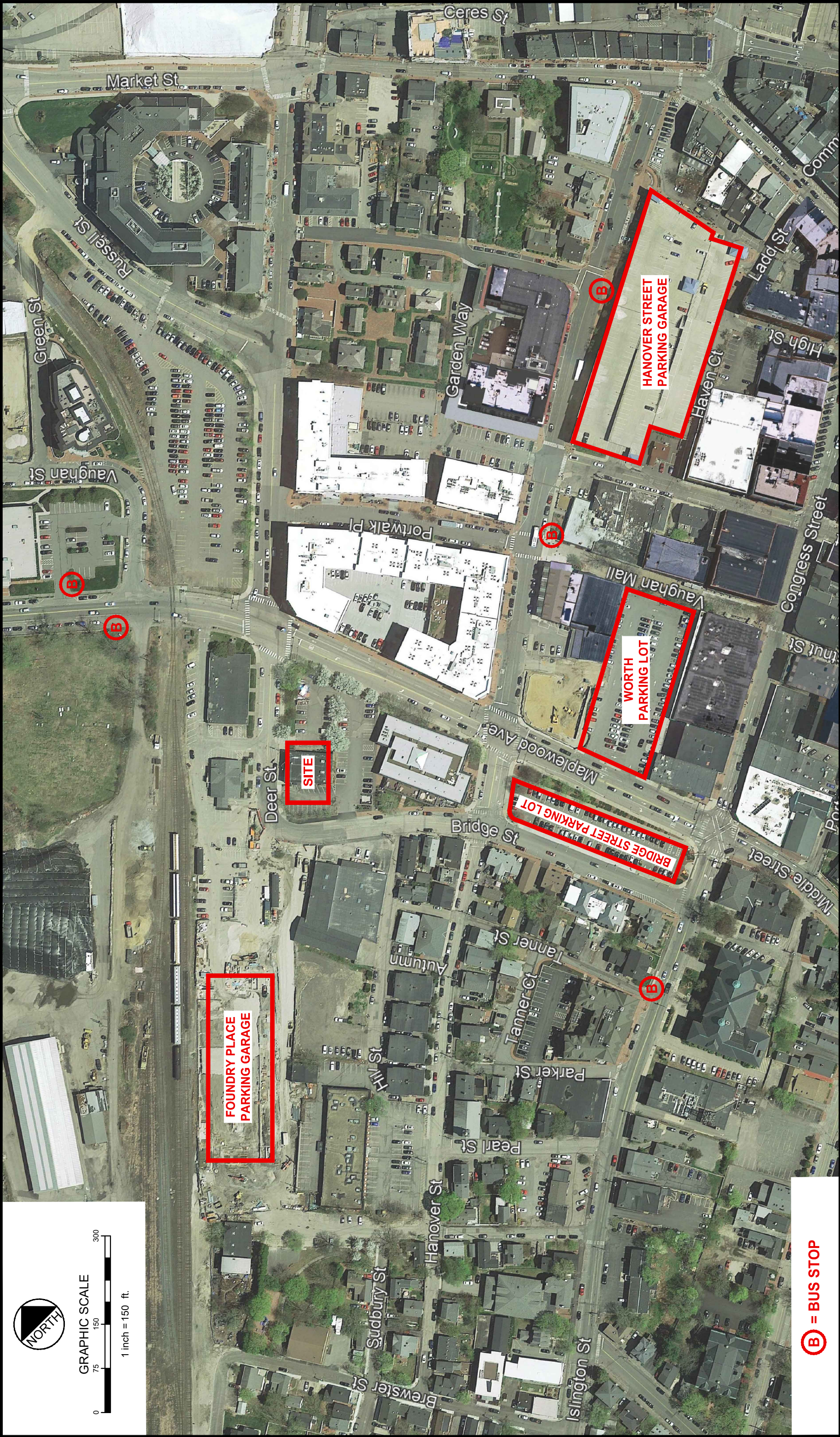
Section 10.1112.312
(21 units) / (5 spaces per dwelling unit) = 4.2 spaces (**round up to 5 spaces**)

Section 10.1115.23
Reduction of 4 spaces due to location within Downtown Overlay District

Summary of Off-Street parking Spaces Required	
Section 10.1115.21	0 Spaces
Section 10.1112.311	11 Spaces
Section 10.1112.312	5 Spaces
Section 10.1115.23	-4 Spaces
Total Required Spaces	12 Spaces

Bicycle Parking Requirement:

Section 10.1116.11
(21 units) X (1 bicycle space per 5 units) = 4.2 Bicycle spaces (**round to 5 bicycle spaces**)



238 DEER STREET MIXED USE BUILDING

238 DEER STREET, LLC

238 DEER STREET
PORTSMOUTH, NEW HAMPSHIRE
PERMIT PLANS

OWNER/APPLICANT:

238 DEER STREET, LLC
238 DEER STREET
PORTSMOUTH, N.H. 03801
Tel. (978) 479-1718

CIVIL ENGINEER & LAND SURVEYOR:

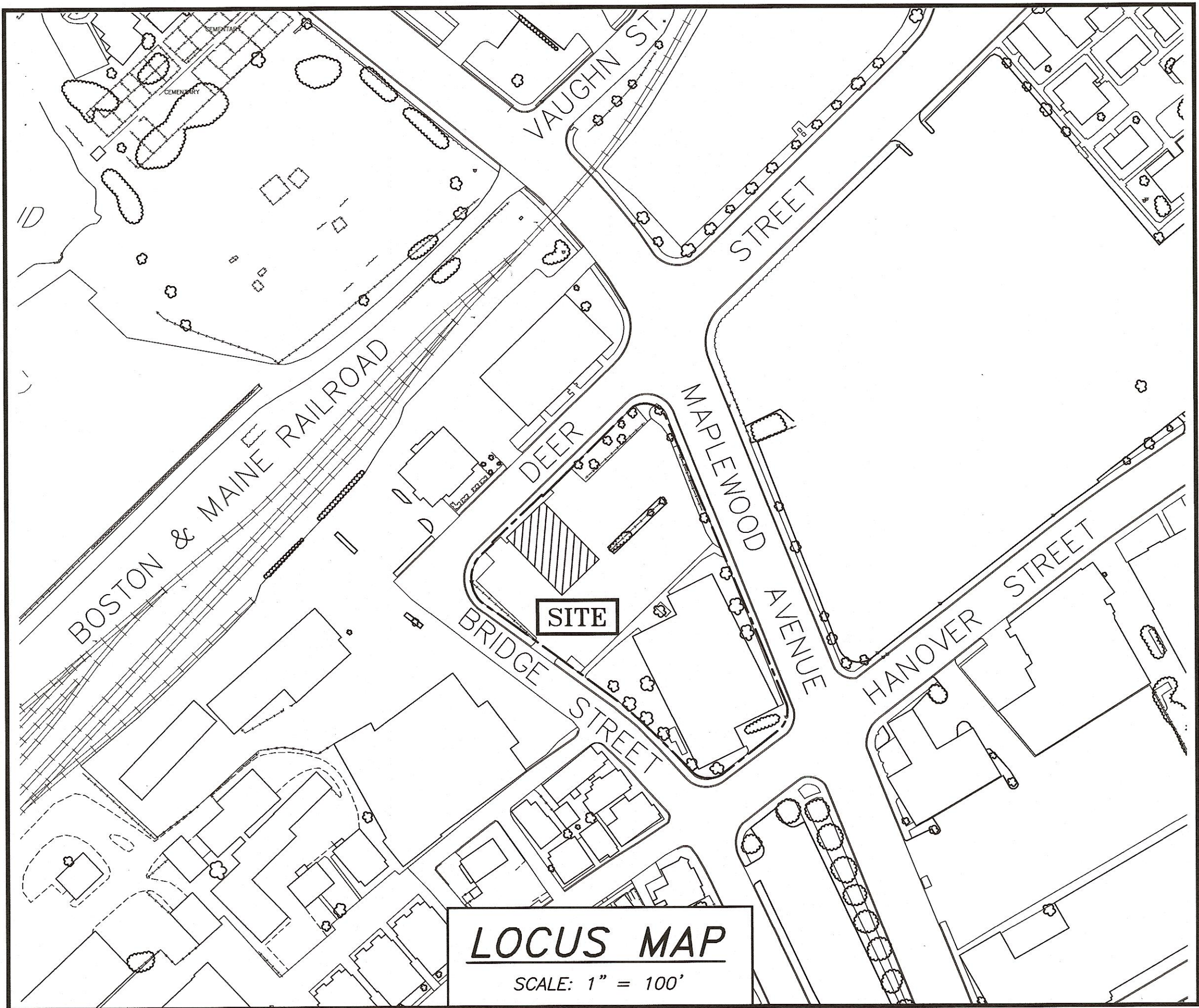
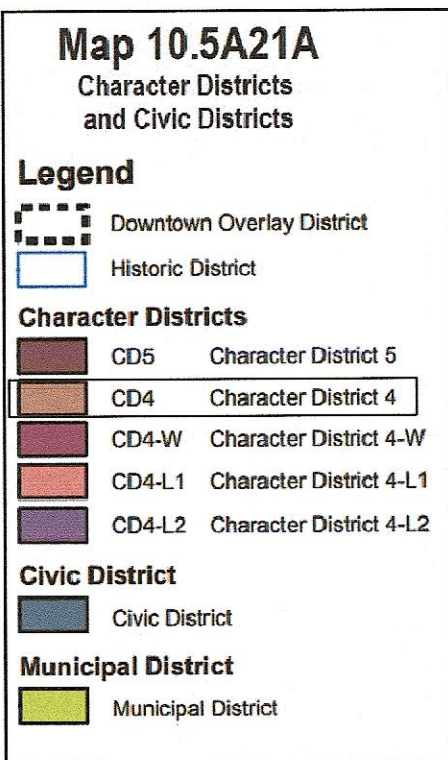
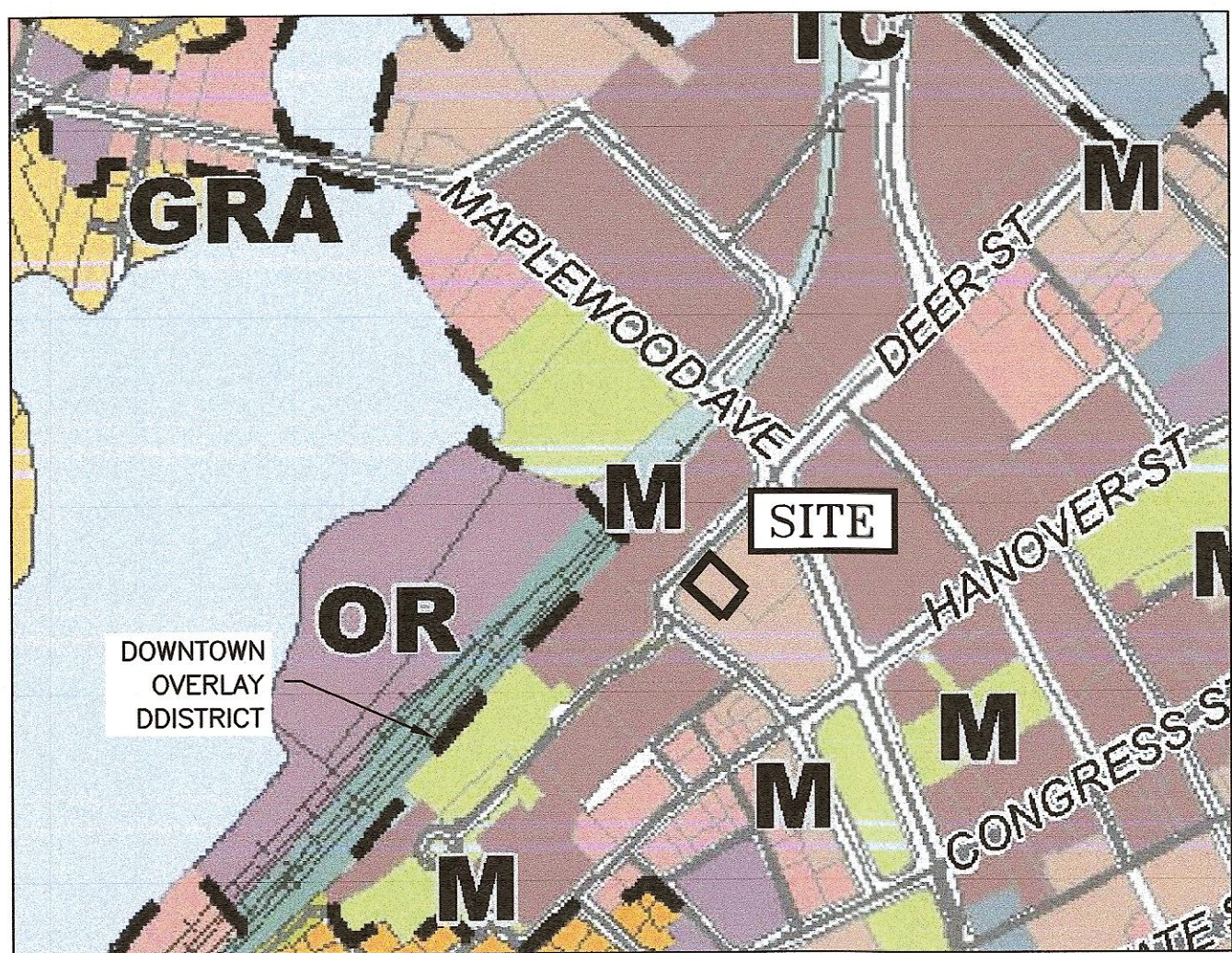
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ARCHITECT:

McHENRY ARCHITECTURE
4 MARKET STREET
PORTSMOUTH, N.H. 03801
TEL. (603) 430-0274

PARKING CONSULTANT

GORRILL PALMER
707 SABLE OAKS DRIVE,
SUITE 30
SOUTH PORTLAND, ME 04106
TEL. (207) 772-2515



PERMIT LIST:

PORTSMOUTH HDC: PENDING
PORTSMOUTH ZONING BOARD: GRANTED 9/28/21
PORTSMOUTH SITE REVIEW: PENDING
PORTSMOUTH CONDITIONAL USE PERMIT: APPROVED 2/18/21

LEGEND:

EXISTING	PROPOSED	
---	---	PROPERTY LINE
---	---	SETBACK
S	S	SEWER PIPE
SL	SL	SEWER LATERAL
G	G	GAS LINE
D	D	STORM DRAIN
W	W	WATER LINE
WS	WS	WATER SERVICE
UGE	UGE	UNDERGROUND ELECTRIC
OHW	OHW	OVERHEAD ELECTRIC/WIRES
---	---	FOUNDATION DRAIN
---	---	EDGE OF PAVEMENT (EP)
---	---	CONTOUR
---	---	SPOT ELEVATION
---	---	UTILITY POLE
---	---	WALL MOUNTED EXTERIOR LIGHTS
---	---	TRANSFORMER ON CONCRETE PAD
---	---	ELECTRIC HANDHOLD
---	---	SHUT OFFS (WATER/GAS)
---	---	GATE VALVE
---	---	HYDRANT
---	---	CATCH BASIN
---	---	SEWER MANHOLE
---	---	DRAIN MANHOLE
---	---	TELEPHONE MANHOLE
---	---	PARKING SPACE COUNT
---	---	PARKING METER
---	---	LANDSCAPED AREA
TBD	TBD	TO BE DETERMINED
CI	CI	CAST IRON PIPE
COP	COP	COPPER PIPE
DI	DI	DUCTILE IRON PIPE
PVC	PVC	POLYVINYL CHLORIDE PIPE
RCP	RCP	REINFORCED CONCRETE PIPE
AC	AC	ASBESTOS CEMENT PIPE
VC	VC	VITRIFIED CLAY PIPE
EP	EP	EDGE OF PAVEMENT
EL	EL	ELEVATION
FF	FF	FINISHED FLOOR
INV	INV	INVERT
S =	S =	SLOPE FT/FT
TBM	TBM	TEMPORARY BENCH MARK
TYP	TYP	TYPICAL



UTILITY CONTACTS

ELECTRIC:
EVERSOURCE
1700 LAFAYETTE ROAD
PORTSMOUTH, N.H. 03801
Tel. (603) 436-7708, Ext. 555.5678
ATTN: MICHAEL BUSBY, P.E. (MANAGER)

NATURAL GAS:
UNITIL
325 WEST ROAD
PORTSMOUTH, N.H. 03801
Tel. (603) 294-5144
ATTN: DAVE BEAULIEU

CABLE:
COMCAST
155 COMMERCE WAY
PORTSMOUTH, N.H. 03801
Tel. (603) 679-5695 (X1037)
ATTN: MIKE COLLINS

SEWER & WATER:
PORTSMOUTH DEPARTMENT OF PUBLIC WORKS
680 PEVERLY HILL ROAD
PORTSMOUTH, N.H. 03801
Tel. (603) 427-1530
ATTN: JIM TOW

COMMUNICATIONS:
FAIRPOINT COMMUNICATIONS
JOE CONSIDINE
1575 GREENLAND ROAD
GREENLAND, N.H. 03840
Tel. (603) 427-5525

INDEX OF SHEETS

DWG NO.	
C1	EXISTING CONDITIONS PLAN
C2	DEMOLITION PLAN
C3	SITE PLAN
C4	UTILITY PLAN
C5	GRADING PLAN
D1-D2	DETAIL SHEETS
A1-A7	ARCHITECTURAL PLANS

PORTSMOUTH APPROVAL CONDITIONS NOTE:
ALL CONDITIONS ON THIS PLAN SET SHALL REMAIN IN EFFECT IN
PERPETUITY PURSUANT TO THE REQUIREMENTS OF THE CITY OF
PORTSMOUTH SITE PLAN REVIEW REGULATIONS.

APPROVED BY THE PORTSMOUTH PLANNING BOARD

CHAIRMAN

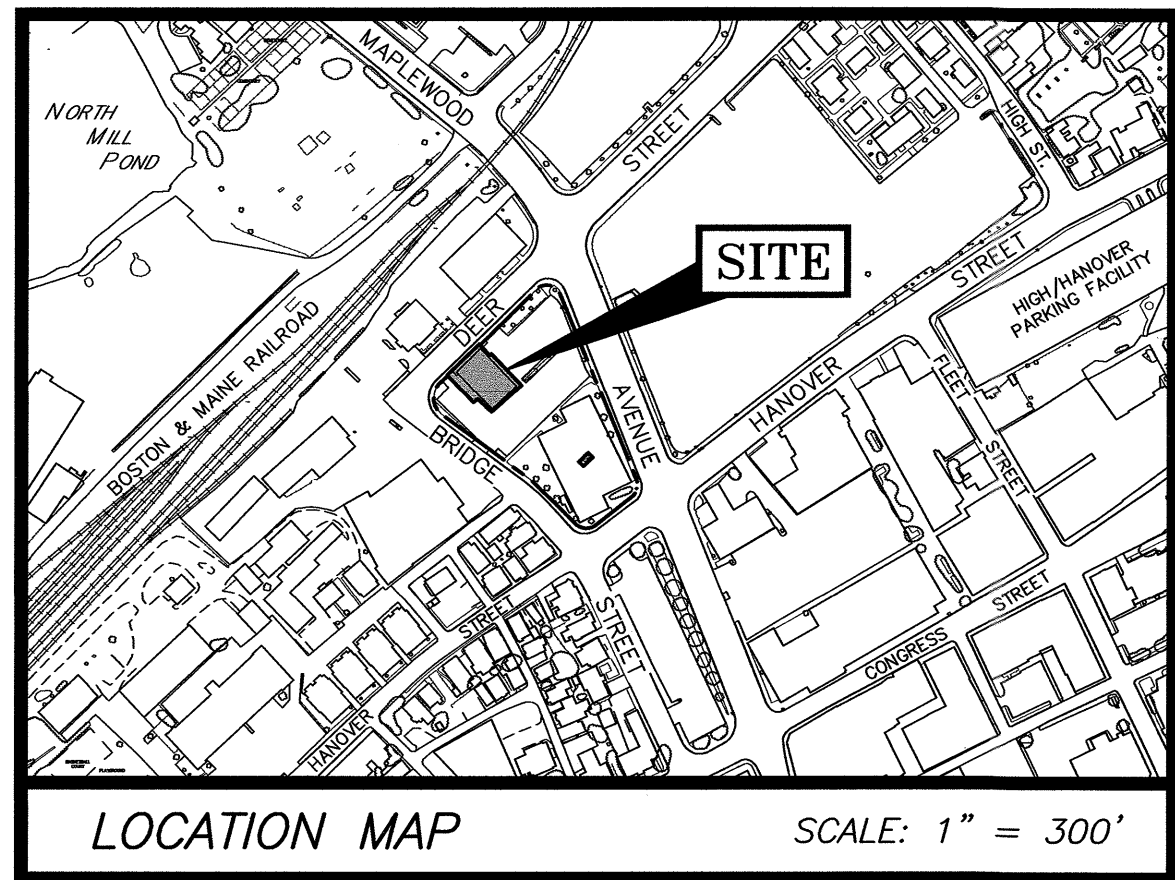
DATE

PERMIT PLANS
238 DEER STREET, LLC
238 DEER STREET
PORTSMOUTH, N.H.



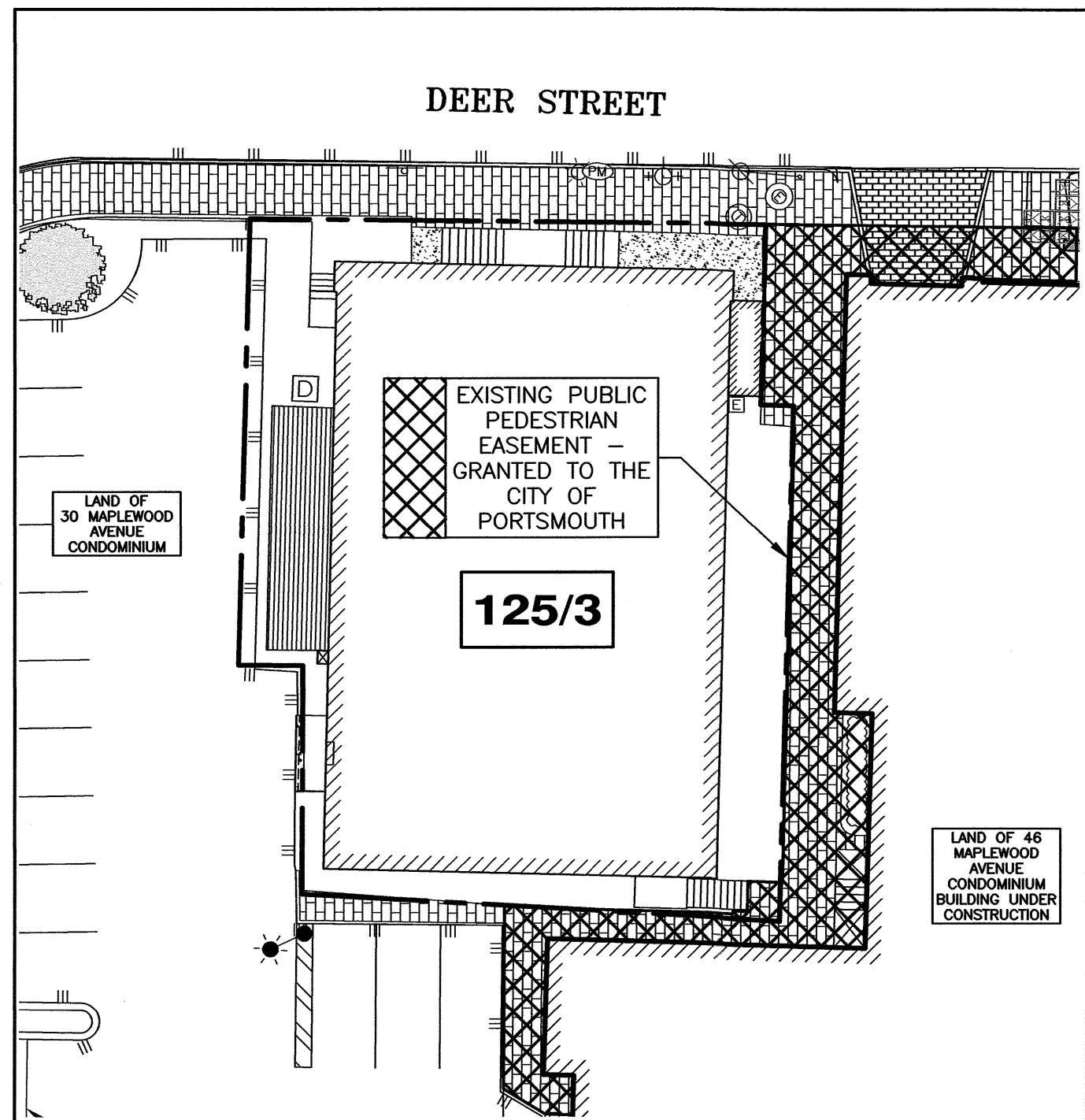
AMBIT ENGINEERING, INC.
Civil Engineers & Land Surveyors
200 Griffin Road - Unit 3
Portsmouth, N.H. 03801-7114
Tel (603) 430-9282
Fax (603) 436-2315

PLAN SET SUBMITTAL DATE: 18 OCTOBER 2021



PLAN REFERENCES:

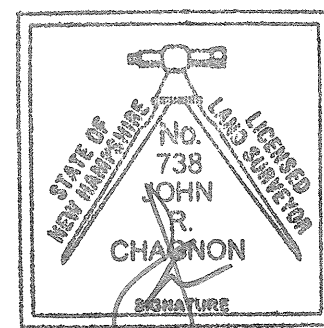
1. VAUGHAN STREET URBAN RENEWAL PROJECT N.H. R-10, PORTSMOUTH, NEW HAMPSHIRE, DISPOSITION PLAN PARCEL 7. DATED OCT. 1973 BY ANDERSON-NIHOLS & CO., INC. RCRD #D-4119.
2. VAUGHAN STREET URBAN RENEWAL PROJECT N.H. R-10, PORTSMOUTH, NEW HAMPSHIRE, DISPOSITION PLAN PARCEL 10. DATED OCT. 1973 BY ANDERSON-NIHOLS & CO., INC. RCRD #D-4125.
3. VAUGHAN STREET URBAN RENEWAL PROJECT N.H. R-10, PORTSMOUTH, NEW HAMPSHIRE, DISPOSITION MAP. DATED NOV. 1969 BY ANDERSON-NIHOLS & CO., INC. RCRD #D-2408.
4. EASEMENT SITE PLAN, TAX MAP 125 - LOT 2, 30 MAPLEWOOD, LLC TO PUBLIC SERVICE COMPANY OF NEW HAMPSHIRE (PSNH), SCALE: 1" = 20', OCTOBER 2013 BY AMBIT ENGINEERING. RCRD D-38148.
5. PROPOSED EASEMENT TO CITY OF PORTSMOUTH, SCALE: 1" = 10', 9/18/13 BY AMBIT ENGINEERING. BK 5512, PG 1046.
6. CONDOMINIUM SITE PLAN, TAX MAP 125 - LOT 2, BY AMBIT ENGINEERING. RCRD D-38936; AMENDED AT RCRD D-39005.
7. SUBDIVISION PLAN TAX MAP 125 - LOT 2, OWNER: 30 MAPLEWOOD, LLC, 30-46 MAPLEWOOD AVENUE, CITY OF PORTSMOUTH, COUNTY OF ROCKINGHAM, STATE OF NEW HAMPSHIRE, PREPARED BY AMBIT ENGINEERING, INC., SCALE 1" = 20', DATED OCTOBER 2015 REVISED 4/18/17, RCRD D-40246
8. PLAN OF LAND NO. 238 DEER ST. PORTSMOUTH, N.H., SCALE: 1IN = 10 FT., DATED MAY 1954 PREPARED BY JOHN W. DURGIN CIVIL ENGINEERS RCRD #02164



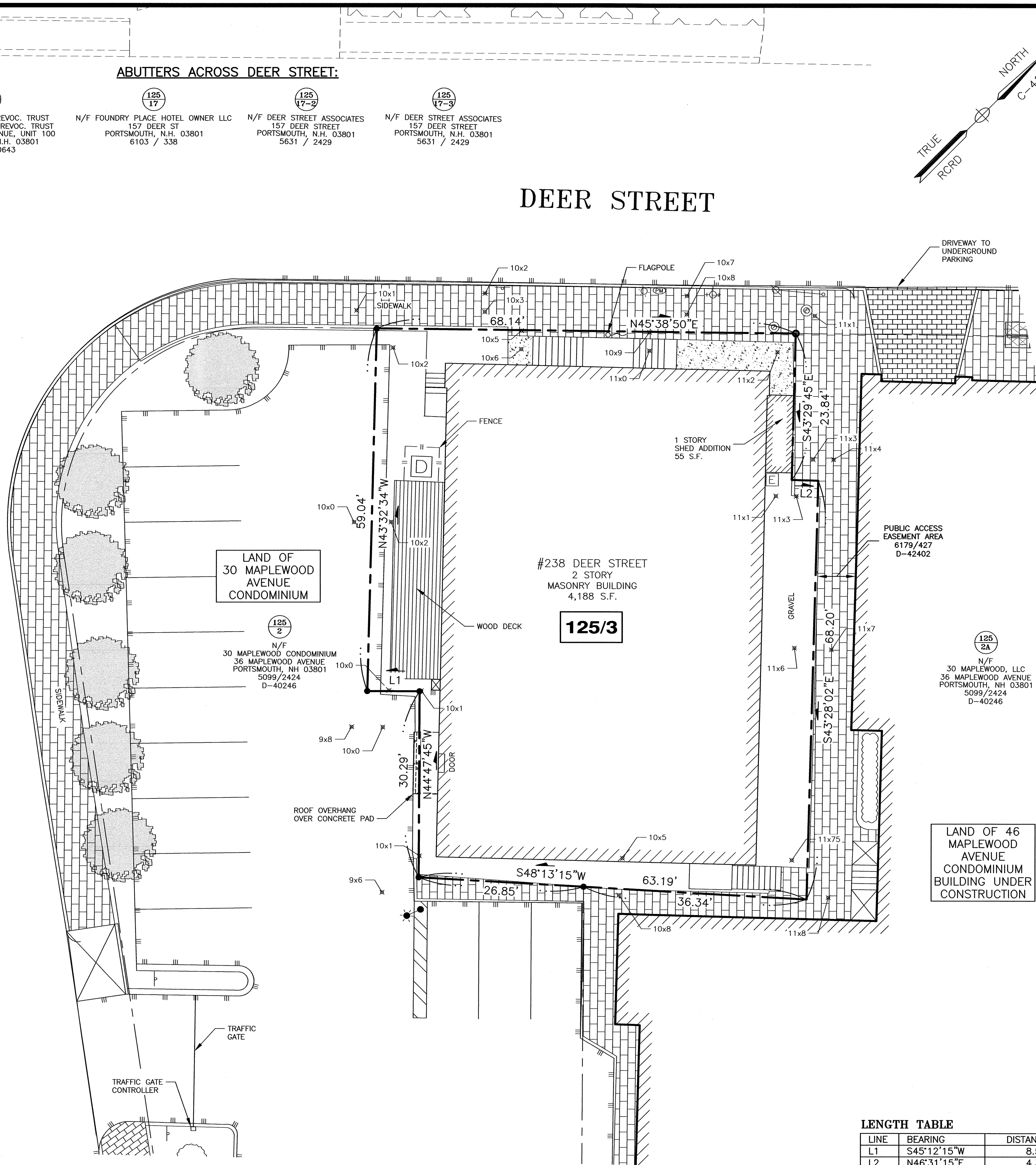
I CERTIFY THAT THIS PLAN WAS PREPARED UNDER MY DIRECT SUPERVISION, THAT IT IS THE RESULT OF A FIELD SURVEY BY THIS OFFICE AND HAS AN ACCURACY OF THE CLOSED TRAVERSE THAT EXCEEDS THE PRECISION OF 1:15,000.

JOHN R. CHAGNON, LLS 738

9.1.21
DATE

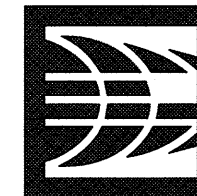


BRIDGE STREET



LENGTH TABLE

LINE	BEARING	DISTANCE
L1	S45°12'15"W	8.50'
L2	N46°31'15"E	4.30'



AMBIT ENGINEERING, INC.
Civil Engineers & Land Surveyors
200 Griffin Road - Unit 3
Portsmouth, N.H. 03801-7114
Tel (603) 430-9282
Fax (603) 436-2315

NOTES:

- 1) PARCEL IS SHOWN ON THE CITY OF PORTSMOUTH ASSESSORS MAP 125 AS LOT 3.
- 2) OWNER OF RECORD:
238 DEER STREET, LLC.
238 DEER STREET
PORTSMOUTH, NH 03801
5890/1712
RCRD #02164
- 3) PARCEL IS LOCATED IN THE CHARACTER DISTRICT 4, HISTORIC DISTRICT, AND DOWNTOWN OVERLAY DISTRICT.
- 4) DIMENSIONAL REQUIREMENTS:
CHARACTER DISTRICT 4 (CD4):
MIN. LOT AREA: NO REQUIREMENT
FRONTAGE: NO REQUIREMENT
SETBACKS:
FRONT (MAX.) 10 FEET (PRIMARY)
SIDE NO REQUIREMENT
REAR 5 FEET
MAXIMUM STRUCTURE HEIGHT: 40 FEET
MAXIMUM STRUCTURE COVERAGE: 90%
MAXIMUM BUILDING FOOTPRINT: 15,000 S.F.
MINIMUM OPEN SPACE: 10%
MINIMUM FRONT LOT LINE BUILDOUT: 50%
- 5) LOT AREA: 6,181 S.F., 0.1419 ACRES.
- 6) PARCEL IS NOT IN A FLOOD HAZARD ZONE AS SHOWN ON FIRM PANEL 33015C0259F, JANUARY 29, 2021
- 7) THE PURPOSE OF THIS PLAN IS TO SHOW THE BOUNDARY AND EXISTING CONDITIONS ON MAP 125, LOT 3.

SITE DEVELOPMENT
238 DEER STREET, LLC
238 DEER STREET
PORTSMOUTH, N.H.

0	ISSUED FOR COMMENT	9/1/21
NO.	DESCRIPTION	DATE

REVISIONS

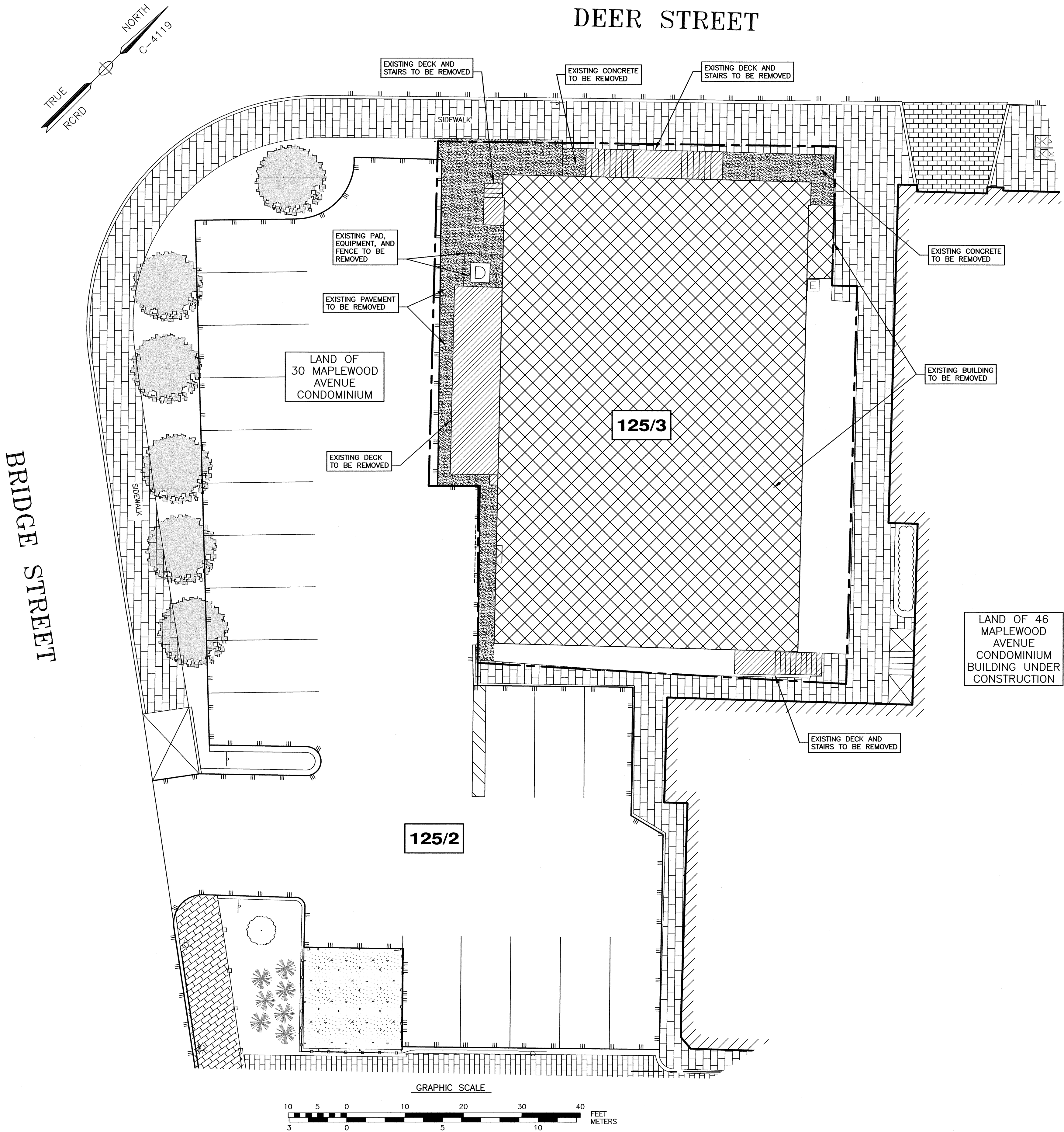
SCALE: 1" = 10' AUGUST 2021

EXISTING
CONDITIONS PLAN

C1

DEMOLITION NOTES:

- A) THE LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE AND THE LOCATIONS ARE NOT GUARANTEED BY THE OWNER OR THE DESIGNER. IT IS THE CONTRACTORS' RESPONSIBILITY TO LOCATE UTILITIES AND ANTICIPATE CONFLICTS. CONTRACTOR SHALL REPAIR EXISTING UTILITIES DAMAGED BY THEIR WORK AND RELOCATE EXISTING UTILITIES THAT ARE REQUIRED TO BE RELOCATED PRIOR TO COMMENCING ANY WORK IN THE IMPACTED AREA OF THE PROJECT.
- B) ALL MATERIALS SCHEDULED TO BE REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTORS UNLESS OTHERWISE SPECIFIED. THE CONTRACTOR SHALL DISPOSE OF ALL MATERIALS OFF-SITE IN ACCORDANCE WITH ALL FEDERAL, STATE, AND LOCAL REGULATIONS, ORDINANCES AND CODES. THE CONTRACTOR SHALL COORDINATE REMOVAL, RELOCATION, DISPOSAL, OR SALVAGE OF UTILITIES WITH THE OWNER AND APPROPRIATE UTILITY COMPANY.
- C) ANY EXISTING WORK OR PROPERTY DAMAGED OR DISRUPTED BY CONSTRUCTION/ DEMOLITION ACTIVITIES SHALL BE REPLACED OR REPAIRED TO THE ORIGINAL EXISTING CONDITIONS BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- D) THE CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UTILITIES AND CALL DIG SAFE AT LEAST 72 HOURS PRIOR TO THE COMMENCEMENT OF ANY DEMOLITION/CONSTRUCTION ACTIVITIES.
- E) SAWCUT AND REMOVE PAVEMENT ONE FOOT OFF PROPOSED EDGE OF PAVEMENT OR EXISTING CURB LINE IN AREAS WHERE PAVEMENT TO BE REMOVED ABUTS EXISTING PAVEMENT OR CONCRETE TO REMAIN.
- F) IT IS THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE THEMSELVES WITH THE CONDITIONS OF ALL THE PERMIT APPROVALS.
- G) THE CONTRACTOR SHALL OBTAIN AND PAY FOR ADDITIONAL CONSTRUCTION PERMITS, NOTICES AND FEES NECESSARY TO COMPLETE THE WORK AND ARRANGE FOR AND PAY FOR ANY INSPECTIONS AND APPROVALS FROM THE AUTHORITIES HAVING JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY ADDITIONAL AND OFF-SITE DISPOSAL OF MATERIALS REQUIRED TO COMPLETE THE WORK.
- H) THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL EXISTING STRUCTURES, CONCRETE, UTILITIES, VEGETATION, PAVEMENT, AND CONTAMINATED SOIL WITHIN THE WORK LIMITS SHOWN UNLESS SPECIFICALLY IDENTIFIED TO REMAIN.
- I) ALL WORK WITHIN THE CITY OF PORTSMOUTH RIGHT OF WAY SHALL BE COORDINATED WITH THE CITY OF PORTSMOUTH DEPARTMENT OF PUBLIC WORKS (DPW).
- J) CONTRACTOR SHALL PROTECT ALL PROPERTY MONUMENTATION THROUGHOUT DEMOLITION AND CONSTRUCTION OPERATIONS. SHOULD ANY MONUMENTATION BE DISTURBED, THE CONTRACTOR SHALL EMPLOY A NH LICENSED LAND SURVEYOR TO REPLACE THEM.
- K) PROVIDE INLET PROTECTION BARRIERS AT ALL CATCH BASINS WITHIN CONSTRUCTION LIMITS AND MAINTAIN FOR THE DURATION OF THE PROJECT. INLET PROTECTION BARRIERS SHALL BE HIGH FLOW SILT SACK BY ACF ENVIRONMENTAL OR APPROVED EQUAL. INSPECT BARRIERS WEEKLY AND AFTER EACH RAIN OF 0.25 INCHES OR GREATER. CONTRACTOR SHALL COMPLETE A MAINTENANCE INSPECTION REPORT AFTER EACH INSPECTION. SEDIMENT DEPOSITS SHALL BE REMOVED AFTER EACH STORM EVENT OR MORE OFTEN IF WARRANTED OR FABRIC BECOMES CLOGGED. EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE START OF ANY CLEARING OR DEMOLITION ACTIVITIES.
- L) THE CONTRACTOR SHALL PAY ALL COSTS NECESSARY FOR TEMPORARY PARTITIONING, BARRICADING, FENCING, SECURITY AND SAFETY DEVICES REQUIRED FOR THE MAINTENANCE OF A CLEAN AND SAFE CONSTRUCTION SITE.
- M) ANY CONTAMINATED MATERIAL REMOVED DURING THE COURSE OF THE WORK WILL REQUIRE HANDLING IN ACCORDANCE WITH NHDES REGULATIONS. CONTRACTOR SHALL HAVE A HEALTH AND SAFETY PLAN IN PLACE, AND COMPLY WITH ALL APPLICABLE PERMITS, APPROVALS, AUTHORIZATIONS, AND REGULATIONS



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Civil Engineers & Land Surveyors

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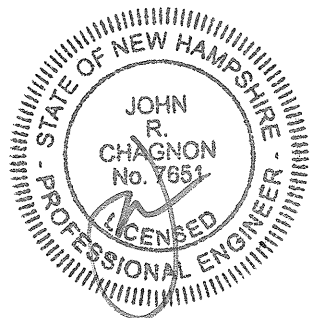
NOTES:

- 1) THE CONTRACTOR SHALL NOTIFY DIG SAFE AT 1-888-DIG-SAFE (1-888-344-7233) AT LEAST 72 HOURS PRIOR TO COMMENCING ANY EXCAVATION ON PUBLIC OR PRIVATE PROPERTY.
- 2) UNDERGROUND UTILITY LOCATIONS ARE BASED UPON BEST AVAILABLE EVIDENCE AND ARE NOT FIELD VERIFIED. LOCATING AND PROTECTING ANY ABOVEGROUND OR UNDERGROUND UTILITIES IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND/OR THE OWNER. UTILITY CONFLICTS SHOULD BE REPORTED AT ONCE TO THE DESIGN ENGINEER.
- 3) CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE "NEW HAMPSHIRE STORMWATER MANUAL, VOLUME 3, EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION. (NHDES DECEMBER 2008).

SITE DEVELOPMENT
238 DEER STREET, LLC
238 DEER STREET
PORTSMOUTH, N.H.

0	TAC WORKSHOP	3/2/21
NO.	DESCRIPTION	DATE

REVISIONS



SCALE: 1" = 10' NOVEMBER 2020

DEMOLITION
PLAN

C2

ZONING DEVELOPMENT STANDARD

CD4: CHARACTER DISTRICT 4

BUILDING PLACEMENT (PRINCIPLE):

	REQUIRED	EXISTING	PROPOSED
MAX. PRINCIPLE FRONT YARD:	10.0'	1'	0'
MAX. SECONDARY FRONT YARD:	N/A	N/A	N/A
MIN. SIDE YARD:	NR	0'	0'
MIN. REAR YARD:	5.0'	3.5'	3.5'
FRONT LOT LINE BUILDOUT:	50% MIN.	78%	92%

BUILDING TYPES:

ALLOWED BUILDING TYPES: ROWHOUSE, APARTMENT, LIVE/WORK, SMALL/LARGE COMMERCIAL
PROHIBITED: HOUSE & DUPLEX

ALLOWED FACADE TYPE: STOOP, STEP, SHOPFRONT, OFFICEFRONT, RECESSED-ENTRY
PROHIBITED: PORCH & FORECOURT

BUILDING FORM:

	REQUIRED	EXISTING	PROPOSED
MAX. STRUCTURE HEIGHT:	40.0' + 2.0' PENTHOUSE	23' +/-	42'
STRUCTURE HEIGHT (IN STORIES):	3	1	3 + PENTHOUSE
PENTHOUSE AREA:	50% MAX. OF STORY BELOW	N/A	3,206 S.F. - 60% 1,907 S.F. - 35.6%
PENTHOUSE SETBACK:	15.0'	N/A	8.0'
MAX. FINISHED FLOOR SURFACE OF GROUND FLOOR ABOVE SIDEWALK GRADE:	36 INCHES	6'	1'
MIN. GROUND STORY HEIGHT:	12.0'	14.0'	12.0'
MIN. SECOND STORY HEIGHT:	10.0'	N/A	10.5'
FACADE GLAZING (OTHER):	20% MIN. TO 50% MAX.	N/A	42%

ROOF TYPE ALLOWED: FLAT, GABLE, HIP, GAMBREL, MANSARD

LOT OCCUPATION:

	REQUIRED	EXISTING	PROPOSED
MAX. BUILDING BLOCK:	200'	53'	63'
MAX. FACADE MOD. LENGTH:	80'	53'	21'
MIN. ENTRANCE SPACING:	50'	N/A	N/A
MAX. BUILDING COVERAGE:	90%	74%	85%
MAX. BUILDING FOOTPRINT:	15,000 SF	4,243 S.F.	5,286 S.F.
GROSS BUILDING:		8,346 S.F.	19,190 S.F.
MIN. LOT AREA:	NR	6,181 S.F.	6,181 S.F.
MIN. LOT AREA/DWELLING (LOT AREA/# OF UNITS):	NR	N/A	N/A
MIN. OPEN SPACE :	10%	9.67%	2.7%

IMPERVIOUS SURFACE AREAS

(TO PROPERTY LINE)

STRUCTURE	PRE-CONSTRUCTION IMPERVIOUS (S.F.)	POST-CONSTRUCTION IMPERVIOUS (S.F.)
BUILDING	4,243	5,286
DECKS	264	0
STAIRS	194	0
CONCRETE	137	0
PAVEMENT	458	70
BRICK WALKWAY	104	559
GRAVEL	531	0
TOTAL	5931	5915
LOT SIZE	6,181	6,181
% LOT COVERAGE	96.0%	95.7%

LANDSCAPE SCHEDULE

No.	ITEM	SIZE	QTY
1	PURPLE LOVEGRASS	2 GAL.	31
2	ERGAROSTIS SPECTABILIS	1 1/2" CAL.	2
3	PINK SPIRES CRABAPPLE	18"-24" SPD.	4
	MALUS "PINK SPIRES"		
	EVER-LOW YEW		
	TAXUS MEDIA "EVER-LOW"		

PLANNING BOARD CUP APPROVAL CONDITIONS:

1) A minimum of 7 off-street parking spaces shall be provided via a long-term lease, shared parking agreement or option to enter into a long-term lease or share parking agreement with a property owner in the vicinity of the project. The lease, shared parking agreement or option for the off-site parking spaces shall be reviewed annually with the property owner and Planning Director and shall be renewed as needed for a period of up to 5 years from the issuance of the final certificate of occupancy for the property.

2) Revise the draft lease agreement related to the tenants' obligation to secure off-site parking if the tenant owns a car by removing paragraph 2 of the draft lease agreement presented by the applicant. The final lease agreement shall be reviewed and approved by the Planning Director and City Attorney.

BOARD OF ADJUSTMENT APPROVAL CONDITIONS:

1) Penthouse-level units shall not exceed 500 square feet.



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Tel (603) 430-9282
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NOTES:

1) PARCEL IS SHOWN ON THE CITY OF PORTSMOUTH ASSESSORS MAP 125 AS LOT 3.

2) OWNER OF RECORD:
238 DEER STREET, LLC.
238 DEER STREET
PORTSMOUTH, NH 03801
5890/1712
RCRD #02164

3) PARCEL IS LOCATED IN THE CHARACTER DISTRICT 4, HISTORIC DISTRICT, AND DOWNTOWN OVERLAY DISTRICT.

4) DIMENSIONAL REQUIREMENTS:

CHARACTER DISTRICT 4 (CD4):
MIN. LOT AREA: NO REQUIREMENT
FRONTAGE: NO REQUIREMENT
SETBACKS:
FRONT (MAX.) 10 FEET (PRIMARY)
SIDE NO REQUIREMENT
REAR 5 FEET
MAXIMUM STRUCTURE HEIGHT: 40 FEET
MAXIMUM STRUCTURE COVERAGE: 90%
MAXIMUM BUILDING FOOTPRINT: 15,000 S.F.
MINIMUM OPEN SPACE: 10%
MINIMUM FRONT LOT LINE BUILDOUT: 50%

5) LOT AREA: 6,181 S.F., 0.1419 ACRES.

6) PARCEL IS NOT IN A FLOOD HAZARD ZONE AS SHOWN ON FIRM PANEL 33015C0259F, JANUARY 29, 2021

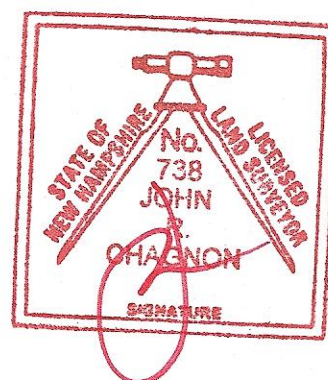
7) THE PURPOSE OF THIS PLAN IS TO SHOW A PROPOSED REPLACEMENT STRUCTURE ON MAP 125, LOT 3.

SITE DEVELOPMENT

238 DEER STREET, LLC
238 DEER STREET
PORTSMOUTH, N.H.

NO.	DESCRIPTION	DATE
0	ISSUED FOR COMMENT	9/1/21

REVISIONS



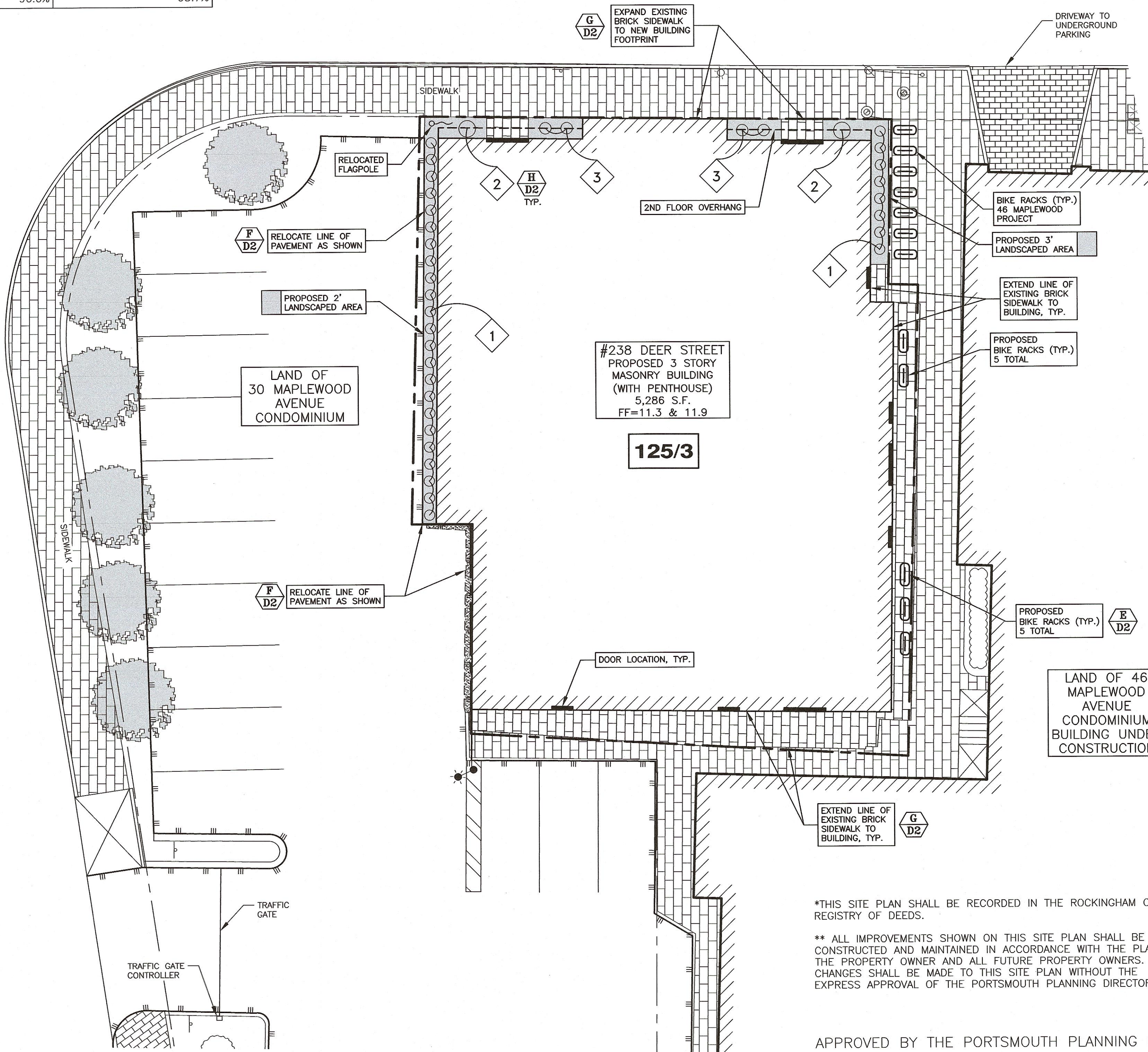
SCALE: 1" = 10'

AUGUST 2021

SITE PLAN

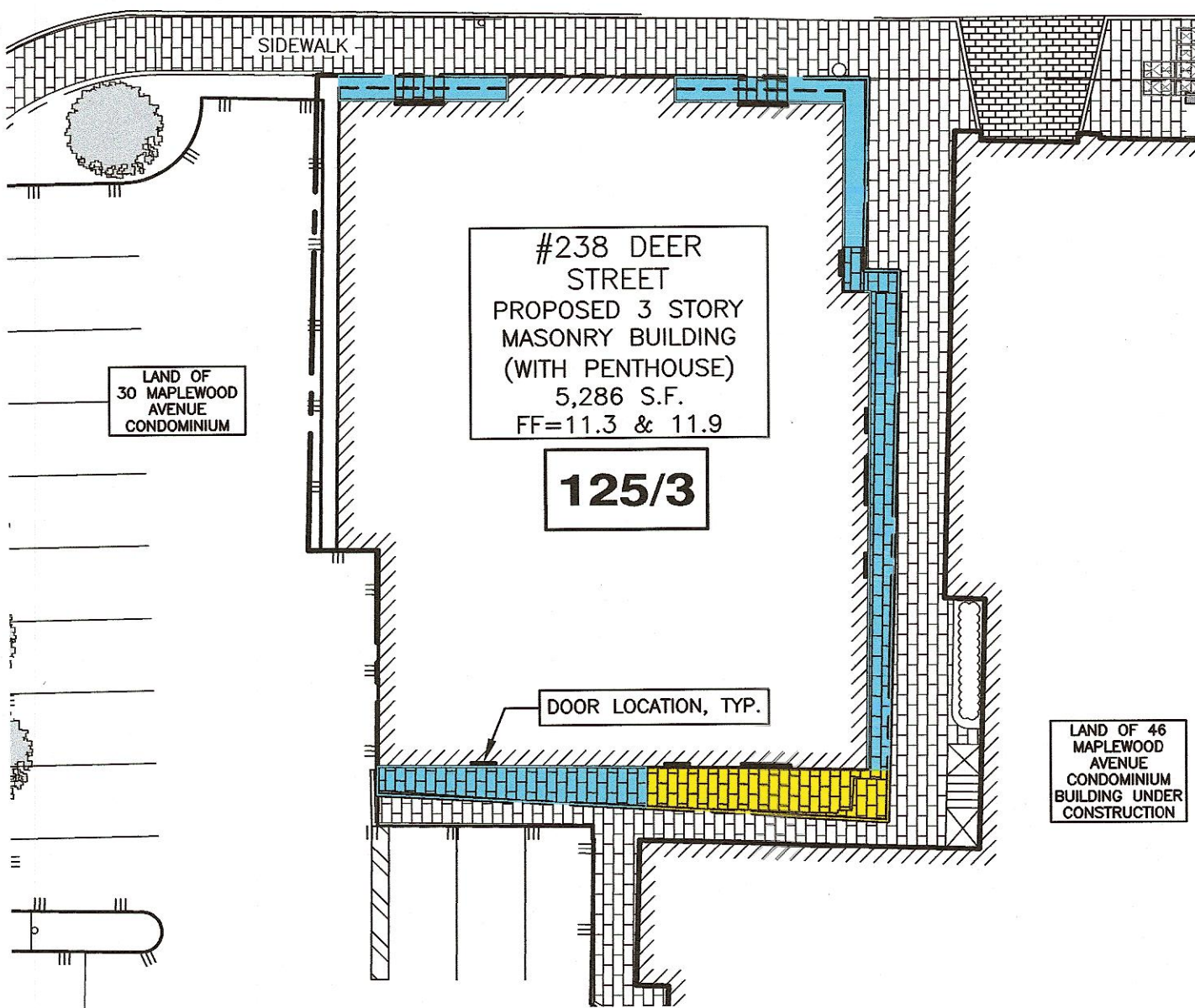
C3

DEER STREET



DEER STREET

BRIDGE STREET

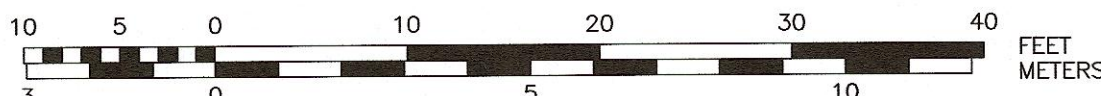


OPEN SPACE EXHIBIT

1"=20'

MEETS ORDINANCE CRITERIA	169 S.F. (2.7%)
MEETS ORDINANCE INTENT	536 S.F. (8.7%)
TOTAL	705 S.F. (11.4%)

GRAPHIC SCALE



*THIS SITE PLAN SHALL BE RECORDED IN THE ROCKINGHAM COUNTY REGISTRY OF DEEDS.

** ALL IMPROVEMENTS SHOWN ON THIS SITE PLAN SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE PLAN BY THE PROPERTY OWNER AND ALL FUTURE PROPERTY OWNERS. NO CHANGES SHALL BE MADE TO THIS SITE PLAN WITHOUT THE EXPRESS APPROVAL OF THE PORTSMOUTH PLANNING DIRECTOR.

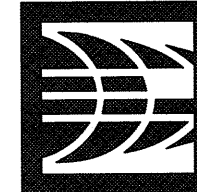
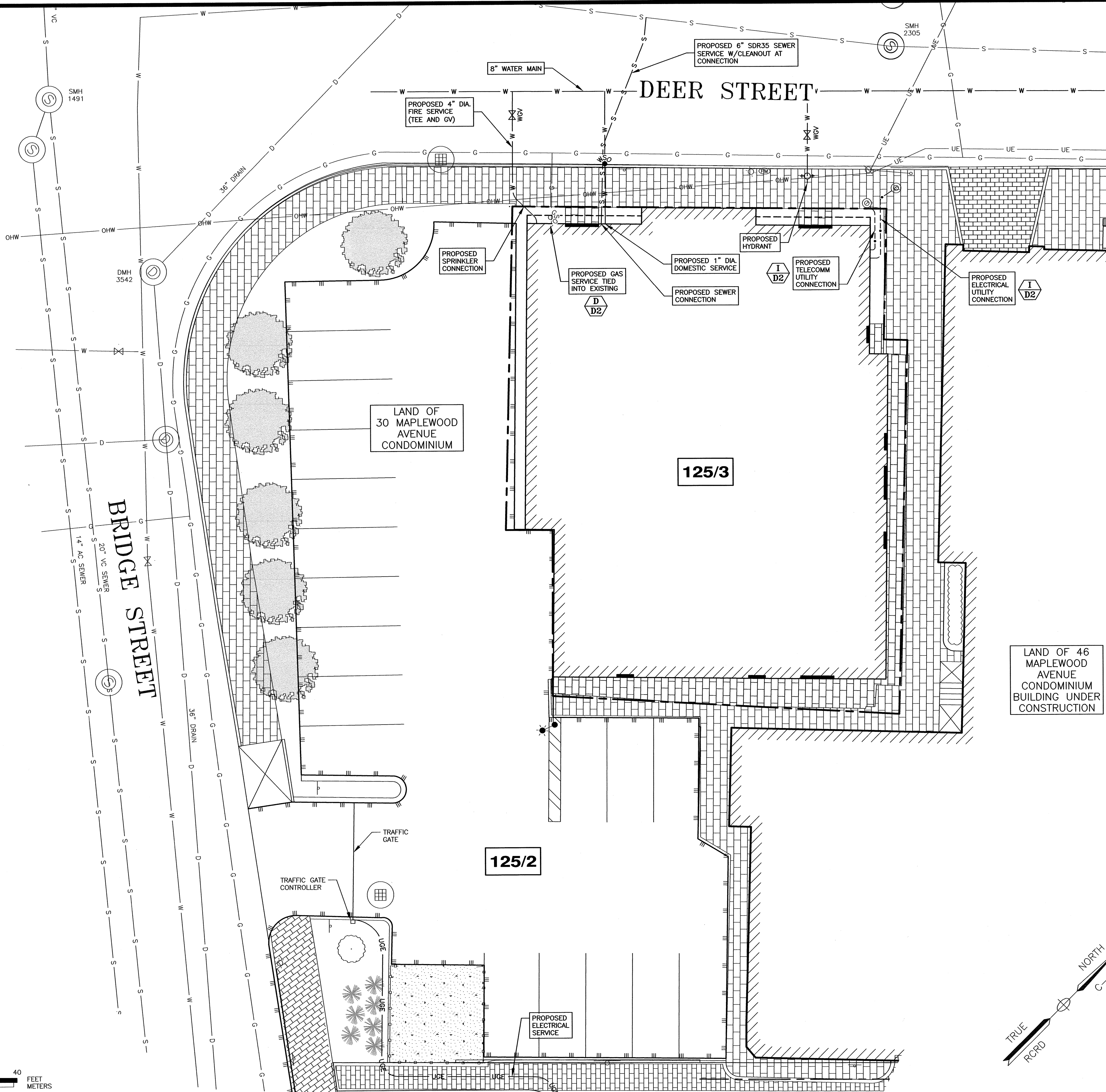
APPROVED BY THE PORTSMOUTH PLANNING BOARD

CHAIRMAN

DATE

UTILITY NOTES:

- 1) SEE EXISTING CONDITIONS PLAN FOR BENCHMARK INFORMATION.
- 2) COORDINATE ALL UTILITY WORK WITH APPROPRIATE UTILITY.
- 3) CONTRACTOR SHALL COORDINATE ALL ELECTRIC WORK INCLUDING BUT NOT LIMITED TO: CONDUIT CONSTRUCTION, MANHOLE CONSTRUCTION, UTILITY POLE CONSTRUCTION, OVERHEAD WIRE RELOCATION, AND TRANSFORMER CONSTRUCTION WITH POWER COMPANY.
- 4) ALL WATER MAIN INSTALLATIONS SHALL BE CLASS 52, POLYWRAPPED, CEMENT LINED DUCTILE IRON PIPE.
- 5) ALL WATERMAIN INSTALLATIONS SHALL BE PRESSURE TESTED AND CHLORINATED AFTER CONSTRUCTION AND BEFORE ACTIVATING THE SYSTEM. CONTRACTOR SHALL COORDINATE WITH THE CITY OF PORTSMOUTH.
- 6) ALL SEWER PIPE SHALL BE PVC SDR 35 UNLESS OTHERWISE STATED.
- 7) ALL WORK WITHIN CITY R.O.W. SHALL BE COORDINATED WITH CITY OF PORTSMOUTH.
- 8) CONTRACTOR SHALL MAINTAIN UTILITY SERVICES TO ABUTTING PROPERTIES THROUGHOUT CONSTRUCTION.
- 9) ANY CONNECTION TO EXISTING WATERMAIN SHALL BE CONSTRUCTED BY THE CITY OF PORTSMOUTH.
- 10) EXISTING UTILITIES TO BE REMOVED SHALL BE CAPPED AT THE MAIN AND MEET THE DEPARTMENT OF PUBLIC WORKS STANDARDS FOR CAPPING OF WATER AND SEWER SERVICES.
- 11) ALL ELECTRICAL MATERIAL WORKMANSHIP SHALL CONFORM TO THE NATIONAL ELECTRIC CODE, LATEST EDITION, AND ALL APPLICABLE STATE AND LOCAL CODES.
- 12) THE EXACT LOCATION OF NEW UTILITY SERVICES AND CONNECTIONS SHALL BE COORDINATED WITH BUILDING DRAWINGS AND UTILITY COMPANIES.
- 13) ADJUST ALL MANHOLES, CATCH BASINS, CURB BOXES, ETC. WITHIN LIMITS OF WORK TO FINISH GRADE.
- 14) ALL UNDERGROUND CONDUITS SHALL HAVE NYLON PULL ROPES TO FACILITATE PULLING CABLES.
- 15) THE CONTRACTOR SHALL OBTAIN, PAY FOR, AND COMPLY WITH ALL REQUIRED PERMITS, ARRANGE FOR ALL INSPECTIONS, AND SUBMIT COPIES OF ACCEPTANCE CERTIFICATED TO THE OWNER PRIOR TO THE COMPLETION OF PROJECT.
- 16) THE CONTRACTOR SHALL PROVIDE AND INSTALL ALL MANHOLES, BOXES, FITTINGS, CONNECTORS, COVER PLATES AND OTHER MISCELLANEOUS ITEMS NOT NECESSARILY DETAILED IN THESE DRAWING TO RENDER INSTALLATION OF UTILITIES COMPLETE AND OPERATIONAL.
- 17) CONTRACTOR SHALL PROVIDE EXCAVATION, BEDDING, BACKFILL AND COMPACTION FOR NATURAL GAS SERVICES.
- 18) A 10-FOOT MINIMUM EDGE TO EDGE HORIZONTAL SEPARATION SHALL BE PROVIDED BETWEEN ALL WATER AND SANITARY SEWER LINES. AN 18-INCH MINIMUM OUTSIDE TO OUTSIDE VERTICAL SEPARATION SHALL BE PROVIDED AT ALL WATER/SANITARY SEWER CROSSINGS WATER ABOVE SEWER.
- 19) SAWCUT AND REMOVE PAVEMENT AND CONSTRUCT PAVEMENT TRENCH PATCH FOR ALL PROPOSED UTILITIES LOCATED IN EXISTING PAVEMENT AREAS TO REMAIN.
- 20) GATE VALVES, FITTINGS, ETC. SHALL MEET THE REQUIREMENTS OF THE CITY OF PORTSMOUTH.
- 21) COORDINATE TESTING OF SEWER CONSTRUCTION WITH THE CITY OF PORTSMOUTH.
- 22) ALL SEWER PIPES WITH LESS THAN 6' COVER SHALL BE INSULATED.



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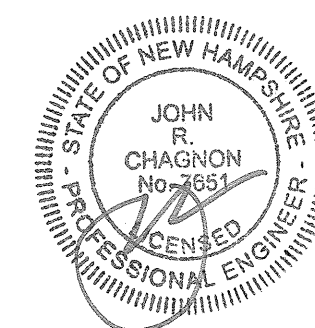
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- 2) UNDERGROUND UTILITY LOCATIONS ARE BASED UPON BEST AVAILABLE EVIDENCE AND ARE NOT FIELD VERIFIED. LOCATING AND PROTECTING ANY ABOVEGROUND OR UNDERGROUND UTILITIES IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND/OR THE OWNER. UTILITY CONFLICTS SHOULD BE REPORTED AT ONCE TO THE DESIGN ENGINEER.
- 3) CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE "NEW HAMPSHIRE STORMWATER MANUAL, VOLUME 3, EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION. (NHDES DECEMBER 2008).
- 4) INSTALL CATCH BASIN INLET PROTECTION ON ALL EXISTING AND PROPOSED CATCH BASINS UNTIL CONSTRUCTION IS COMPLETED AND THE SITE IS STABILIZED.
- 5) ALL WATER MAIN AND SANITARY SEWER WORK SHALL MEET THE STANDARDS OF THE NEW HAMPSHIRE STATE PLUMBING CODE AND CITY OF PORTSMOUTH DEPARTMENT OF PUBLIC WORKS.
- 6) UTILITY AS-BUILTS SHALL BE SUBMITTED TO THE CITY OF PORTSMOUTH DEPARTMENT OF PUBLIC WORKS UPON COMPLETION OF THE PROJECT.
- 7) EVERSOURCE WORK ORDER #6893710
- 8) PROPOSED SEWER FLOW:
21 UNITS X 170 GPD/UNITS = 3,570 GPD
2,585 S.F. COMMERCIAL X 5 GPD/100 S.F. = 130 GPD
TOTAL FLOW: 3,700 GPD
- 9) THE APPLICANT SHALL HAVE A COMMUNICATIONS SITE SURVEY CONDUCTED BY A MOTOROLA COMMUNICATIONS CARRIER APPROVED BY THE PORTSMOUTH'S COMMUNICATIONS DIVISION. THE RADIO COMMUNICATIONS CARRIER MUST BE FAMILIAR AND CONVERSANT WITH THE PORTSMOUTH POLICE AND FIRE RADIO SYSTEMS CONFIGURATION. IF THE SITE SURVEY INDICATES THAT IT IS NECESSARY TO INSTALL A SIGNAL REPEATER EITHER ON OR NEAR THE PROPOSED PROJECT, THOSE COSTS SHALL BE THE RESPONSIBILITY OF THE PROPERTY OWNER. THE PROPERTY OWNER WILL BE REQUIRED TO MAINTAIN ANY INSTALLED EQUIPMENT. THE PROPERTY OWNER SHALL BE RESPONSIBLE TO PAY FOR THE SITE SURVEY WHETHER OR NOT THE SURVEY INDICATES THAT EQUIPMENT IS NECESSARY. THE OWNER SHALL COORDINATE WITH THE SUPERVISOR OF RADIO COMMUNICATIONS FOR PORTSMOUTH. THE SURVEY SHALL BE COMPLETED AND ANY REQUIRED EQUIPMENT INSTALLED, TESTED, AND ACCEPTED PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY.
- 10) FINAL CONDUIT LOCATION SUBJECT TO CONFIRMATION FROM UTILITY PROVIDERS.

SITE DEVELOPMENT
238 DEER STREET, LLC
238 DEER STREET
PORTSMOUTH, N.H.

NO.	DESCRIPTION	DATE
2	TAC SUBMISSION	10/18/21
1	UPDATE UTILITIES	3/23/21
0	TAC WORKSHOP	3/2/21

REVISIONS



SCALE: 1" = 10' NOVEMBER 2020

UTILITY
PLAN

C4



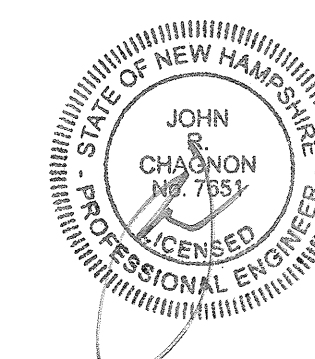
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**SITE DEVELOPMENT
238 DEER STREET, LLC
238 DEER STREET
PORTSMOUTH, N.H.**

0	TAC SUBMISSION	10/18/21
NO.	DESCRIPTION	DATE
REVISIONS		



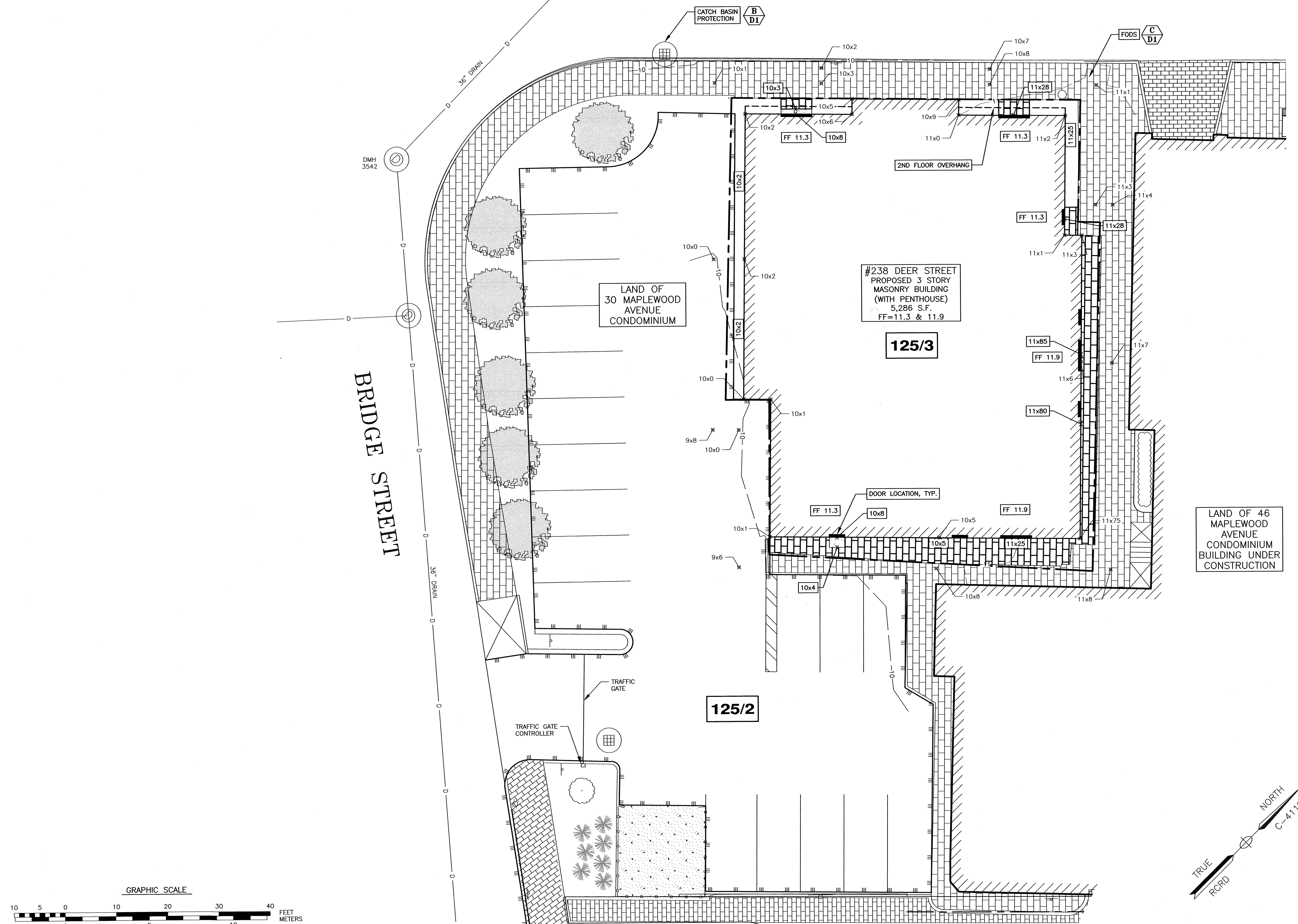
SCALE: 1" = 10' NOVEMBER 2020

GRADING PLAN

C5

FB 410 PG 75

2916



EROSION CONTROL NOTES

CONSTRUCTION SEQUENCE

DO NOT BEGIN CONSTRUCTION UNTIL ALL LOCAL, STATE AND FEDERAL PERMITS HAVE BEEN APPLIED FOR AND RECEIVED.

IF REQUIRED THE CONTRACTOR SHALL OBTAIN AN NPDES PHASE II STORMWATER PERMIT AND SUBMIT A NOTICE OF INTENT (N.O.I) BEFORE BEGINNING CONSTRUCTION AND SHALL HAVE ON SITE A STORMWATER POLLUTION PREVENTION PLAN (S.W.P.P.P.) AVAILABLE FOR INSPECTION BY THE PERMITTING AUTHORITY DURING THE CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CARRYING OUT THE S.W.P.P.P. AND INSPECTING AND MAINTAINING ALL BMP'S CALLED FOR BY THE PLAN. THE CONTRACTOR SHALL SUBMIT A NOTICE OF TERMINATION (N.O.T.) FORM TO THE REGIONAL EPA OFFICE WITHIN 30 DAYS OF FINAL STABILIZATION OF THE ENTIRE SITE OR TURNING OVER CONTROL OF THE SITE TO ANOTHER OPERATOR.

INSTALL PERIMETER CONTROLS, CATCH BASIN PROTECTION ON ALL CATCH BASINS IN PROJECT AREA BEFORE ANY EARTH MOVING OPERATIONS. THE USE OF HAYBALES IS NOT ALLOWED.

PLACE FODS AS NEEDED THROUGHOUT PROJECT.

CUT AND GRUB ALL TREES, SHRUBS, SAPLINGS, BRUSH, VINES AND REMOVE OTHER DEBRIS AND RUBBISH AS REQUIRED. DEMOLISH EXISTING BUILDING, REMOVE IMPACTED UTILITIES.

ROUGH GRADE SITE.

LAYOUT AND INSTALL ALL BURIED UTILITIES AND SERVICES UP TO 10' OF THE PROPOSED BUILDING FOUNDATIONS. CAP AND MARK TERMINATIONS OR LOG SWING TIES.

CONSTRUCT BUILDING FOUNDATION.

CONNECT UTILITIES.

CONSTRUCT BUILDING.

PLACE BINDER LAYER OF PAVEMENT FOR SIDEWALKS.

AFTER BUILDINGS ARE COMPLETED, FINISH ALL REMAINING LANDSCAPED WORK.

CONSTRUCT SIDEWALKS AND INSTALL BIKE RACKS.

REMOVE TRAPPED SEDIMENTS FROM COLLECTION DEVICES AS APPROPRIATE, AND THEN REMOVE TEMPORARY EROSION CONTROL MEASURES UPON COMPLETION OF FINAL STABILIZATION OF THE SITE.

GENERAL CONSTRUCTION NOTES

THE EROSION CONTROL PROCEDURES SHALL CONFORM TO SECTION 645 OF THE "STANDARD SPECIFICATION FOR ROAD AND BRIDGE CONSTRUCTION" OF THE NHDOT, AND "STORM WATER MANAGEMENT AND EROSION AND SEDIMENT CONTROL HANDBOOK FOR URBAN AND DEVELOPING AREAS IN NEW HAMPSHIRE". THE PROJECT IS TO BE MANAGED IN A MANNER THAT MEETS THE REQUIREMENTS AND INTENT OF RSA 430:53 AND CHAPTER AGR 3800 RELATIVE TO INVASIVE SPECIES.

DURING CONSTRUCTION AND THEREAFTER, EROSION CONTROL MEASURES ARE TO BE IMPLEMENTED AS NOTED. THE SMALLEST PRACTICAL AREA OF LAND SHOULD BE EXPOSED AT ANY ONE TIME DURING DEVELOPMENT. NO DISTURBED AREA SHALL BE LEFT UNSTABILIZED FOR MORE THAN 45 DAYS.

ANY DISTURBED AREAS WHICH ARE TO BE LEFT TEMPORARILY, AND WHICH WILL BE REGRADED LATER DURING CONSTRUCTION SHALL BE MACHINE HAY MULCHED AND SEEDED WITH RYE GRASS TO PREVENT EROSION.

DUST CONTROL: IF TEMPORARY STABILIZATION PRACTICES, SUCH AS TEMPORARY VEGETATION AND MULCHING, DO NOT ADEQUATELY REDUCE DUST GENERATION, APPLICATION OF WATER OR CALCIUM CHLORIDE SHALL BE APPLIED IN ACCORDANCE WITH BEST MANAGEMENT PRACTICES.

SILT FENCES AND SILT/STOXX SHALL BE PERIODICALLY INSPECTED DURING THE LIFE OF THE PROJECT AND AFTER EACH STORM. ALL DAMAGED FENCES AND SILT/STOXX SHALL BE REPAIRED. SEDIMENT DEPOSITS SHALL PERIODICALLY BE REMOVED AND DISPOSED IN A SECURED LOCATION.

AVOID THE USE OF FUTURE OPEN SPACES (LOAM AND SEED AREAS) WHEREVER POSSIBLE DURING CONSTRUCTION. CONSTRUCTION TRAFFIC SHALL USE THE ROADBEDS OF FUTURE ACCESS DRIVES AND PARKING AREAS.

ADDITIONAL TOPSOIL REQUIRED FOR THE ESTABLISHMENT OF VEGETATION SHALL BE STOCKPILED IN AMOUNTS NECESSARY TO COMPLETE FINISHED GRADING OF ALL EXPOSED AREAS---CONSTRUCT SILT FENCE OR SILT/STOXX AROUND TOPSOIL STOCKPILE.

AREAS TO BE FILLED SHALL BE CLEARED, GRUBBED AND STRIPPED OF TOPSOIL TO REMOVE TREES, VEGETATION, ROOTS OR OTHER OBJECTIONABLE MATERIAL. STUMPS SHALL BE DISPOSED OF IN AN APPROVED FACILITY.

ALL FILLS SHALL BE PLACED AND COMPACTED TO REDUCE EROSION, SLIPPAGE, SETTLEMENT, SUBSIDENCE OR OTHER RELATED PROBLEMS.

ALL NON--STRUCTURAL, SITE--FILL SHALL BE PLACED AND COMPACTED TO 90% MODIFIED PROCTOR DENSITY IN LAYERS NOT EXCEEDING 18 INCHES IN THICKNESS UNLESS OTHERWISE NOTED.

FROZEN MATERIAL OR SOFT, MUCKY OR HIGHLY COMPRESSIBLE MATERIAL, TRASH, WOODY DEBRIS, LEAVES, BRUSH OR ANY DELTERIOUS MATTER SHALL NOT BE INCORPORATED INTO FILLS.

FILL MATERIAL SHALL NOT BE PLACED ON FROZEN FOUNDATION SUBGRADE.

DURING CONSTRUCTION AND UNTIL ALL DEVELOPED AREAS ARE FULLY STABILIZED, ALL EROSION CONTROL MEASURES SHALL BE INSPECTED WEEKLY AND AFTER EACH ONE HALF INCH OF RAINFALL.

THE CONTRACTOR SHALL MODIFY OR ADD EROSION CONTROL MEASURES AS NECESSARY TO ACCOMMODATE PROJECT CONSTRUCTION.

ALL ROADWAYS AND PARKING AREAS SHALL BE STABILIZED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE. ALL CUT AND FILL SLOPES SHALL BE SEEDED/LOAMED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE.

AN AREA SHALL BE CONSIDERED STABLE IF ONE OF THE FOLLOWING HAS OCCURRED:

- BASE COURSE GRAVELS HAVE BEEN INSTALLED ON AREAS TO BE PAVED
- A MINIMUM OF 85% VEGETATED GROWTH HAS BEEN ESTABLISHED
- A MINIMUM OF 3 INCHES OF NON--EROSIVE MATERIAL SUCH AS STONE OR RIPRAP HAS BEEN INSTALLED
- EROSION CONTROL BLANKETS HAVE BEEN INSTALLED

VEGETATIVE PRACTICE

FOR PERMANENT MEASURES AND PLANTINGS:

LIMESTONE SHALL BE THOROUGHLY INCORPORATED INTO THE LOAM LAYER AT A RATE OF 2 TONS PER ACRE.

FERTILIZER SHALL BE SPREAD ON THE TOP LAYER OF LOAM AND WORKED INTO THE SURFACE. FERTILIZER APPLICATION RATE SHALL BE 500 POUNDS PER ACRE OF 10--20--20 FERTILIZER.

SEED SHALL BE SOWN AT THE RATES SHOWN IN THE TABLE BELOW. IMMEDIATELY BEFORE SEEDING, THE SOIL SHALL BE LIGHTLY RAKED. ONE HALF THE SEED SHALL BE SOWN IN ONE DIRECTION AND THE OTHER HALF AT RIGHT ANGLES TO THE ORIGINAL DIRECTION. IT SHALL BE LIGHTLY RAKED INTO THE SOIL TO A DEPTH NOT OVER 1/4 INCH AND ROLLED WITH A HAND ROLLER WEIGHING NOT OVER 100 POUNDS PER LINEAR FOOT OF WIDTH. HAY MULCH SHALL BE APPLIED IMMEDIATELY AFTER SEEDING AT A RATE OF 1.5 TO 2 TONS PER ACRE, AND SHALL BE HELD IN PLACE USING APPROPRIATE TECHNIQUES FROM THE EROSION AND SEDIMENT CONTROL HANDBOOK.

THE SURFACE SHALL BE WATERED AND KEPT MOIST WITH A FINE SPRAY AS REQUIRED, WITHOUT WASHING AWAY THE SOIL. UNTIL THE GRASS IS WELL ESTABLISHED, ANY AREAS WHICH ARE NOT SATISFACTORILY COVERED SHALL BE RESEDED, AND ALL NOXIOUS WEEDS REMOVED.

A GRASS SEED MIXTURE CONTAINING THE FOLLOWING SEED REQUIREMENTS SHALL BE:

GENERAL COVER	PROPORTION	SEEDING RATE
CREEPING RED FESCUE	50%	100 LBS/ACRE
KENTUCKY BLUEGRASS	50%	
SLOPE SEED (USED ON ALL SLOPES GREATER THAN OR EQUAL TO 3:1)		
CREEPING RED FESCUE	42%	
TALL FESCUE	42%	48 LBS/ACRE
BIRDSFOOT TREFOIL	16%	

IN NO CASE SHALL THE WEED CONTENT EXCEED ONE PERCENT BY WEIGHT. ALL SEED SHALL COMPLY WITH APPLICABLE STATE AND FEDERAL SEED LAWS.

FOR TEMPORARY PROTECTION OF DISTURBED AREAS:
MULCHING AND SEEDING SHALL BE APPLIED AT THE FOLLOWING RATES:

PERENNIAL RYE:	0.7 LBS/1,000 S.F.
MULCH:	1.5 TONS/ACRE

MAINTENANCE AND PROTECTION

THE CONTRACTOR SHALL MAINTAIN ALL LOAM & SEED AREAS UNTIL FINAL ACCEPTANCE AT THE COMPLETION OF THE CONTRACT. MAINTENANCE SHALL INCLUDE WATERING, WEEDING, REMOVAL OF STONES AND OTHER FOREIGN OBJECTS OVER 1/2 INCHES IN DIAMETER WHICH MAY APPEAR AND THE FIRST TWO (2) CUTTINGS OF GRASS NO CLOSER THEN TEN (10) DAYS APART. THE FIRST CUTTING SHALL BE ACCOMPLISHED WHEN THE GRASS IS FROM 2 1/2 TO 3 INCHES HIGH. ALL BARE AND DEAD SPOTS WHICH BECOME APPARENT SHALL BE PROPERLY PREPARED, LIMED AND FERTILIZED, AND RESEDED BY THE CONTRACTOR AT HIS EXPENSE AS MANY TIMES AS NECESSARY TO SECURE GOOD GROWTH. THE ENTIRE AREA SHALL BE MAINTAINED, WATERED AND CUT UNTIL ACCEPTANCE OF THE LAWN BY THE OWNER'S REPRESENTATIVE.

THE CONTRACTOR SHALL TAKE WHATEVER MEASURES ARE NECESSARY TO PROTECT THE GRASS WHILE IT IS DEVELOPING.

TO BE ACCEPTABLE, SEEDED AREAS SHALL CONSIST OF A UNIFORM STAND OF AT LEAST 90 PERCENT ESTABLISHED PERMANENT GRASS SPECIES, WITH UNIFORM COUNT OF AT LEAST 100 PLANTS PER SQUARE FOOT.

SEEDED AREAS WILL BE FERTILIZED AND RESEDED AS NECESSARY TO INSURE VEGETATIVE ESTABLISHMENT.

THE SWALES WILL BE CHECKED WEEKLY AND REPAIRED WHEN NECESSARY UNTIL ADEQUATE VEGETATION IS ESTABLISHED.

THE SILT FENCE OR SILT/STOXX BARRIER SHALL BE CHECKED AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL.

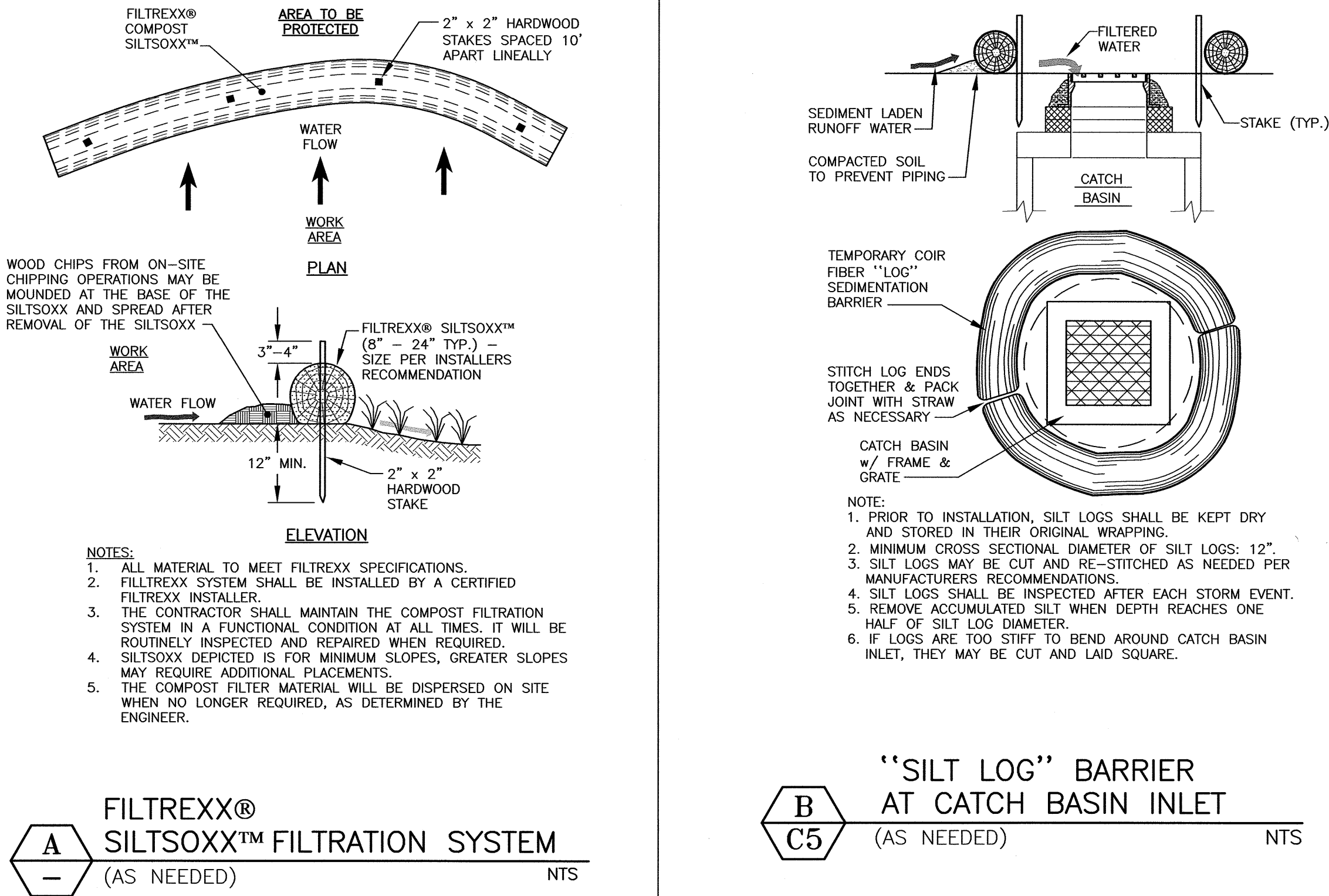
SILT FENCING AND SILT/STOXX SHALL BE REMOVED ONCE VEGETATION IS ESTABLISHED, AND DISTURBED AREAS RESULTING FROM SILT FENCE AND SILT/STOXX REMOVAL SHALL BE PERMANENTLY SEEDED.

WINTER NOTES

ALL PROPOSED VEGETATED AREAS WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY OCTOBER 15TH, OR WHICH ARE DISTURBED AFTER OCTOBER 15TH, SHALL BE STABILIZED BY SEEDING AND INSTALLING EROSION CONTROL BLANKETS ON SLOPES GREATER THAN 3:1, AND SEEDING AND PLACING 3 TO 4 TONS OF MULCH PER ACRE, SECURED WITH ANCHORED NETTING. ELSEWHERE, THE INSTALLATION OF EROSION CONTROL BLANKETS OR MULCH AND NETTING SHALL NOT OCCUR OVER ACCUMULATED SNOW OR ON FROZEN GROUND AND SHALL BE COMPLETED IN ADVANCE OF THAW OR SPRING MELT EVENTS.

ALL DITCHES OR SWALES WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY OCTOBER 15TH, OR WHICH ARE DISTURBED AFTER OCTOBER 15TH, SHALL BE STABILIZED TEMPORARILY WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW CONDITIONS.

AFTER NOVEMBER 15TH, INCOMPLETE ROAD OR PARKING SURFACES, WHERE WORK HAS STOPPED FOR THE WINTER SEASON, SHALL BE PROTECTED WITH A MINIMUM OF 3 INCHES OF CRUSHED GRAVEL PER NHDOT ITEM 304.3.



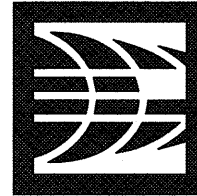
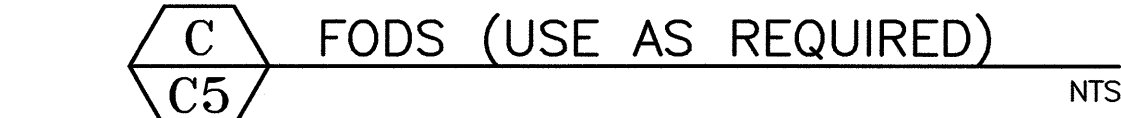
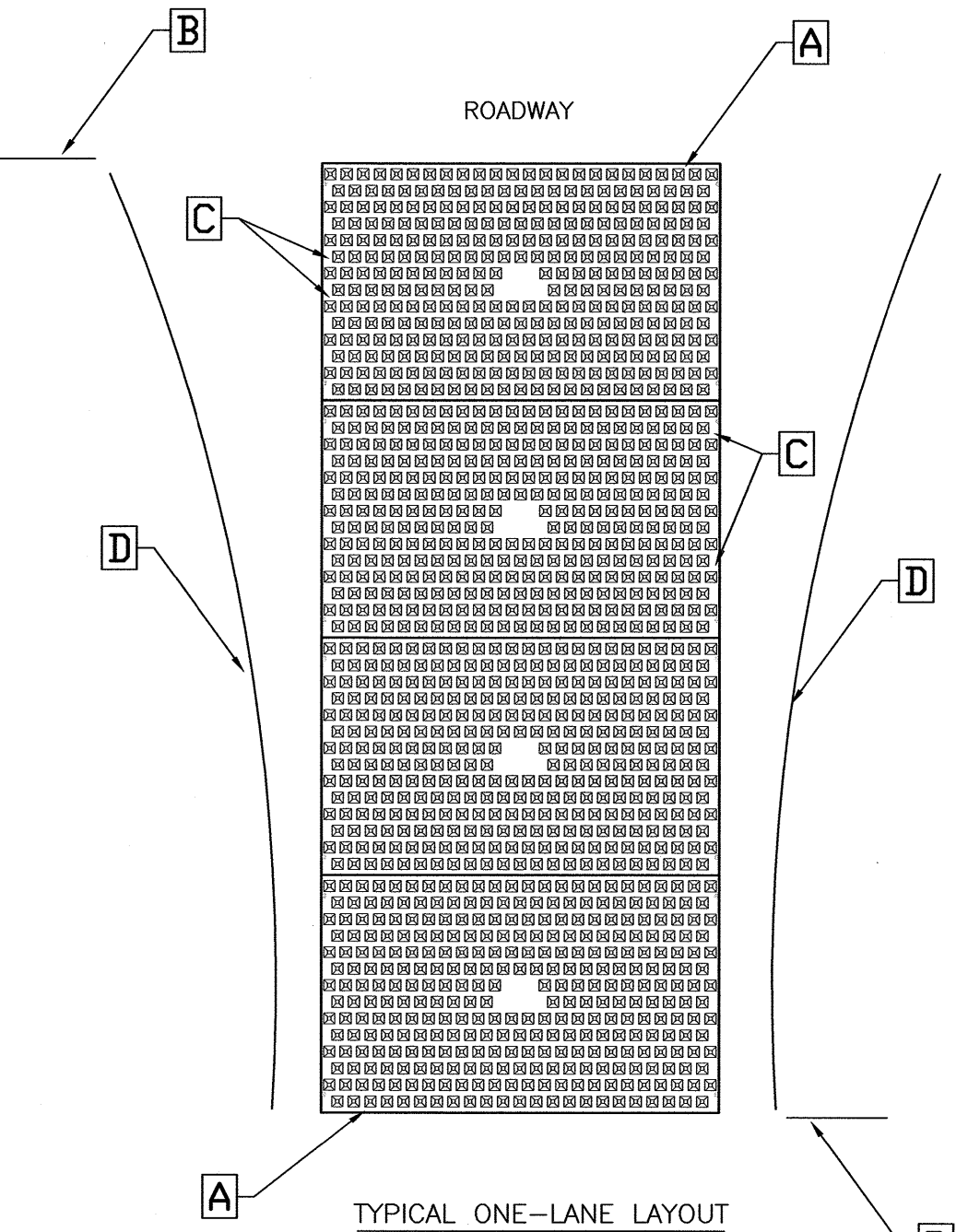
FODS TRACKOUT CONTROL SYSTEM

INSTALLATION:

THE PURPOSE AND DESIGN OF THE FODS TRACKOUT CONTROL SYSTEM IS TO EFFECTIVELY REMOVE MOST SEDIMENT FROM VEHICLE TIRES AS THEY EXIT A DISTURBED LAND AREA ONTO A PAVED STREET. THIS MANUAL IS A PLATFORM FROM WHICH TO INSTALL A FODS TRACKOUT CONTROL SYSTEM. (NOTE: THIS IS NOT A ONE SIZE FITS ALL GUIDE.) THE INSTALLATION MAY NEED TO BE MODIFIED TO MEET THE EXISTING CONDITIONS, EXPECTATIONS, OR DEMANDS OF A PARTICULAR SITE. THIS IS A GUIDELINE. ULTIMATELY THE FODS TRACKOUT CONTROL SYSTEM SHOULD BE INSTALLED SAFELY WITH PROPER ANCHORING AND SIGNS PLACED AT THE ENTRANCE AND EXIT TO CAUTION USERS AND OTHERS.

KEY NOTES:

- FODS TRACKOUT CONTROL SYSTEM MAT.
- FODS SAFETY SIGN.
- ANCHOR POINT.
- SILT OR ORANGE CONSTRUCTION FENCE.



AMBIT ENGINEERING, INC.

Civil Engineers & Land Surveyors

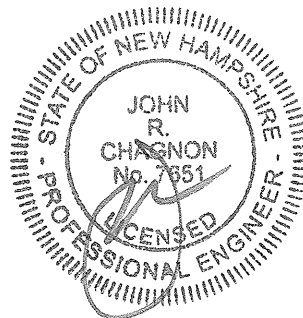
200 Griffin Road - Unit 3
Portsmouth, N.H. 03801-7114
Tel (603) 430-9282
Fax (603) 436-2315

NOTES:

- 1) THE CONTRACTOR SHALL NOTIFY DIG SAFE AT 1-888-DIG-SAFE (1-888-344-7233) AT LEAST 72 HOURS PRIOR TO COMMENCING ANY EXCAVATION ON PUBLIC OR PRIVATE PROPERTY.
- 2) UNDERGROUND UTILITY LOCATIONS ARE BASED UPON BEST AVAILABLE EVIDENCE AND ARE NOT FIELD VERIFIED. LOCATING AND PROTECTING ANY ABOVEGROUND OR UNDERGROUND UTILITIES IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND/OR THE OWNER. UTILITY CONFLICTS SHOULD BE REPORTED AT ONCE TO THE DESIGN ENGINEER.
- 3) CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE "NEW HAMPSHIRE STORMWATER MANUAL, VOLUME 3, EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION. (NHDES DECEMBER 2008).

SITE DEVELOPMENT
238 DEER STREET, LLC
238 DEER STREET
PORTSMOUTH, N.H.

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SCALE: AS SHOWN OCTOBER 2021

EROSION PROTECTION
NOTES AND DETAILS

D1



SCOPE OF WORK:

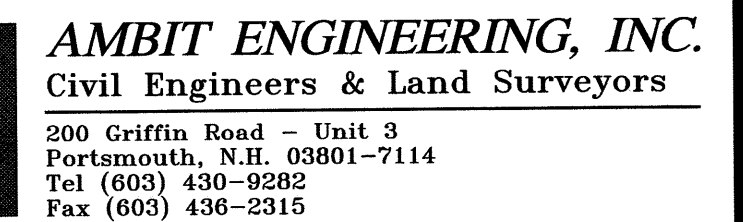
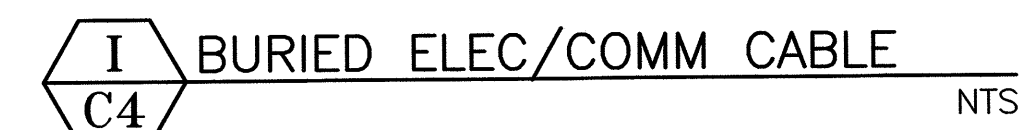
- 1) THE WORK SHALL CONSIST OF CONSTRUCTING/RECONSTRUCTING THE SUB-BASE AND CONSTRUCTING A NEW BRICK SIDEWALK AS DIRECTED IN THE FIELD BY THE ENGINEER.
- 2) REVEAL SHALL BE 5" (COORDINATE WITH PORTSMOUTH DPW).

METHODS OF CONSTRUCTION:

- A) ALL LABOR AND MATERIALS SHALL CONFORM TO THE STATE OF NEW HAMPSHIRE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 608, AND CITY OF PORTSMOUTH SPECIFICATIONS FOR NEW BRICK SIDEWALK, SECTION 6.
- B) ALL BRICKS SHALL CONFORM TO THE REQUIREMENTS OF ASTM STANDARD SPECIFICATIONS FOR BUILDING BRICKS: CLASS SX, TYPE 1, APPLICATION PX. THE BRICKS SHALL BE NO. 1, WIRE CUT TYPE FOR PAVING, WITH A COMPRESSIVE STRENGTH OF NOT LESS THAN 6,000 POUNDS PER SQUARE INCH. THE BRICKS SHALL NOT BE CORED OR HAVE FROGS AND SHALL BE OF A STANDARD SIZE (2.25" X 4 X 8").
- C) EXCAVATION FOR SIDEWALKS SHALL BE AT A DEPTH OF 10 INCHES BELOW FINISH GRADE. IN AREAS NOT BUTTING CURBING OR BUILDINGS, THE EXCAVATION SHALL BE 6 INCHES WIDER THAN THE FINISHED SIDEWALK WIDTH. AT ALL DRIVE CROSSINGS, THE DEPTH OF EXCAVATION SHALL BE INCREASED ACCORDINGLY. THE CONTRACTOR SHALL PROVIDE NEAT AND SQUARE CUTTING OF EXISTING ASPHALT ROAD SURFACE AS NEEDED. ALL UNSUITABLE MATERIAL SHALL BE REMOVED AND DISPOSED OF OFF-SITE AT THE CONTRACTOR'S OWN EXPENSE.
- D) THE BASE MATERIAL SHALL CONSIST OF A MIXTURE OF STONES OR ROCK FRAGMENTS AND PARTICLES WITH 100% PASSING THE 3 INCH SIEVE, 95% TO 100% PASSING THE 2 INCH SIEVE, 53% TO 95% PASSING THE 1 INCH SIEVE, AND 27% TO 52% PASSING THE NO. 4 SIEVE. AT LEAST 50% OF THE MATERIALS RETAINED ON THE 1 INCH SIEVE SHALL HAVE A FRACTURED FACE. THE BASE MATERIAL SHALL BE THOROUGHLY COMPACTED TO THE DEPTH SPECIFIED OR DIRECTED. IN THE CASE OF ALL DRIVE CROSSINGS THE BASE WILL BE INCREASED TO A COMPACTED DEPTH OF 12 INCHES. GRAVEL REQUIREMENTS FOR RECONSTRUCTION WILL BE AS DIRECTED, BASED ON SITE CONDITIONS. THE WORK INCLUDES BACKFILL UP ANY AND ALL CURB BEING INSTALLED BY OTHERS ON BOTH SIDES.
- E) THE CLAY BRICK PAVERS SHALL BE LAID IN A 1 INCH BED OF A SAND MIXTURE COMPRISED OF: 3 PARTS SAND MIXED WITH 1 PART PORTLAND CEMENT.
- F) THE CONTRACTOR SHALL LAY THE BRICKS SO THAT APPROXIMATELY 4.5 BRICKS SHALL COVER ONE SQUARE FOOT.
- G) THE SIDEWALK SHALL PITCH TOWARDS THE STREET AS SHOWN ON THE GRADING PLAN.
- H) IN AREAS WHERE THE FRONT OF THE BRICK SIDEWALK IS NOT ADJACENT TO GRANITE CURBING, THE CONTRACTOR SHALL INSTALL EDGING TO HOLD THE BRICKS IN PLACE. SUCH EDGING SHALL BE INSTALLED PER THE MANUFACTURER'S RECOMMENDATIONS.
- I) THE CONTRACTOR SHALL SUBMIT A SAMPLE OF THE BRICKS FOR APPROVAL BY THE CITY BEFORE BRICKS ARE INSTALLED.



- NOTES:
- 1) ALL CONDUIT TO BE U.L. LISTED, SCH. 80 UNDER ALL TRAVEL WAYS, & SCHED. 40 FOR THE REMAINDER.
 - 2) NORMAL CONDUIT SIZES FOR PSNH ARE 3 INCH FOR SINGLE PHASE PRIMARY AND SECONDARY VOLTAGE TABLES, 4 INCH FOR THREE PHASE SECONDARY, AND 5 INCH FOR THREE PHASE PRIMARY.
 - 3) ALL WORK TO CONFORM TO THE NATIONAL ELECTRICAL CODE (LATEST REVISION)
 - 4) INSTALL A 200# PULL ROPE FOR EACH CONDUIT
 - 5) VERIFY ALL CONDUIT SPECIFICATIONS WITH UTILITY COMPANY'S PRIOR TO ANY CONSTRUCTION.

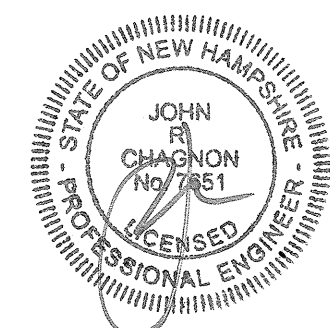


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**SITE DEVELOPMENT
238 DEER STREET, LLC
238 DEER STREET
PORTSMOUTH, N.H.**

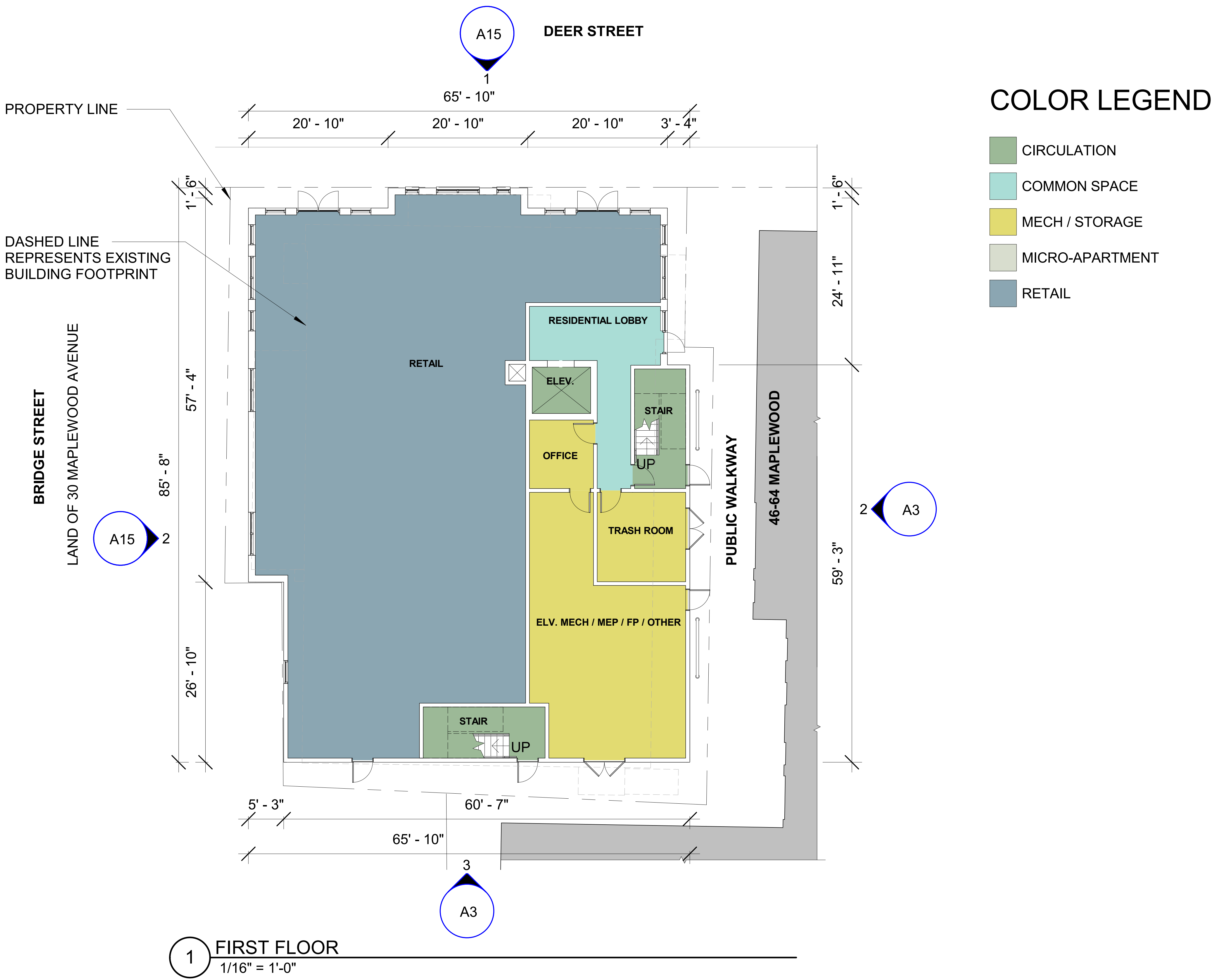
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SCALE: AS SHOWN OCTOBER 2021

DETAILS

D2



DEER ST. MIXED-USE BUILDING

238 DEER STREET
PORTSMOUTH, NH 03801

FLOOR PLANS

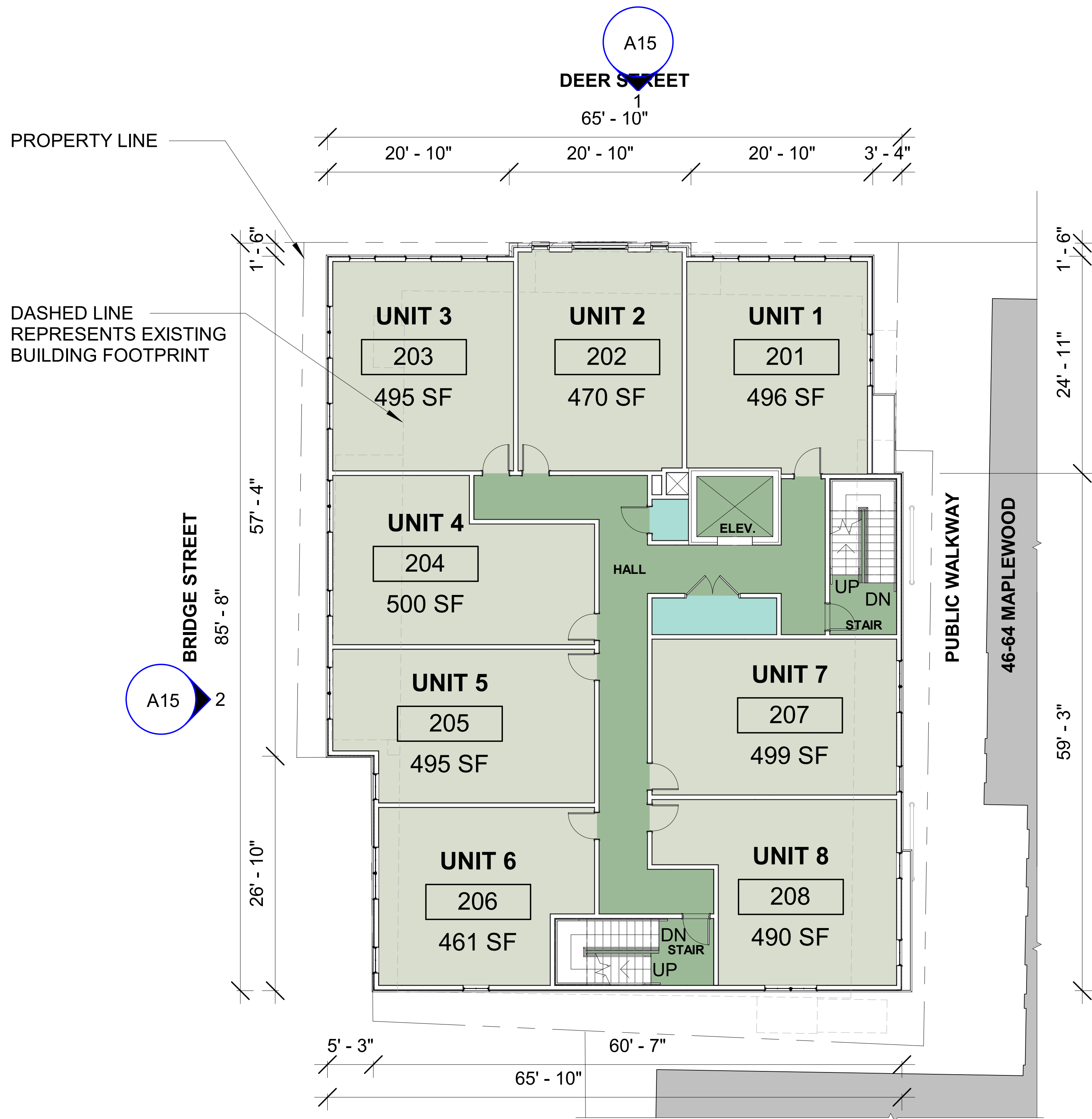
HISTORIC DISTRICT COMMISSION, OCTOBER 2021

McHENRY ARCHITECTURE

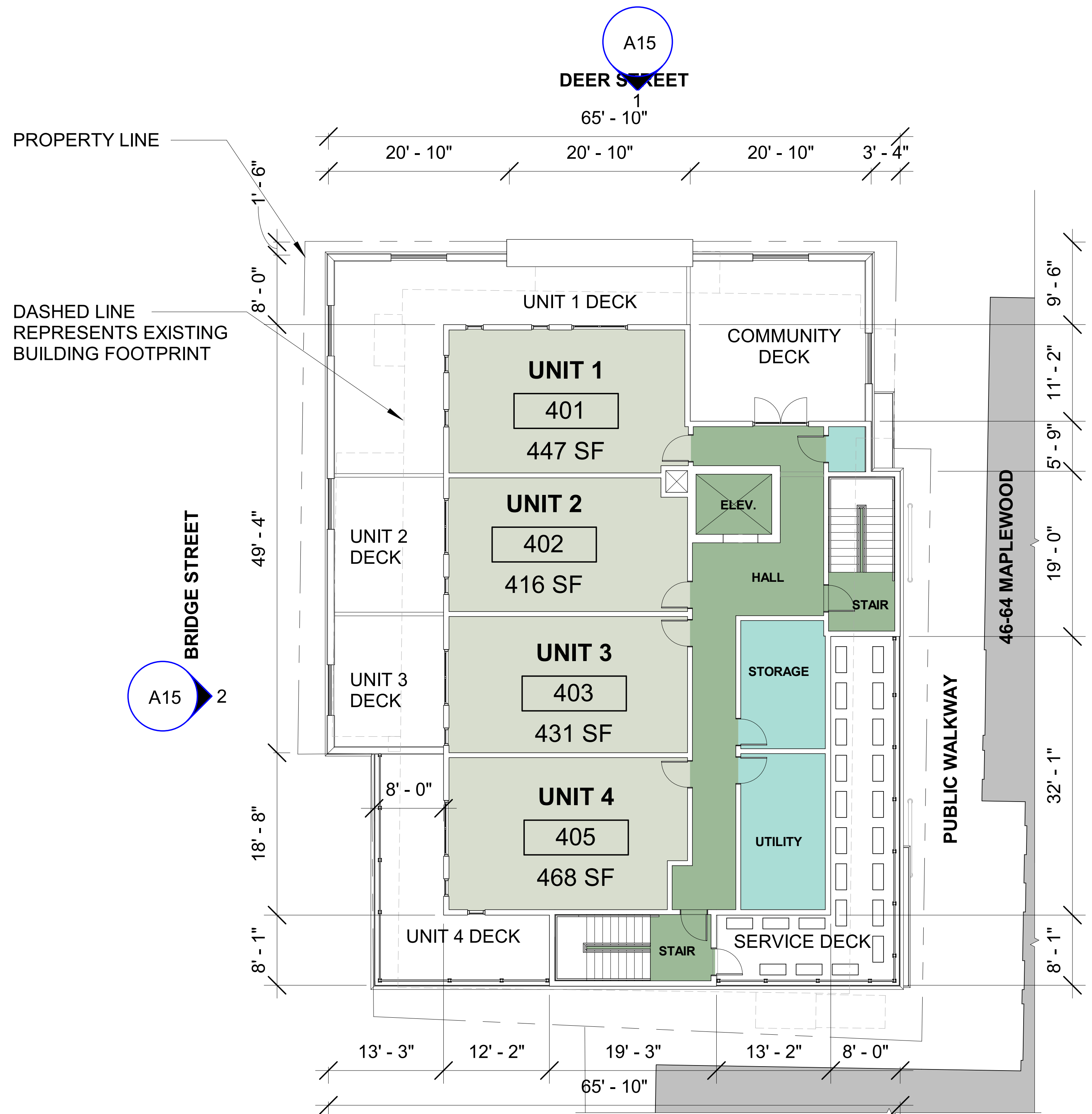
4 Market Street
Portsmouth, New Hampshire

A1

10/18/2021
McHA: SM/RD/MG
Scale: 1/16" = 1'-0"



1 SECOND AND THIRD FLOOR
1/16" = 1'-0"



2 FOURTH FLOOR
1/16" = 1'-0"

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DEER ST. MIXED-USE BUILDING
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FLOOR PLANS
HISTORIC DISTRICT COMMISSION, OCTOBER 2021

McHENRY ARCHITECTURE
4 Market Street
Portsmouth, New Hampshire

A2

10/18/2021
McHA: SM/RD/MG
Scale: 1/16" = 1'-0"

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1 NORTH ELEVATION (DEER STREET)
1/16" = 1'-0"



2 EAST ELEVATION (PUBLIC WALKWAY)
1/16" = 1'-0"

TO. ROOF	41' - 4"
FOURTH FLOOR	32' - 4"
THIRD FLOOR	22' - 2"
SECOND FLOOR	12' - 0"
FIRST FLOOR	0' - 0"
AVG. GRADE	-0' - 7 29/32"



3 SOUTH ELEVATION (REAR)
1/16" = 1'-0"



4 WEST ELEVATION (BRIDGE STREET)
1/16" = 1'-0"

TO. ROOF	41' - 4"
FOURTH FLOOR	32' - 4"
THIRD FLOOR	22' - 2"
SECOND FLOOR	12' - 0"
FIRST FLOOR	0' - 0"
AVG. GRADE	-0' - 7 29/32"

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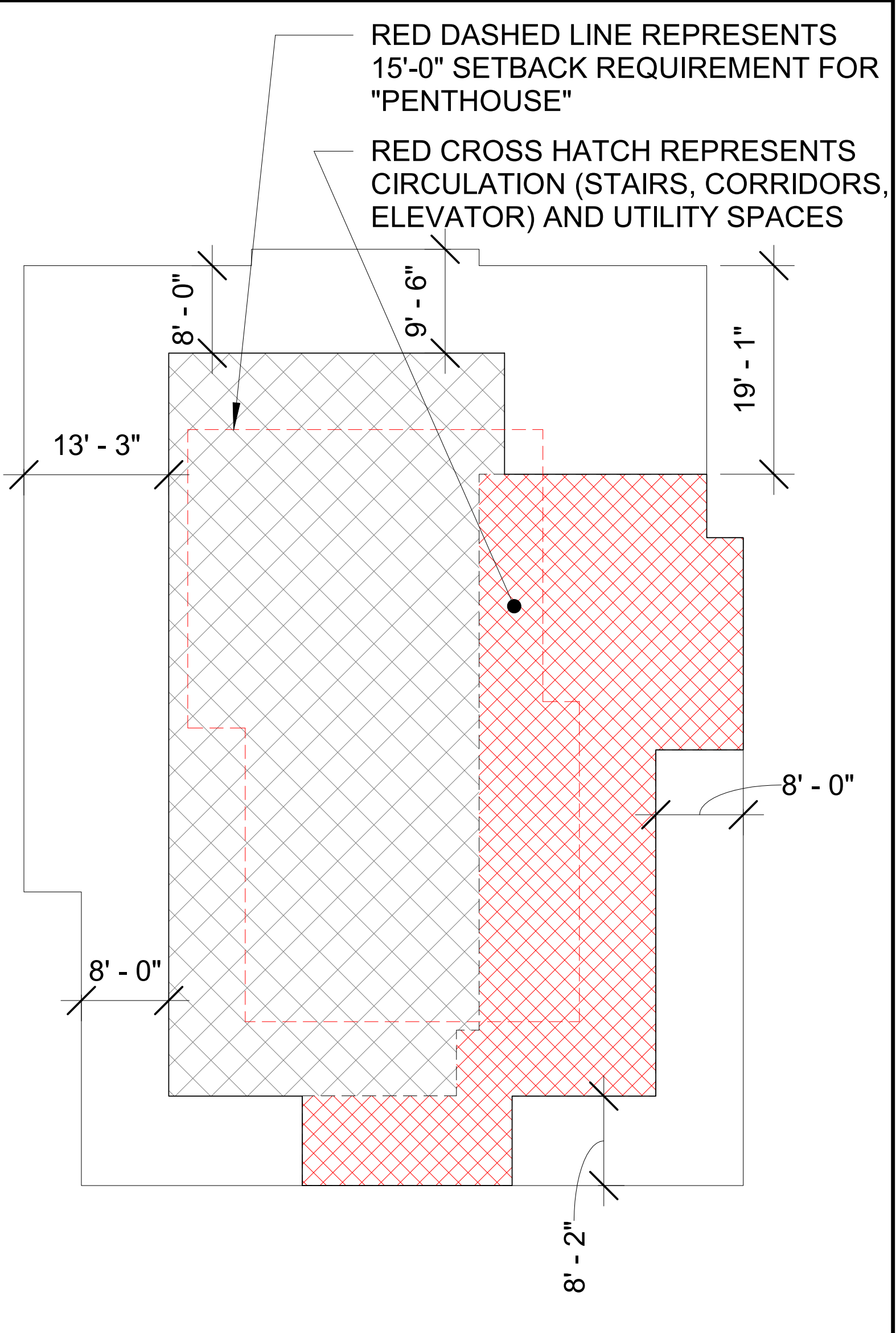
OVERALL ELEVATIONS
HISTORIC DISTRICT COMMISSION, OCTOBER 2021

McHENRY ARCHITECTURE
4 Market Street
Portsmouth, New Hampshire

A3

10/18/2021
McHA: SM/RD/MG
Scale: 1/16" = 1'-0"

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AERIAL RENDERING
HISTORIC DISTRICT COMMISSION, OCTOBER 2021

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A5

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NOT TO SCALE



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DEER STREET RENDERING

HISTORIC DISTRICT COMMISSION, OCTOBER 2021

McHENRY ARCHITECTURE

4 Market Street
Portsmouth, New Hampshire

A6

10/18/2021
McHA: SM/RD/MG
NOT TO SCALE



EXAMPLE EFFICIENCY UNIT FLOOR PLAN - 400SF



EXAMPLE EFFICIENCY UNIT



OWNER CONCEPT
PRECEDENT:
EXAMPLE
EFFICIENCY UNIT

EXAMPLE EFFICIENCY UNIT



EXAMPLE EFFICIENCY UNIT

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INTERIOR CONCEPT / OWNER
INSPIRATION

McHENRY ARCHITECTURE
4 Market Street
Portsmouth, New Hampshire

A7

10/18/2021
McHA: SM/RD/MG
NOT TO SCALE