HALEY WARD

22 December 2025

Peter Stith, TAC Committee Chair
City of Portsmouth
1 Junkins Avenue
Portsmouth, NH 03801

RE: Request for Technical Advisory Committee (TAC) Review at 134 Pleasant Street, Proposed Revision to Retail Banking and Banking Offices, Physical Therapy, Health Club and Apartments

Dear Mr. Stith and Technical Advisory Committee Members:

On behalf of Double MC, LLC, we are pleased to submit the attached **Site Plan and Associated Supplemental Material** for the above-mentioned project and request that we be placed on the agenda for your **January 6, 2026,** Technical Advisory Committee Meeting. The property is shown on the City of Portsmouth Assessors Map 116 as Lot 30.

The project is located at 134 Pleasant Street, adjacent to a Right-of-Way which is a part of the city Parrott Avenue Parking lot. The project is within the City of Portsmouth Character District 4 (CD4) and Historic District Commission (HDC) Zoning Districts. The site is currently developed with an existing building and a surface parking lot. The current use is a banking facility with drive-up, and offices.

Project Overview

The project consists of the adaptive reuse of the existing structure, with horizontal and vertical building expansions, and below grade (basement) parking. New exterior treatments and entrance locations are proposed, along with re-imaged pedestrian friendly sidewalks and landscaping improvements. The proposal is to create a new structure for banking with expanded offices and financial services, keeping the drive through in the same location. Apartments will be added along with the second-floor offices. The existing building will be renovated with a stair tower addition and vertical expansion. This re-purposed building with host Physical Therapy, Health Club and Apartment uses.

Site Zoning and Parking

The project is within the City of Portsmouth Character District 4 (CD4) Zone. Proposed residential use at the site is allowed by right. The proposed redevelopment meets the required parking under the Portsmouth Zoning Ordinance (PZO) for the proposed uses. The site will provide 48 parking spaces in the basement level, and 37 exterior surface parking spaces, for a total of 85 onsite spaces. The total parking required for the redevelopment is 91 spaces, based on the parking calculations and off-site needs associated with another property. Therefore, there is a 6-space deficit, which will require a Parking Conditional Use Permit.

200 Griffin Rd., Unit 14, Portsmouth, NH 03801
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Site History

First National Stores, Inc. purchased and merged the Universalist Church's two lots in 1952: the empty lot of the burned church, and the parsonage house lot. They also purchased a vacant lot behind these two lots from the City of Portsmouth and merged it with the other two lots, creating one lot, large enough to support a modern grocery store with parking lot. First National Stores, Incorporated was formed in April 1925 as a merger of three New England grocers; the John. T. Connor Company, Ginter Company and O'Keefe's, Incorporated. The Economy Grocery Company (Connecticut) was acquired in 1929. The Nicholson-Thackray Company (Rhode Island), Davey Brothers Company (Connecticut) and Modern Grocery Company (also of Connecticut), were bought during 1929, as well. With these mergers, First National Stores operated 2,002 units. They developed a prototype architectural design in the 1950's, with large storefront windows, and a decorative trellis banding along the wide flat roof. This style was replicated across the nation and became the basis of design for the mid-century supermarket chain. First National's "private label" merchandise underwent a massive re-labeling program during 1968, with the new "Finast" branding used. In 1978, Pic-N-Pay acquired First National Stores. In 1982, they sold the Pleasant Avenue property to Portsmouth Trust Bank, preferring to focus on their existing Pic-N-Pay store on Islington Street. At this time the building underwent a major remodel, recladding the stone with brick and granite, and adding large canopies to the east and south. Portsmouth Trust sold to First NH Bank, who later sold to Citizens Bank. In 1998 it was purchased by a private landowner, and then was sold in 2023 to the current owner, Double MC, LLC.

Vehicular and Pedestrian Circulation

The proposed site pedestrian connections are as follows:

- The east banking storefront from the Pleasant Street sidewalk
- The main banking entrance on the south side adjacent to the parking area
- The Physical Therapy, Health Club and Apartment entrance on the south side of the renovated building
- The west entrance adjacent to the vehicular ramp to the garage basement parking level adjacent to the proposed outdoor seating area.
- The proposed structure pedestrian residential entrance on the north side of the building.

Two-way vehicular access is from Pleasant Street, with an outlet to the driveway adjoining the Court House. The existing drive up exit on to Pleasant Street is relocated northwest, to allow the proposed building to expand along the street frontage, as required in the Character District requirements. The plan shows a revision to the west end of the current city / site parking arrangement. An existing awkward turning movement is being replaced with an easier design and landscape area, which creates three additional parking spaces in the Parrott Avenue parking lot, and also contributes to a situation where cars looking to get to the Parrott Avenue parking lot from Court Street will be less likely to cut through the PHA Housing / City Fire Station properties. The design is detailed on the Site Plan.

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Screening and Landscaping

The site landscape consists of the preservation of significant trees on the south and east sides, and the addition of landscaping along Pleasant Street fronting the building, and on the west side along a pedestrian access corridor to Court Street, where a pocket park feature has been added. The design is detailed on the Landscape Plans.

Water and Sewage Systems

Water and sewer services are provided by the city, and service mains are adjacent to the site. The Utility Plan shows the connections to serve the redeveloped site. Sewer services will be constructed to tie to the existing city sewer west of the development site. Water services, domestic and sprinkler, will come from Pleasant street re-using existing connection points. The proposed water and sewer flow is a modest 4,700 gallons per day. The municipal water and sewer systems should not be impacted by the change in the proposed flow with the building renovation and additional structure.

Stormwater Management

The site drainage patterns will remain, with the roof drainage collection providing opportunities to increase stormwater treatment through the use of downspout filters. Site development results in a modest 5,500 square foot increase in impervious surfaces. Site Open Space is 12.4%; this is more than the 10% Character District requirement.

Site Lighting

Pole mounted lighting will be introduced on the south side of the site to provide parking lot lighting, shown on the Lighting Plan. The sidewalks and other areas adjacent to the buildings will be illuminated by building mounted lighting, as shown on the attached Nighttime Lighting exhibit.

Site Utilities and Solid Waste

Site utilities include natural gas, underground electric and communications services. Gas services will be provided from connections on Pleasant Street, as shown on the Utility Plan. Electrical service will come from a new pole on Pleasant Street to provide a drop connection top serve a new ground mounted transfer on private property, with services underground to the existing and proposed structures. Communication services will follow the same paths. Solid waste will be collected within the building basement and disposed privately, see Architectural Plan PB.01.

Submission

The submission includes a Site Orthophoto Plan, showing the site surroundings and depicting the 250-foot DES Shoreland Zone setback line, which just clips the southwest corner of the development parcel. The site is adjacent to an access right of way. A copy of the plan showing the adjacent right-of-way is included in the submission.

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Supplemental Submission material includes the following:

- Application Checklist
- Site Cost Estimate
- Green Building Statement
- Inspection and Maintenance Plan
- Parking Analysis
- Trip Generation
- Lighting Specifications
- Will Serve Letter
- Site photos and Timeline
- Drive-up window location
- Existing Average Grade Plane
- Historic Plan - Easements

The following full-size plans are included in our submission:

- Cover Sheet: This shows the Development Team, Legend, Site Location, and Site Zoning.
- Existing Conditions Plan V101: This plan shows the site boundary and the existing development area conditions in detail.
- Existing Utility Plan V102: This plan shows the invert and rim elevation details for the existing utilities.
- Site Orthophoto Plan V103: This plan shows the existing site, and the site relationship to the NHDES Shoreland Zone.
- Demolition Plan C101: This plan shows site demolition.
- Site Plan C102: This plan shows the proposed building expansion and site improvements, along with surface area calculations.
- Landscape Plans L1-L2: These plan show the proposed site landscaping.
- PB 0.1 to 2.1 Architectural Plans - Floor Plans, Roof Plans, Elevations, and Vignettes showing the proposed building construction.
- Utility Plan C103: This plan shows the proposed utility connections.
- Grading Plan C104: This plan shows the proposed site grading.
- Lighting Plan C105: This plan shows the proposed parking lot lighting.
- Detail Plans D501 – D506: These plans show the proposed construction details.

We look forward to an in-person presentation to the Technical Advisory Committee and the review of this submission, and request approval of the proposed design.

Sincerely,

John R. Chagnon, PE

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City of Portsmouth, New Hampshire

Site Plan Application Checklist

This site plan application checklist is a tool designed to assist the applicant in the planning process and for preparing the application for Planning Board review. The checklist is required to be completed and uploaded to the Site Plan application in the City's online permitting system. A pre-application conference with a member of the planning department is strongly encouraged as additional project information may be required depending on the size and scope. The applicant is cautioned that this checklist is only a guide and is not intended to be a complete list of all site plan review requirements. Please refer to the Site Plan review regulations for full details.

Applicant Responsibilities (Section 2.5.2): Applicable fees are due upon application submittal along with required attachments. The application shall be complete as submitted and provide adequate information for evaluation of the proposed site development. Waiver requests must be submitted in writing with appropriate justification.

Name of Applicant: Double MC, LLC Date Submitted: 12/22/2025

_____ Application # (in City's online permitting): LU 25 - 138

Site Address: 134 Pleasant Street _____ Map: 116 Lot: 30

Application Requirements			
☑	**Required Items for Submittal**	**Item Location (e.g. Page or Plan Sheet/Note #)**	**Waiver Requested**
☐	Complete application form submitted via the City's web-based permitting program (2.5.2.1**(2.5.2.3A)**	Online	N/A
☐	All application documents, plans, supporting documentation and other materials uploaded to the application form in viewpoint in digital Portable Document Format (PDF). One hard copy of all plans and materials shall be submitted to the Planning Department by the published deadline. **(2.5.2.8)**	Online	N/A

Site Plan Review Application Required Information			
☑	**Required Items for Submittal**	**Item Location (e.g. Page/line or Plan Sheet/Note #)**	**Waiver Requested**
☐	Statement that lists and describes "green" building components and systems. **(2.5.3.1B)**	Online	
☐	Existing and proposed gross floor area and dimensions of all buildings and statement of uses and floor area for each floor. **(2.5.3.1C)**	Architectural Plans	N/A
☐	Tax map and lot number, and current zoning of all parcels under Site Plan Review. **(2.5.3.1D)**	V101 Existing Conditions	N/A

Site Plan Review Application Required Information			
☑	**Required Items for Submittal**	**Item Location (e.g. Page/line or Plan Sheet/Note #)**	**Waiver Requested**
☐	Owner's name, address, telephone number, and signature. Name, address, and telephone number of applicant if different from owner. **(2.5.3.1E)**	Cover Page	N/A
☐	Names and addresses (including Tax Map and Lot number and zoning districts) of all direct abutting property owners (including properties located across abutting streets) and holders of existing conservation, preservation or agricultural preservation restrictions affecting the subject property. **(2.5.3.1F)**	V101 Existing Conditions	N/A
☐	Names, addresses and telephone numbers of all professionals involved in the site plan design. **(2.5.3.1G)**	Cover Sheet	N/A
☐	List of reference plans. **(2.5.3.1H)**	Existing Conditions Plan	N/A
☐	List of names and contact information of all public or private utilities servicing the site. **(2.5.3.1I)**	Cover Sheet	N/A

Site Plan Specifications			
☑	**Required Items for Submittal**	**Item Location (e.g. Page/line or Plan Sheet/Note #)**	**Waiver Requested**
☐	Full size plans shall not be larger than 22 inches by 34 inches with match lines as required, unless approved by the Planning Director.. **(2.5.4.1A)**	Required on all plan sheets	N/A
☐	Scale: Not less than 1 inch = 60 feet and a graphic bar scale shall be included on all plans. **(2.5.4.1B)**	Required on all plan sheets	N/A
☐	GIS data should be referenced to the coordinate system New Hampshire State Plane, NAD83 (1996), with units in feet. **(2.5.4.1C)**	Complies	N/A
☐	Plans shall be drawn to scale and stamped by a NH licensed civil engineer. **(2.5.4.1D)**	Required on all plan sheets	N/A
☐	Wetlands shall be delineated by a NH certified wetlands scientist and so stamped. **(2.5.4.1E)**	N/A	N/A
☐	Title (name of development project), north point, scale, legend. **(2.5.4.2A)**	Cover Sheet	N/A
☐	Date plans first submitted, date and explanation of revisions. **(2.5.4.2B)**	All Sheets	N/A
☐	Individual plan sheet title that clearly describes the information that is displayed. **(2.5.4.2C)**	Required on all plan sheets	N/A
☐	Source and date of data displayed on the plan. **(2.5.4.2D)**	V101 Existing Conditions Plan	N/A

Site Plan Specifications – Required Exhibits and Data			
☑	**Required Items for Submittal**	**Item Location (e.g. Page/line or Plan Sheet/Note #)**	**Waiver Requested**
☐	**1. Existing Conditions: (2.5.4.3A)** • Surveyed plan of site showing existing natural and built features; • Existing building footprints and gross floor area; • Existing parking areas and number of parking spaces provided; • Zoning district boundaries; • Existing, required, and proposed dimensional zoning requirements including building and open space coverage, yards and/or setbacks, and dwelling units per acre; • Existing impervious and disturbed areas; • Limits and type of existing vegetation; • Wetland delineation, wetland function and value assessment (including vernal pools); • SFHA, 100-year flood elevation line and BFE data, as required.	V101 Existing Conditions Plan	
☐	**2. Buildings and Structures: (2.5.4.3B)** • Plan view: Use, size, dimensions, footings, overhangs, 1st fl. elevation; • Elevations: Height, massing, placement, materials, lighting, façade treatments; • Total Floor Area; • Number of Usable Floors; • Gross floor area by floor and use.	Architectural Plans	
☐	**3. Access and Circulation: (2.5.4.3C)** • Location/width of access ways within site; • Location of curbing, right of ways, edge of pavement and sidewalks; • Location, type, size and design of traffic signing (pavement markings); • Names/layout of existing abutting streets; • Driveway curb cuts for abutting prop. and public roads; • If subdivision; Names of all roads, right of way lines and easements noted; • AASHTO truck turning templates, description of minimum vehicle allowed being a WB-50 (unless otherwise approved by TAC).	C102 Site Plan	
☐	**4. Parking and Loading: (2.5.4.3D)** • Location of off street parking/loading areas, landscaped areas/buffers; • Parking Calculations (# required and the # provided).	C102 Site Plan	
☐	**5. Water Infrastructure: (2.5.4.3E)** • Size, type and location of water mains, shut-offs, hydrants & Engineering data; • Location of wells and monitoring wells (include protective radii).	C103 Utility Plan	
☐	**6. Sewer Infrastructure: (2.5.4.3F)** • Size, type and location of sanitary sewage facilities & Engineering data, including any onsite temporary facilities during construction period.	C103 Utility Plan	

☐	**7. Utilities: (2.5.4.3G)** • The size, type and location of all above & below ground utilities; • Size type and location of generator pads, transformers and other fixtures.	C103 Utility Plan	
☐	**8. Solid Waste Facilities: (2.5.4.3H)**	Architectural Plan	
	• The size, type and location of solid waste facilities.		
☐	**9. Storm water Management: (2.5.4.3I)** • The location, elevation and layout of all storm-water drainage. • The location of onsite snow storage areas and/or proposed off-site snow removal provisions. • Location and containment measures for any salt storage facilities • Location of proposed temporary and permanent material storage locations and distance from wetlands, water bodies, and stormwater structures.	C 104 Grading Plan	
☐	**10. Outdoor Lighting: (2.5.4.3J)** • Type and placement of all lighting (exterior of building, parking lot and any other areas of the site) and photometric plan.	C105 Lighting Plan	
☐	**11.** Indicate where dark sky friendly lighting measures have been implemented. **(10.1)**	All Fixtures	
☐	**12. Landscaping: (2.5.4.3K)** • Identify all undisturbed area, existing vegetation and that which is to be retained; • Location of any irrigation system and water source.	L1 & L2 Landscape Plans	
☐	**13. Contours and Elevation: (2.5.4.3L)** • Existing/Proposed contours (2 foot minimum) and finished grade elevations.		
☐	**14. Open Space: (2.5.4.3M)** • Type, extent and location of all existing/proposed open space.	C102 Site Plan	
☐	**15. All easements, deed restrictions and non-public rights of ways. (2.5.4.3N)**	C 104 Grading Plan	
☐	**16. Character/Civic District (All following information shall be included): (2.5.4.3P)** • Applicable Building Height (10.5A21.20 & 10.5A43.30); • Applicable Special Requirements (10.5A21.30); • Proposed building form/type (10.5A43); • Proposed community space (10.5A46).	Cover Letter & Architectural Plans	
☐	**17. Special Flood Hazard Areas (2.5.4.3Q)** • The proposed development is consistent with the need to minimize flood damage; • All public utilities and facilities are located and construction to minimize or eliminate flood damage; • Adequate drainage is provided so as to reduce exposure to flood hazards.	N/A	

Other Required Information			
☑	**Required Items for Submittal**	**Item Location (e.g. Page/line or Plan Sheet/Note #)**	**Waiver Requested**
☐	Traffic Impact Study or Trip Generation Report, as required. **(3.2.1-2)**	Supplemental Materials	
☐	Indicate where Low Impact Development Design practices have been incorporated. **(7.1)**	Detail Sheets	
☐	Indicate whether the proposed development is located in a wellhead protection or aquifer protection area. Such determination shall be approved by the Director of the Dept. of Public Works. **(7.3.1)**	N/A	
☐	Stormwater Management and Erosion Control Plan. **(7.4)**	Supplemental Materials	
☐	Inspection and Maintenance Plan **(7.6.5)**	Supplemental Materials	

Final Site Plan Approval Required Information			
☑	**Required Items for Submittal**	**Item Location (e.g. Page/line or Plan Sheet/Note #)**	**Waiver Requested**
☐	All local approvals, permits, easements and licenses required, including but not limited to: • Waivers; • Driveway permits; • Special exceptions; • Variances granted; • Easements; • Licenses. **(2.5.3.2A)**	Cover Sheet	
☐	Exhibits, data, reports or studies that may have been required as part of the approval process, including but not limited to: • Calculations relating to stormwater runoff; • Information on composition and quantity of water demand and wastewater generated; • Information on air, water or land pollutants to be discharged, including standards, quantity, treatment and/or controls; • Estimates of traffic generation and counts pre- and post-construction; • Estimates of noise generation; • A Stormwater Management and Erosion Control Plan; • Endangered species and archaeological / historical studies; • Wetland and water body (coastal and inland) delineations; • Environmental impact studies. **(2.5.3.2B)**	Online & On Plans	
☐	A document from each of the required private utility service providers indicating approval of the proposed site plan and indicating an ability to provide all required private utilities to the site. **(2.5.3.2D)**	Supplemental Materials	

Final Site Plan Approval Required Information			
☑	**Required Items for Submittal**	**Item Location (e.g. Page/line or Plan Sheet/Note #)**	**Waiver Requested**
☐	A list of any required state and federal permit applications required for the project and the status of same. **(2.5.3.2E)**	Cover Sheet	
☐	A note shall be provided on the Site Plan stating: "All conditions on this Plan shall remain in effect in perpetuity pursuant to the requirements of the Site Plan Review Regulations." **(2.5.4.2E)**	Cover	N/A
☐	For site plans that involve land designated as "Special Flood Hazard Areas" (SFHA) by the National Flood Insurance Program (NFIP) confirmation that all necessary permits have been received from those governmental agencies from which approval is required by Federal or State law, including Section 404 of the Federal Water Pollution Control Act Amendments of 1972, 33 U.S.C. 1334. **(2.5.4.2F)**	N/A	
☐	Plan sheets submitted for recording shall include the following notes: a. "This Site Plan shall be recorded in the Rockingham County Registry of Deeds." b. "All improvements shown on this Site Plan shall be constructed and maintained in accordance with the Plan by the property owner and all future property owners. No changes shall be made to this Site Plan without the express approval of the Portsmouth Planning Director." **(2.13.3)**	C102 Site Plan	N/A

Applicant's Signature:  **Date:** 12/19/2025

PROJECT NAME & ADDRESS: Double MC , LLC

DATE: 12/19/2025

MAP & LOT: 116/30

Description	Quantity	Unit	Unit Price	Total	
Site Preparation				**$ 87,111.00**	
Common Excavation	1500	CY	$ 16.00	$ 24,000.00	*1
Ledge Excavation	500	CY	$ 125.00	$ 62,500.00	*1
Saw Cut Pavement	235	LF	$ 2.60	$ 611.00	
Roads				**$ 258,279.00**	
Gravel Backfill (304.2)	684	CY	$ 36.00	$ 24,624.00	
Crushed Gravel (304.3)	382	CY	$ 36.00	$ 13,752.00	*2
Hot Bituminous Pavement - Machine Method	489	TON	$ 150.00	$ 73,350.00	
Fine Grading	1912	SY	$ 3.00	$ 5,736.00	
Concrete Sidewalk 4"	297	SY	$ 62.00	$ 18,414.00	*3
Straight Granite Curb	1890	LF	$ 40.00	$ 75,600.00	
Light pole Base and Fixture	3	EA	$ 4,500.00	$ 13,500.00	
Conduit Duct Bank (4" SCH 80 - 4 Pipes)	255	LF	$ 120.00	$ 30,600.00	
Retroflected Paint Marking - 4" Line	850	LF	$ 1.20	$ 1,020.00	
Retroflected Paint Pavement Marking - Symbol or Word	33	EA	$ 51.00	$ 1,683.00	
Structural				**$ 81,000.00**	
Retaining Wall - Modular Block	1500	SF	$ 54.00	$ 81,000.00	
Utilities				**$ 225,670.00**	
Catch Basin with Frame, Grate & Hood	5	EA	$ 4,800.00	$ 24,000.00	
Sewer MH - 4' Dia. w/ Base and Top	20	VF	$ 480.00	$ 9,600.00	
Remove CB, DI Grate and Frame	2	EA	$ 3,000.00	$ 6,000.00	
Reconstruct / Adjust Drainage Manholes	2	EA	$ 3,500.00	$ 7,000.00	
12" Plastic Corrugated/Smooth Pipe	165	LF	$ 85.00	$ 14,025.00	
PVC Sewer Gravity Main > 8"	190	LF	$ 180.00	$ 34,200.00	
PVC Sewer Service Pipe and Fittings	60	LF	$ 120.00	$ 7,200.00	
Reconstruct / Adjust Sewer Manholes	1	EA	$ 1,000.00	$ 1,000.00	
Manhole Covers and Frames (Drainage and Sewer)	2	EA	$ 1,200.00	$ 2,400.00	
6" Ductile Iron Water Pipe CL 52	250	LF	$ 150.00	$ 37,500.00	
2" Copper Water Pipe	250	LF	$ 120.00	$ 30,000.00	
Curb Stop w/ Box & Rod	2	EA	$ 385.00	$ 770.00	
6" Gate Valve	2	EA	$ 2,100.00	$ 4,200.00	
2" Water Service Tap & Corporation	2	EA	$ 425.00	$ 850.00	
Chlorine Injection Tap	2	EA	$ 1,400.00	$ 2,800.00	
Gas Service	175	LF	$ 195.00	$ 34,125.00	
Subdivision Electrification (Cost from Utility)	1	LS	$ 10,000.00	$ 10,000.00	
Site Stabilization/Stormwater/Landscaping				**$ 131,367.50**	
Silt Fence (or equal)	200	LF	$ 4.70	$ 940.00	
Erosion Control Monitoring	60	HR	$ 90.00	$ 5,400.00	

Retaining Wall Fall Protection Fencing	300	LF	$	85.00	$	25,500.00
New Trees - Deciduous - General	17	EA	$	510.00	$	8,670.00
Benches and Hardware	1	LS	$	30,000.00	$	30,000.00
Irrigation	1	LS	$	12,500.00	$	12,500.00
Shrubs - Small	535	EA	$	80.00	$	42,800.00
Loam and Seed	585	SY	$	9.50	$	5,557.50
	0					
Quality Control					**$**	**-**
Physical Testing Laboratory Services	0	AC	$	3,400.00	$	-
Officers - 4 hr min	0	HR	$	98.00	$	-
Flagger	0	HR	$	44.00	$	-
Site Maintenance					**$**	**6,656.00**
Site Clean Up (Removal of Construction Materials/Debris)	0.94	AC	$	6,400.00	$	6,016.00
Clean Out Catch Basins	8	EA	$	80.00	$	640.00
Dumpster (10 CY - 4 Ton)	0	WK	$	675.00	$	-
Demolition					**$**	**50,000.00**
Specialized or Site Specific Demolition ($1,200 Min)	1	LS	$	50,000.00	$	50,000.00
Mobilization/Demobilization			**5% of total**		**$**	**42,004.18**
				Subtotal:	**$**	**882,087.68**

Notes

*1. 12' cut for basement

*2. Asphalt and Concrete Flatwork

*3. Includes Drive Thru Concrete Island and Steps

*4

134 Pleasant Street

2025.12.22

Green Building Statement

WATER

- By reducing surface parking lot, water quality will be better protected.
- The project targets the use of low-flow fixtures to reduce indoor water consumption by at least 30% over current building code requirements, in alignment with EPAct 2005.

ENERGY

- By conducting early-phase energy study analysis, the basis of design for both the existing building with a deep energy retrofit and the new addition will target an Energy Use Intensity lower than code-compliant performance (IECC 2018), with a projected reduction of 30% (estimated EUI = 32). This will be validated by energy modeling in the next phase of design. The project will utilize a high-performance envelope (walls, roofs, floors, and fenestration). The project is also targeting an envelope design that reduces air infiltration and thermal bridging, paired with high-efficiency mechanical (HVAC) systems that include a heat-recovery ventilator (ERV) to recapture conditioned air and reduce overall energy consumption.
- The project will use LED lighting throughout with daylight sensors and other energy-management devices.
- Reduce Low level ozone (smog) -- Provide safe and secure bicycle storage. Use only low-VOC products for construction and operation.

MATERIALS & RESOURCES

- Minimize waste (during construction and operation)
- Use regional, renewable, low carbon footprint materials

INDOOR ENVIRONMENTAL QUALITY

- Thermal comfort -- Meet ASHRAE 55 Thermal Comfort Code. Address thermal envelope per above. Provide multiple zones of heating and cooling in each space.

- Indoor air quality (before and during occupancy) -- MEET ASHRAE 62 Ventilation Code in all occupied spaces.
- Views / connection to outdoors -- Provide views to outdoors for every regularly occupied space.
- Daylighting -- Achieve Daylight Factor of 2% minimum for every regularly occupied space.
- Individual controls (light, heat etc...) -- Provide individual controls for temperature and lighting.

STORMWATER INSPECTION & MAINTENANCE PLAN

FOR

134 Pleasant Street
Portsmouth, NH

Introduction

The intent of this plan is to provide Double MC, LLC, (herein referred to as "owner") with a list of procedures that document the inspection and maintenance requirements of the drainage structures for this development.

The following inspection and maintenance program is necessary to keep the stormwater management system functioning properly. These measures will also help minimize potential environmental impacts. By following the enclosed procedures, the owner will be able to maintain the functionality of the drainage structures and maximize their ability to drain the site effectively from stormwater runoff.

Annual Report

The owner shall prepare an annual Inspection & Maintenance Report. The report shall include a summary of the system's maintenance and repair by transmission of the Inspection & Maintenance Log and other information as required. A copy of the report shall be delivered annually to the City of Portsmouth Public Works Department.

Inspection & Maintenance Checklist/Log

The following pages contain a Stormwater Management System Inspection & Maintenance Checklist and a blank copy of the Stormwater Management System Inspection & Maintenance Log. These forms are provided to the owner as a guideline for performing the inspection and maintenance. This is a guideline and should be periodically reviewed for conformance with current practice and standards.

DRAINAGE STRUCTURE COMPONENTS

Non-Structural BMP's

Non-Structural best management practices (BMP's) include temporary and permanent measures that typically require less labor and capital inputs and are intended to provide protection against erosion of soils. Examples of non-structural BMP's on this project include but are not limited to: temporary and permanent mulching, temporary and permanent grass cover, trees, shrubs and ground covers, miscellaneous landscape plantings, dust control, tree protection, topsoiling, sediment barriers, and a stabilized construction entrance.

Structural BMP's

Structural BMP's are more labor and capital-intensive structures or installations that require more specialized personnel to install. Examples on this project include but are not limited to storm drain catch basins, roof drains and pipes.

Inspection and Maintenance Requirements

The following summarizes the inspection and maintenance requirements for the various BMP's that may be found on this project.

1. **Landscaped areas:** After each rain event of 0.5" or more during a 24-hour period, inspect landscaped areas for signs of disturbance, such as erosion. If damaged areas are discovered, immediately repair the damage. Repairs may include adding new topsoil, lime, seed, fertilizer and mulch.
2. **Plantings**: Planting and landscaping (trees, shrubs) shall be monitored bi-monthly during the first year to insure viability and vigorous growth. Replace dead or dying vegetation with new stock and adjust the conditions that caused the dead or dying vegetation. During dryer times of the year, provide weekly watering or irrigation during the establishment period of the first year. Make the necessary adjustments to ensure long-term health of the vegetated covers, i.e. provide more permanent mulch or compost or other means of protection. Clean up dead leaves yearly to avoid drainage issues.
3. **Storm Drain Catch Basins and Pipes:** Monitor drain inlets and outlets during construction. Monitor sediment levels in catch basin sumps and remove as necessary.
4. **Roof Drains:** Maintain roof drains and review periodically for clogs. Roof drain filters will be installed within the buildings. Follow the Maintenance Specification as Detailed in the attached.

Stormwater Management System

Inspection & Maintenance Checklist for Post Construction Condition—for 134 Pleasant Street, Portsmouth, NH

BMP/System Component	Minimum Inspection Frequency	Minimum Inspection Requirements	Maintenance/Cleanout Threshold
Closed Drainage System			
Drainage Pipes and Roof Drain Filters	Yearly	*Check for sediment clogging, or soiled runoff.*	Clean entire drainage system and remove all sediments if discovered in piping.
Catch Basins	Bi-Annually	*Check for excessive accumulation of sediment in sump*	Remove sediment as necessary
Annual Report	Yearly	*Prepare Annual Report, including all Inspection & Maintenance Logs. Provide to City (if required).*	N/A

Stormwater Management System Maintenance Summary

Inspection & Maintenance Log—for 134 Pleasant Street, Portsmouth, NH

BMP/System Component	Date Inspected	Inspector	Problems Noted, Required Maintenance *(List Items/Comments)*	Date of Maintenance	Performed By

Data Sheets

CLOSED DRAINAGE STRUCTURE MAINTENANCE SHEET

INSPECTION REQUIREMENTS		
ACTION TAKEN	**FREQUENCY**	**MAINTENANCE REQUIREMENTS**
-Bio Clean Filters *-Drain Manholes*	**Every other Month**	***Check for erosion or short-circuiting*** ***Check for sediment accumulation*** ***Check for floatable contaminants***
-Drainage Pipes	**1 time per 2 years**	***Check for sediment accumulation/clogging, or soiled runoff.***

MAINTENANCE LOG	
PROJECT NAME	
INSPECTOR NAME	**INSPECTOR CONTACT INFO**
DATE OF INSPECTION	**REASON FOR INSPECTION** ☐LARGE STORM EVENT ☐PERIODIC CHECK-IN
IS CORRECTIVE ACTION NEEDED? ☐YES ☐NO	**DESCRIBE ANY PROBLEMS, NEEDED MAINTENANCE**
DATE OF MAINTENANCE	**PERFORMED BY**
NOTES	

Bio Clean

A Forterra Company

Downspout Filter

A Stormwater Solution

OVERVIEW

The Bio Clean Downspout Filter is the industry's leading solution for treatment of roof runoff. This technology is used to treat commercial and industrial rooftops along with highrise buildings, parking structures, and residential buildings.

Available in 3 sizes, this filter can easily adapt to downspouts 2" to 12" in diameter. The filter comes standard with rubber boots that allow for easy installation to the downspout.

Proven since 2003, the Downspout Filter has been used on hundreds of installations throughout the United States. All internal components are constructed of stainless steel.

The sleek in-line design allows the filter to be used in tight spaces. Approved by the IAPMO, this filter can meet all your needs.

PERFORMANCE

93% REMOVAL OF TSS

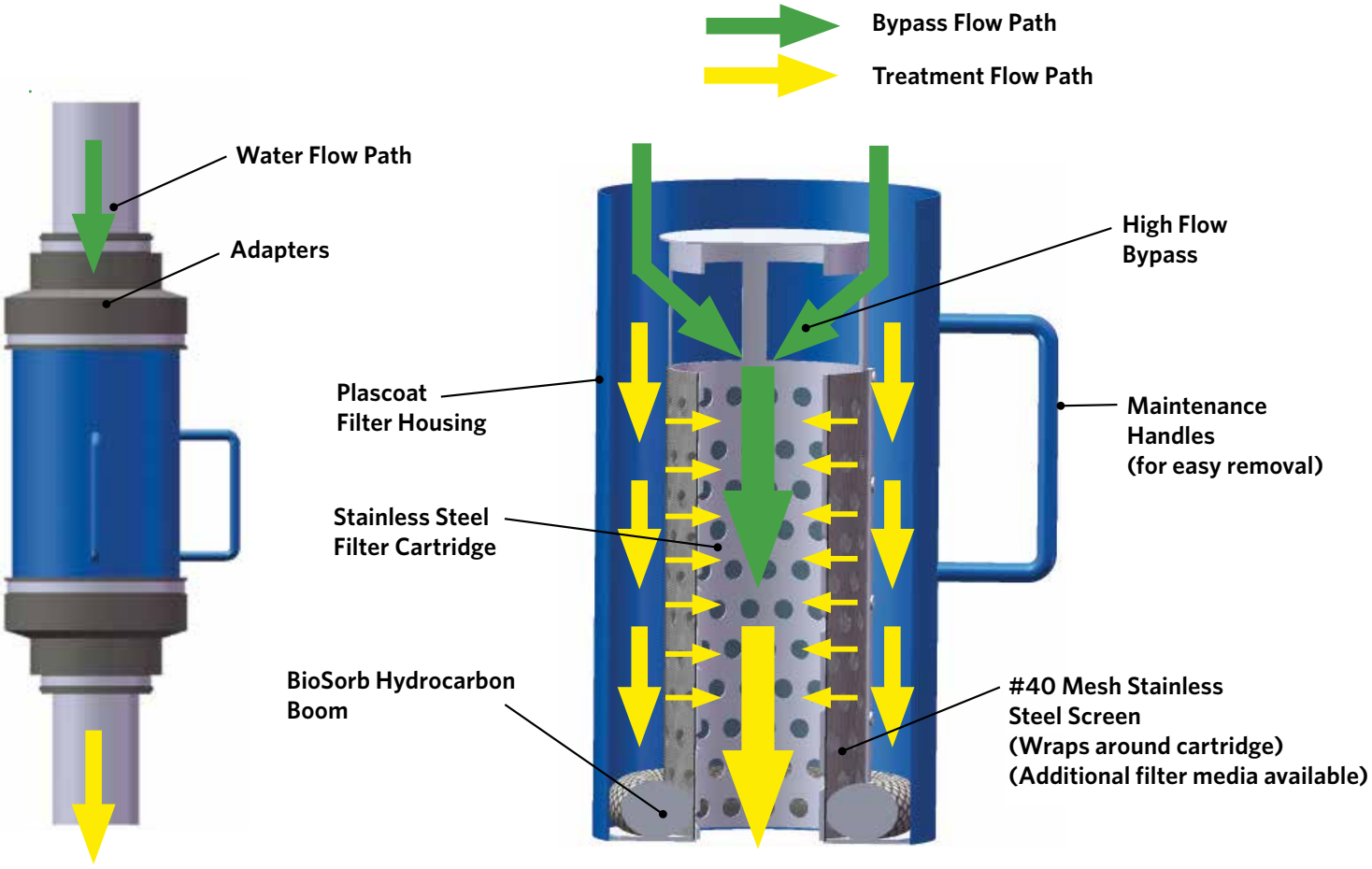
87% REMOVAL OF HYDROCARBONS

- EFFECTIVE AT REMOVING METALS, NUTRIENTS, AND BACTERIA (MEDIA TYPE)

ADVANTAGES

- 1-YEAR WARRANTY
- NO NETS OR GEOFABRICS
- SLEEK IN-LINE DESIGN
- HIGH TREATMENT FLOW RATE
- HIGH BYPASS FLOW RATE
- LOW COST

OPERATION



SPECIFICATIONS

MODEL #	INLET ID (dia., in.)	FILTER OD (dia., in.)	STORAGE CAP. (cu. ft.)	FILTERED FLOW (gpm)	BYPASS FLOW (gpm)
BC-DF4	4	6.625	0.09	249	566
BC-DF6	6	8.625	0.21	509	1006
BC-DF8	8	8.625	0.21	509	1006
BC-DF10	10	12.75	0.77	1145	2264
BC-DF12	12	12.75	0.77	1145	2264

APPLICATION

Easily adapts to square or rectangular downspouts.

Fits in-line with iron, steel, or plastic pipe.

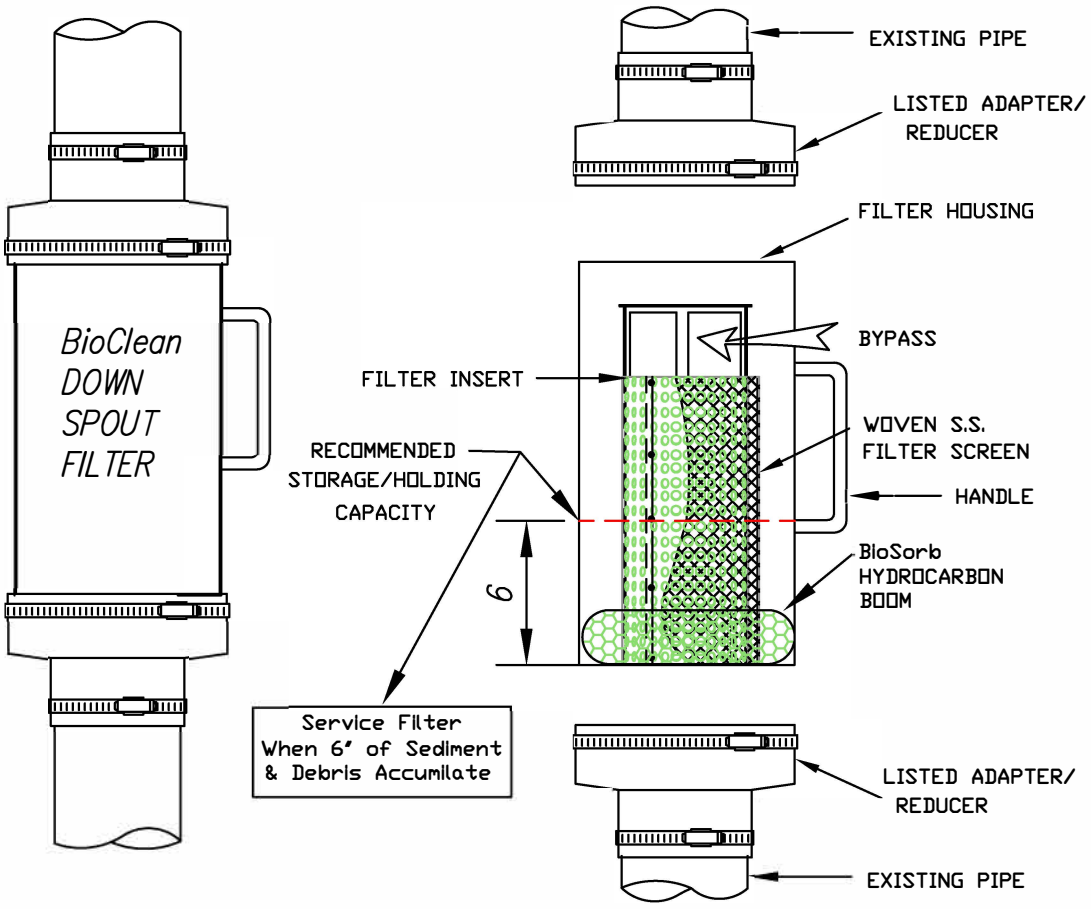
- Commercial
- Residential
- Parking Structures
- Mixed-Use

Bio Clean
A Forterra Company
5796 Armada Drive Suite 250
Carlsbad, CA 92008
855.566.3938
stormwater@forterrabp.com
biocleanenvironmental.com

SERVICE MANUAL

(Cleaning Procedures)

Bio Clean DOWNSPOUT FILTER
Screen Type With Hydrocarbon Boom



DETAIL OF PARTS

TOOLS AND EQUIPMENT NEEDED:

1. Medium size flat scred driver
2. BioSorb hydrocarbon boom. 25-1/2" X 2" dia.
   (Call Bio Clean to order)
3. Trash container or bag
4. Wooden dowel approx. 3' x 1/2' dia.

Bio Clean
A Forterra Company

P.O. BOX 869, Oceanside, Ca. 92049
(760) 433-7640 Fax (760) 433-3176
www.biocleanenvironmental.net

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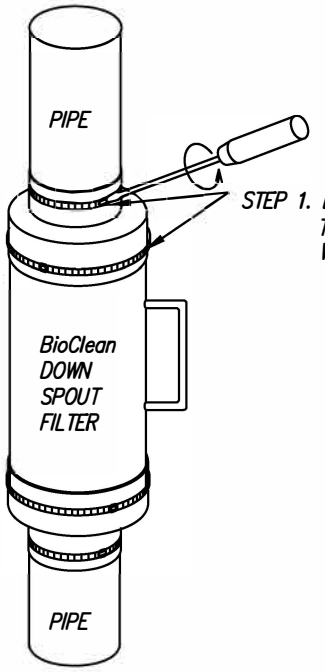
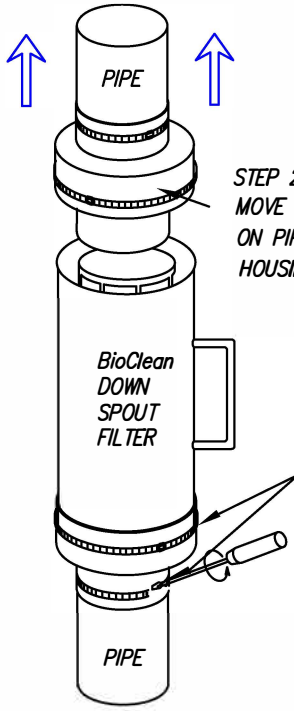
REMOVING FILTER



STEP 1. LOOSEN BOTH TOP CLAMPS WITH SCREW DRIVER.

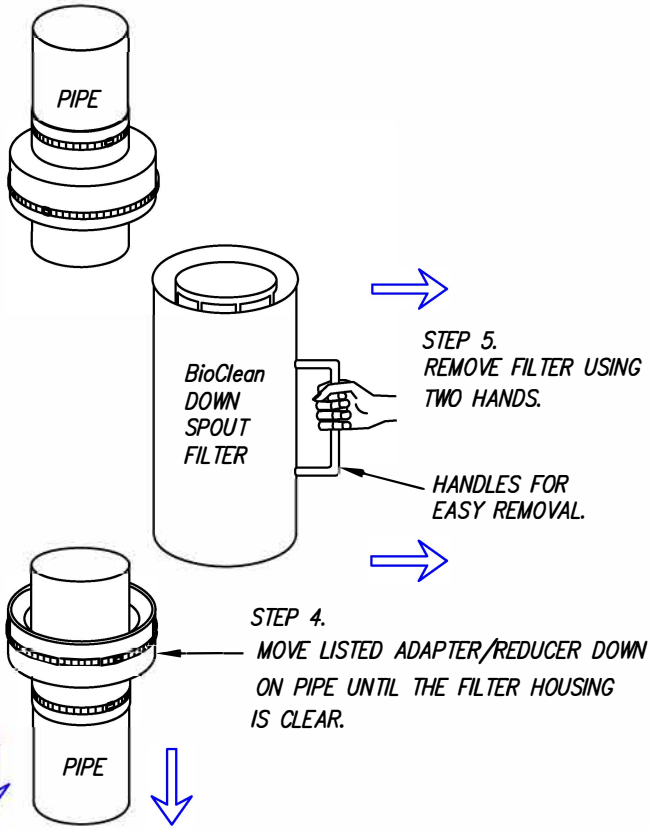


STEP 2.
MOVE LISTED ADAPTER/REDUCER UP ON PIPE UNTIL THE FILTER HOUSING IS CLEAR TO REMOVE.

STEP 3.
LOOSEN BOTH BOTTOM CLAMPS WITH SCREW DRIVER.

(STEP 4. AT BOTTOM LEFT OF PAGE)

CLEANING FILTER

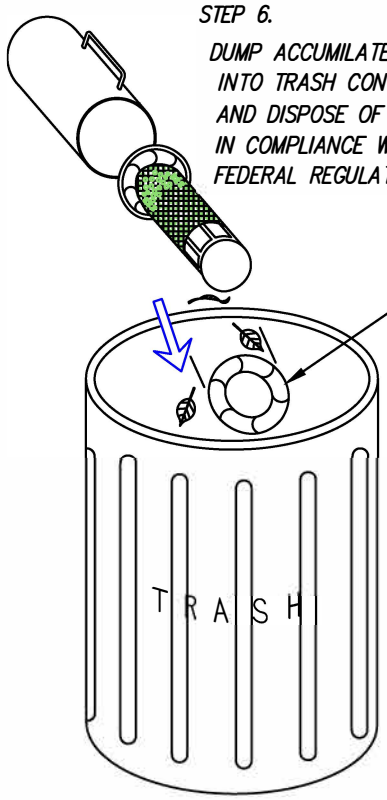


STEP 5.
REMOVE FILTER USING TWO HANDS.

HANDLES FOR EASY REMOVAL.

STEP 4.
MOVE LISTED ADAPTER/REDUCER DOWN ON PIPE UNTIL THE FILTER HOUSING IS CLEAR.

STEP 6.
DUMP ACCUMILATED DEBRIS OUT OF FILTER INTO TRASH CONTAINER. REMOVE FILTER INSERT AND DISPOSE OF HYDROCARBON BOOM IN COMPLIANCE WITH LOCAL STATE & FEDERAL REGULATIONS.



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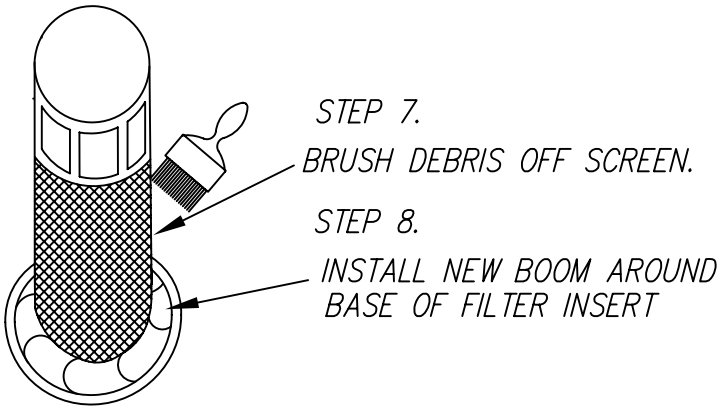
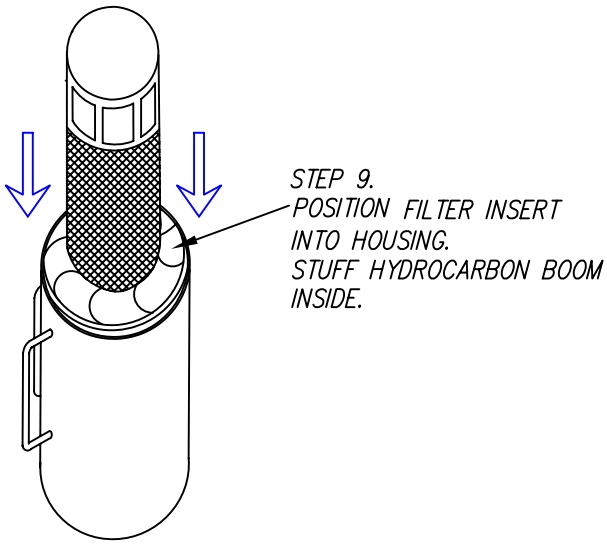
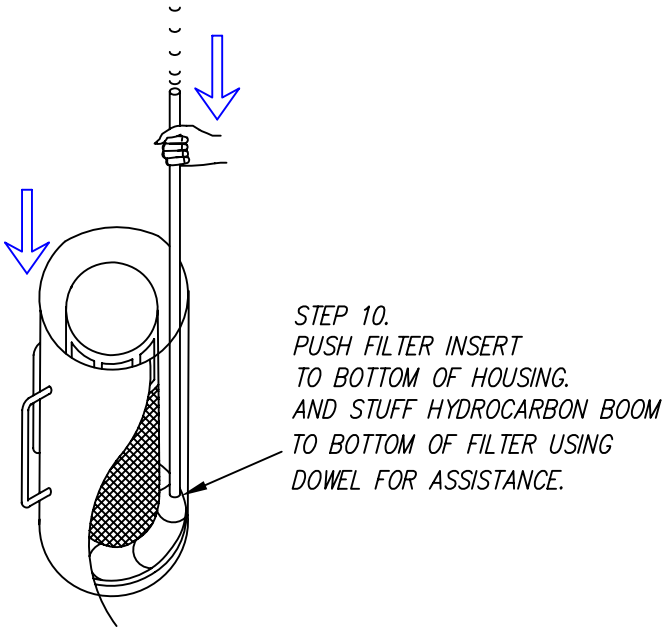
A Forterra Company
P.O. BOX 869, Oceanside, Ca. 92049
(760) 433-7640 Fax (760) 433-3176
www.biocleanenvironmental.net



REPLACING FILTER INSERT





REPLACING FILTER

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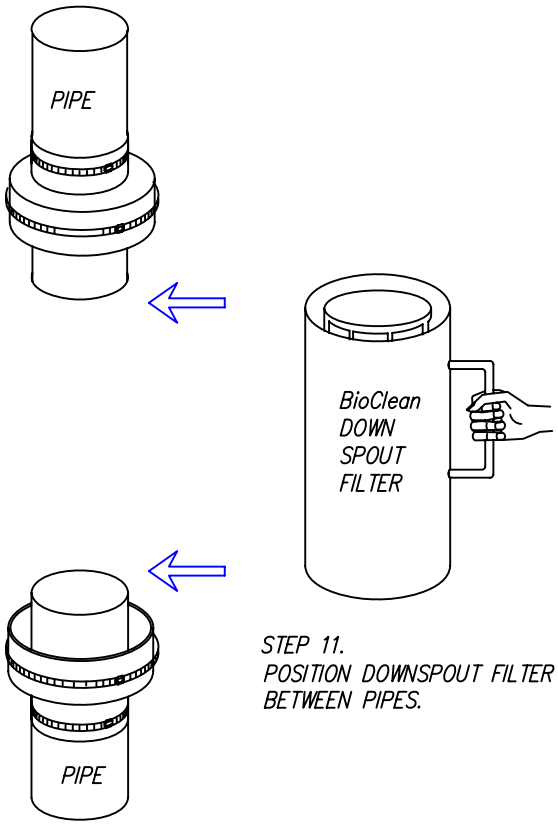
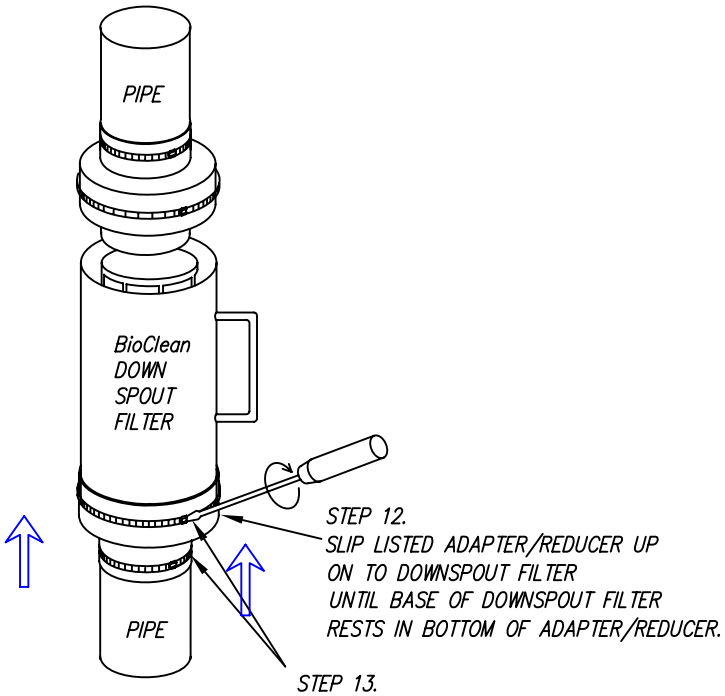
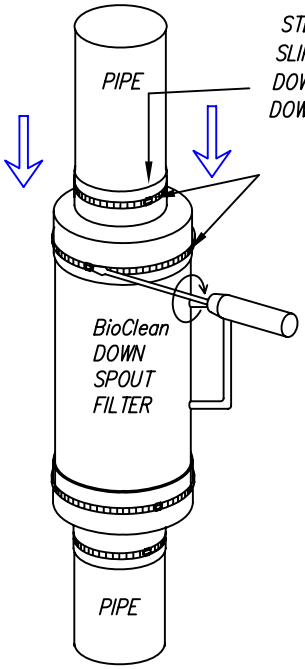


STEP 11.
POSITION DOWNSPOUT FILTER
BETWEEN PIPES.



STEP 12.
SLIP LISTED ADAPTER/REDUCER UP
ON TO DOWNSPOUT FILTER
UNTIL BASE OF DOWNSPOUT FILTER
RESTS IN BOTTOM OF ADAPTER/REDUCER.

STEP 13.
SECURE BY TIGHTENING
BOTH CLAMPS.



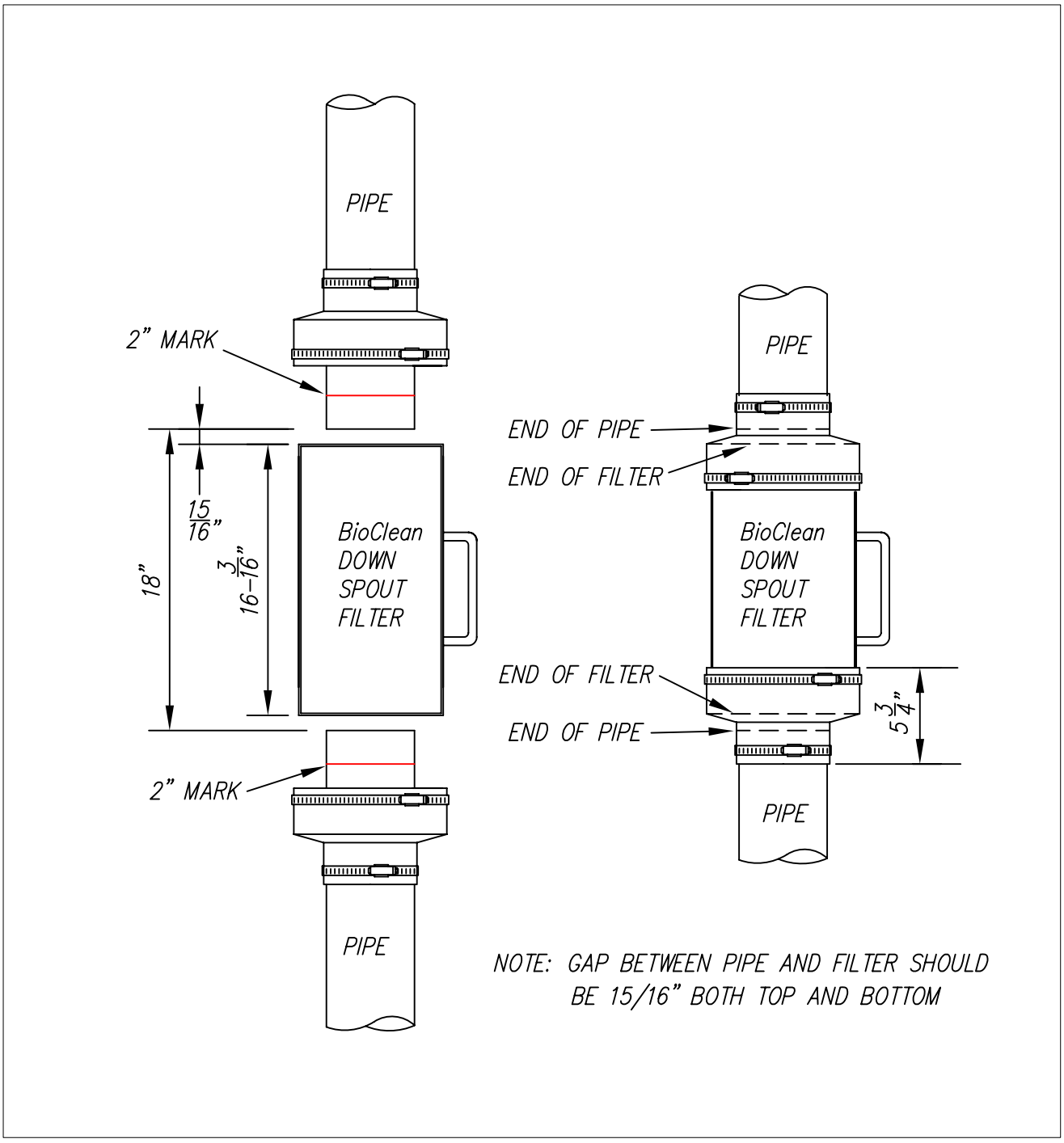
STEP 14.
SLIP LISTED ADAPTER/REDUCER
DOWNWARD ON TO
DOWNSPOUT FILTER.

STEP 15.
SECURE BY TIGHTENING
BOTH CLAMPS
WITH SCREWDRIVER.

ENSURE CLAMPS
ARE PROPERLY TIGHTENED.
SERVICE COMLPLETE.

APPROPRIATE INSTALLATION

FILTER CENTERED BETWEEN PIPES WITH EVEN GAPS ON TOP AND BOTTOM



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134 Pleasant Street

December 22, 2025

Parking Demand Analysis

Residential - dwelling units				parking		
Level	**UNIT No.**	**QTY bedrooms**	**Area (sf)**	**spaces/unit**	**TOTAL**	
LEVEL 2 A	201	2	735	1	1	
LEVEL 2 A	202	1	666	1	1	
LEVEL 2 A	203	1	753	1	1	
LEVEL 2 A	204	1	718	1	1	
LEVEL 2 A	205	2	751	1	1	
LEVEL 2 A	206	2	772	1	1	
LEVEL 2 A	207	2	741	1	1	
LEVEL 2 A	208	1	604	1	1	
LEVEL 2 A	209	1	666	1	1	
LEVEL 2 A	210	1	666	1	1	
LEVEL 2 A	211	2	761	1	1	
LEVEL 2 A	212	1	565	1	1	
LEVEL 2 A	213	1	540	1	1	
LEVEL 2 A	214	1	583	1	1	
LEVEL 2 B	221	3	1,604	1	1	
LEVEL 2 B	222	3	1,392	1	1	
PENTHOUSE APARTMENT 3BR	301	3	1,634	1	1	
PENTHOUSE APARTMENT 3BR	302	3	1,317	1	1	
subtotal, gross	**18**	**31**	**15,466**		**18**	
Visitor Parking - 1 space per every 5 dwelling units (apartments) rounded up					4	
total, gross					**22**	
Bicycle Parking , Residential						
use	spaces required per use			TOTAL		
multifamily dwellings	1 bicycle per 5 dwelling units where > 4 dwelli			4		
bicycle parking provided	outside			5		
bicycle parking provided	inside			13		
total bike parking provided - Residential				18		
Car parking deduction = 1 car space for 6 excess bikes provided, max 5%:						
max deduction 5% allowed =	1.1	parking spaces deduction		Rounded dn	(1)	
Net Total Residential parking required (round up)					**21**	

Commercial						
level	**Suite No.**	**Name**	**Area (sf)**	**sf/parking space**	**TOTAL spaces**	
LEVEL 1 B	103	RETAIL BANKING	8,323	350	24	
LEVEL 2 B	200	FINANCIAL SERVICES	4,087	350	12	
LEVEL 1 A	102	HEALTH CLUB / PHYSICAL THERAPY	11,144	250	45	
subtotal, gross					**81**	
Bicycle Parking , Commercial						
use	spaces required per use			TOTAL		
Commercial Use	1 bicycle per 10 spaces required			9		
bicycle parking provided	outside			5		
bicycle parking provided	inside			13		
total bike parking provided - Commercial				18		
Car parking deduction = 1 car space for 6 excess bikes provided, max 5%:						
max deduction 5% allowed =	4.1	parking spaces deduction		Rounded dn	(4)	
Net Total Commercial parking required (round up)					**77**	

Shared Parking 10.1112.60										
Total Parking Required	**Weekday**				**Weekend**				**Nighttime**		
Land Use	**daytime (8am-5pm)**	**spaces required**	**evening (6-12pm)**	**spaces required**	**daytime (8am-5pm)**	**spaces required**	**evening (6-midnight)**	**spaces required**	**(midnight 6am)**	**spaces required**	**Maximum TOTAL Required**
Apartments (Residential)	60%	12.6	100%	21.0	80%	16.8	100%	21.0	100%	21.0	
Retail Banking (Service)	60%	14.4	90%	21.6	100%	24.0	70%	16.8	5%	1.2	
Financial Services (Office)	100%	12.0	20%	2.4	10%	1.2	5%	0.6	5%	0.6	
Health Club / PT (Service)	60%	27.0	90%	40.5	100%	45.0	70%	31.5	5%	2.3	
Shared Parking TOTAL Required		66.0		85.50		87.0		69.9		25.1	**86**
PARKING EASEMENT - 93 PLEASANT ST (HOTEL)											**5**
NET TOTAL PARKING REQUIRED (residential + commercial)											**91**

SUMMARY						
parking proposed - outside						37
parking proposed - inside		20 tandems (*2) + 8 singles = 48				48
Total parking proposed						**85**
EXCESS beyond Required *(Defecit)*						**(6)**
Existing total available onsite today						38
Proposed Net **Increase** (reduction) in parking						**47**

HALEY WARD
200 Griffin Road, Unit 14, Portsmouth, NH 03801
Phone (603) 430-9282

22 December, 2025

Trip Generation
Proposed Development
134 Pleasant Street
Portsmouth, NH

On behalf of Double MC, LLC, we hereby submit this Trip Generation in support of the applicant's filing with the Portsmouth Technical Advisory Committee for Site Plan approval. The project consists of the adaptive reuse of the existing structure, with horizontal and vertical building expansions, and below grade (basement) parking.

In developing the expected trips, Haley Ward considered the standard trip generation rates and equations published in the Institute of Transportation Engineers (ITE) Trip Generation Manual. The land use categories that best correlate with the use of the proposed facility are listed below:

Residential: ITE Land Use 220 (Multifamily Housing Low-Rise)

Health Club / Physical: ITE Land Use 630 (Walk-In Clinic)

Financial Services: ITE Land Use 712 (Small Office Building)

Retailing Banking: ITE Land Use 912 (Drive-In Banking)

Using these descriptions, the proposed site generates use the following AM and PM Peak Hour Trips:

<u>Proposed Trip Generation (Weekday)</u>

Residential: ITE Land Use 220 (Multifamily Housing Low-Rise)

Weekday AM Peak Hour of Generator 0.47 Trips per Dwelling Unit:
18 units X 0.47 Trip Ends per Dwelling Unit = 9 trips
9 Trips (27% entering; 73% exiting)

Weekday PM Peak Hour of Generator 0.62 Trips per Dwelling Unit:
18 units X 0.62 Trip Ends per Dwelling Unit = 12 trips
12 Trips (60% entering; 40% exiting)

Health Club / Physical: ITE Land Use 630 (Walk-In Clinic)

Weekday AM Peak Hour of Generator 1.55 Trips per Employee:
10 Employees (Assumed) X 1.55 Trip Ends per Dwelling Unit = 16 trips
16 Trips (51% entering; 49% exiting)

Weekday PM Peak Hour of Generator 2.36 Trips per Employee:
10 Employees (Assumed) X 2.36 Trip Ends per Dwelling Unit = 24 trips
24 Trips (43% entering; 57% exiting)

Financial Services: ITE Land Use 712 (Small Office Building)

Weekday AM Peak Hour of Generator 2.61 Trips per 1,000 SF GFA:
9,910 / 1,000 SF X 2.61 Trip Ends per SF = 26 trips
26 Trips (60% entering; 40% exiting)

Weekday PM Peak Hour of Generator 2.93 Trips per 1,000 SF GFA:
9,910 / 1,000 SF X 2.93 Trip Ends per SF = 29 trips
29 Trips (42% entering; 58% exiting)

Retailing Banking: ITE Land Use 912 (Drive-In Banking)

Weekday AM Peak Hour of Generator 14.86 Trips per 1,000 SF GFA:
2,500 (for Retail Banking) / 1,000 SF X 14.86 Trip Ends per SF = 38 trips
38 Trips (53% entering; 47% exiting)

Weekday PM Peak Hour of Generator 20.92 Trips per 1,000 SF GFA:
2,500 (for Retail Banking) / 1,000 SF X 20.92 Trip Ends per SF = 53 trips
53 Trips (42% entering; 58% exiting)

<u>Total Trips</u>

Weekday AM Peak Hour of Generator = 89 Trips
Weekday PM Peak Hour of Generator = 118 Trips

The added trip generation from the site is not excessive, will not impact the adjacent street network, and can be accommodated without any changes to the roadway network. Please feel free to call if you have any questions or comments about this application.

Sincerely,

John R. Chagnon, PE
Haley Ward

P:\NH\5010156-McNabb_Properties\1532-134 Pleasant St., Portsmouth-JRC\04-Support_Files\Reference_Materials\ITE Trip Gen\134 Pleasant Trip Generation 12-22-25.doc

Multifamily Housing (Low-Rise)
Not Close to Rail Transit (220)

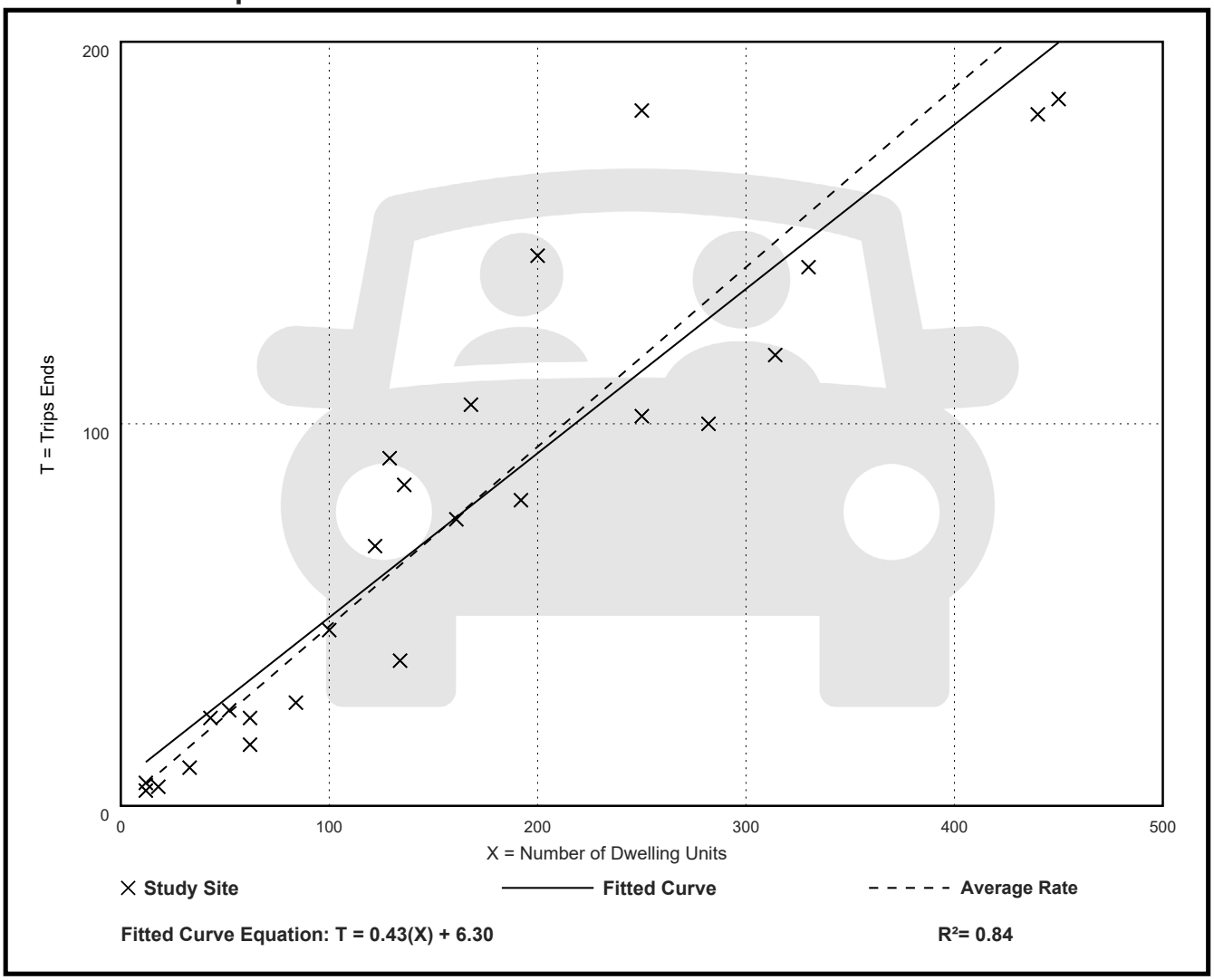
Vehicle Trip Ends vs: **Dwelling Units**
On a: **Weekday,**
AM Peak Hour of Generator

Setting/Location: **General Urban/Suburban**
Number of Studies: 25
Avg. Num. of Dwelling Units: 161
Directional Distribution: 27% entering, 73% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.47	0.26 - 0.73	0.13

Data Plot and Equation



Fitted Curve Equation: T = 0.43(X) + 6.30

R^2= 0.84

Multifamily Housing (Low-Rise)
Not Close to Rail Transit (220)

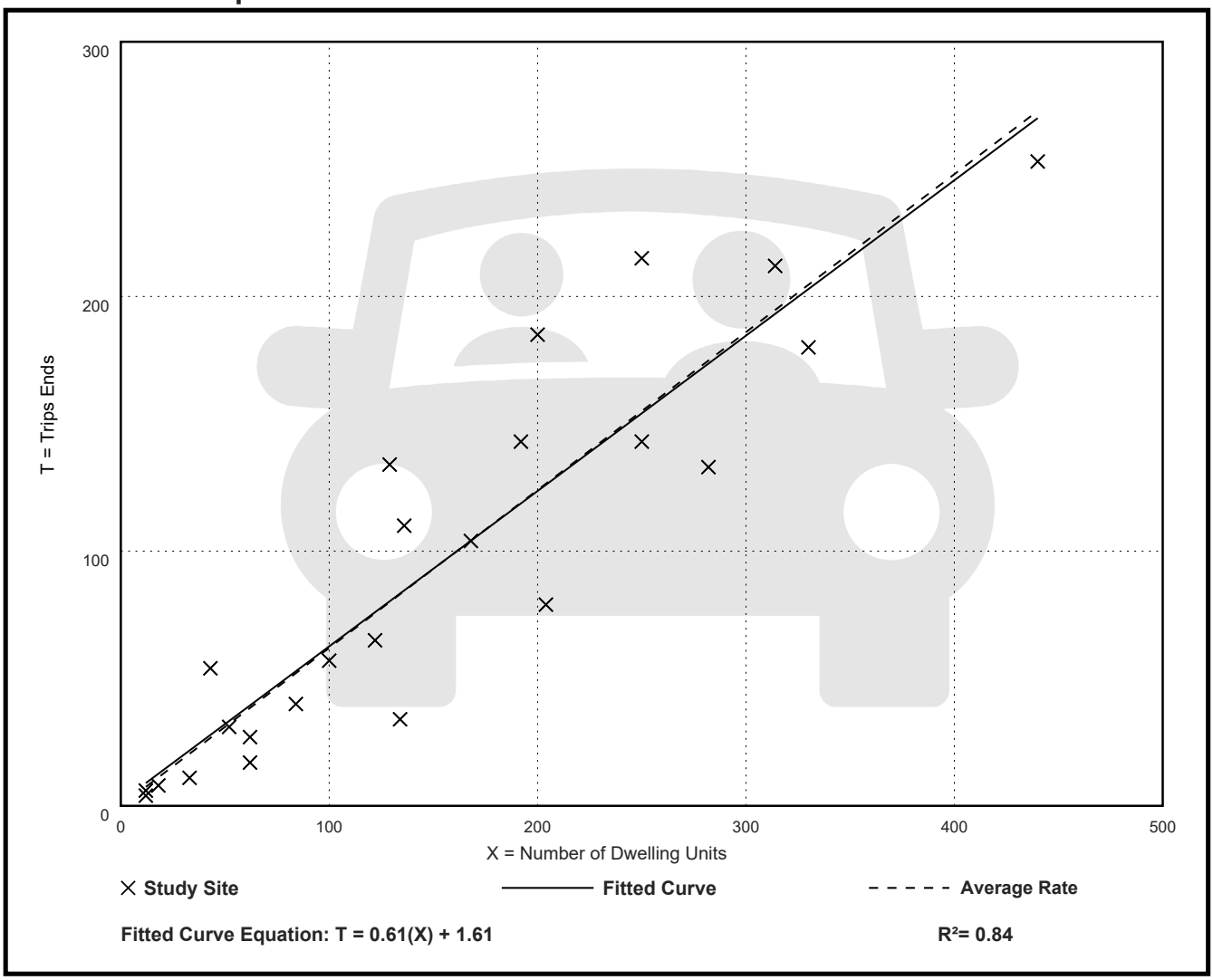
Vehicle Trip Ends vs: **Dwelling Units**
On a: **Weekday,**
PM Peak Hour of Generator

Setting/Location: **General Urban/Suburban**
Number of Studies: 24
Avg. Num. of Dwelling Units: 151
Directional Distribution: 60% entering, 40% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.62	0.25 - 1.26	0.20

Data Plot and Equation



Fitted Curve Equation: T = 0.61(X) + 1.61

R^2= 0.84

Walk-In Clinic
(630)

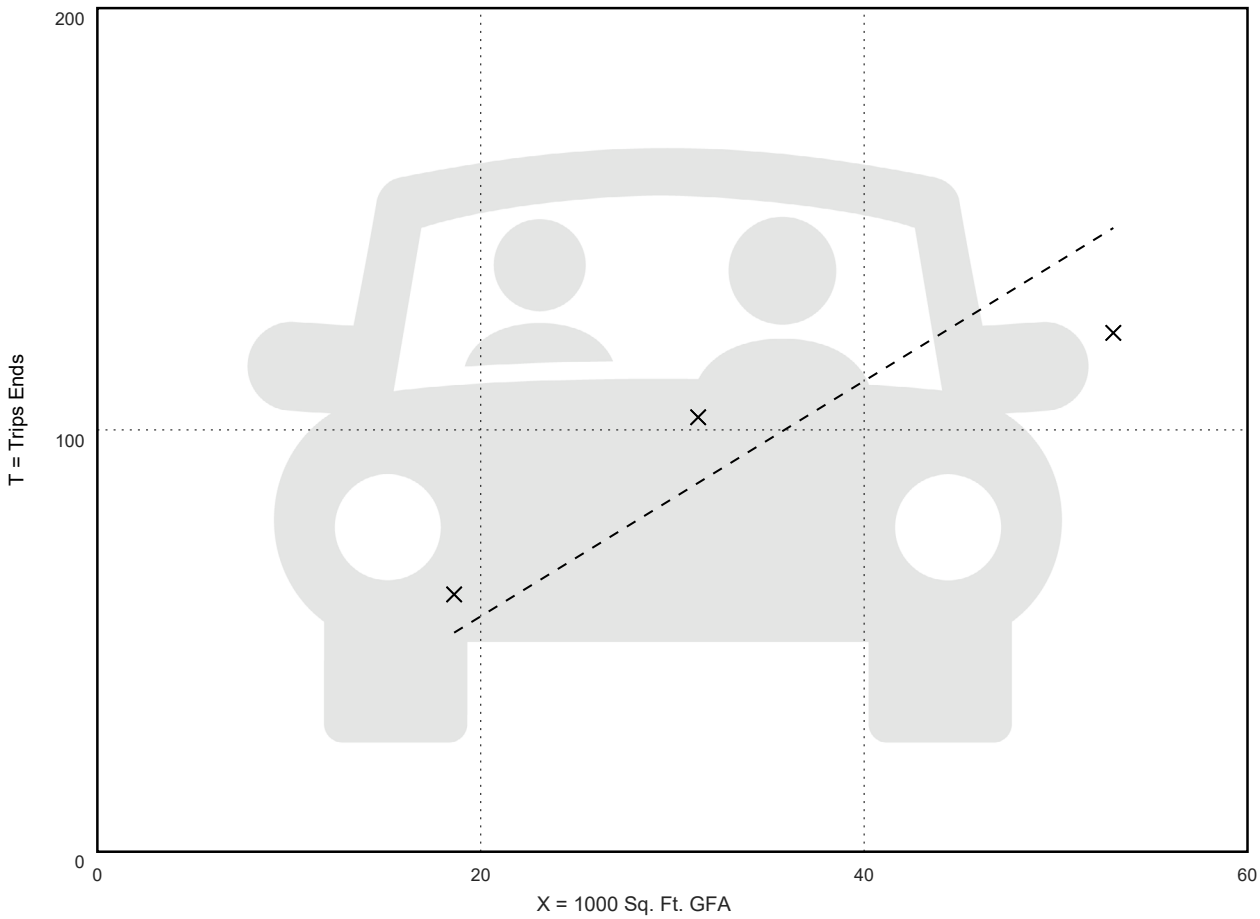
Vehicle Trip Ends vs: **1000 Sq. Ft. GFA**
On a: **Weekday,**
AM Peak Hour of Generator

Setting/Location: **General Urban/Suburban**
Number of Studies: 3
Avg. 1000 Sq. Ft. GFA: 34
Directional Distribution: 64% entering, 36% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
2.79	2.32 - 3.29	0.59

Data Plot and Equation



Fitted Curve Equation: Not Given

R^2= *

Walk-In Clinic
(630)

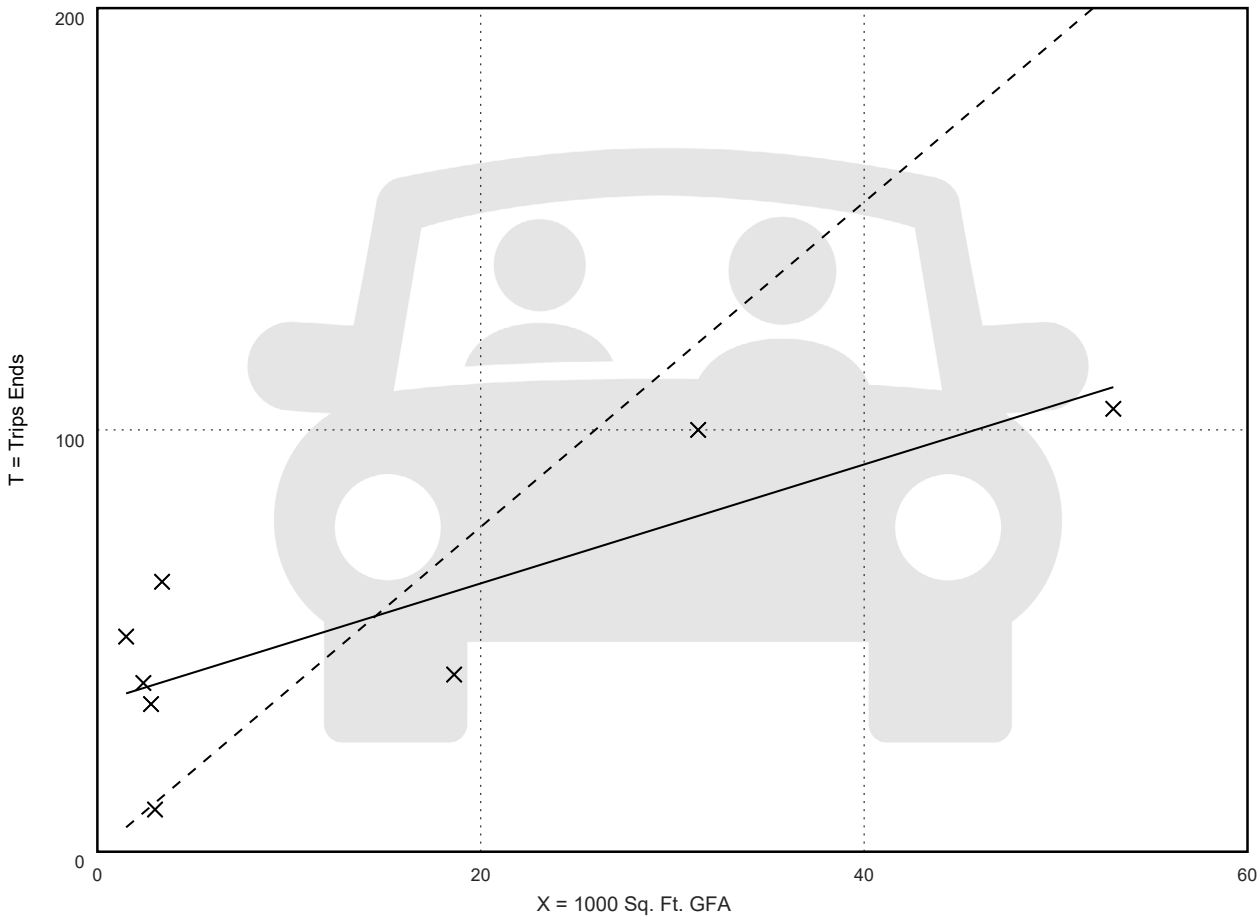
Vehicle Trip Ends vs: **1000 Sq. Ft. GFA**
On a: **Weekday,**
PM Peak Hour of Generator

Setting/Location: **General Urban/Suburban**
Number of Studies: 8
Avg. 1000 Sq. Ft. GFA: 15
Directional Distribution: 44% entering, 56% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
3.85	1.98 - 34.00	5.42

Data Plot and Equation



Fitted Curve Equation: T = 1.41(X) + 35.41

R^2= 0.67

Small Office Building
(712)

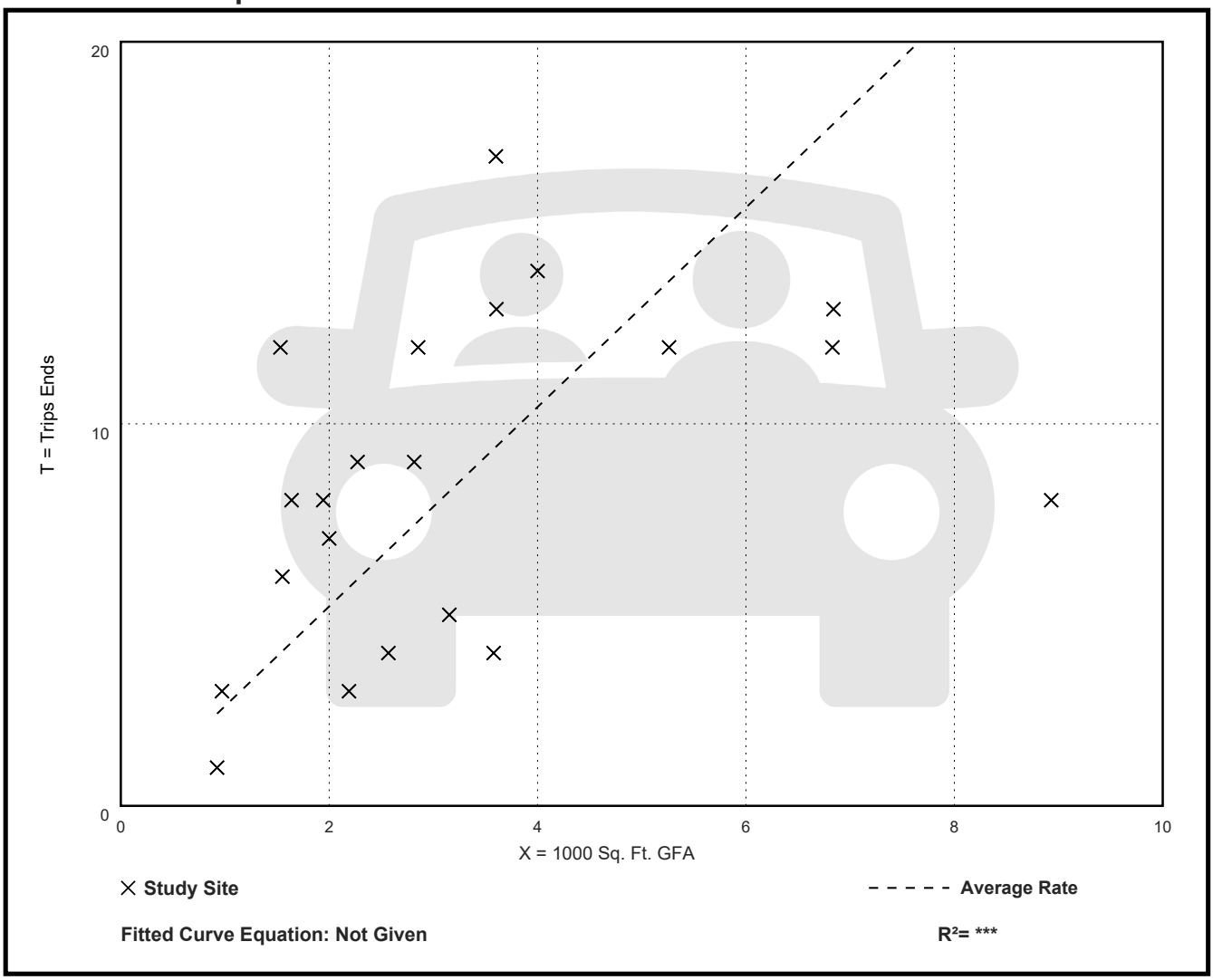
Vehicle Trip Ends vs: **1000 Sq. Ft. GFA**
On a: **Weekday,**
AM Peak Hour of Generator

Setting/Location: **General Urban/Suburban**
Number of Studies: 21
Avg. 1000 Sq. Ft. GFA: 3
Directional Distribution: 60% entering, 40% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
2.61	0.90 - 7.83	1.50

Data Plot and Equation



Fitted Curve Equation: Not Given

R^2= ***

Small Office Building
(712)

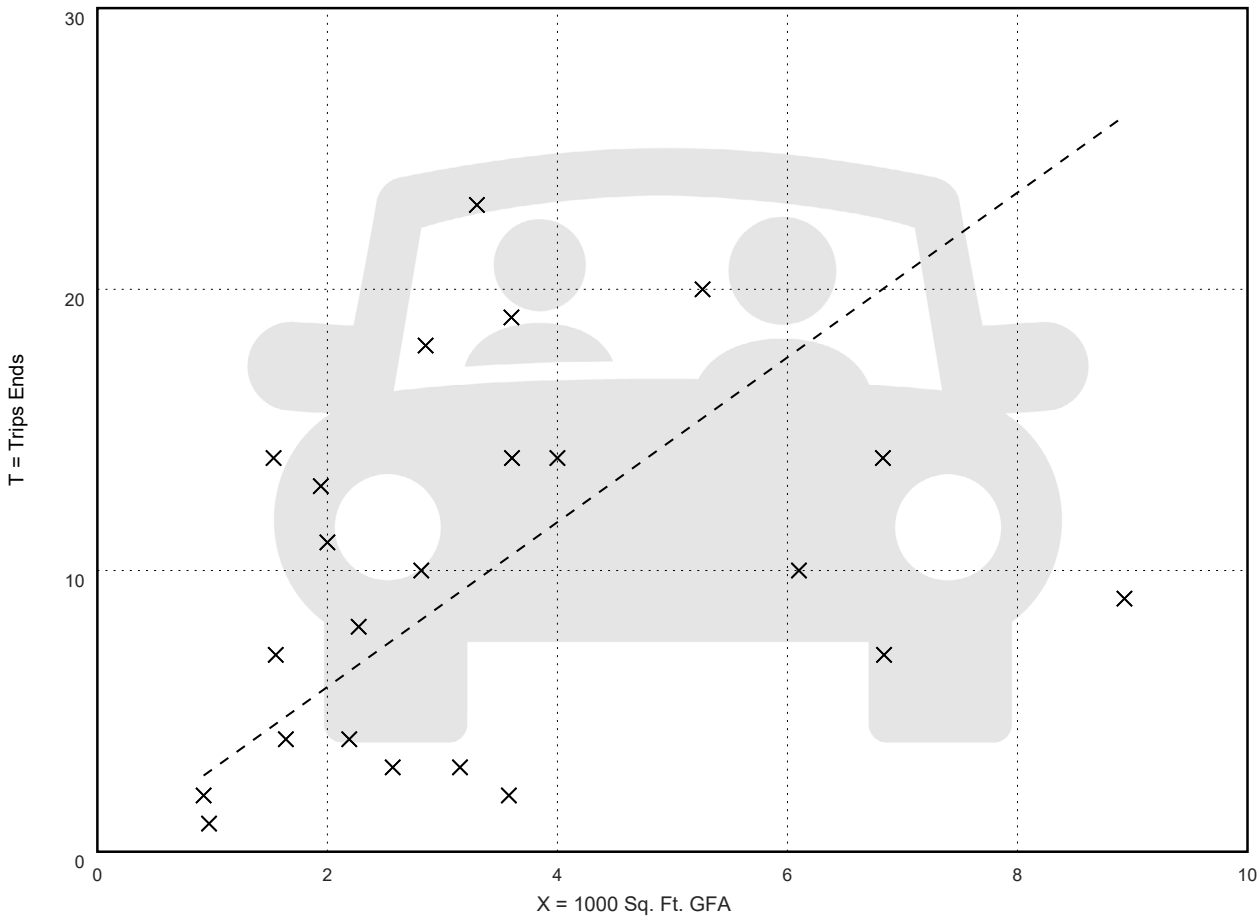
Vehicle Trip Ends vs: **1000 Sq. Ft. GFA**
On a: **Weekday,**
PM Peak Hour of Generator

Setting/Location: **General Urban/Suburban**
Number of Studies: 23
Avg. 1000 Sq. Ft. GFA: 3
Directional Distribution: 42% entering, 58% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
2.93	0.56 - 9.14	2.13

Data Plot and Equation



Fitted Curve Equation: Not Given

R^2= ***

Drive-in Bank
(912)

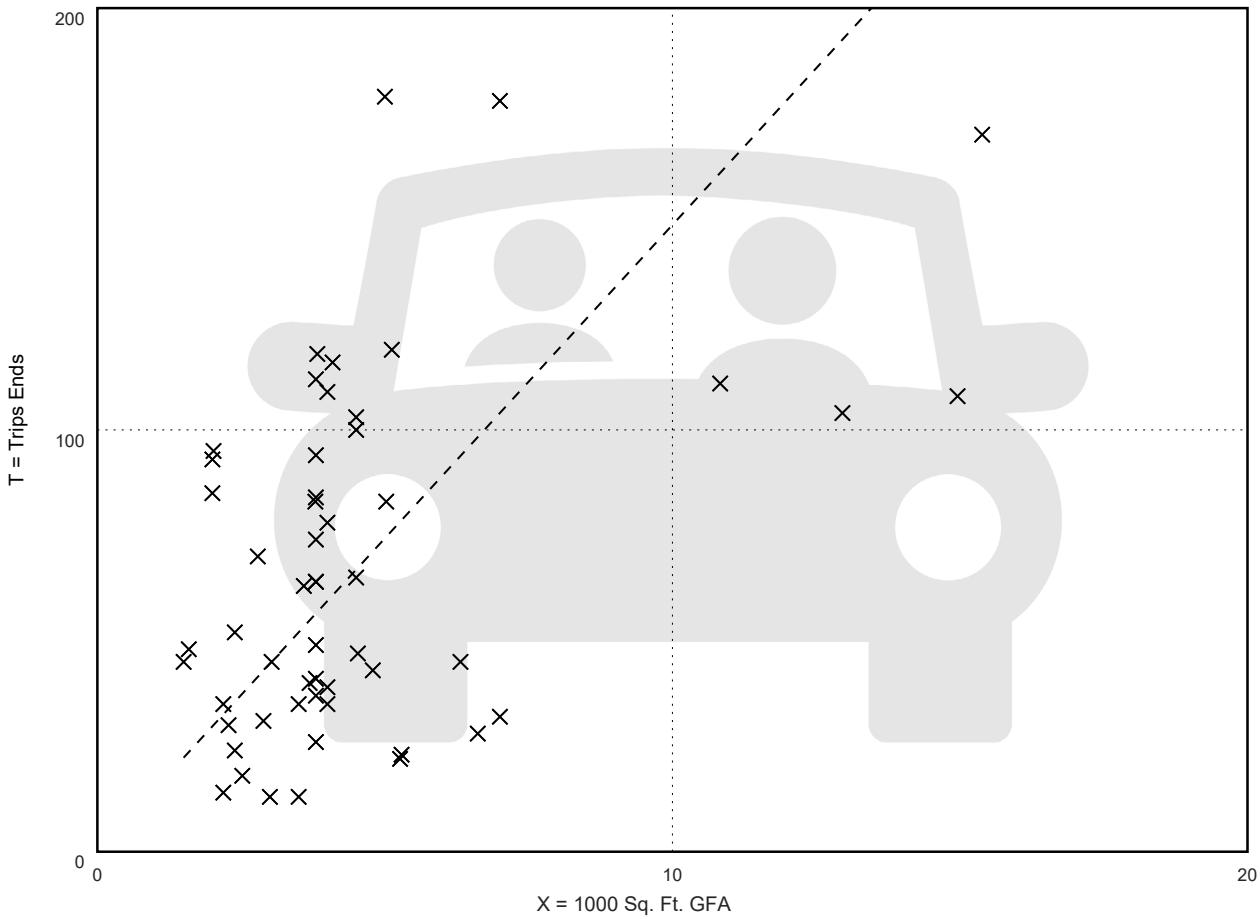
Vehicle Trip Ends vs: **1000 Sq. Ft. GFA**
On a: **Weekday,**
AM Peak Hour of Generator

Setting/Location: **General Urban/Suburban**
Number of Studies: 54
Avg. 1000 Sq. Ft. GFA: 5
Directional Distribution: 53% entering, 47% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
14.86	3.71 - 47.03	9.58

Data Plot and Equation



Fitted Curve Equation: Not Given

R^2= *

Drive-in Bank
(912)

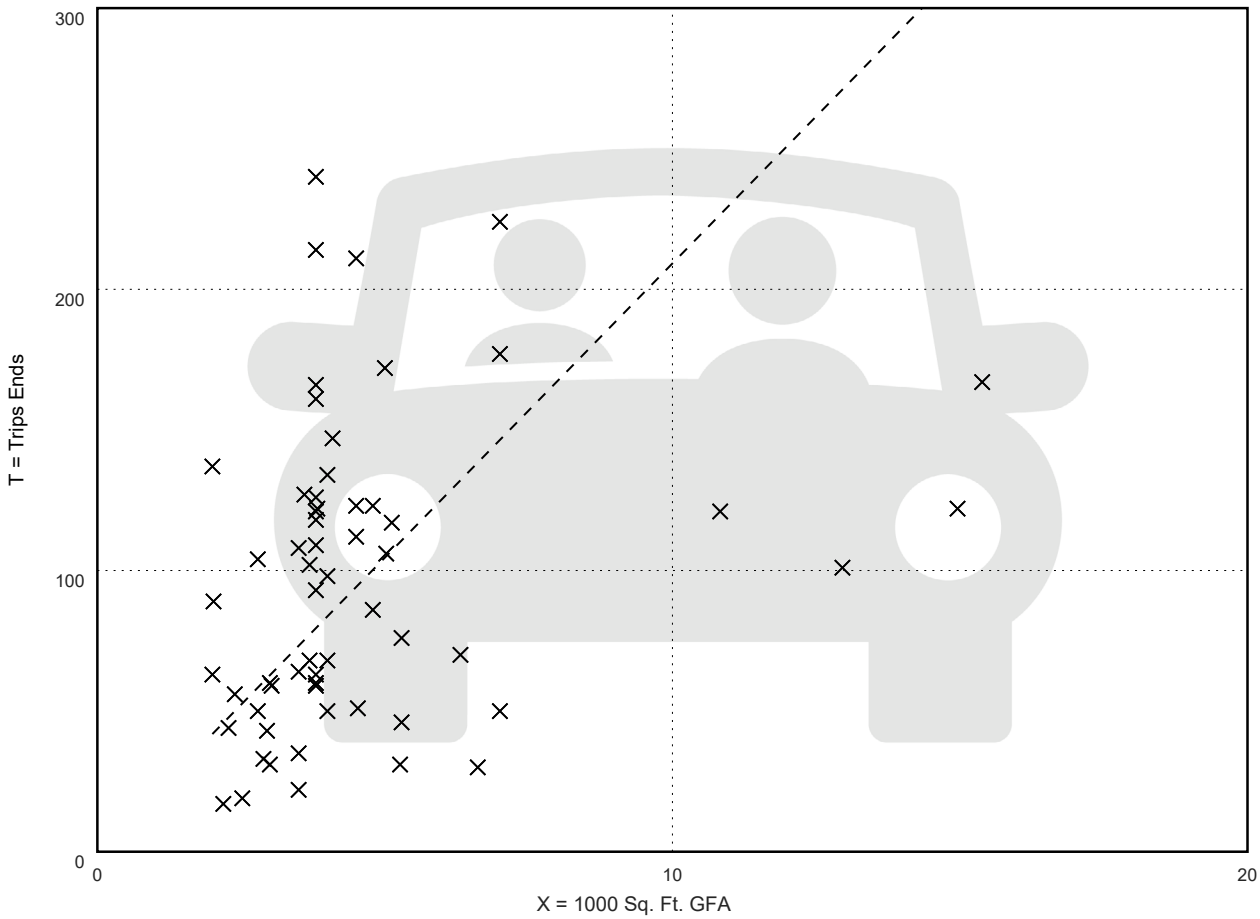
Vehicle Trip Ends vs: **1000 Sq. Ft. GFA**
On a: **Weekday,**
PM Peak Hour of Generator

Setting/Location: **General Urban/Suburban**
Number of Studies: 60
Avg. 1000 Sq. Ft. GFA: 5
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
20.92	4.54 - 68.50	13.45

Data Plot and Equation



Fitted Curve Equation: Not Given

R^2= ***

Catalog # : _____ Project : _____ Type : _____

Prepared By : _____ Date : _____

V-Locity Small (VALS)

Outdoor LED Area Light

OVERVIEW	
Lumen Package	6,000 - 27,000
Wattage Range	36 - 178
Efficacy Range (LPW)	142 - 171
Weight lbs(kg)	20 (9.1)
Control Options	IMSBT, ALB, ALS, 7-Pin, PCI

QUICK LINKS

Ordering Guide | Performance | Photometrics | Dimensions

FEATURES & SPECIFICATIONS

Construction

- Rugged die-cast aluminum housing contains factory prewired driver and optical units. Cast aluminum wiring access door located underneath.
- Self-contained optic, board and heat sink assembly can be rotated or replaced in the field.
- Fixtures are finished with LSI's DuraGrip® polyester powder coat finishing process. The DuraGrip finish withstands extreme weather changes without cracking or peeling. Other standard LSI finishes available. Consult factory.
- Shipping weight: TBD lbs in carton.

Optical System

- State-of-the-Art acrylic optics delivers industry leading optical control with an integrated gasket to provide IP66 rated seal.
- Proprietary refractor optics provide exceptional coverage and uniformity in distribution types 2, 3M, 3W, 4M, 4W, 4F, FTM, 5QN, 5QM, 5Q, 5QW, AM, WF and LC/RC.
- Available in 5000K, 4000K, 3500K, 3000K and 2700K color temperatures per ANSI C78.377 as well as phosphor converted amber.
- Minimum CRI of 80 (optional 70 CRI for 5000K and 4000K).
- Factory or field installable integral shielding available for enhanced spill light control.
- Zero Uplight (excludes adjustable arms).

Electrical

- High-performance driver features over-voltage, under voltage, short-circuit and over temperature protection.
- 0-10 volt dimming (10% - 100%) standard, must specify EXT option for dimming leads to be extended to fixture exterior.
- Standard Universal Voltage (120-277 VAC) Input 50/60 Hz or optional High Voltage (347-480 VAC).
- L70 Calculated Life: >60k Hours
- Total harmonic distortion: <20%
- Operating temperature: -40°C to +50°C (-40°F to +122°F).
- Power factor: >.90
- Input power stays constant over life.
- Field replaceable 10kV surge protection device meets a minimum Category C Low operation (per ANSI/IEEE C62.41.2).
- High-efficacy LEDs mounted to metal-core circuit board to maximize heat dissipation
- Driver is fully encased in potting material for moisture resistance and complies with FCC standards. Driver and key electronic components can easily be accessed.

Controls

- Optional integral passive infrared Bluetooth™ motion and photocell sensor. Fixtures operate independently and can be commissioned via iOS or Android configuration app.
- LSI's AirLink™ wireless control system options reduce energy and maintenance costs while optimizing light quality 24/7.

Installation

- REDiMount pole mount arm allows for a true one person installation.
- Side arm pole mount designed to mount to square or round poles.
- Pole mount arms can accommodate pole drill patterns from 2.4 to 5" on center and utilize LSI's reduced B3 or traditional B5 drill patterns.
- Additional mounting options are available including a mast arm or adjustable slip fitter that allow for luminaire attachment to a 2 3/8" tenon or mast arm.
- Adjustable arms allow for 65° of tilt (-5°below horizontal to +60° above horizontal) in 5° increments.

Warranty

- LSI luminaires carry a 5-year limited warranty. Refer to https://www.lsicorp.com/resources/terms-conditions-warranty/ for more information.

Listings

- Listed to UL 1598 and UL 8750.
- Meets Buy American Act requirements.
- DarkSky approved with 3000K or warmer color temperature selection (side arm pole and mast arm mounting only).
- Title 24 Compliant; see local ordinance for qualification information.
- Suitable for wet locations.
- IP66 rated Luminaire per IEC 60598-1.
- 3G rated for ANSI C136.31 high vibration applications are qualified.

V-Locity Small (VALS) Outdoor LED Area Light

Type : _____

Have questions? Call us at (800) 436-7800

ORDERING GUIDE

Back to Quick Links

TYPICAL ORDER EXAMPLE: VALS 18L 4W UNV 40K8 BLK SA ALBMR2LR IS

Prefix	Lumen Package	Distribution	Orientation[2]	Voltage	Color Temp /Rendering	Finish
VALS - V-Locity Area Light Small	**6L** - 6,000 lms, 36W **9L** - 9,000 lms, 54W **12L** - 12,000 lms, 74W **15L** - 15,000 lms, 91W **18L** - 18,000 lms, 111W **21L** - 21,000 lms, 130W **24L** - 24,000 lms, 155W **27L** - 27,000 lms, 178W Custom Lumen Packages[1]	**2** - Type 2 **3M** - Type 3 Medium **3W** - Type 3 Wide **4M**- Type 4 Medium **4F** - Type 4 Forward **4W** - Type 4 Wide **FTM** - Forward Throw Medium **5QN** - Type 5 Square Narrow **5QM** - Type 5 Square Medium **5Q** - Type 5 Square **5QW** - Type 5 Square Wide **AM**- Automotive Merchandise **WF** - 6x6 Wide Flood **LC** - Left Corner **RC** - Right Corner	**(Blank)** - Standard **L**- Optics rotated left 90° **R** - Optics rotated right 90°	**UNV** - Universal Voltage (120-277V) **HV** - High Voltage (347-480V)	**50K7**- 5000 CCT - 70 CRI **50K8**- 5000 CCT - 80 CRI **40K7** - 4000 CCT - 70 CRI **40K8**- 4000 CCT - 80 CRI **35K8**- 3500 CCT - 80 CRI **30K8** - 3000 CCT - 80 CRI **27K8** - 2700 CCT - 80 CRI **AMB** - Phosphor Converted Amber	**BLK -** Black **BRZ -** Dark Bronze **GMG -** Gun Metal Gray **GPT -** Graphite **MSV -** Metallic Silver **PLP -** Platinum Plus **SVG -** Satin Verde Green **WHT -** White

Mounting	Controls (Choose One)		Options
SA - Universal Side Arm Pole Mount **SF** - Adjustable Slip Fitter **UA**- Universal Adjustable Pole Mount Arm **MA** - Mast Arm	**(Blank)** - None **Wireless Controls System** **ALSC** - AirLink Synapse Control System **ALSCS2** - AirLink Synapse Control System with 12-20' MH Motion Sensor **ALSCS4 -** AirLink Synapse Control System with 20-40' MH Motion Sensor **ALBMR1LR** - AirLink Blue Long Range Wireless Multi-Range Sensor Controller (8-15' MH)[4] **ALBMR2LR** - AirLink Blue Long Range Wireless Multi-Range Sensor Controller (16-40' MH)[4]	**Stand-Alone Controls** **EXT** - 0-10v Dimming leads extended to housing exterior **CR7P** - 7 Pin Control Receptacle ANSI C136.41[3] **IMSBTL1** - Integral Bluetooth™ Motion and Photocell Sensor (8-24' MH)[4] **IMSBTL2** - Integral Bluetooth™ Motion and Photocell Sensor (25-40' MH)[4] **PCIU** - 120-277 Button Photocell **PCI347** - 347V Button Photocell	**IS -** Integral Shield[2] **TE** - Tooless Entry (Mast Arm Only)

Need more information?
Click here for our glossary

Have additional questions?
Call us at (800) 436-7800

Accessory Ordering Information[5]

CONTROLS ACCESSORIES	
Description	**Order Number**
Twist Lock Photocell (120V) for use with CR7P	**122514**
Twist Lock Photocell (208-277) for use with CR7P	**122515**
Twist Lock Photocell (347V) for use with CR7P	**122516**
Twist Lock Photocell (480V) for use with CR7P	**1225180**
AirLink 5 Pin Twist Lock Controller	**661409**
AirLink 7 Pin Twist Lock Controller	**661410**
Shorting Cap for use with CR7P	**149328**

FUSING OPTIONS[7]	
Description	**Order Number**
Single Fusing (120V)	See Fusing Accessory Guide
Single Fusing (277V)	
Double Fusing (208V, 240V)	
Double Fusing (480V)	
Double Fusing (347V)	

EXTERNAL SHIELDING OPTIONS	
Description	**Order Number**
1.5" External Shield	See Shielding Guide
3" External Shield	

1. Custom lumen and wattage packages available, consult factory. Values are within industry standard tolerances but not DLC listed.
2. Not available on Type 5 or wide flood distributions.
3. Control device or shorting cap must be ordered separately. See Accessory Ordering Information.
4. Motion sensors are field configurable via the LSI app that can be downloaded from your smartphone's native app store.
5. Accessories are shipped separately and field installed.
6. "CLR" denotes finish. See Finish options.
7. Fusing must be located in hand hole of pole. See Fusing Accessory Guide for compatability.

V-Locity Small (VALS) Outdoor LED Area Light

Type : _____

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ACCESSORIES

MOUNTING ACCESSORIES

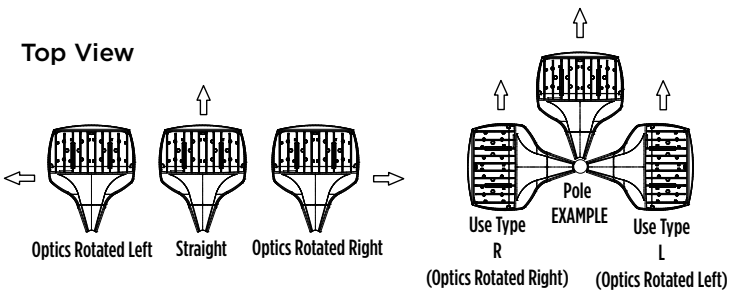
Side Arm	**Bullhorn Bracket** Mounts onto a 2" (51mm) IP, 2.375" (60mm) O.D. tenon and allows for mounting up to 4 adjustable slip fitter mount luminaires **Part Number: BKS PTB 2 CLR / BKS PTB 12 CLR (2 Luminaires)** **Part Number: BKS PTB 3-30 CLR / BKS PTB 13-30 CLR (3 Luminaires)** **Part Number: BKS PTB 4 CLR / BKS PTB 14 CLR (4 Luminaires)**	
	Flood Wall Mount Bracket Mounts onto vertical wall surface (hardware/anchors not included) **Part Number: BKS FMW 5 CLR (5" Straight Version)** **Part Number: BKS FMW 18 CLR (18" Curved Version)**	
	Bolt on Side Tenon Mount Bolt on Side Tenon Mount **Part Number: BKSFB0 5 CLR (5" Straight Version)** **Part Number: BKSFB0 18 CLR (18" Curved Version)** **Part Number: BKSFB0 30 CLR (30" Dual Tenon Version)**	
Tenon / Slipfitter	**Square Tenon Top** Allows to seamlessly integrate LSI luminaires to open top 4", 5" or 6" square poles with 11 ga. Or 7 ga. wall thickness **Part Number: BKA ISF10 _SQ CLR**	
	Square Tenon Top Mounts onto a 2" (51mm) IP, 2.375" (60mm) O.D. tenon and allows for mounting up to 4 luminaires **Part Number: BKA XNM ***	
	Square Internal Slipfitter Mounts inside 4" or 5" square pole and allows for mounting up to 4 lumianires **Part Number: BKA X_ISF * CLR**	
Wall Mount/ Wood Pole	**Wall Mount Bracket** Mounts onto vertical wall surface (hardware/anchors not included) **Part Number: BKS XB0 WM CLR**	
	Wood Pole Bracket Mounts onto wooden poles (6" minimum OD, hardware/anchors not inlcuded) **Part Number: BKS XB0 WP CLR**	

SHIELDING, POLES & MISC. ACCESSORIES

Shielding	**Integral Shield for Asymmetric Distributions** Field Install Integral shield provides maximum backlight control by shiedling each individual row of LEDS with minimal impact on Streett/front side distribution **Part Number: TBD**	
	Integral Shield for Left and Right Corner Distributions Field install integral shield provides cutoff from two sides for maximum spill light control when utilizing LC or RC corner distributions **Part Number: TBD**	
	External Shield External shield blocks view of light source from anyside of luminaire, additional shielding configurations available **Part Number: TBD (1.5") / TBD (3")**	
Poles	**Square Poles** 14 - 39' steel and aluminum poles in 4", 5" and 6" sizes for retrofit and new construction **Part Number: 4SQ/5SQ/6SQ**	
	Round Poles 10 - 30' steel and aluminum poles in 4" and 5" sizes for retrofit and new construction **Part Number: 4RP/5RP**	
	Tapered Poles 20' - 39' steel and aluminum poles for retrofit and new construction **Part Number: RTP**	
Misc.	**Bird Spikes** 10' linear bird spike (3' recommended per luminaire) silcone adhesive (covers approximately 25' linear fee of bird spike) and application tool **Spike Part Number: 751631** **Adhesive Part Number: 751632** **Caulk Gun Part Number: 751636**	

1 Replace CLR with paint finish description
2 Repleace XX with SQ for square pole or RD for round pole (≥3" OD)
3 Replace * with S (Single), D180 (Double @180°), D90 (Double @90°), T90 (Triple), Q90 (Quad)
4 Replace _ with 4 (4" square pole) or 5 (5" square pole)

OPTICS ROTATION

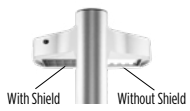


ACCESSORIES/OPTIONS

Integral Shield (IS)

Integral Shield (IS) available for improved backlight control without sacrificing Streett side performance. LSI's Integral Shield (IS) option delivers backlight control that significantly reduces spill light behind the pole for applications with pole locations close to adjacent properties. The design maximizes forward reflected light while reducing glare, maintaining the optical distribution selected, and most importantly eliminating light trespass. Shields rotate with the optical distribution.

Integral Shield (IS)



Luminaire Shown with AirLink Blue Sensor Option

7 Pin Photoelectric Control

7-pin ANSI C136.41-2013 control receptacle option available for twist lock photocontrols or wireless control modules. Control accessories sold separately. Dimming leads from the receptacle will be connected to the driver dimming leads (Consult factory for alternate wiring).

Luminaire Shown with CR7P

V-Locity Small (VALS) Outdoor LED Area Light

Type : _____

Have questions? Call us at (800) 436-7800

PHOTOMETRICS

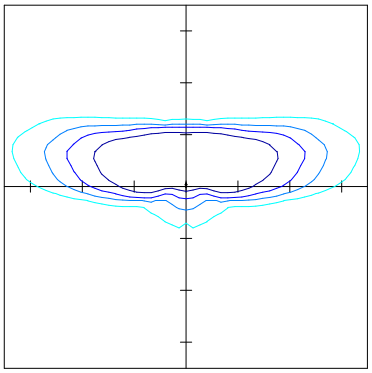
Back to Quick Links

Luminaire photometry has been conducted by a NVLAP accredited testing laboratory in accordance with IESNA LM-79-08. As specified by IESNA LM-79-08 the entire luminaire is tested as the source resulting in a luminaire efficiency of 100%.

See the individual product page on https://www.lsicorp.com/ for detailed photometric data.

VALS 18L 2 40K8

V-LOCITY Area Small, 18,000lm, Type 2 Distribution



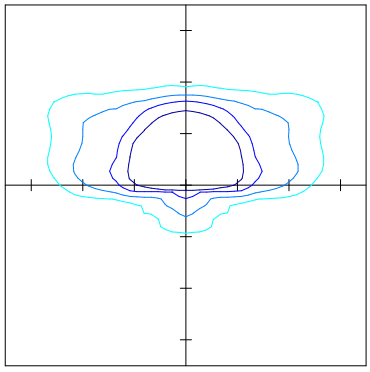
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	II Medium
BUG Rating	B2-U0-G4
Street Side Lumen %	88.9%
House Side Lumen %	11.1%
Uplight %	0.0%

VALS 18L 3M 40K8

V-LOCITY Area Small, 18,000lm, Type 3 Medium Distribution



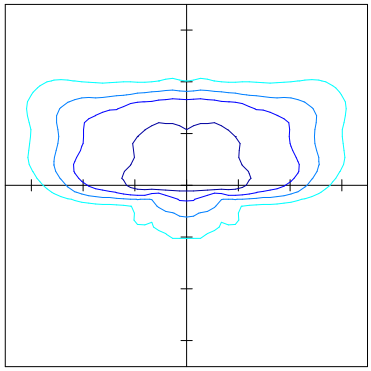
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	III Very Short
BUG Rating	B2-U0-G3
Street Side Lumen %	90.1%
House Side Lumen %	9.9%
Uplight %	0.0%

VALS 18L 3W 40K8

V-LOCITY Area Small, 18,000lm, Type 3 Wide Distribution



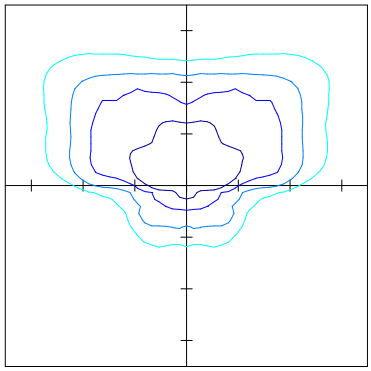
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	IV Medium
BUG Rating	B2-U0-G4
Street Side Lumen %	86.1%
House Side Lumen %	13.9%
Uplight %	0.0%

VALS 18L 4W 40K8

V-LOCITY Area Small, 18,000lm, Type 4 Wide Distribution



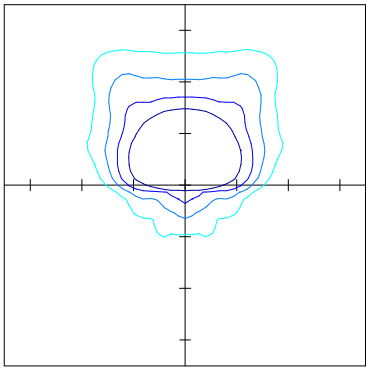
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	IV Medium
BUG Rating	B2-U0-G5
Street Side Lumen %	86.3%
House Side Lumen %	13.7%
Uplight %	0.0%

VALS 18L 4M 40K8

V-LOCITY Area Small, 18,000lm, Type 4 Medium Distribution



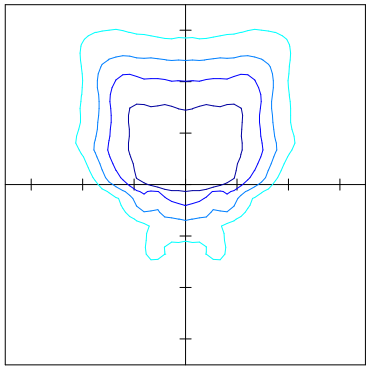
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	IV Very Short
BUG Rating	B1-U0-G3
Street Side Lumen %	91.0%
House Side Lumen %	9.0%
Uplight %	0.0%

VALS 18L 4F 40K8

V-LOCITY Area Small, 18,000lm, Type 4 Forward Distribution



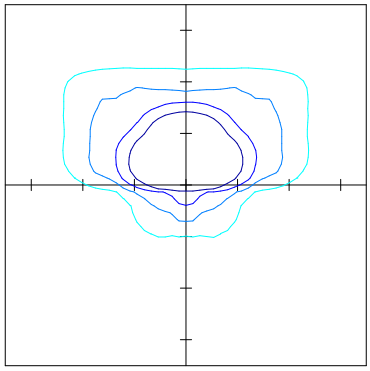
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	IV Short
BUG Rating	B2-U0-G4
Street Side Lumen %	87.9%
House Side Lumen %	12.1%
Uplight %	0.0%

VALS 18L FTM 40K8

V-LOCITY Area Small, 18,000lm, Forward Throw Medium Distribution



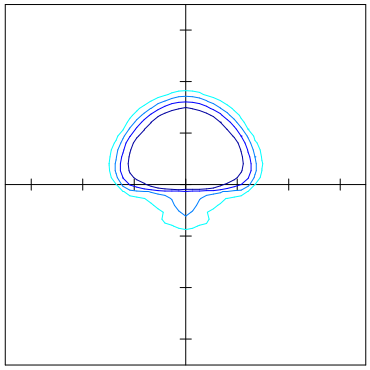
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	III Very Short
BUG Rating	B1-U0-G2
Street Side Lumen %	90.2%
House Side Lumen %	9.8%
Uplight %	0.0%

VALS 18L AM 40K8

V-LOCITY Area Small, 18,000lm, Automotive Merchandise Distribution



20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	III Very Short
BUG Rating	B1-U0-G2
Street Side Lumen %	94.1%
House Side Lumen %	5.9%
Uplight %	0.0%

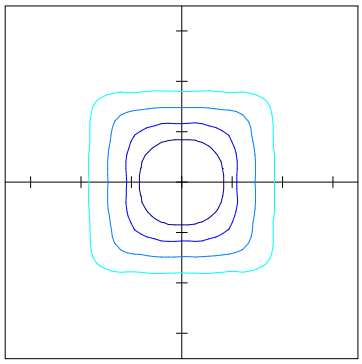
PHOTOMETRICS (CONTINUED)

Luminaire photometry has been conducted by a NVLAP accredited testing laboratory in accordance with IESNA LM-79-08. As specified by IESNA LM-79-08 the entire luminaire is tested as the source resulting in a luminaire efficiency of 100%.

See the individual product page on https://www.lsicorp.com/ for detailed photometric data.

VALS 18L 5QN 40K8

V-LOCITY Area Small, 18,000lm, Type 5 Square Narrow Distribution



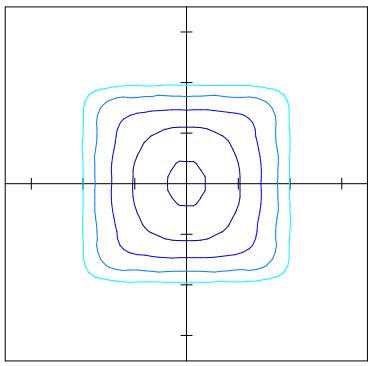
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	VS Very Short
BUG Rating	B3-U0-G2
0 - 60° Zonal Lumens	73.8%
60 - 90° Zonal Lumens	26.2%
Uplight %	0.0%

VALS 18L 5QM 40K8

V-LOCITY Area Small, 18,000lm, Type 5 Square Medium Distribution



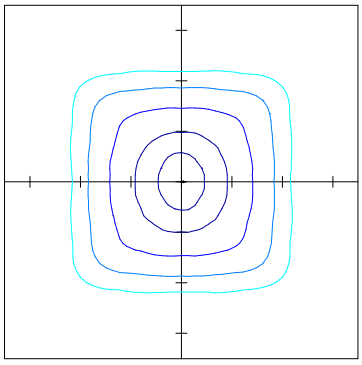
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	VS Short
BUG Rating	B4-U0-G2
0 - 60° Zonal Lumens	51.0%
60 - 90° Zonal Lumens	49.0%
Uplight %	0.0%

VALS 18L 5Q 40K8

V-LOCITY Area Small, 18,000lm, Type 5 Square Distribution



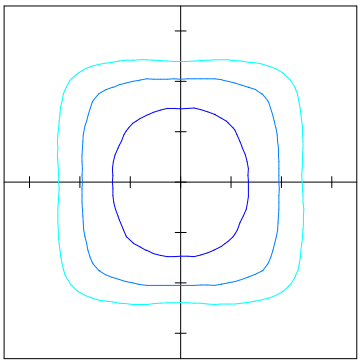
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	VS Medium
BUG Rating	B5-U0-G3
0 - 60° Zonal Lumens	41.8%
60 - 90° Zonal Lumens	58.2%
Uplight %	0.0%

VALS 18L 5QW 40K8

V-LOCITY Area Small, 18,000lm, Type 5 Square Wide Distribution



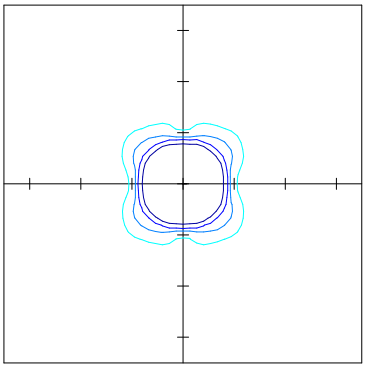
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	VS Medium
BUG Rating	B5-U0-G3
0 - 60° Zonal Lumens	33.7%
60 - 90° Zonal Lumens	66.3%
Uplight %	0.0%

VALS 18L WF 40K8

V-LOCITY Area Small, 18,000lm, Wide Flood Distribution



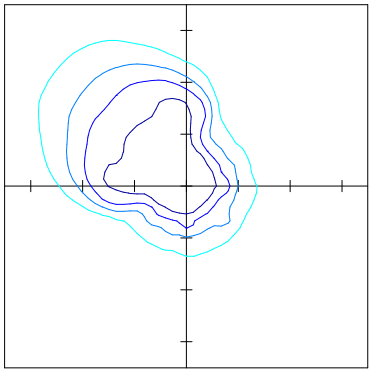
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

NEMA Type	6x6
Max Candela	9,352
Max Candela Angle	-19.5H x -29V
Beam Angle	80.7 x 90.0°
Field Angle	109.9 x 117.7°

VALS 18L LC 40K8

V-LOCITY Area Small, 18,000lm, Left Corner Distribution



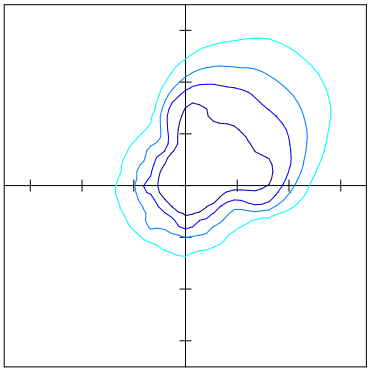
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	N/A
BUG Rating	B3-U0-G4
Street Side Lumen %	74.3%
House Side Lumen %	25.7%
Uplight %	0.0%

VALS 18L RC 40K8

V-LOCITY Area Small, 18,000lm, Right Corner Distribution



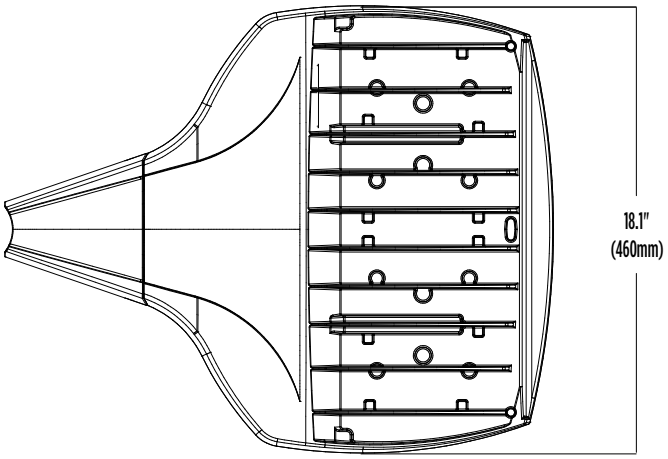
20' Mounting Height / 30' Grid Spacing

2 FC 1 FC 0.5 FC 0.2 FC

IES Type	N/A
BUG Rating	B3-U0-G4
Street Side Lumen %	74.3%
House Side Lumen %	25.7%
Uplight %	0.0%

PRODUCT DIMENSIONS

Back to Quick Links



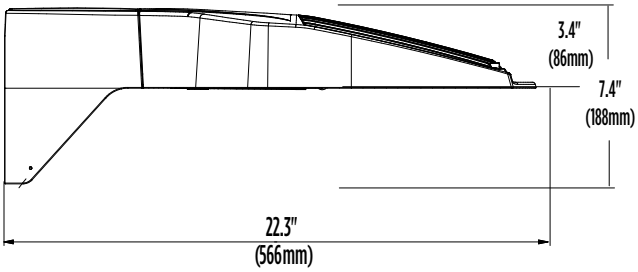
Luminaire EPA Chart - SA Side Arm		
Tilt Degree		0°
	Single	0.7
	D180°	1.3
	D90°	1.0
	T90°	1.4
	TN120°	1.5
	Q90°	1.4

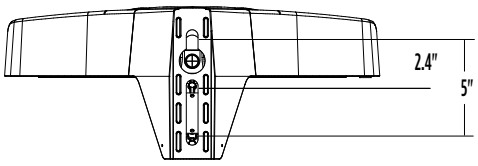
Luminaire EPA Chart - UA Universal Adjustable Arm						
Tilt Degree		0°	15°	30°	45°	60°
	Single	0.8	0.8	1.3	1.8	2.1
	D180°	1.5	1.9	2.5	3.2	3.8
	D90°	1.5	1.9	2.5	3.2	3.8
	T90°	1.8	2.1	2.7	3.2	3.8
	TN120°	1.8	2.3	3.4	4.3	5.0
	Q90°	1.8	2.1	2.7	3.2	3.8

Luminaire EPA Chart - MA Mast Arm		
Tilt Degree		0°
	Single	0.5

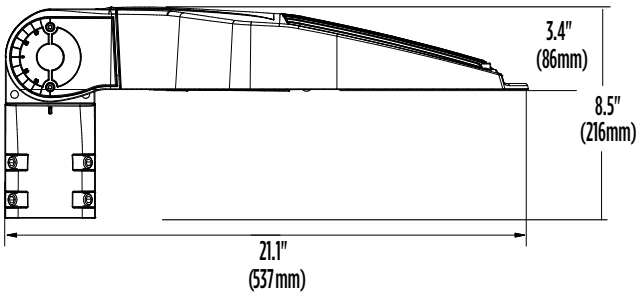
Luminaire EPA Chart - SF Adjustable Slip Fitter						
Tilt Degree		0°	15°	30°	45°	60°
	Single	0.7	1.3	1.4	1.8	2.2

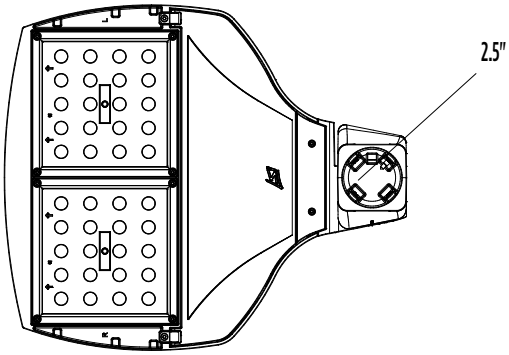
SA Universal Side Arm Mount



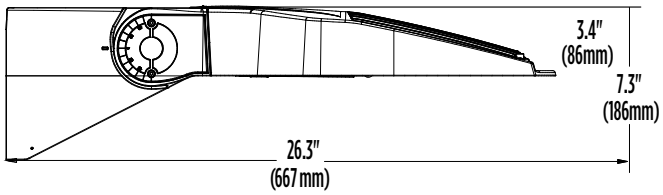


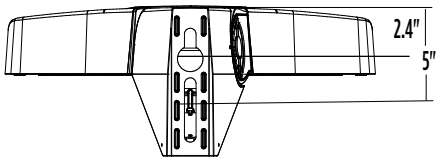
SF Adjustable Slipfitter



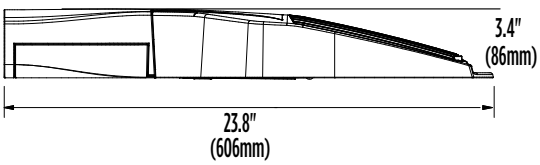


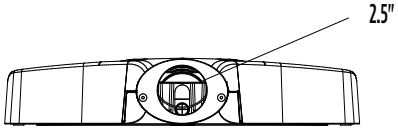
UA Universal Adjustable Arm





MA Mast Arm





CONTROLS

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Integral Bluetooth™ Motion and Photocell Sensor (IMSBTxL)

Slim low profile sensor provides multi-level control based on motion and/or daylight. Sensor controls 0-10 VDC LED drivers and is IP66 rated for cold and wet locations (-40°F to 167°F). Two unique PIR lenses are available and used based on fixture mounting height. All control parameters are adjustable via an iOS or Android App capable of storing and transmitting sensor profiles.

Click here to learn more details about IMSBT

LEVITON App | Apple | Android

AirLink Blue (ALBMRxLR, ALBCSx)

Wireless Bluetooth Mesh Outdoor Lighting Control System that provides energy savings, code compliance and enhanced safety/security for parking lots and parking garages. Three key components; Bluetooth wireless radio/sensor controller, Time Keeper and an iOS App. Capable of grouping multiple fixtures and sensors as well as scheduling time-based events by zone. Radio/Sensor Controller is factory integrated into Area/Site, Wall Mounted, Parking Garage and Canopy luminaires.

Click here to learn more details about AirLink Blue

AirLink Blue App | Apple

Sensor Sequence of Operations

Standard Programming	On Event	Off Event	On Light Level	Dim Light Level	Daylight Harvesting	Delay To Off	Sensitivity
OMSBTxL/IMSBTxL	Motion	No Motion	100%	N/A	On; Auto Calibration	20 minutes	High
OMS	Motion	No Motion	N/A	N/A	N/A	30 seconds	Auto

Operation	Description
On Event	Trigger that activates lights to turn on; either automatic via motion detected or manually activated via push of button.
Off Event	Trigger that activates lights to turn off; either automatic via no motion detected or manually activated via push of button.
On Light Level	The light level that the fixtures will turn on to when ON EVENT occurs.
Dim Light Level	The light level that the fixtures will dim down to when no motion is detected.
Delay to Dim	The amount of time after which no motion is detected that the fixtures will be triggered to dim down. This sequence is optional, and sensor can be programmed to only trigger the fixture to turn off by entering 100% in this field.
Delay to Off	The amount of time after which no motion is detected that the fixtures will be triggered to turn off. If delay to dim is part of the programmed functionality, this is the amount of time after which no motion is detected after the fixture have already dimmed down.
Sensitivity	The sensitivity can be set to high, medium, low, or auto where applicable. High will detect smaller, simple motions. Low will only detect larger more complex motions. Auto temperature calibration adjusts the PIR sensitivity as ambient temperature rises to increase detection of heat movement through the field of view.

Catalog # : _____ Project : _____ Type : _____

Prepared By : _____ Date : _____

Steel Poles

Square Straight

QUICK LINKS

Ordering Guide | Configurations | Dimensions | EPA

FEATURES & SPECIFICATIONS

Pole Shaft

- Straight poles are 4”, 5”, or 6” square.
- Pole shaft is electro-welded ASTM-A500 Grade C steel tubing with a minimum yield strength of 50,000 psi.
- On Tenon Mount steel poles, tenon is 2-3/8” O.D. high-strength pipe. Tenon is 4-3/4” in length.

Hand-Hole

- Standard hand-hole location is 12” above pole base.
- Poles 22’ and above have a 3x6 reinforced hand hole.

Base

- Pole base is ASTM-A36 hot-rolled steel plate with a minimum yield strength of 36,000 psi.
- Two-piece square base cover is optional.

Anchor Bolts

- Anchor bolts are not provided as standard on pole orders. Anchor bolts must be ordered as an accessory, anchor bolts feature zinc-plated double nuts and washers. Galvanized anchor bolts are optional.
- Anchor Bolts conform to ASTM F 1554-07a Grade 55 with a minimum yield strength of 55,000 PSI.

Ground Lug

- Ground lug is standard.

Duplex Receptacle

- Weatherproof duplex receptacle is optional.

Ground Fault Circuit Interrupter

- Self-testing Ground fault circuit interrupter is optional.

Finishes

- Every pole is provided with the DuraGrip Protection System and a 5-year limited warranty. NOTE: Warranty only applies to the finish.
- When the top-of-the line DuraGrip Plus Protection System is selected, in addition to the DuraGrip Protection System, a non-porous, automotive-grade corrosion coating is applied to the lower portion of the pole interior sealing and further protecting it from corrosion. This option extends the limited warranty to 7 years. NOTE: Warranty only applies to the finish.

Determining The Luminaire/Pole Combination For Your Application:

- Select luminaire from luminaire ordering information.
- Select bracket configuration if required
- Determine EPA value from luminaire/bracket EPA chart
- Select Pole Height
- Select MPH to match wind speed in the application area (See windspeed maps).
- Confirm pole EPA equal to or exceeding value of luminaire/bracket EPA
- Consult factory for special wind load requirements and banner brackets.

Pole Vibration Damper

- A pole vibration damper is recommended in open terrain areas of the country where low steady state winds are common.
- Non-tapered poles and lightly loaded poles are more susceptible to destructive vibration if a damper is not installed.

Warranty

- LSI poles come standard with a 1 year materials and workmanship warranty. Refer to https://www.lsicorp.com/resources/terms-conditions-warranty/ for more information.

Listings

- UL Listed
- BAA/TAA Compliant
- BABAA Compliant *If Specified

Steel Poles Square Straight

Type : _____

Have questions? Call us at (800) 436-7800

ORDERING GUIDE

Back to Quick Links

TYPICAL ORDER EXAMPLE: 4SQ B3 S11G 24 S PLP DGP

Pole Series	Mounting Method	Material	Height [2]	Mounting Configuration	Pole Finish	Options
4SQ - 4" x 4" Square Straight Pole (New Build) **5SQ** - 5" x 5" Square Straight Pole (New Build) **6SQ** - 6" x 6" Square Straight Pole (New Build) **5SQU** - 5" x 5" Square Straight Pole (Retrofit) **6SQU** - 6" x 6" Square Straight Pole (Retrofit)	**Bolt-On Mount**[1] - See pole selection guide for patterns and fixture matches **B5** - 5" Traditional Drilling Pattern **B3** - 3" Reduced Pattern **B2** - 2" Low Profile Drilling Pattern **B2R** - Raised 2" Low Profile Drilling Pattern **(LAL4 ONLY)** **T** - Tenon Mount - See pole selection guide for tenon and fixture/bracket matches **I** - No Mounting Holes[1]	**S11G** – 11 Ga. Steel (4SQ/4SQU and 5SQ/5SQU Only) **S07G** – 07 Ga. Steel	**8'** **10'** **12'** **13'** **14'** **15'** **16'** **17'** **17'6"** **18'** **20'** **22'** **22'6"** **23'** **24'** **25'** **26'** **27'** **28'** **30'** **32'** **35'** **39'**	**S** – Single/Parallel **D180** – Double **D90** – Double **DN90** – Double **T90** – Triple **TN120** – Triple **Q90** – Quad **QN90** – Quad **N** – Tenon Mount (Standard Tenon size is 2-3/8" O.D.)[8] **(Blank)** - Use with I for Mounting Method	**BRZ** – Bronze **BLK** – Black **PLP** – Platinum Plus **WHT** – White **SVG** – Satin Verde Green **GPT** – Graphite **MSV** – Metallic Silver **BZA** – Alternate Bronze **GMG** - Gun Metal Gray	**GA** – Galvanized Anchor Bolts **SF** – Single Flood[3] **DF** – Double Flood [3] **DGP** – DuraGrip® Plus **LAB** – Less Anchor Bolts **CRXX** - Conduit Raceway[4] **USA** - BABAA Compliant[5]

Need more information?
Click here for our glossary

Have additional questions?
Call us at (800) 436-7800

ACCESSORY ORDERING INFORMATION

Part Number	Description
122559CLR	4BC – 4" Square Base Cover
122561CLR	5BC – 5" Square Base Cover
122563CLR	6BC – 6" Square Base Cover
132488CLR	5BC - 5' Square Universal Base Cover
131252CLR	6BC - 6' Square Universal Base Cover
122566CLR	ER2 – Weatherproof Duplex Receptacle
122567CLR	GFI – Ground Fault Circuit Interrupter
132336	MH5 - mounting Hole Plugs for use with 5" traditional drill pattern (3 set of 3 plugs)
681126	MH3 - mounting Hole Plugs for use with 3" reduced drill pattern (3 set of 3 plugs)
725841	MH2 - Mounting Hole Plugs for use with 2" reduced drill pattern (3 sets of 3 plugs)
172539	Vibration Damper - 4" Square Pole (bolt-on mount only)
172538	Vibration Damper - 5" Square Pole (bolt-on mount only)
178361	Vibration Damper - 6" Square Pole (bolt-on mount only)

ANCHOR BOLT KIT ORDERING INFORMATION

Part Number	Description
AB KIT 122584	4SQ S11G/S07G Anchor Bolts (Steel Plated)
AB KIT 122609	4SQ S11G/S07G Anchor Bolts (Galvanized)
AB KIT 1225850	5SQ S11G Anchor Bolts (Steel Plated)
AB KIT 122610	5SQ S11G Anchor Bolts (Galvanized)
AB KIT 122586	5SQ S07G Anchor Bolts (Steel Plated)
AB KIT 122611	5SQ S07G Anchor Bolts (Galvanized)
AB KIT 122612	6SQ S07G Anchor Bolts (Galvanized)

1 - See Area Light Brackets - 3" Reduced Drill Pattern and Area Light Brackets - 5" Traditional Drill Pattern Spec Sheets.
2 - Pole heights will have +/- 1/2" tolerance.
3 - See Flood Lighting Brackets section for choice of FBO brackets.
4 - CR selection must indicate required height and side of pole mounting location. Mounting template required at time of order.
5 - USA only applies to 4SQ poles up to 26' and 5SQ/5SQU poles up to 30'.

Steel Poles Square Straight

Type : _____

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DRILLING LOCATIONS

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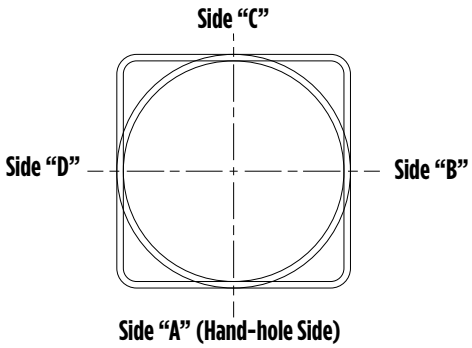
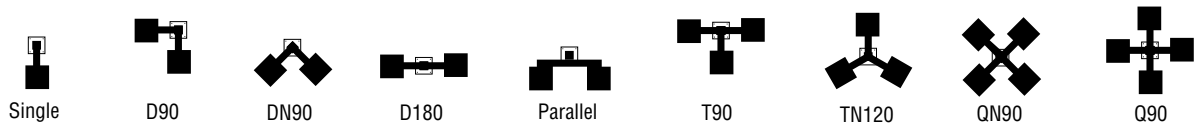
Sides	A	B	C	D
Hand-hole	X			
Single	X			
D180		X		X
D90	X			X
DN90[1]				
T90	X	X		X
TN120[2]				
Q90	X	X	X	X
QN90[3]				
Single FBO	X			
Double FBO		X		X



1 - Two locations will be 45° to the left and right of Side A.
2 - Other two locations will be 120° to the left and right of Side A.
3 - Two locations will be 45° to the left and right of Side A and two locations will be 135° to the left and right of Side A.

Consult factory for custom variations. Standard SF and DF pole preparations are located 3/4 of the height of the pole from the base, except on 20' poles. Maximum height for SF and DF pole preparations on 20' poles is 13' from the base.

FIXTURE CONFIGURATIONS



BOLT CIRCLE

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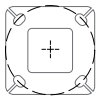
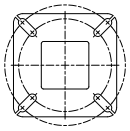
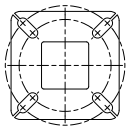
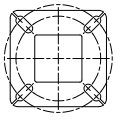
STANDARD BASEPLATE

	4" (102mm) square 10-1/8" (257mm) sq.	5" (127mm) square 10-1/8" (257mm) sq.	5" (127mm) square 10-1/8" (257mm) sq.	6" (152mm) square 12" (305mm) sq.
				
	11" (279mm) Dia. Bolt Circle	11" (279mm) Dia. Bolt Circle	11" (279mm) Dia. Bolt Circle	12" (305mm) Dia. Bolt Circle
Bolt Circle Designator	**B**	**C**	**D**	**J**
Bolt Circle	Slotted 8"-11" (203mm-279mm)	Slotted 9"-11" (229mm-279mm)	Slotted 9"-11" (229mm-279mm)	Slotted 12" (305mm)
Anchor Bolt Size	3/4" x 24" (19mm x 609mm)	3/4" x 24" (19mm x 609mm)	1"x30" (25mm x 762mm)	1"x30" (25mm x 762mm)
Anchor Bolt Projection	3-1/4" (83mm)	3-1/4" (83mm)	4" (102mm)	4" (102mm)
Base Plate Opening for Wireway Entry	3-5/8" (92mm)	4-3/4" (121mm)	4-5/8" (117mm)	5-5/8" (143mm)
Base Plate Dimensions	10-1/8" sq. x 3/4" thk. (257mm x 19mm)	10-1/8" sq. x 3/4" thk. (257mm x 19mm)	10-1/8" sq. x 1" thk. (257mm x 25mm)	12" sq. x 1-1/8" thk. (305mm x 29mm)
Pole Gauge	11	11	7	7

Note: Base plate illustrations may change without notice. Do not use for setting anchor bolts. Consult factory for the appropriate anchor bolt template.

UNIVERSAL BASEPLATE

	5" (127mm) square 11.125" (283mm) sq.	5" (127mm) square 11.75" (298mm) sq.	6" (152mm) square 12-1/2" (318mm) sq.
			
	5SQ	5SQ	14" (356mm) Dia. Bolt Circle
Bolt Circle Designator	**F**	**G**	**H**
Bolt Circle	Slotted 10-13"	Slotted 10-13"	Slotted 11"-14" (279mm-356mm)
Anchor Bolt Size	3/4x 24" (19mm x 609 mm)	1"x30" (25mm x 762mm)	1"x30" (25mm x 762mm)
Anchor Bolt Projection	3-1/4" (83 mm)	4" (102 mm)	4" (102mm)
Base Plate Opening for Wireway Entry	4-3/4" (121mm)	5-1/8" (130 mm)	5-5/8" (143mm)
Base Plate Dimensions	11-1/8 sq. x 3/4" thk. (283 mm x 19 mm)	11-3/4" sq. x 1" thk. (298 mm x 25 mm)	12 1/2" sq. x 1 1/8" thk. (318mm x 29mm)
Pole Gauge	11	7	7

Note: Base plate illustrations may change without notice. Do not use for setting anchor bolts. Consult factory for the appropriate anchor bolt template.

1 - Full Galvanized option is 1" x 30" straight headed anchor bolt. Contact factory for questions

Steel Poles Square Straight

Type : _____

Have questions? Call us at (800) 436-7800

PRODUCT DIMENSIONS

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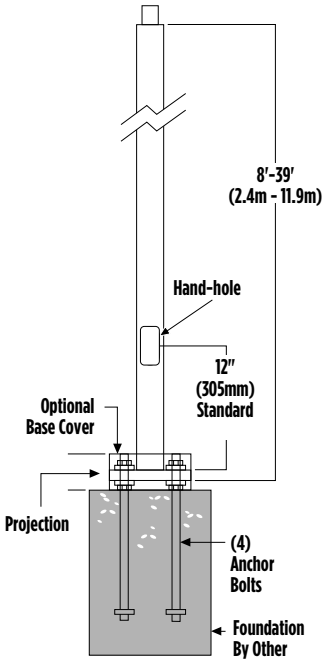
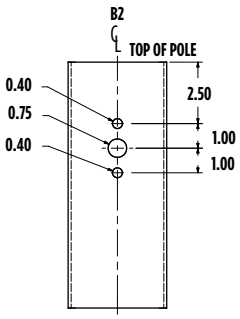
SQT –
N= 2-3/8" (60mm) O.D. x 4-3/4" (121mm) Tenon

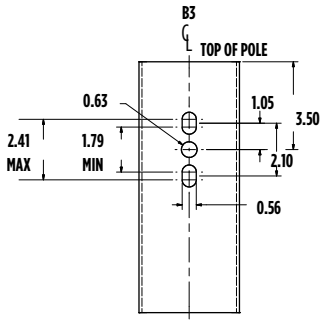


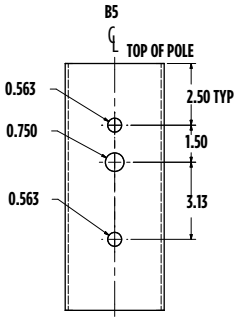
SF –
Single Flood
Pole Preparation

SHIPPING WEIGHTS	
4"(102mm) sq. 11 Ga. is approximately	7.50 lbs./ft.
4"(102mm) sq. 07 Ga. is approximately	10.00 lbs./ft.
5"(127mm) sq. 11 Ga. is approximately	9.00 lbs./ft.
5"(127mm) sq. 07 Ga. is approximately	12.50 lbs./ft.
6"(152mm) sq. 07 Ga. is approximately	15.40 lbs./ft.
Anchor Bolts (3/4" x 24")(19mm x 609mm)	15 lbs. (7kg)/set
Anchor Bolts (1" x 30") (25mm x 762mm)	30 lbs. (14kg)/set

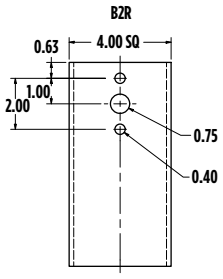
Bolt-On Mount 2-Bolt Pattern

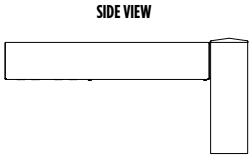






ONLY FOR USE WITH LAL4 PRODUCT
B2R- Raised 2" low profile drilling pattern





Steel Poles Square Straight

Type : _____

Have questions? Call us at (800) 436-7800

WIND SPEED

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EPA Information

All LSI Industries' poles are guaranteed to meet the EPA requirements listed. LSI Industries is not responsible if a pole order has a lower EPA rating than the indicated wind-loading zone where the pole will be located.

CAUTION: This guarantee does not apply if the pole/bracket/fixture combination is used to support any other items such as flags, pennants, or signs, which would add stress to the pole. LSI Industries cannot accept responsibility for harm or damage caused in these situations.

NOTE: Pole calculations include a 1.3 gust factor over steady wind velocity. Example: poles designed to withstand 80 MPH steady wind will withstand gusts to 104 MPH. EPAs are for locations 100 miles away from hurricane ocean lines. Consult LSI for other areas. Note: Hurricane ocean lines are the Atlantic and Gulf of Mexico coastal areas. For applications in Florida or Canada, consult factory.

POLE[1]	Mtg. Height Length (ft)	Wall Thick (ga)	BOLT CIRCLE			EPA								
			Designator	Dia. (in)	Anchor bolt Dia {in}	110 MPH	115 MPH	120 MPH	130 MPH	140 MPH	150 MPH	160 MPH	170 MPH	180 MPH
4" x 11-ga x 12'	12	11	B	8" - 11"	0.75	13.9	12.5	11.3	9.2	7.6	6.3	5.2	4.3	3.6
4" x 11-ga x 14'	14	11	B	8" - 11"	0.75	10.7	9.5	8.5	6.8	5.4	4.4	3.5	2.7	2.1
4" x 11-ga x 16'	16	11	B	8" - 11"	0.75	8.2	7.2	6.4	4.9	3.8	2.9	2.1	1.5	1.0
4" x 11-ga x 18'	18	11	B	8" - 11"	0.75	6.3	5.4	4.7	3.4	2.4	1.6	1.0	0.4	n/a
4" x 11-ga x 20'	20	11	B	8" - 11"	0.75	4.6	3.9	3.2	2.1	1.2	0.6	n/a	n/a	n/a
4" x 11-ga x 22'	22	11	B	8" - 11"	0.75	7.6	6.6	5.7	4.2	3.0	2.0	1.2	0.5	n/a
4" x 11-ga x 24'	24	11	B	8" - 11"	0.75	6.0	5.1	4.3	2.9	1.8	0.9	n/a	n/a	n/a
4" x 11-ga x 26'	26	11	B	8" - 11"	0.75	4.6	3.7	3.0	1.7	0.7	n/a	n/a	n/a	n/a
4" x 7-ga x 14'	14	7	B	8" - 11"	0.75	18.3	16.4	14.9	12.2	10.2	8.5	7.1	5.9	5.0
4" x 7-ga x 16'	16	7	B	8" - 11"	0.75	14.7	13.2	11.8	9.6	7.8	6.3	5.2	4.2	3.4
4" x 7-ga x 18'	18	7	B	8" - 11"	0.75	11.9	10.5	9.3	7.4	5.9	4.6	3.6	2.8	2.1
4" x 7-ga x 20'	20	7	B	8" - 11"	0.75	9.6	8.4	7.4	5.7	4.3	3.2	2.3	1.6	0.9
4" x 7-ga x 22'	22	7	B	8" - 11"	0.75	7.7	6.6	5.7	4.2	3.0	2.0	1.2	0.5	n/a
4" x 7-ga x 24'	24	7	B	8" - 11"	0.75	6.0	5.1	4.3	2.9	1.8	0.9	n/a	n/a	n/a
4" x 7-ga x 26'	26	7	B	8" - 11"	0.75	4.6	3.7	3.0	1.7	0.7	n/a	n/a	n/a	n/a
4" x 7-ga x 28"[2]	28	7	B	8" - 11"	0.75	3.3	2.5	1.8	0.7	n/a	n/a	n/a	n/a	n/a
4" x 7-ga x 30"[2]	30	7	B	8" - 11"	0.75	2.2	1.4	0.8	n/a	n/a	n/a	n/a	n/a	n/a
5" x 11-ga x 14'	14	11	C	9" - 11"	0.75	17.4	15.7	14.1	11.5	9.3	7.7	6.3	5.2	4.2
5" x 11-ga x 16'	16	11	C	9" - 11"	0.75	13.8	12.3	10.9	8.7	6.9	5.5	4.3	3.3	2.5
5" x 11-ga x 18'	18	11	C	9" - 11"	0.75	10.8	9.6	8.4	6.5	4.9	3.7	2.6	1.8	1.1
5" x 11-ga x 20'	20	11	C	9" - 11"	0.75	8.5	7.3	6.3	4.6	3.2	2.1	1.2	0.5	n/a
5" x 11-ga x 22'	22	11	C	9" - 11"	0.75	10.9	9.5	8.3	6.2	4.5	3.2	2.1	1.2	0.5
5" x 11-ga x 24'	24	11	C	9" - 11"	0.75	8.8	7.5	6.4	4.5	3.0	1.8	0.8	n/a	n/a
5" x 11-ga x 26'	26	11	C	9" - 11"	0.75	6.8	5.7	4.6	3.0	1.6	0.6	n/a	n/a	n/a
5" x 11-ga x 28'	28	11	C	9" - 11"	0.75	5.2	4.1	3.2	1.6	0.4	n/a	n/a	n/a	n/a
5" x 11-ga x 30'	30	11	C	9" - 11"	0.75	3.6	2.7	1.8	0.4	n/a	n/a	n/a	n/a	n/a
5" x 7-ga x 20'	20	7	D	9" - 11"	1.00	21.6	19.3	17.3	14.0	11.3	9.2	7.4	6.0	4.8
5" x 7-ga x 22'	22	7	D	9" - 11"	1.00	20.7	18.6	16.6	13.3	10.7	8.5	6.8	5.4	4.2
5" x 7-ga x 24'	24	7	D	9" - 11"	1.00	17.7	15.6	13.8	10.8	8.5	6.6	5.0	3.7	2.6
5" x 7-ga x 26'	26	7	D	9" - 11"	1.00	14.9	13.1	11.4	8.8	6.6	4.9	3.5	2.3	1.3
5" x 7-ga x 28'	28	7	D	9" - 11"	1.00	12.5	10.9	9.4	6.9	4.9	3.4	2.1	1.0	n/a
5" x 7-ga x 30'	30	7	D	9" - 11"	1.00	10.3	8.9	7.5	5.2	3.4	2.0	0.8	n/a	n/a
5" x 7-ga x 35'	35	7	D	9" - 11"	1.00	6.0	4.8	3.6	1.8	n/a	n/a	n/a	n/a	n/a
6" x 7-ga x 24'	24	7	J	12"	1.00	18.6	16.4	14.3	11.2	8.6	6.5	4.8	3.4	2.2
6" x 7-ga x 26'	26	7	J	12"	1.00	15.6	13.4	11.7	8.8	6.5	4.6	3.0	1.8	0.7
6" x 7-ga x 28'	28	7	J	12"	1.00	12.9	10.9	9.3	6.7	4.6	2.8	1.5	n/a	n/a
6" x 7-ga x 30'	30	7	J	12"	1.00	10.4	8.8	7.3	4.8	2.9	1.3	n/a	n/a	n/a
6" x 7-ga x 32'	32	7	J	12"	1.00	8.3	6.8	5.5	3.1	1.3	n/a	n/a	n/a	n/a
6" x 7-ga x 34'	34	7	J	12"	1.00	6.5	5.0	3.7	1.6	n/a	n/a	n/a	n/a	n/a
6" x 7-ga x 35'	35	7	J	12"	1.00	5.5	4.2	2.9	0.9	n/a	n/a	n/a	n/a	n/a
6" x 7-ga x 39'	39	7	J	12"	1.00	2.3	1.0	n/a	n/a	n/a	n/a	n/a	n/a	n/a

Steel Poles Square Straight

Type : _____

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WIND SPEED

POLE[1]	Mtg. Height Length (ft)	Wall Thick (ga)	BOLT CIRCLE			EPA								
			Designator	Dia. (in)	Anchor Bolt Dia (in)	110 MPH	115 MPH	120 MPH	130 MPH	140 MPH	150 MPH	160 MPH	170 MPH	180 MPH
5" x 11-ga x 14'	14	11	F	11"	0.75	17.6	15.8	14.2	11.5	9.4	7.7	6.3	5.2	4.3
5" x 11-ga x 14'	14	11	F	13"	0.75	17.6	15.8	14.2	11.5	9.4	7.7	6.3	5.2	4.3
5" x 11-ga x 16'	16	11	F	11"	0.75	13.9	12.2	11.0	8.8	7.0	5.5	4.3	3.4	2.5
5" x 11-ga x 16'	16	11	F	13"	0.75	13.9	12.2	11.0	8.8	7.0	5.5	4.3	3.4	2.5
5" x 11-ga x 18'	18	11	F	11"	0.75	11.0	9.6	8.4	6.5	5.0	3.7	2.7	1.8	1.1
5" x 11-ga x 18'	18	11	F	13"	0.75	11.0	9.6	8.4	6.5	5.0	3.7	2.7	1.8	1.1
5" x 11-ga x 20'	20	11	F	11"	0.75	8.6	7.4	6.4	4.6	3.3	2.2	1.3	0.5	-
5" x 11-ga x 20'	20	11	F	13"	0.75	8.6	7.4	6.4	4.6	3.3	2.2	1.3	0.5	-
5" x 11-ga x 22'	22	11	F	11"	0.75	12.7	11.1	9.6	7.4	5.6	4.1	3.0	2.0	1.1
5" x 11-ga x 22'	22	11	F	12"	0.75	10.3	8.9	7.7	5.7	4.1	2.8	1.8	0.9	-
5" x 11-ga x 22'	22	11	F	13"	0.75	8.6	7.4	6.4	4.6	3.1	2.0	1.1	-	-
5" x 11-ga x 24'	24	11	F	11"	0.75	10.2	8.9	7.6	5.6	4.0	2.6	1.6	0.7	-
5" x 11-ga x 24'	24	11	F	12"	0.75	8.0	6.9	5.8	4.0	2.6	1.5	0.5	-	-
5" x 11-ga x 24'	24	11	F	13"	0.75	6.7	5.5	4.6	3.0	1.7	0.7	-	-	-
5" x 11-ga x 26'	26	11	F	11"	0.75	8.1	6.9	5.8	4.0	2.5	1.3	-	-	-
5" x 11-ga x 26'	26	11	F	12"	0.75	6.2	5.1	4.1	2.6	1.3	-	-	-	-
5" x 11-ga x 26'	26	11	F	13"	0.75	5.0	4.0	3.1	1.6	0.5	-	-	-	-
5" x 11-ga x 28'	28	11	F	11"	0.75	6.3	5.2	4.3	2.5	1.1	-	-	-	-
5" x 11-ga x 28'	28	11	F	12"	0.75	4.6	3.6	2.7	1.2	-	-	-	-	-
5" x 11-ga x 28'	28	11	F	13"	0.75	3.4	2.5	1.7	-	-	-	-	-	-
5" x 11-ga x 30'	30	11	F	11"	0.75	4.7	3.7	2.8	1.2	-	-	-	-	-
5" x 11-ga x 30'	30	11	F	12"	0.75	3.1	2.2	1.4	-	-	-	-	-	-
5" x 11-ga x 30'	30	11	F	13"	0.75	2.0	1.2	0.5	-	-	-	-	-	-
5" x 7-ga x 20'	20	7	G	11"	0.75	19.0	17.0	15.0	12.2	9.7	7.8	6.2	5.0	3.8
5" x 7-ga x 20'	20	7	G	12"	0.75	21.4	19.1	17.1	13.8	11.2	9.1	7.3	5.9	4.7
5" x 7-ga x 20'	20	7	G	13"	0.75	21.4	19.2	17.2	13.9	11.3	9.2	7.4	6.0	4.8
5" x 7-ga x 20'	20	7	G	11"	1	21.7	19.4	17.4	14.0	11.4	9.3	7.5	6.0	4.8
5" x 7-ga x 20'	20	7	G	13"	1	21.7	19.4	17.4	14.0	11.4	9.3	7.5	6.0	4.8
5" x 7-ga x 22'	22	7	G	11"	0.75	16.0	14.1	12.5	9.8	7.6	5.9	4.4	3.3	2.3
5" x 7-ga x 22'	22	7	G	12"	0.75	17.7	15.9	14.2	11.2	8.7	7.0	5.4	4.1	3.0
5" x 7-ga x 22'	22	7	G	13"	0.75	19.9	17.3	15.6	12.6	10.0	8.0	6.3	5.0	3.8
5" x 7-ga x 22'	22	7	G	11"	1	21.0	18.7	16.7	13.4	10.6	8.5	6.8	5.4	4.2
5" x 7-ga x 22'	22	7	G	12"	1	23.4	20.6	18.4	15.0	12.2	9.9	8.0	6.4	5.1
5" x 7-ga x 22'	22	7	G	13"	1	21.3	18.8	17.0	13.7	11.0	8.8	7.0	5.6	4.3
5" x 7-ga x 24'	24	7	G	11"	0.75	13.3	11.6	10.0	7.7	5.7	4.2	2.9	1.9	1.0
5" x 7-ga x 24'	24	7	G	12"	0.75	15.0	13.0	11.6	8.9	6.8	5.1	3.8	2.6	1.7
5" x 7-ga x 24'	24	7	G	13"	0.75	16.6	14.6	12.9	10.2	8.0	6.1	4.6	3.3	2.3
5" x 7-ga x 24'	24	7	G	11"	1	17.5	15.7	13.9	10.9	8.6	6.7	5.0	3.7	2.7
5" x 7-ga x 24'	24	7	G	12"	1	20.0	17.4	15.4	12.3	9.9	7.8	6.0	4.7	3.5
5" x 7-ga x 24'	24	7	G	13"	1	18.1	16.0	14.2	11.0	8.7	6.7	5.3	3.9	2.8
5" x 7-ga x 26'	26	7	G	11"	0.75	10.9	9.3	8.0	5.9	4.1	2.7	1.6	0.6	-
5" x 7-ga x 26'	26	7	G	12"	0.75	12.4	10.9	9.5	7.0	5.1	3.6	2.3	1.3	-
5" x 7-ga x 26'	26	7	G	13"	0.75	14.0	12.3	10.7	8.1	6.0	4.4	3.1	2.0	1.0
5" x 7-ga x 26'	26	7	G	11"	1	15.0	13.2	11.5	8.8	6.7	4.9	3.5	2.3	1.3

Steel Poles Square Straight

Type : _____

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WIND SPEED

POLE[1]	Mtg. Height Length (ft)	Wall Thick (ga)	BOLT CIRCLE			EPA								
			Designator	Dia. (in)	Anchor Bolt Dia (in)	110 MPH	115 MPH	120 MPH	130 MPH	140 MPH	150 MPH	160 MPH	170 MPH	180 MPH
5" x 7-ga x 26'	26	7	G	12"	1	17.0	14.8	13.0	10.2	7.9	6.0	4.4	3.1	2.1
5" x 7-ga x 26'	26	7	G	13"	1	15.3	13.5	11.8	9.0	6.8	5.0	3.6	2.5	1.4
5" x 7-ga x 28'	28	7	G	11"	0.75	8.9	7.4	6.3	4.3	2.7	1.4	-	-	-
5" x 7-ga x 28'	28	7	G	12"	0.75	10.2	8.8	7.5	5.3	3.5	2.1	1.0	-	-
5" x 7-ga x 28'	28	7	G	13"	0.75	11.8	10.2	8.8	6.4	4.5	3.0	1.7	0.7	-
5" x 7-ga x 28'	28	7	G	11"	1	12.5	10.9	9.5	7.0	5.0	3.3	2.1	1.0	-
5" x 7-ga x 28'	28	7	G	12"	1	14.2	12.4	11.0	8.2	6.0	4.3	3.0	1.7	0.8
5" x 7-ga x 28'	28	7	G	13"	1	12.9	11.0	9.7	7.2	5.2	3.6	2.2	1.1	-
5" x 7-ga x 30'	30	7	G	11"	0.75	7.0	5.8	4.7	2.8	1.3	-	-	-	-
5" x 7-ga x 30'	30	7	G	12"	0.75	8.4	7.0	5.8	3.8	2.2	0.9	-	-	-
5" x 7-ga x 30'	30	7	G	13"	0.75	9.7	8.2	7.0	4.8	3.0	1.6	0.5	-	-
5" x 7-ga x 30'	30	7	G	11"	1	10.4	8.8	7.6	5.3	3.4	2.0	0.8	-	-
5" x 7-ga x 30'	30	7	G	12"	1	12.0	10.3	9.0	6.4	4.4	2.9	1.6	0.5	-
5" x 7-ga x 30'	30	7	G	13"	1	10.6	9.1	7.7	5.5	3.6	2.1	1.0	-	-
5" x 7-ga x 35'	35	7	G	11"	0.75	3.2	2.2	1.2	-	-	-	-	-	-
5" x 7-ga x 35'	35	7	G	12"	0.75	4.4	3.2	2.2	0.5	-	-	-	-	-
5" x 7-ga x 35'	35	7	G	13"	0.75	5.5	4.2	3.1	1.3	-	-	-	-	-
5" x 7-ga x 35'	35	7	G	11"	1	6.0	4.8	3.6	1.8	-	-	-	-	-
5" x 7-ga x 35'	35	7	G	12"	1	7.3	6.0	4.8	2.7	1.1	-	-	-	-
5" x 7-ga x 35'	35	7	G	13"	1	6.3	5.0	3.8	1.9	-	-	-	-	-
6" x 7-ga x 24'	24	7	H	11"	1	16.5	14.4	12.6	9.6	7.2	5.3	3.8	2.5	1.4
6" x 7-ga x 24'	24	7	H	12-1/2"	1	19.8	17.5	15.4	12.0	9.2	7.0	5.3	3.8	2.7
6" x 7-ga x 24'	24	7	H	14"	1	23.0	20.5	18.0	14.3	11.2	8.9	6.9	5.3	3.8
6" x 7-ga x 26'	26	7	H	11"	1	13.7	11.8	10.2	7.5	5.3	3.6	2.1	1.0	-
6" x 7-ga x 26'	26	7	H	12-1/2"	1	16.5	14.6	12.6	9.6	7.0	5.2	3.6	2.2	1.1
6" x 7-ga x 26'	26	7	H	14"	1	19.6	17.3	15.2	11.7	8.9	6.7	5.0	3.5	2.2
6" x 7-ga x 28'	28	7	H	11"	1	11.0	9.3	7.8	5.5	3.5	1.9	0.6	-	-
6" x 7-ga x 28'	28	7	H	12-1/2"	1	13.8	12.0	10.2	7.5	5.2	3.4	1.9	0.7	-
6" x 7-ga x 28'	28	7	H	14"	1	16.4	14.5	12.5	9.4	6.9	4.7	3.2	1.8	0.7
6" x 7-ga x 30'	30	7	H	11"	1	9.0	7.3	6.0	3.6	1.9	0.5	-	-	-
6" x 7-ga x 30'	30	7	H	12-1/2"	1	11.4	9.6	8.0	5.5	3.4	1.7	-	-	-
6" x 7-ga x 30'	30	7	H	14"	1	14.0	12.0	10.0	7.2	5.0	3.2	1.6	-	-
6" x 7-ga x 32'	32	7	H	11"	1	7.0	5.5	4.2	2.0	-	-	-	-	-
6" x 7-ga x 32'	32	7	H	12-1/2"	1	9.2	7.6	6.0	3.8	1.8	-	-	-	-
6" x 7-ga x 32'	32	7	H	14"	1	11.4	9.7	8.0	5.4	3.2	1.6	-	-	-
6" x 7-ga x 34'	34	7	H	11"	1	5.1	3.7	2.5	0.6	-	-	-	-	-
6" x 7-ga x 34'	34	7	H	12-1/2"	1	7.2	5.6	4.4	2.2	-	-	-	-	-
6" x 7-ga x 34'	34	7	H	14"	1	9.3	7.6	6.2	3.6	1.7	-	-	-	-
6" x 7-ga x 35'	35	7	H	11"	1	4.2	3.0	1.8	-	-	-	-	-	-
6" x 7-ga x 35'	35	7	H	12-1/2"	1	6.2	4.8	3.6	1.4	-	-	-	-	-
6" x 7-ga x 35'	35	7	H	14"	1	8.2	6.6	5.2	2.9	1.0	-	-	-	-
6" x 7-ga x 39'	39	7	H	11"	1	1.0	-	-	-	-	-	-	-	-
6" x 7-ga x 39'	39	7	H	12-1/2"	1	3.0	1.6	0.5	-	-	-	-	-	-
6" x 7-ga x 39'	39	7	H	14"	1	4.6	3.3	2.0	-	-	-	-	-	-

Unitil
Northern Utilities, Inc.

August 14th, 2025

John Chagnon
Vice President/ Senior Project Manager
Haley Ward
200 Griffin Road Unit 14
Portsmouth, NH 03801

Natural Gas to Mixed-Use Development at 134 Pleasant Street, Portsmouth NH

Good morning, John.

Unitil/Northern Utilities Natural Gas Division has reviewed the proposed site for natural gas Service.

Unitil hereby confirms that natural gas is at the existing structure and will be available for the proposed development at 134 Pleasant Street in Portsmouth, NH.

If you have any questions, please contact me at 603-264-2033.

Sincerely,

Josh Wilk
Senior Business Development Rep
Unitil

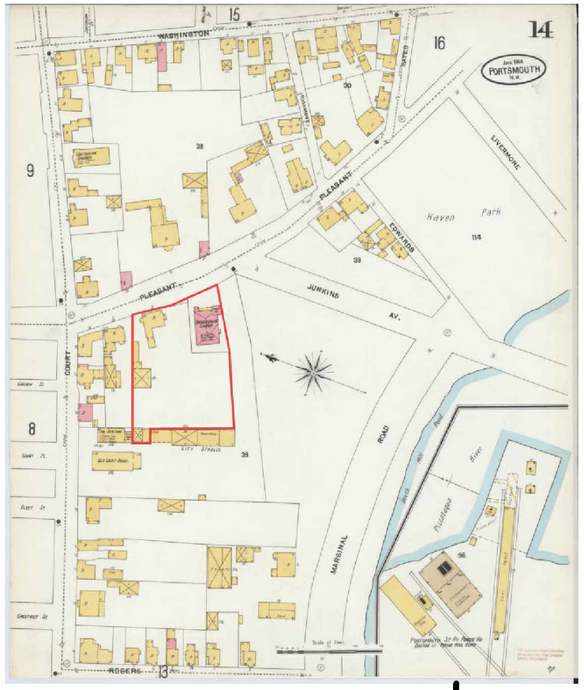
T 603.227.4628
M 603.264.2033
F 603.294.5264
Email wilkj@unitil.com

H0.2 EXISTING PHOTOS
134 PLEASANT STREET
PORTSMOUTH, NH 03801

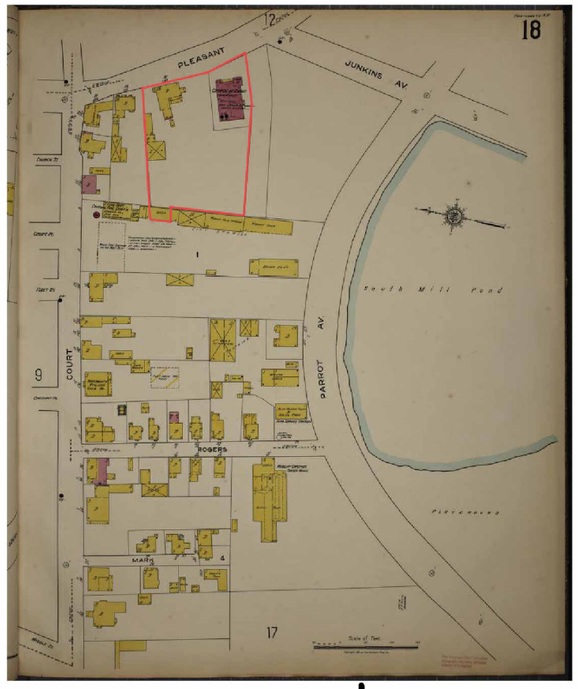
07/25/2025

ARCOVE ARCHITECTS

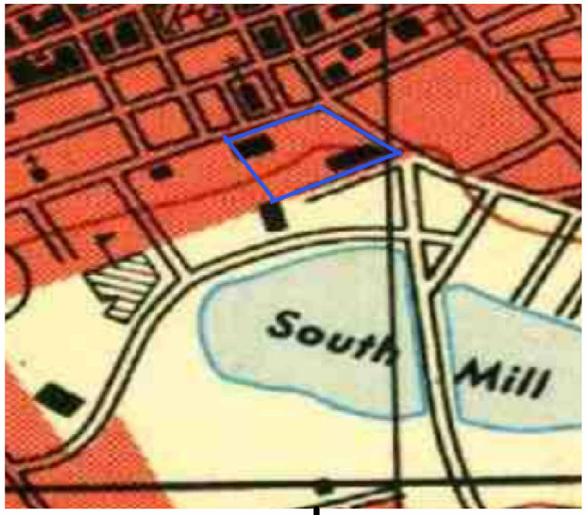
1813
UNIVERSALIST CHURCH & HOUSING



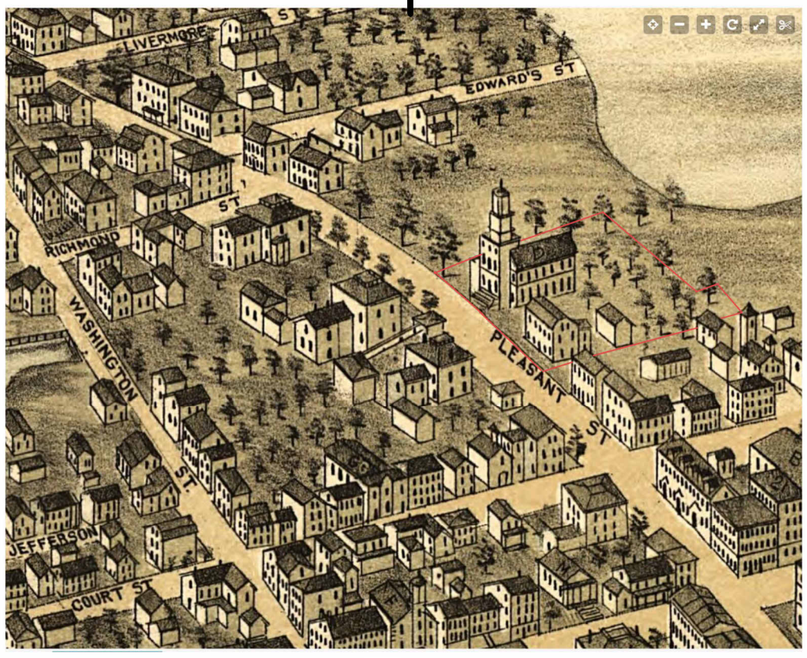
1904
UNIVERSALIST CHURCH & HOUSING



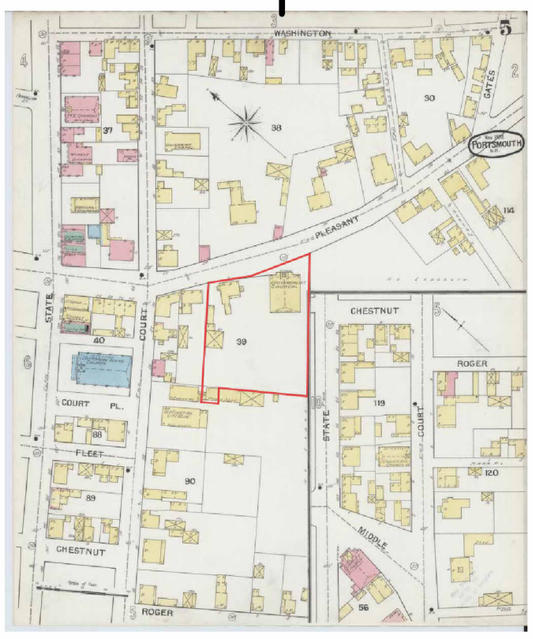
1920
UNIVERSALIST CHURCH & HOUSING



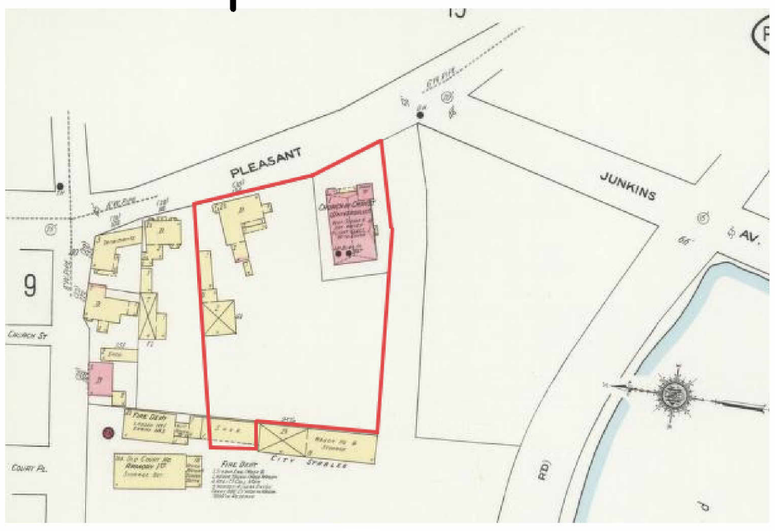
1952
GROCERY IN CONSTRUCTION TILL 1953
CHURCH DEMOLISHED AFTER



1877
UNIVERSALIST CHURCH & HOUSING



1892
UNIVERSALIST CHURCH & HOUSING



1910
UNIVERSALIST CHURCH & HOUSING

H0.4

HISTORIC TIMELINE
134 PLEASANT STREET
PORTSMOUTH, NH 03801

07/25/2025

ARCOVE ARCHITECTS

EXISTING DRIVE UP WINDOW DIMENSION SKETCH

OWNER: DOUBLE MC, LLC
PROPERTY LOCATION: 134 PLEASANT STREET
CITY OF PORTSMOUTH
COUNTY OF ROCKINGHAM
STATE OF NEW HAMPSHIRE

GRAPHIC SCALE

40 0 20 40 80 120

(IN FEET)
1 inch = 40 ft.

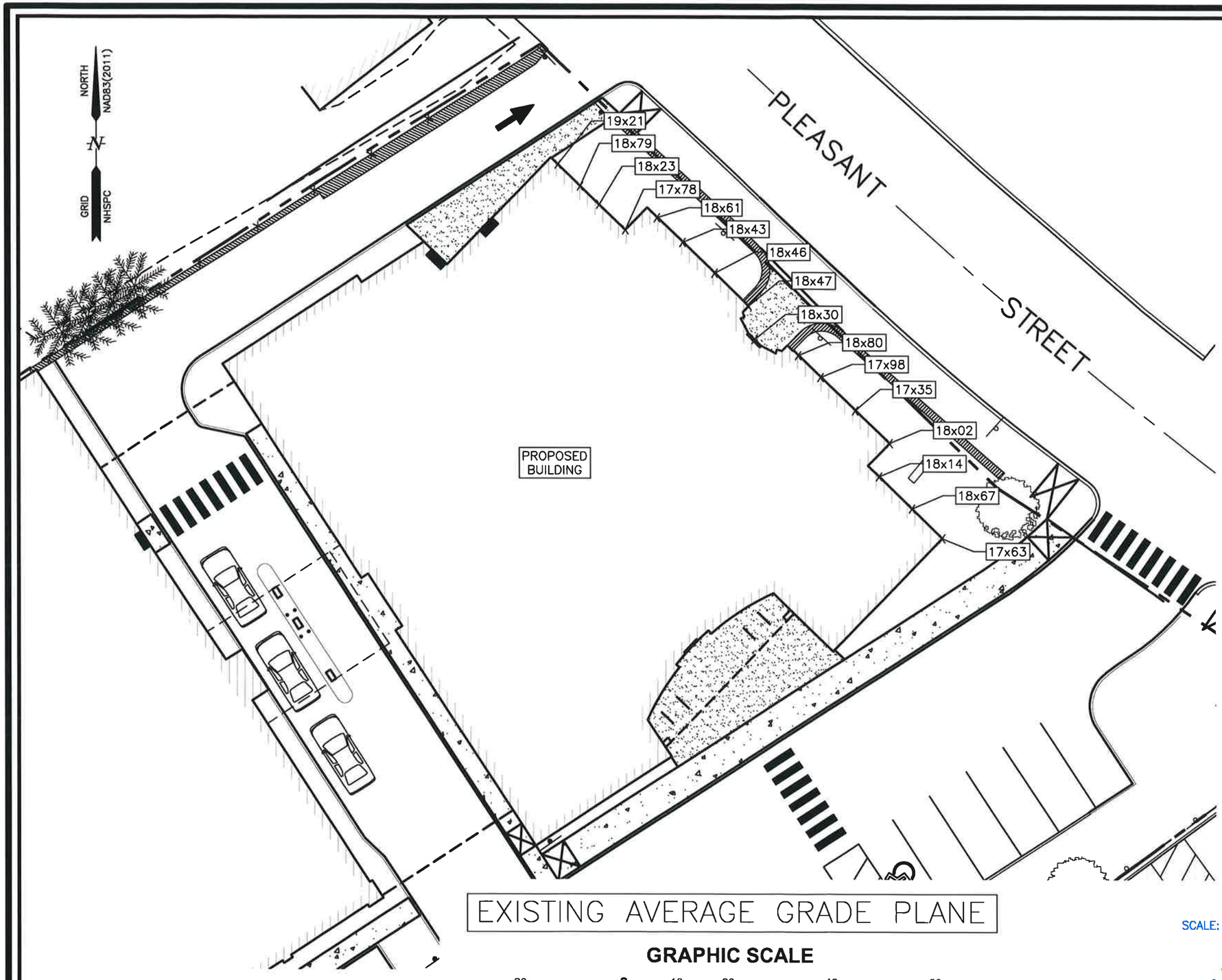
SCALE: 1"=40'

DATE: AUGUST 4, 2025

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Rd. Unit 14
Portsmouth, New Hampshire 03801
603.430.9282
WWW.HALEYWARD.COM

FB 276 PG 60

JOB # 5010156.1532



EXISTING AVERAGE GRADE

	SOUTH	WEST	NORTH	EAST
	N/A	N/A	N/A	19.21
				18.79
				18.23
				17.78
				18.61
				18.43
				18.46
				18.47
				18.30
				18.80
				17.98
				17.35
				18.02
				18.14
				18.67
				17.63
TOTAL	**N/A**	**N/A**	**N/A**	**292.87**
/# GRADES	**N/A**	**N/A**	**N/A**	**292.87/16**
AVG. GRAD	**N/A**	**N/A**	**N/A**	**18.30**

***SPOT GRADES TAKEN AT POINTS 6' ON CENTER ALONG PROPOSED BUILDING FOOTPRINT.**

EXISTING AVERAGE GRADE: 18.30

EXISTING AVERAGE GRADE PLANE

GRAPHIC SCALE

20 0 10 20 40 60

(IN FEET)
1 inch = 20 ft.

SITE REDEVELOPMENT
134 PLEASANT STREET
PORTSMOUTH, N.H.

STATE OF NEW HAMPSHIRE — No. 738 — JOHN R. CHAGNON — LICENSED LAND SURVEYOR — SIGNATURE

SCALE: 1"=20'

SEPTEMBER 2025
REVISED 12-22-2025

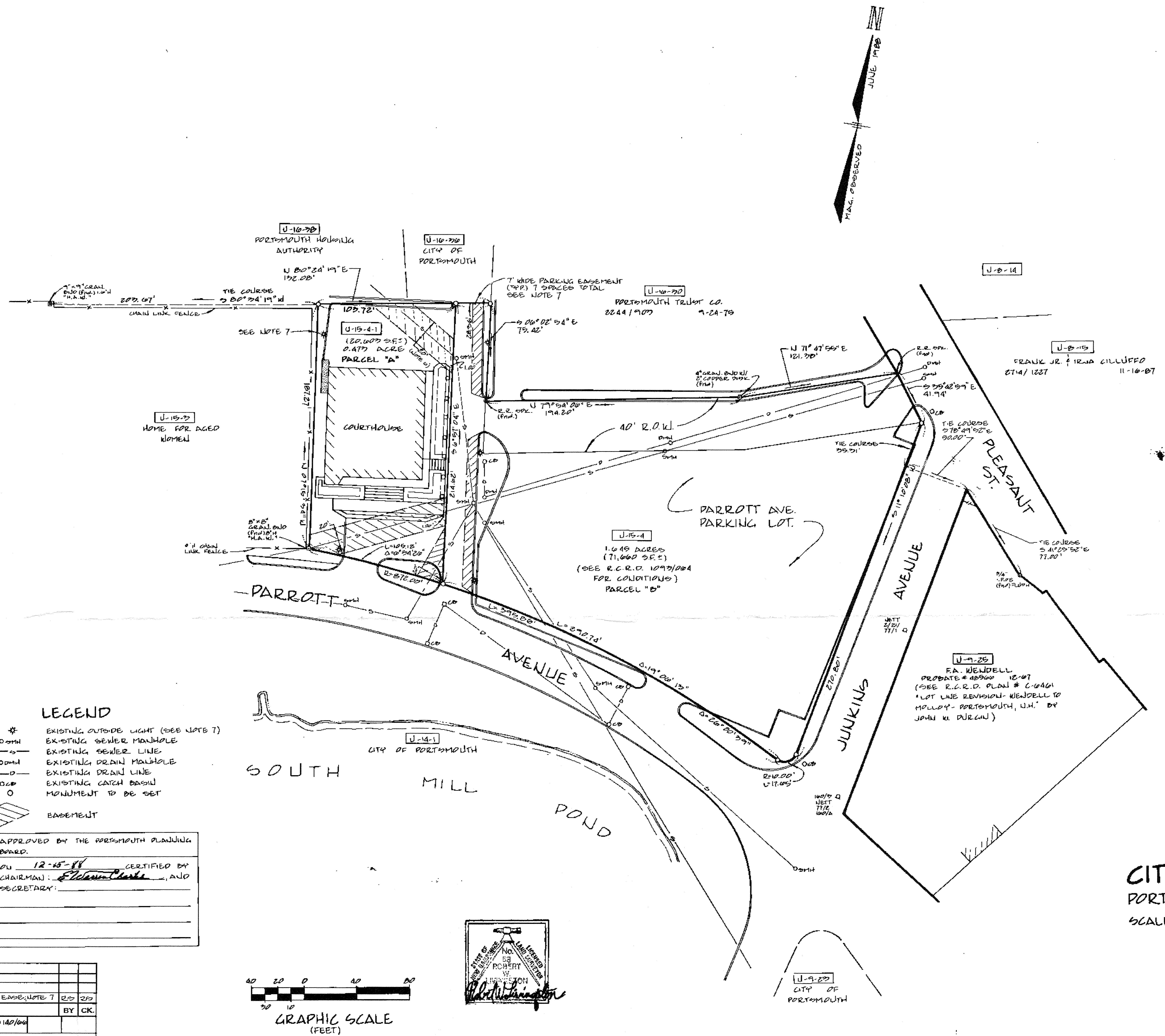
HALEYWARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Rd. Unit 14
Portsmouth, New Hampshire 03801
603.430.9282
WWW.HALEYWARD.COM

FB 276 PG 60

5010156.1532

Dec 15 3 40 PM '88 61814

Dec 16 3:48 PM '88



STATE OF NEW HAMPSHIRE LICENSED LAND SURVEYOR No. 53 ROBERT W. LIVINGSTON

Robert W. Livingston

LEGEND

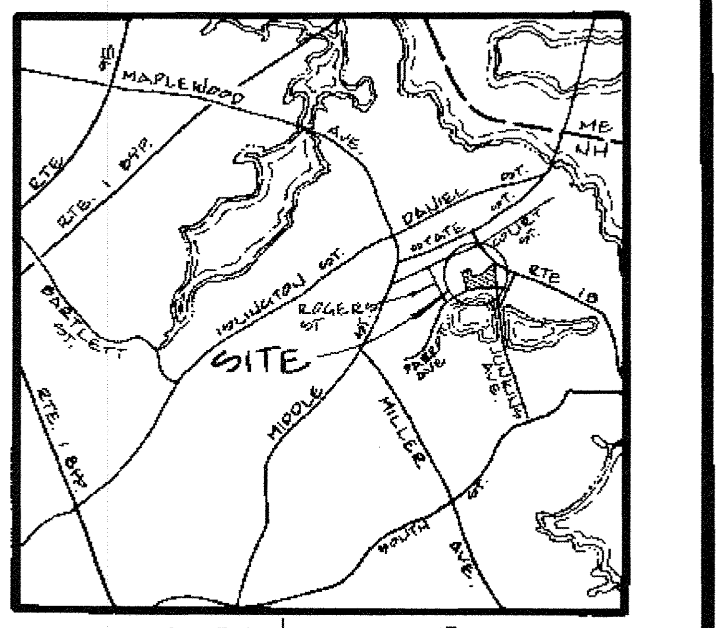
☼	EXISTING OUTSIDE LIGHT (SEE NOTE 7)
○SMH	EXISTING SEWER MANHOLE
—S—	EXISTING SEWER LINE
○DMH	EXISTING DRAIN MANHOLE
—D—	EXISTING DRAIN LINE
○CB	EXISTING CATCH BASIN
○	MONUMENT TO BE SET
▨	EASEMENT

APPROVED BY THE PORTSMOUTH PLANNING BOARD.

ON 12-15-88 CERTIFIED BY
CHAIRMAN: Eleanor Clarke, AND
SECRETARY: ____

REVISIONS DATE	DESCRIPTION	BY	CK.
9-19-88	ADDED OUTSIDE LIGHTS; PKG EASE; NOTE 7	RS	RD

FILE		SIZE		TYPE		FIELD BOOKS & PAGES		
FILE	45590.00	SIZE	D	TYPE	F	M 140/66		



LOCUS MAP
SCALE: 1" = 2000' ±

REFERENCE PLANS:

1. "PLAN OF A TRACT OF LAND SITUATE ON PLEASANT STREET IN THE TOWN OF PORTSMOUTH" BY BENJAMIN SHERMAN, 1822, R.C.R.D. BK. 307, PG 174.
2. "SUSAN J. WENTWORTH EST. - PORTSMOUTH, N.H." - SCALE: 1"=50', MARCH 1940, BY JOHN W. DURGIN, CIVIL ENGINEER. RCRD PLAN # 0937
3. "PARROT AVENUE COURTHOUSE SITE - CITY OF PORTSMOUTH - PORTSMOUTH, NEW HAMPSHIRE" APRIL 1979, BY WARD & WILLIAMS ASSOC.
4. "PLAN OF BOUNDARY LINE - PARROT AVENUE PARKING LOT - FOR - CITY OF PORTSMOUTH, N.H." SCALE: 1"=20', APRIL 1984, BY PARKER SURVEY ASSOC., INC. RCRD PLAN # C-13957
5. "PLAN OF LOT - PARROTT AVE., PORTSMOUTH, N.H. - FOR - HOME FOR AGED WOMEN" SCALE: 1"=10', DEC. 1950, BY JOHN W. DURGIN, CIVIL ENGINEERS.
6. "PLAN OF LOT - PORTSMOUTH, N.H. - FOR PORTSMOUTH TRUST CO." SCALE: 1"=20', AUGUST 1975, BY JOHN W. DURGIN, CIVIL ENGINEERS, PROF. ASSOC.

NOTES:

1. OWNER OF RECORD: CITY OF PORTSMOUTH, R.C.R.D. 1093/064, 11-26-47 & 654/358, 6-4-1910
2. 4-15-4 INDICATES TAX MAP AND LOT NUMBER.
3. TRAVERSE ERROR OF CLOSURE IS NOT GREATER THAN 1:10,000.
4. TOTAL AREA OF PARCEL IS 2.118 ACRES (92,263 S.F.±)
5. SEWER AND UNDERGROUND DRAINAGE LINES TAKEN FROM: "PROPOSED SEWER EXTENSION PROJECT - PORTSMOUTH, N.H. - PARROTT AVENUE" - BY WHITMAN & HOWARD, INC. JAN. 1986, REV. 3-20-87; AND FROM "SEWER SYSTEM MAP - PORTSMOUTH - NEW HAMPSHIRE" - 1981, PORTSMOUTH D.P.W.; AND FROM ACTUAL FIELD LOCATION OF MANHOLES BY THIS OFFICE.
6. CITY OF PORTSMOUTH TO RETAIN SEWER AND DRAINAGE EASEMENTS TO MAINTAIN EXISTING LINES.
7. PARKING EASEMENTS AS SHOWN TO BE GRANTED TO PARCEL "A" WITH THE CONDITION THAT SAID PARCEL "A" OWNS, MAINTAINS AND OPERATES EXISTING OUTSIDE LIGHTS AS SHOWN ON PLAN, WITH APPURTENANT UNDERGROUND CONDUIT AND WIRING.

COURTHOUSE LOT
SUBDIVISION OF LAND
PREPARED FOR
CITY OF PORTSMOUTH
PORTSMOUTH, NEW HAMPSHIRE
SCALE: 1" = 40'
JULY 1, 1988

THOMAS F. MORAN INC.
surveyors
civil engineers
land planners

Bedford, NH 03102 Milford, NH 03055 Dover, NH 03820

D-18880

-14

PROPOSED MIXED USE DEVELOPMENT

134 PLEASANT STREET, PORTSMOUTH, NEW HAMPSHIRE

PERMIT PLANS

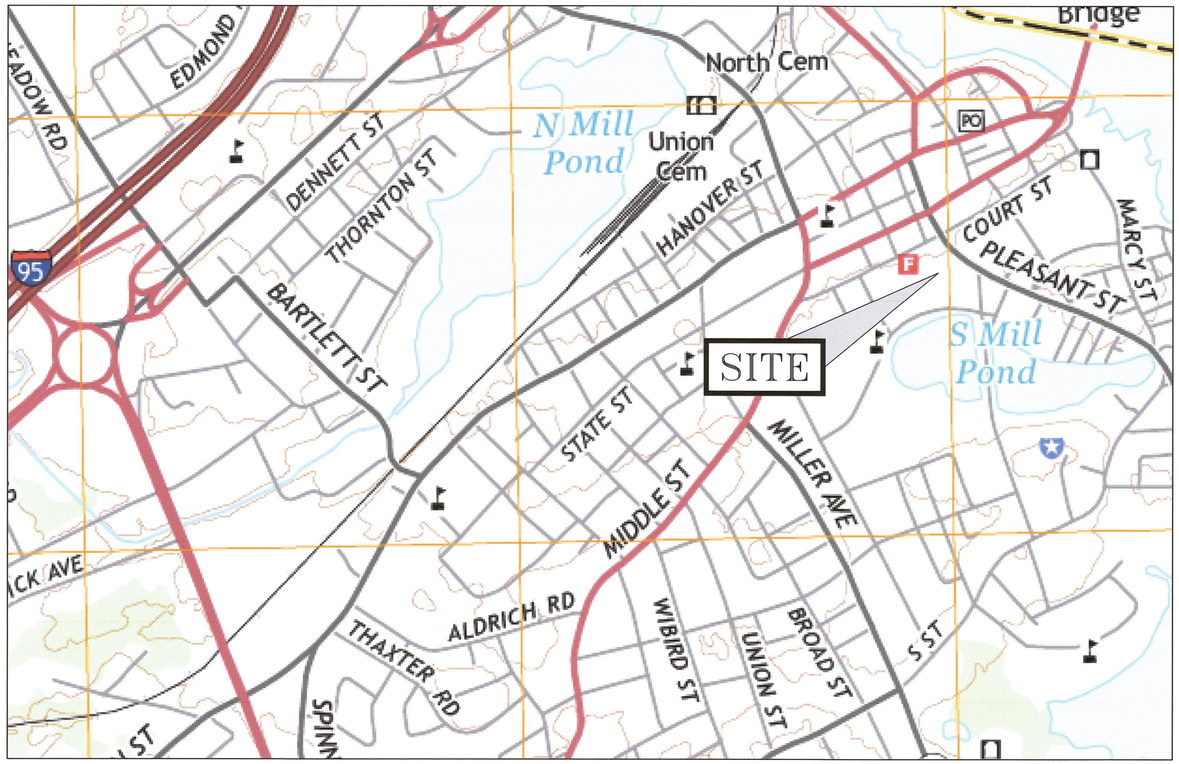
OWNER/APPLICANT
DOUBLE MC, LLC
10 PLEASANT STREET SUITE 300
PORTSMOUTH, N.H. 03801
TEL. (603) 427–0725

CIVIL ENGINEER & LAND SURVEYOR:
HALEY WARD, INC.
200 GRIFFIN ROAD, UNIT 14
PORTSMOUTH, N.H. 03801
TEL. (603) 430–9282

ARCHITECT:
ARCOVE LLC
767 ISLINGTON STREET
PORTSMOUTH, NH 03801
TEL. (603) 731–5187

LANDSCAPE ARCHITECT:
TERRA FIRMA LANDSCAPE ARCHITECTURE
163A COURT STREET
PORTSMOUTH, NH 03801
TEL. (603) 430–8388

DIG SAFE – ME – NH – RI – VT – 1-888-344-7233 – CALL TOLL FREE



SCALE: NTS

INDEX OF SHEETS

PORTSMOUTH APPROVAL CONDITIONS NOTE:
ALL CONDITIONS ON THIS PLAN SET SHALL REMAIN IN EFFECT IN PERPETUITY PURSUANT TO THE REQUIREMENTS OF THE CITY OF PORTSMOUTH SITE PLAN REVIEW REGULATIONS.

APPROVED BY THE PORTSMOUTH PLANNING BOARD

CHAIRMAN _____ DATE _____

UTILITY CONTACTS

ELECTRIC:
EVERSOURCE
1700 LAFAYETTE ROAD
PORTSMOUTH, N.H. 03801
Tel. (603) 436–7708
ATTN: NICHOLAS KOSKO X3327565

NATURAL GAS:
UNITIL
325 WEST ROAD
PORTSMOUTH, N.H. 03801
TEL. (603) 294–5144
ATTN: DAVE BEAULIEU

CABLE:
XFINITY BY COMCAST
180 GREENLEAF AVE.
PORTSMOUTH, N.H. 03801
Tel. (603) 266–2278
ATTN: MIKE COLLINS

SEWER & WATER:
PORTSMOUTH DEPARTMENT OF PUBLIC WORKS
680 PEVERLY HILL ROAD
PORTSMOUTH, N.H. 03801
TEL. (603) 427–1530
ATTN: DOUG SPARKS

COMMUNICATIONS:
CONSOLIDATED COMMUNICATIONS
1575 GREENLAND ROAD
GREENLAND, N.H. 03840
Tel. (603) 427–5525
ATTN: BENJAMIN WILLS

PERMIT LIST:
SITE PLAN: TBD
HISTORIC DISTRICT: TBD

LEGEND:

N/F	NOW OR FORMERLY
RP	RECORD OF PROBATE
RCRD	ROCKINGHAM COUNTY REGISTRY OF DEEDS
11/21	MAP 11/LOT 21
IR FND	IRON ROD FOUND
IP FND	IRON PIPE FOUND
IR SET	IRON ROD SET
DH FND	DRILL HOLE FOUND
DH SET	DRILL HOLE SET
	GRANITE BOUND w/IRON ROD FOUND

EXISTING	PROPOSED	
FM	FM	FORCE MAIN
S	S	SEWER PIPE
SL	SL	SEWER LATERAL
G	PG	GAS LINE
D	D	STORM DRAIN
FD	FD	FOUNDATION DRAIN
W	W	WATER LINE
FS	FS	FIRE SERVICE LINE
UE	UGE	UNDERGROUND ELECTRIC SUPPLY
		UNDERGROUND ELECTRIC SERVICE
OHW	OHW	OVERHEAD ELECTRIC/WIRES
		RETAINING WALL
		EDGE OF PAVEMENT (EP)
100	100	CONTOUR
97x3	98x0	SPOT ELEVATION
		UTILITY POLE
E	E	ELECTRIC METER
		TRANSFORMER ON CONCRETE PAD
W C.O.	W C.O.	WATER SHUT OFF/CURB STOP
C.O.	C.O.	PIPE CLEANOUT
	GV	GATE VALVE
	HYD	HYDRANT
CB	CB	CATCH BASIN
S	SMH	SEWER MANHOLE
D	DMH	DRAIN MANHOLE
M	WMH	WATER METER MANHOLE
#5		TEST BORING
TP 1		TEST PIT
LA		LANDSCAPED AREA
CI	CI	CAST IRON PIPE
COP	COP	COPPER PIPE
CMP	CMP	CORRUGATED METAL PIPE
DI	DI	DUCTILE IRON PIPE
PVC	PVC	POLYVINYL CHLORIDE PIPE
RCP	RCP	REINFORCED CONCRETE PIPE
HYD	HYD	HYDRANT
℄	℄	CENTERLINE
EP	EP	EDGE OF PAVEMENT
EL.	EL.	ELEVATION
FF	FF	FINISHED FLOOR
INV	INV	INVERT
TBM	TBM	TEMPORARY BENCH MARK
TYP	TYP	TYPICAL
TBR	TBR	TO BE REMOVED

PROPOSED MIXED USE DEVELOPMENT
134 PLEASANT STREET
PORTSMOUTH, N.H.
PERMIT PLANS

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Rd. Unit 14
Portsmouth, New Hampshire 03801
603.430.9282
WWW.HALEYWARD.COM

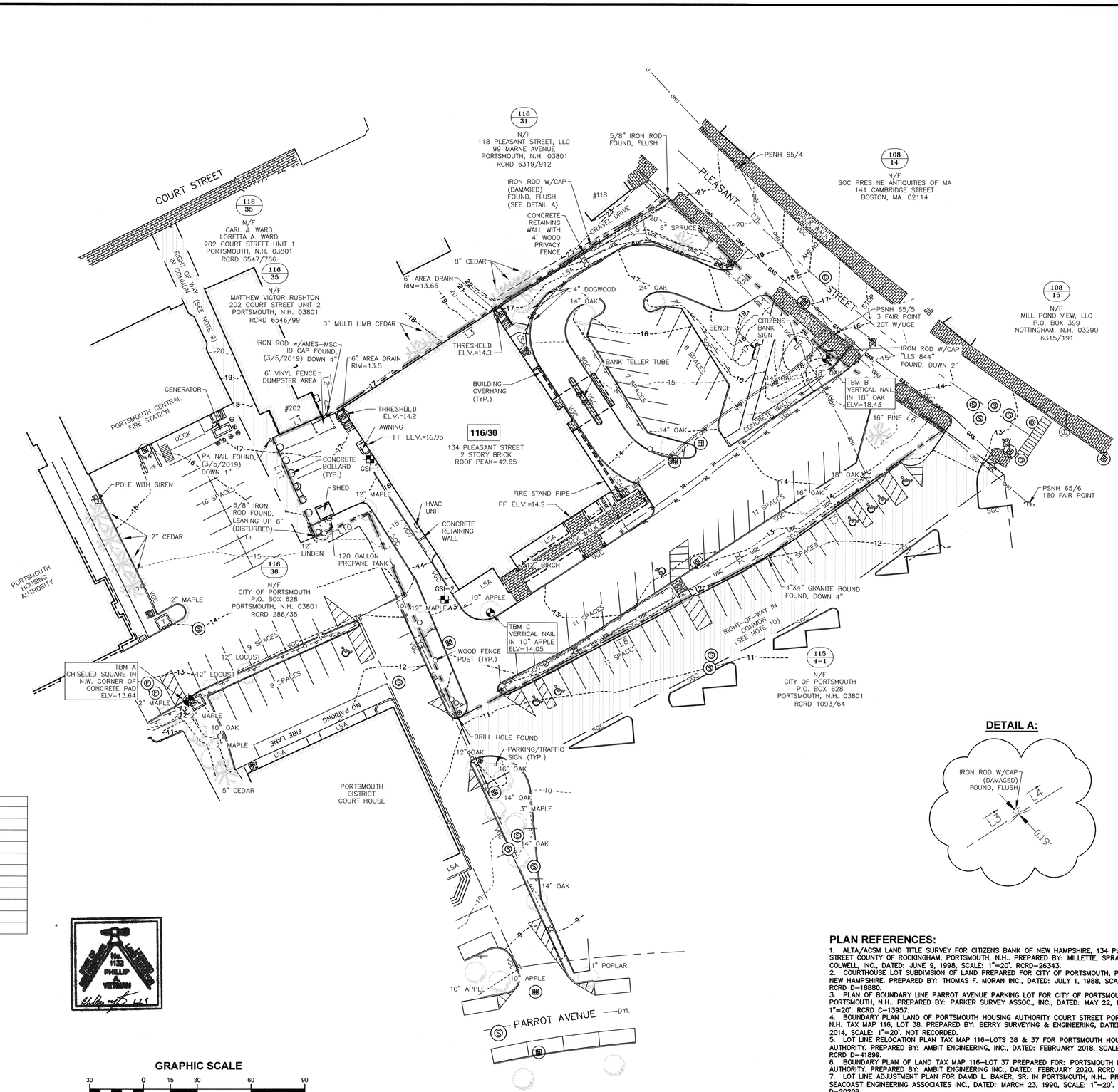
PLAN SET SUBMITTAL DATE: 22 DECEMBER 2025

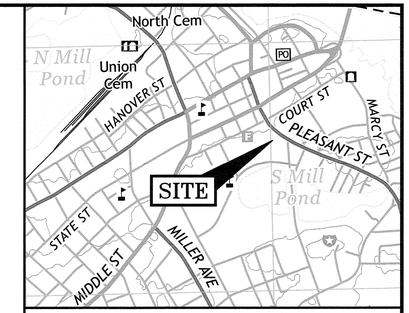
5010156.1532

P:\PWO\5010156-McNally_Properties\1532-134 Pleasant St, Portsmouth (MCM)\CAD\Plan\Cad\5010156.1532-Cover 2025-Dec. 12/18/2025 1:58:37 PM

LEGEND:

DESCRIPTION	EXISTING
RCRD	ROCKINGHAM COUNTY REGISTRY OF DEEDS
1234/123	DEED BOOK/PAGE
N/F	NOW OR FORMALLY
TYP.	TYPICAL
TBS	TO BE SET
LSA	LANDSCAPE AREA
INV.	INVERT
ELV.	ELEVATION
FF	FINISHED FLOOR
DYL	DOUBLE YELLOW LINE
SGC	SLOPED GRANITE CURB
VGC	VERTICAL GRANITE CURB
MAP 21 LOT 8	
BENCHMARK	
RAILROAD SPIKE	
BOUND	
IRON ROD/PIPE FOUND	
SET 5/8" REBAR WITH SURVEYOR'S CAP	
SEWER MANHOLE	
DRAIN MANHOLE	
COMMUNICATIONS MANHOLE	
ELECTRIC MANHOLE	
UTILITY POLE	
CATCH BASIN	
HYDRANT	
GATE VALVE	GGV WGV SGV
GAS/WATER SHUTOFF	
BOLLARD	
BORING	GSI-1
TREES	
GAS/ELECTRIC METER	
LIGHT POLE	
MAIL BOX	
AIR CONDITIONER	
SIGN	
SPOT GRADE	12x1
ELECTRIC PEDESTAL	
ELECTRIC HANDHOLD	
PROPERTY LINE	
APPROXIMATE EXTERIOR PROPERTY LINE	
EDGE OF PAVEMENT	
EDGE OF GRAVEL	
STONE WALL	
FENCE	
SHRUBLINE	
MINOR FOOT CONTOUR	98
MAJOR FOOT CONTOUR	100
GAS LINE	GAS GAS
WATER LINE	WL WL
STORM DRAIN LINE	SD SD
SANITARY SEWER LINE	SS SS
OVERHEAD UTILITY LINE	OHU OHU
UNDERGROUND COMMUNICATIONS LINE	UGT UGT





LOCATION MAP SCALE: 1" = 1,000'

NOTES:

1) PARCEL IS SHOWN ON THE CITY OF PORTSMOUTH ASSESSOR'S TAX MAP 116 AS LOT 30.

2) OWNERS OF RECORD:
DOUBLE MC, LLC
10 PLEASANT STREET SUITE 400
PORTSMOUTH, N.H. 03801
RCRD 6505/2411

3) PARCEL IS NOT IN A SPECIAL FLOOD HAZARD AREA AS SHOWN ON FIRM PANEL 33015C0259F. EFFECTIVE JANUARY 29, 2021.

4) EXISTING LOT AREA:
53,084 S.F.
1.22 ACRES

5) PARCEL IS LOCATED IN THE CHARACTER DISTRICT 4 (CD4) DISTRICT.

6) DIMENSIONAL REQUIREMENTS: SEE PORTSMOUTH ORDINANCE FOR REQUIREMENTS.

7) THE PURPOSE OF THIS PLAN IS TO SHOW THE RESULTS OF AN EXISTING CONDITIONS SURVEY OF ASSESSOR'S MAP 116, LOT 30 IN THE CITY OF PORTSMOUTH.

8) VERTICAL DATUM IS NAVD88. BASIS OF VERTICAL DATUM IS REDUNDANT RTN GNSS OBSERVATIONS.

9) PARCEL IS BENEFITED BY A RIGHT OF WAY TO COURT STREET AS DESCRIBED IN RCRD 1257/466.

10) PARCEL IS BENEFITED BY A RIGHT OF WAY AS DESCRIBED IN RCRD 2975/349 AND SHOWN ON RCRD D-18880 AND RCRD D-26343.

11) ABUTTER INFORMATION TAKEN FROM THE CITY OF PORTSMOUTH ASSESSORS GIS WEBSITE.

LINE TABLE:

LINE	BEARING	DISTANCE
L1	N66°28'05"E	32.60'
L2	S04°27'10"E	0.98'
L3	N57°05'10"E	175.24'
L4	N58°07'10"E	50.44'
L5	S45°49'50"E	122.39'
L6	S56°42'50"E	79.29'
L7	S54°09'10"W	121.33'
L8	S62°18'10"W	194.24'
L9	N23°38'50"W	124.67'
L10	S66°22'00"W	32.60'
L11	N23°20'25"W	60.00'

DETAIL A:

PLAN REFERENCES:

1. ALTA/ACSM LAND TITLE SURVEY FOR CITIZENS BANK OF NEW HAMPSHIRE, 134 PLEASANT STREET COUNTY OF ROCKINGHAM, PORTSMOUTH, N.H.. PREPARED BY: MILLETTE, SPRAGUE & COLWELL, INC., DATED: JUNE 9, 1998, SCALE: 1"=20'. RCRD-26343.
2. COURTHOUSE LOT SUBDIVISION OF LAND PREPARED FOR CITY OF PORTSMOUTH, PORTSMOUTH NEW HAMPSHIRE. PREPARED BY: THOMAS F. MORAN INC., DATED: JULY 1, 1988, SCALE: 1"=40'. RCRD D-18880.
3. PLAN OF BOUNDARY LINE PARROT AVENUE PARKING LOT FOR CITY OF PORTSMOUTH IN PORTSMOUTH, N.H.. PREPARED BY: PARKER SURVEY ASSOC., INC., DATED: MAY 22, 1984, SCALE: 1"=20'. RCRD C-13957.
4. BOUNDARY PLAN LAND OF PORTSMOUTH HOUSING AUTHORITY COURT STREET PORTSMOUTH, N.H. TAX MAP 116, LOT 38. PREPARED BY: BERRY SURVEYING & ENGINEERING, DATED: MAY 30, 2014, SCALE: 1"=20'. NOT RECORDED.
5. LOT LINE RELOCATION PLAN TAX MAP 116-LOTS 38 & 37 FOR PORTSMOUTH HOUSING AUTHORITY. PREPARED BY: AMBIT ENGINEERING, INC., DATED: FEBRUARY 2018, SCALE: 1"=30'. RCRD D-41899.
6. BOUNDARY PLAN OF LAND TAX MAP 116-LOT 37 PREPARED FOR: PORTSMOUTH HOUSING AUTHORITY. PREPARED BY: AMBIT ENGINEERING INC., DATED: FEBRUARY 2020. RCRD D-42016.
7. LOT LINE ADJUSTMENT PLAN FOR DAVID L. BAKER, SR. IN PORTSMOUTH, N.H.. PREPARED BY: SEACOAST ENGINEERING ASSOCIATES INC., DATED: MARCH 23, 1990, SCALE: 1"=20'. RCRD D-20209.
8. PLAN OF LOTS NOS. 202, 206 & 222 COURT STREET PORTSMOUTH, N.H.. PREPARED BY: JOHN W. DURGIN CIVIL ENGINEERS, DATED: JANUARY 1974, SCALE: 1"=10'. RCRD C-4259.
9. CONDOMINIUM SITE PLAN FIREHOUSE TOWN HOMES CONDOMINIUM. PREPARED BY: AMBIT ENGINEERING, INC..DATED: JANUARY 2020, SCALE: 1"=10'. RCRD D-43127.

"I CERTIFY THAT THIS PLAN WAS PREPARED UNDER MY DIRECT SUPERVISION, THAT IT IS THE RESULT OF A FIELD SURVEY BY THIS OFFICE AND HAS AN ACCURACY OF THE CLOSED TRAVERSE THAT EXCEEDS THE PRECISION OF 1:15,000."

PHILIP A. YETMAN, LLS 1122 — DATE 7/18/2025

GRAPHIC SCALE
30 0 15 30 60 90
(IN FEET)
1 inch = 30 ft.

REV.	DATE	DESCRIPTION	BY	CHK
0	07-18-2025	ISSUED FOR COMMENT	RJB	PAY

DRAWING ISSUE STATUS
SITE SURVEY

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Road, Unit 14
Portsmouth, NH 03801
603-430-9282
WWW.HALEYWARD.COM

PROJECT
DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE
EXISTING CONDITIONS PLAN

DATE	SCALE
MARCH 2025	1"=30'
DRAWN BY: RJB	DESIGNED BY: ---	CHECKED BY: PAY
PROJECT No.: 5010156.1532	FIELD BOOK / PAGE: FB 276 PG 60
DRAWING No.: V101	REV.: 0

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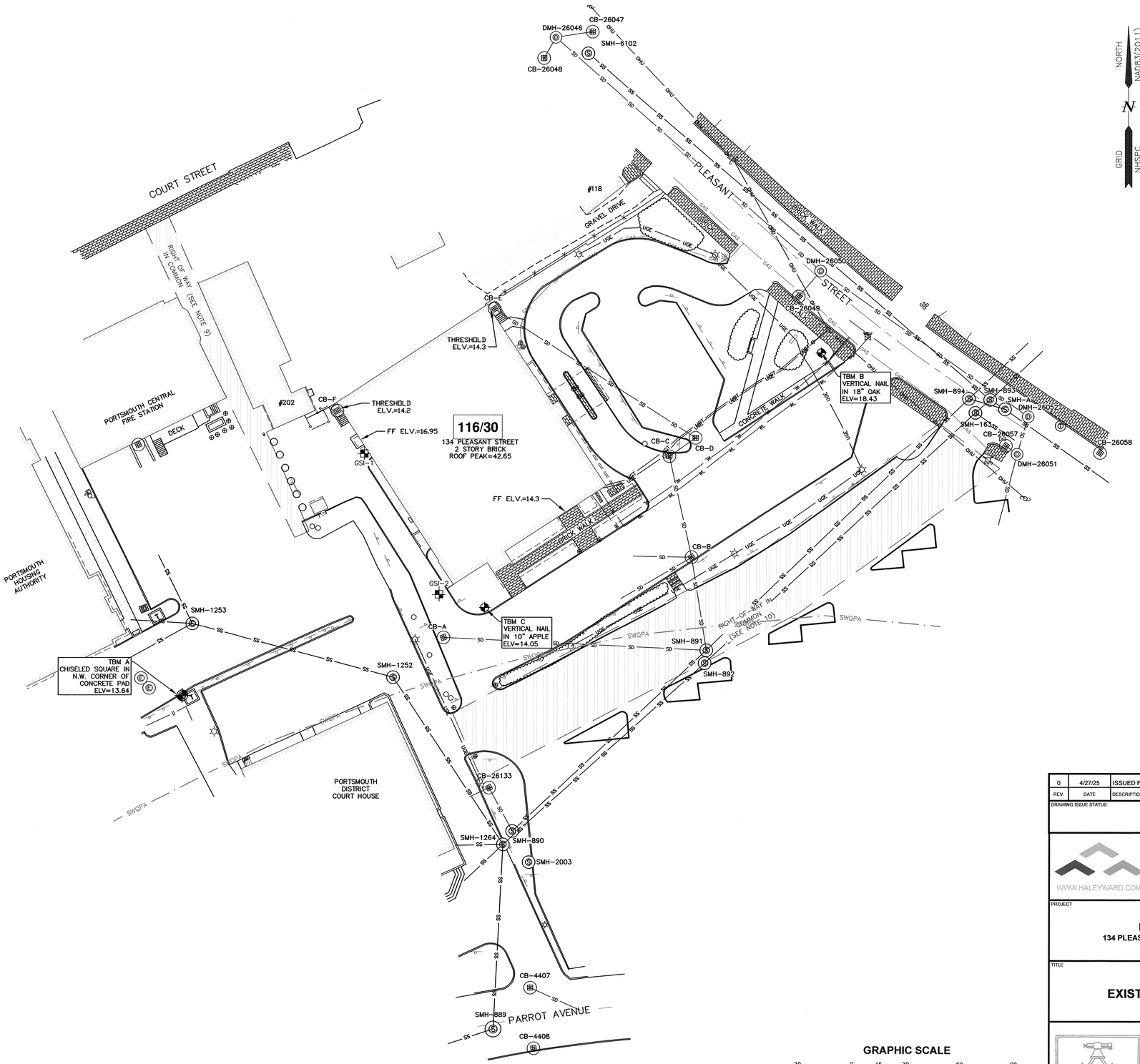
SEWER MANHOLE STRUCTURE TABLE

STRUCTURE	PROP/EX	RIM	PIPE SIZE/TYPE	INVERT IN	INVERT OUT	DIRECTION
SMH-889	EX	8.96	30" AC	0.30		SW
			30" AC		-0.14	N
SMH-1264	EX	9.90	18" VCP	-0.55		SE
			30" RCP	-0.15		SW
			18" AC	COULD NOT	MEASURE	NW
			30" CAST		-0.65	NE
SMH-890	EX	10.48	12" RCP	3.88		NW
			30" CAST		0.13	NE
SMH-2003	EX	9.96	18" VCP	-0.54		NW
					N/A	
SMH-851	EX	9.96				
SMH-1252	EX	11.79	18" CLAY	3.19		NW
			18" CLAY		3.14	SE
SMH-1253	EX	13.98	8" CLAY	6.98		SW
			6" CLAY	6.68		W
			18" CLAY	6.68		NW
			18" CLAY		6.63	SE
SMH-891	EX	11.00	30" CAST	-0.05		SW
			12" AC	6.25		NW
			18" AC	6.30		NE
			30" CAST		-0.05	NE
SMH-892	EX	10.91	30" CAST	-0.29		SW
			30" CAST		-0.29	NE
SMH-163	EX	13.13	36" CAST	-0.87		SW
			6" PVC	1.43		SE
			36" CAST		-0.87	NE
SMH-893	EX	13.24	36" CAST	-0.86		SW
			12" PVC	4.54		NW
			12" CLAY	4.54		SE
			36" CAST		-0.86	NE
SMH-A	EX	13.12	12" RCP (ENCLOSED)	3.57		NW
			12" RCP (ENCLOSED)		3.57	SE
SMH-894	EX	13.43	36" CAST	-0.27		SW
			12" CLAY	4.33		SE
			36" CAST		-0.32	NE
SMH-6102	EX	22.87				

DRAINAGE STRUCTURE TABLE

STRUCTURE	PROP/EX	RIM	PIPE SIZE/TYPE	INVERT IN	INVERT OUT	DIRECTION
CB-4407	EX	8.30	NO VISSIBLE PIPES			
CB-26133	EX	10.86	12" CLAY	4.31		SW
			12" CLAY		4.31	SE
CB-A	EX	12.22	12" AC		8.42	SE
CB-B	EX	12.80	8" CLAY	7.10		NW
			12" AC	7.10		NE
			12" AC	7.00		SW
			18" AC		7.00	S
CB-C	EX	13.47	12" AC	8.87		NE
			12" AC		8.82	S
CB-D	EX	13.54	4" AC	9.34		NW
			12" AC		9.34	SW
CB-E	EX	13.67	SILTED/LOCKED			
CB-F	EX	13.48	SILTED/LOCKED			
CB-26057	EX	12.53	12" CPP W/OIL SEPERATOR		8.33	SE
DMH-26046	EX	22.99				
CB-26048	EX	22.70				
CB-26047	EX	22.89				
DMH-26050	EX	17.45	12" PVC	12.80		NW
			12" PVC	12.80		SW
			12" PVC		12.80	SE
CB-26049	EX	17.08	12"PVC W/OIL SEPERATOR		11.78	NE
DMH-26052	EX	12.93	12" CPP	8.18		NW
			12" CPP		8.13	S
DMH-26051	EX	12.61	12" CPP	7.86		N
			12" CPP	8.11		NW
			12" CPP		7.81	S



GRAPHIC SCALE

30 0 15 30 60 90

(IN FEET)
1 inch = 30 ft.

REV.	DATE	DESCRIPTION	BY	CHK
0	4/27/25	ISSUED FOR COMMENT	RJB	PAY

DRAWING ISSUE STATUS

SITE SURVEY

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Road, Unit 14
Portsmouth, NH 03801
603-430-9282
WWW.HALEYWARD.COM

PROJECT

DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE

EXISTING UTILITIES PLAN

DATE	JUNE 2025
SCALE	1"=30'
DRAWN BY	RJB
DESIGNED BY	---
CHECKED BY	PAY
PROJECT No.	5010156.1532
FIELD BOOK / PAGE	FB 276 PG 60
DRAWING No.	V102
REV.	0

STATE OF NEW HAMPSHIRE No. 738 JOHN R. CHAMPION LICENSED LAND SURVEYOR SIGNATURE

FILE LOCATION: P:\NH\5010156.MCN\MB_PROPERTIES\1532-134 PLEASANT ST - PORTSMOUTH-JRC\02-CAD_FILES\5010156.1532.V-EC.DWG, 2025.07.30, 2:04 PM

GRAPHIC SCALE

40 0 20 40 80 120

(IN FEET)
1 inch = 40 ft.

REV.	DATE	DESCRIPTION	BY	CHK.
0	7/25/25	ISSUED FOR COMMENT	RJB	JRC

DRAWING ISSUE STATUS

SITE SURVEY

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Road, Unit 14
Portsmouth, NH 03801
603-430-9282
WWW.HALEYWARD.COM

PROJECT

DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE

ORTHOPHOTO PLAN

DATE: JULY 2025	SCALE: 1"=40'
DRAWN BY: RJB	DESIGNED BY: —
CHECKED BY: PAY	
PROJECT No.: 5010156.1532	FIELD BOOK / PAGE: FB 276 PG 60
DRAWING No.: V103	REV.: 0

DEMOLITION NOTES:

A) THE LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE AND THE LOCATIONS ARE NOT GUARANTEED BY THE OWNER OR THE DESIGNER. IT IS THE CONTRACTORS' RESPONSIBILITY TO LOCATE UTILITIES AND ANTICIPATE CONFLICTS. CONTRACTOR SHALL REPAIR EXISTING UTILITIES DAMAGED BY THEIR WORK AND RELOCATE EXISTING UTILITIES THAT ARE REQUIRED TO BE RELOCATED PRIOR TO COMMENCING ANY WORK IN THE IMPACTED AREA OF THE PROJECT.

B) ALL MATERIALS SCHEDULED TO BE REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTORS UNLESS OTHERWISE SPECIFIED. THE CONTRACTOR SHALL DISPOSE OF ALL MATERIALS OFF-SITE IN ACCORDANCE WITH ALL FEDERAL, STATE, AND LOCAL REGULATIONS, ORDINANCES AND CODES. THE CONTRACTOR SHALL COORDINATE REMOVAL, RELOCATION, DISPOSAL, OR SALVAGE OF UTILITIES WITH THE OWNER AND APPROPRIATE UTILITY COMPANY.

C) ANY EXISTING WORK OR PROPERTY DAMAGED OR DISRUPTED BY CONSTRUCTION/ DEMOLITION ACTIVITIES SHALL BE REPLACED OR REPAIRED TO THE ORIGINAL EXISTING CONDITIONS BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.

D) THE CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UTILITIES AND CALL DIG SAFE AT LEAST 72 HOURS PRIOR TO THE COMMENCEMENT OF ANY DEMOLITION/CONSTRUCTION ACTIVITIES.

E) SAWCUT AND REMOVE PAVEMENT ONE FOOT OFF PROPOSED EDGE OF PAVEMENT TRENCH IN AREAS WHERE PAVEMENT IS TO BE REMOVED.

F) IT IS THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE THEMSELVES WITH THE CONDITIONS OF ALL THE PERMIT APPROVALS.

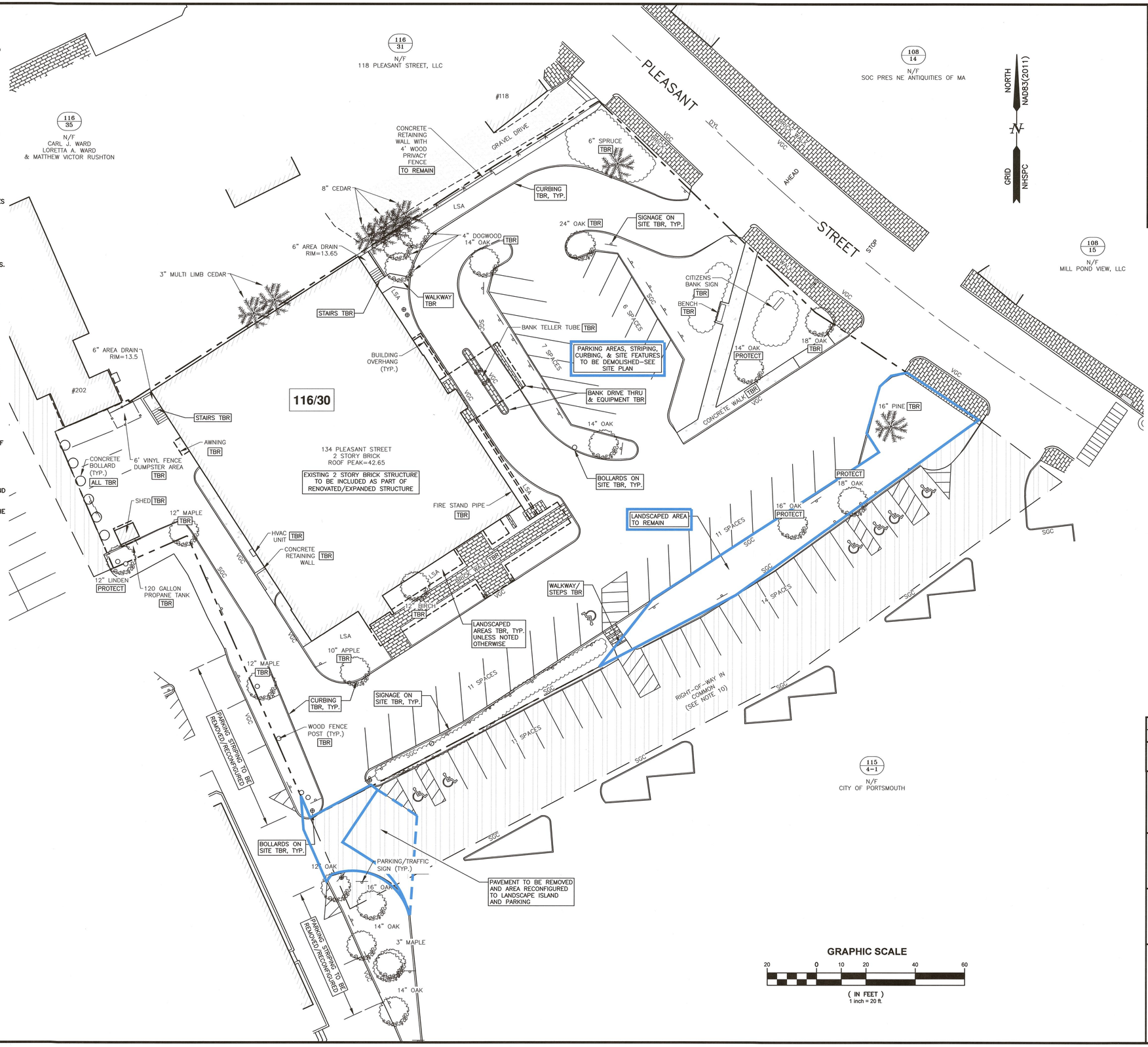
G) THE CONTRACTOR SHALL OBTAIN AND PAY FOR ADDITIONAL CONSTRUCTION PERMITS, NOTICES AND FEES NECESSARY TO COMPLETE THE WORK AND ARRANGE FOR AND PAY FOR ANY INSPECTIONS AND APPROVALS FROM THE AUTHORITIES HAVING JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY ADDITIONAL AND OFF-SITE DISPOSAL OF MATERIALS REQUIRED TO COMPLETE THE WORK.

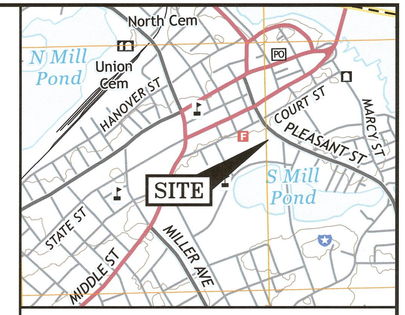
H) THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL EXISTING STRUCTURES, CONCRETE, UTILITIES, VEGETATION, PAVEMENT, AND CONTAMINATED SOIL WITHIN THE WORK LIMITS SHOWN UNLESS SPECIFICALLY IDENTIFIED TO REMAIN. ANY EXISTING DOMESTIC / IRRIGATION SERVICE WELLS IN THE PROJECT AREA IDENTIFIED DURING THE CONSTRUCTION AND NOT CALLED OUT ON THE PLANS SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER AND ENGINEER FOR PROPER CAPPING / RE-USE.

I) PROVIDE INLET PROTECTION BARRIERS AT ALL CATCH BASINS WITHIN CONSTRUCTION LIMITS AND MAINTAIN FOR THE DURATION OF THE PROJECT. INLET PROTECTION BARRIERS SHALL BE HIGH FLOW SILT SACK BY ACF ENVIRONMENTAL OR APPROVED EQUAL. INSPECT BARRIERS WEEKLY AND AFTER EACH RAIN OF 0.25 INCHES OR GREATER. CONTRACTOR SHALL COMPLETE A MAINTENANCE INSPECTION REPORT AFTER EACH INSPECTION. SEDIMENT DEPOSITS SHALL BE REMOVED AFTER EACH STORM EVENT OR MORE OFTEN IF WARRANTED OR FABRIC BECOMES CLOGGED. EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE START OF ANY CLEARING OR DEMOLITION ACTIVITIES.

J) THE CONTRACTOR SHALL PAY ALL COSTS NECESSARY FOR TEMPORARY PARTITIONING, BARRICADING, FENCING, SECURITY AND SAFELY DEVICES REQUIRED FOR THE MAINTENANCE OF A CLEAN AND SAFE CONSTRUCTION SITE.

K) ANY CONTAMINATED MATERIAL REMOVED DURING THE COURSE OF THE WORK WILL REQUIRE HANDLING IN ACCORDANCE WITH NHDES REGULATIONS. CONTRACTOR SHALL HAVE A HEALTH AND SAFETY PLAN IN PLACE, AND COMPLY WITH ALL APPLICABLE PERMITS, APPROVALS, AUTHORIZATIONS, AND REGULATIONS





LOCATION MAP SCALE: 1" = 1,000'

NOTES:

1) THE CONTRACTOR SHALL NOTIFY DIG SAFE AT 1-888-DIG-SAFE (1-888-344-7233) AT LEAST 72 HOURS PRIOR TO COMMENCING ANY EXCAVATION ON PUBLIC OR PRIVATE PROPERTY.

2) UNDERGROUND UTILITY LOCATIONS ARE BASED UPON BEST AVAILABLE EVIDENCE AND ARE NOT FIELD VERIFIED. LOCATING AND PROTECTING ANY ABOVEGROUND OR UNDERGROUND UTILITIES IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND/OR THE OWNER. UTILITY CONFLICTS SHOULD BE REPORTED AT ONCE TO THE DESIGN ENGINEER.

3) CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE "NEW HAMPSHIRE STORMWATER MANUAL, VOLUME 3, EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION. (NHDES DECEMBER 2008).

4) COORDINATE ACCESS IN STREET/ROW AREAS ADJACENT TO DEMOLITION TO INSURE SAFE PASSAGE. UTILIZE DETOURS IF NEEDED.

5) EXISTING UTILITIES TO BE ABANDONED SHALL BE REMOVED TO THE UTILITY MAIN AND CAPPED PER THE UTILITY COMPANY REQUIREMENT.

GRAPHIC SCALE
20 0 10 20 40 60
(IN FEET)
1 inch = 20 ft.

REV.	DATE	DESCRIPTION	BY	CHK.
0	07-18-2025	ISSUED FOR COMMENT	SJR	JRC

DRAWING ISSUE STATUS

PERMIT PLANS

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Road, Unit 14
Portsmouth, NH 03801
603-430-9282
WWW.HALEYWARD.COM

PROJECT

DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE

DEMOLITION PLAN

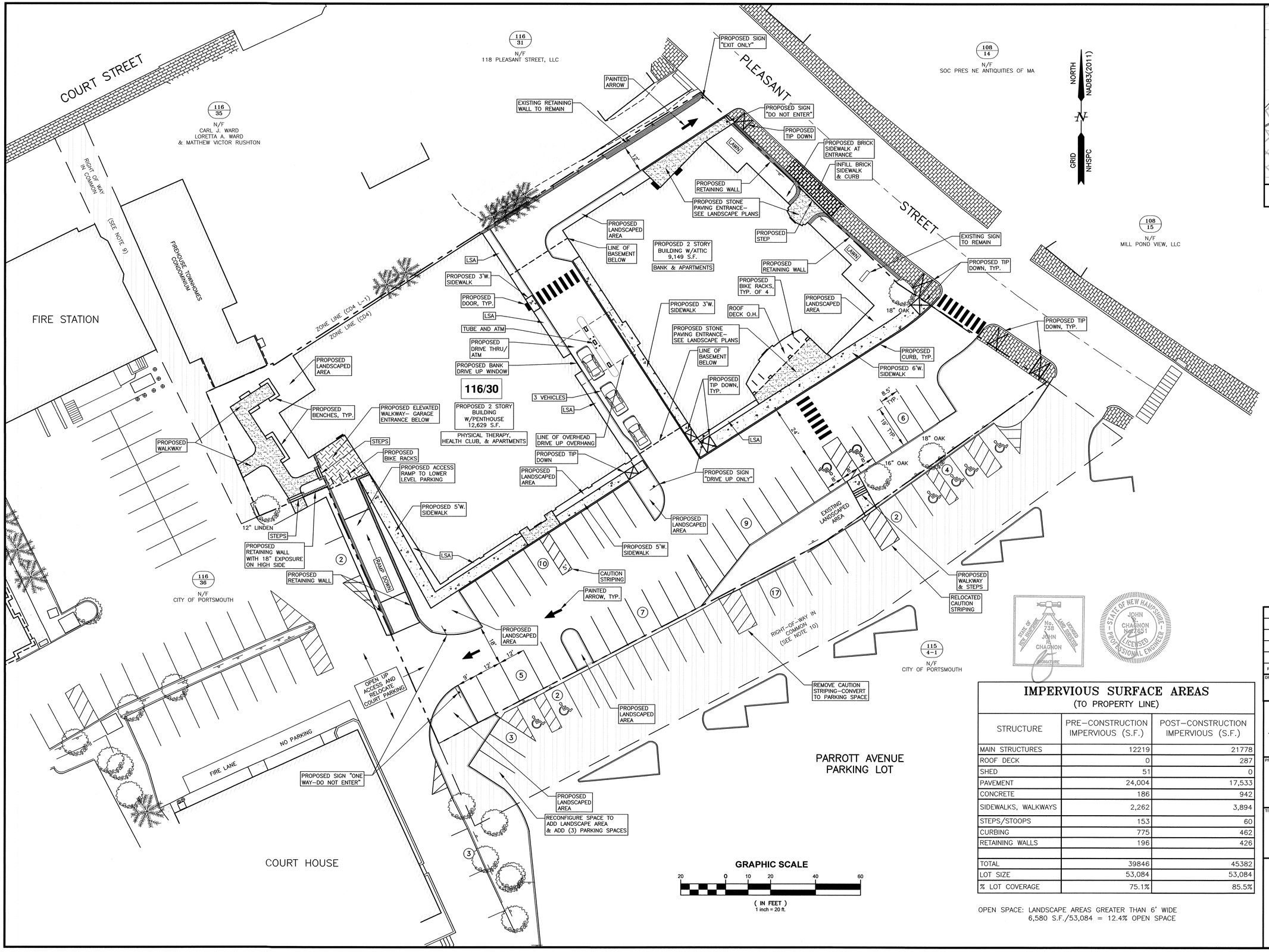
JOHN R. CHAGNON No. 9651 LICENSED PROFESSIONAL ENGINEER - STATE OF NEW HAMPSHIRE -

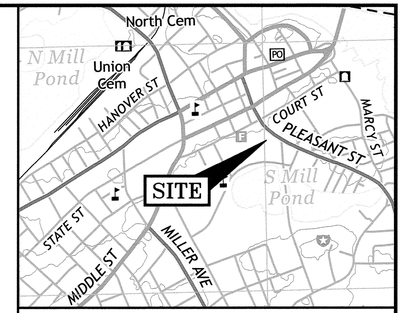
DATE	SCALE
JULY 2025	1"=20'

DRAWN BY	DESIGNED BY	CHECKED BY
SJR	---	JRC

PROJECT No. 5010156.1532
FIELD BOOK / PAGE FB 276 PG 60

DRAWING No. **C101** REV. **0**





LOCATION MAP SCALE: 1" = 1,000'

NOTES:

1) PARCEL IS SHOWN ON THE CITY OF PORTSMOUTH ASSESSOR'S TAX MAP 116 AS LOT 30.

2) OWNERS OF RECORD:
DOUBLE MC, LLC
10 PLEASANT STREET SUITE 400
PORTSMOUTH, N.H. 03801
RCRD 6505/2411

3) PARCEL IS NOT IN A SPECIAL FLOOD HAZARD AREA AS SHOWN ON FIRM PANEL 33015C0259F. EFFECTIVE JANUARY 29, 2021.

4) EXISTING LOT AREA:
53,084 S.F.
1.22 ACRES

5) PARCEL IS LOCATED IN THE CHARACTER DISTRICT 4 (CD4) DISTRICT.

6) DIMENSIONAL REQUIREMENTS: SEE PORTSMOUTH ORDINANCE FOR REQUIREMENTS.

7) THE PURPOSE OF THIS PLAN IS TO SHOW A PROPOSED MIXED USE DEVELOPMENT ON ASSESSOR'S MAP 116, LOT 30 IN THE CITY OF PORTSMOUTH.

8) VERTICAL DATUM IS NAVD88. BASIS OF VERTICAL DATUM IS REDUNDANT RTN GNSS OBSERVATIONS.

9) PARCEL IS BENEFITED BY A RIGHT OF WAY TO COURT STREET AS DESCRIBED IN RCRD 1257/466.

10) PARCEL IS BENEFITED BY A RIGHT OF WAY AS DESCRIBED IN RCRD 2975/349 AND SHOWN ON RCRD D-18880 AND RCRD D-26343.

11) ABUTTER INFORMATION TAKEN FROM THE CITY OF PORTSMOUTH ASSESSORS GIS WEBSITE.

12) PROPOSED USES:
-RETAIL BANKING
-BANK HEADQUARTERS
-APARTMENTS
-PHYSICAL THERAPY
-HEALTH CLUB

IMPERVIOUS SURFACE AREAS
(TO PROPERTY LINE)

STRUCTURE	PRE-CONSTRUCTION IMPERVIOUS (S.F.)	POST-CONSTRUCTION IMPERVIOUS (S.F.)
MAIN STRUCTURES	12219	21778
ROOF DECK	0	287
SHED	51	0
PAVEMENT	24,004	17,533
CONCRETE	186	942
SIDEWALKS, WALKWAYS	2,262	3,894
STEPS/STOOPS	153	60
CURBING	775	462
RETAINING WALLS	196	426
TOTAL	39846	45382
LOT SIZE	53,084	53,084
% LOT COVERAGE	75.1%	85.5%

OPEN SPACE: LANDSCAPE AREAS GREATER THAN 6' WIDE
6,580 S.F./53,084 = 12.4% OPEN SPACE

STATE OF NEW HAMPSHIRE No. 728 LICENSED LAND SURVEYOR JOHN J. CHAGNON SIGNATURE

JOHN J. CHAGNON No. 17651 LICENSED PROFESSIONAL ENGINEER STATE OF NEW HAMPSHIRE

REV.	DATE	DESCRIPTION	BY	CHK.
4	12-22-2025	BUILDING & SITE LAYOUT	SJR	JRC
3	10-08-2025	PLAN NOTES	SJR	JRC
2	09-22-2025	REVISED DRIVE THROUGH	SJR	JRC
1	09-05-2025	REVISED LAYOUT	SJR	JRC
0	07-18-2025	ISSUED FOR COMMENT	SJR	JRC

DRAWING ISSUE STATUS

PERMIT PLANS

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Road, Unit 14
Portsmouth, NH 03801
603-430-9282
WWW.HALEYWARD.COM

PROJECT

DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE

SITE PLAN

DATE	SCALE
JULY 2025	1"=20'

DRAWN BY	DESIGNED BY	CHECKED BY
SJR	---	JRC

PROJECT No.	FIELD BOOK / PAGE
5010156.1532	FB 276 PG 60

DRAWING No.	REV.
C102	4

P:\NH\5010156.NH\DWG\PLANSET\5010156.1532 SITE 2025.DWG, 2025.12.18, 2:52 PM

Plant List - Perennials

ID	Qty	Botanical Name	Common Name	Scheduled Size
ABI	36	Amsonia 'Blue Ice'	Blue Star Flower	1 Gal.
ACT	6	Actaea racemosa	Black Cohosh	1 Gal.
AJM	23	Ajuga reptans 'Mahogany'	Bugleweed	2 QT
AMH	33	Amsonia hubrichtii	Arkansas Amsonia	1 Gal.
AMT	53	Amsonia tabernaemontana	Blue Star Flower	1 Gal.
ARU	4	Aruncus dioicus	Goat's Beard	1 Gal.
ASD	25	Astilbe 'Deutschland'	Early White Astilbe	1 Gal.
AU	46	Arctostaphylos uva-ursi	Bearberry	2 QT
BAP	15	Baptisia australis	False Indigo	1 Gal.
CARY	4	Caryopteris x clandonensis 'First Choice'	First Choice Blue Beard	1 Gal.
CBZ	221	Carex flacca 'Blue Zinger'	Blue Zinger Sedge	1 Gal.
EPI	133	Epimedium rubrum	Barrenwort	2 QT
ERA	70	Eragrostis spectabilis	Purple Lovegrass	1 Gal.
GMB	22	Geranium macrorrhizum 'Bevan's Variety'	Bevan's Variety Geranium	1 Gal.
HMT	69	Hemerocallis 'Mary Todd'	Daylily	1 Gal.
ISP	72	Iberis sempervirons 'Purity'	Candytuft	1 Gal.
PVS	10	Panicum virgatum 'Shenandoah'	Shenandoah Switch Grass	2 Gal.
RBK	80	Rudbeckia 'Goldsturm'	Goldsturm Blackeyed Susan	1 Gal.
SEDK	35	Sedum kamtschaticum	Orange Stonecrop	2 QT
SEDS	247	Sedum sexangulare	Stonecrop	2 QT
SEDW	247	Sedum 'Weihenstephaner Gold'	Weihenstephaner Gold Stonecrop	1 Gal.
SS	409	Scilla siberica	Siberian Squill	Bulb
SSD	247	Sedum Sunsparkler 'Dazzleberry'	Dazzleberry Stonecrop	2 QT

Plant List - Trees and Shrubs

ID	Qty	Botanical Name	Common Name	Scheduled Size
ARB	3	Acer rubrum 'Bowhall'	Bowhall Maple	3 1/2" Cal.
COGN	1	Chamaecyparis obtusa 'Nana Gracilis'	Hinoki Cypress	2 1/2' Ht.
FG	13	Fothergilla gardenii	Dwarf Fothergilla	5 Gal.
GTH	4	Gleditsia triacanthos inermis 'Halka'	Halka Honeylocust	2" Cal.
HYP	36	Hypericum x 'Hidcote'	Hidcote St. John's Wort	2 Gal.
ICS	7	Ilex crenata 'Steeds'	Steeds Japanese Holly	5 Gal.
IGG	33	Ilex glabra 'Gem Box'	Gem Box Inkberry	1-2'
IGS	21	Ilex glabra 'Shamrock'	Shamrock Inkberry Holly	5 Gal.
IST	29	Ilex crenata 'Soft Touch'	Soft Touch Japanese Holly	2-3' Ht.
JBD	45	Juniperus communis depressa 'Blueberry Delight'	Blueberry Delight Juniper	2 Gal.
JPN	15	Juniperus procumbens 'Nana'	Dwarf Japanese Garden Juniper	3-4'
LT	1	Liriodendron tulipifera	Tulip Tree	2 1/2" Cal.
PHC	5	Pinus sylvestris 'Hillside Creeper'	Hillside Creeper Pine	2 Gal.
PJC	11	Pieris japonica 'Cavatine'	Cavatine Andromeda	3-4'
PLH	12	Pieris japonica 'Little Heath'	Little Heath Andromeda	3-4'
RCW	14	Rhododendron catawba 'White'	White CatawbaRhododendron	3-4'
RGL	260	Rhus aromatica 'Grow Low'	Grow Low Sumac	18"+ Ht.
RYP	23	Rhododendron yaku 'Princess'	Yaku Princess Rhody	2 GAL
SAW	7	Spiraea x bumalda 'Anthony Waterer'	Anthony Waterer Spirea	3-4'
TCG	9	Tilia cordata 'Greenspire'	Greenspire Littleleaf Linden	2 1/2" Cal.
VMR	3	Viburnum plicatum tomentosum 'Mariesii'	Marie's Doublefile Viburnum	3-4'

PLEASE NOTE: SHEET SIZE IS SCALED FOR ASME D . DO NOT REDUCE OR ENLARGE.

terra firma
landscape architecture

165-a Court Street Portsmouth, NH 03801
603.531.9109 | terrence@terrafirmalandarch.com

134 PLEASANT

134 PLEASANT STREET
PORTSMOUTH, NH

Project Title

TERRENCE PARKER 00254 — LICENSED LANDSCAPE ARCHITECT — STATE OF NEW HAMPSHIRE

Landscape Architect

Scale
1:240

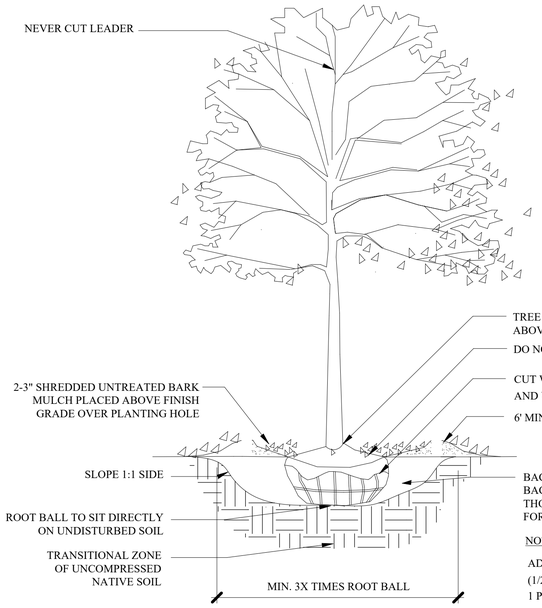
REV.	DATE	DESCRIPTION

NO.	DATE	ISSUE NOTE

Project Manager | Drawn By
Date 9/18/2025 | Reviewed By
Project ID 134 PLEASANT

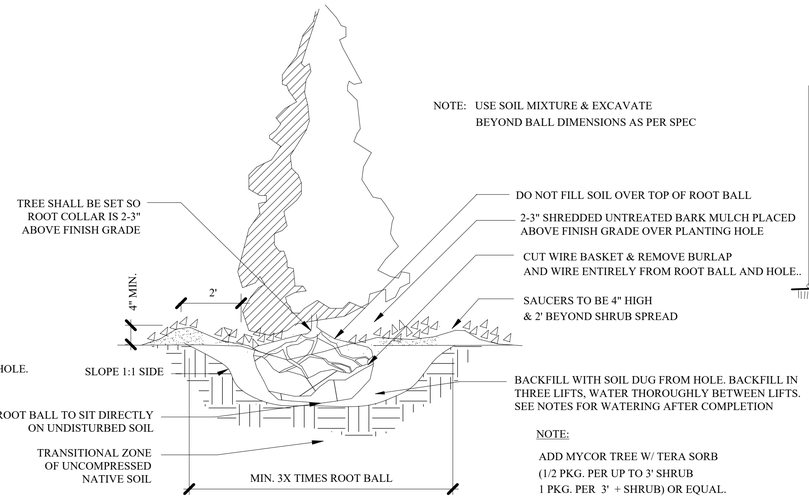
Sheet Title
LANDSCAPE PLAN

Sheet No.
L-1



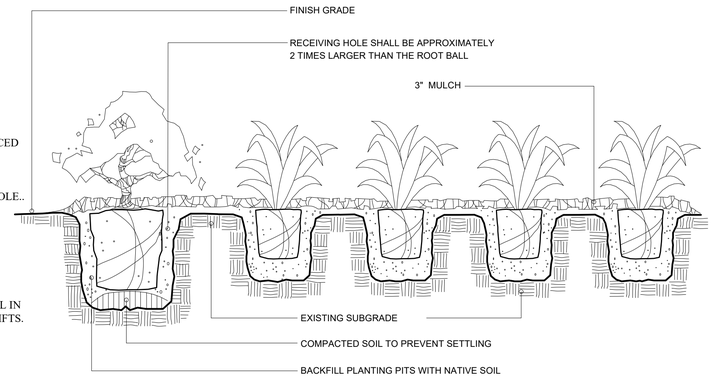
1 L-2

TREE PLANTING - 2"+ CAL.
SCALE: NTS



2 L-2

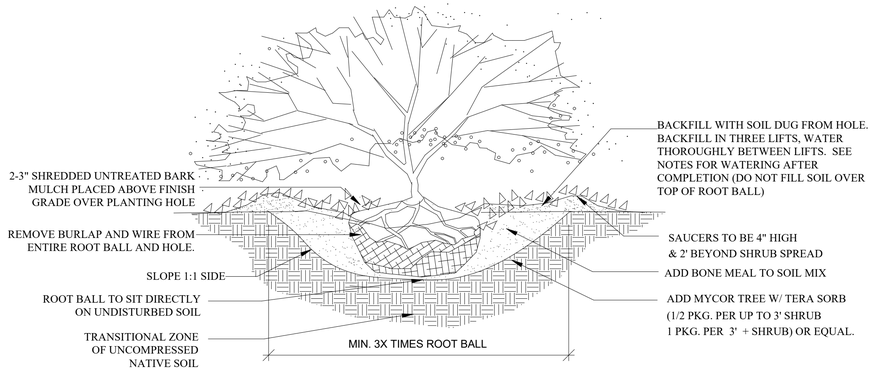
PYRAMIDAL EVERGREEN TREE PLANTING
SCALE: NTS



NOTE: SHRUBS SHALL BE PLANTED A MINIMUM OF 1" & NO MORE THAN 2" ABOVE FINISH GRADE, DEPENDING UPON SITE CONDITIONS.

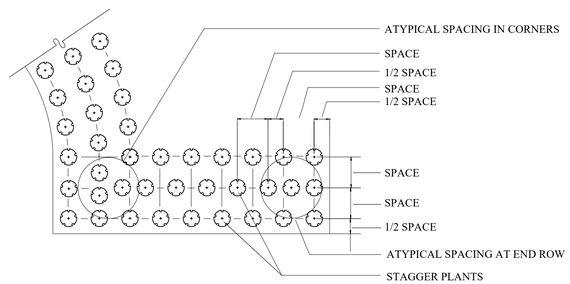
3 L-2

SHRUB/GROUND COVER PLANTING DETAIL
SCALE: NTS



4 L-2

B&B SHRUB PLANTING
SCALE: NTS



NOTES:

1. PLACE PLANTS IN BED AS SHOWN, SPACING AS SPECIFIED IN PLANT SCHEDULE.
2. GROUNDCOVER SHALL BE TRIANGULAR SPACED IN ROWS PARALLEL TO STRAIGHT EDGES AND SHALL BE EVENLY SPACED IN ROWS PARALLEL TO CURVE EDGES.

5 L-2

GROUND COVER SPACING DETAIL
SCALE: NTS

CITY OF PORTSMOUTH PLANTING REQUIREMENTS

1. All planting holes shall be dug by hand- NO MACHINES. The only exceptions are new construction where new planting pits, planting beds with granite curbing, and planting sites with Silva Cells are being created. If a machine is used to dig in any of these situations and planting depth needs to be raised the material in the bottom of the planting hole MUST be firmed with machine to prevent sinking of the root ball.
2. ALL Wire and Burlap shall be removed from the root ball AND planting hole.
3. The root ball of the tree shall be worked so that the root collar of the tree is visible and no girdling roots are present.
4. The root collar of the tree shall be 2"-3" above grade of planting hole for finished depth.
5. All plantings shall be backfilled with soil from the site and amended no more than 20% with Organic Compost. The only exceptions are new construction where engineered soil is being used in conjunction with Silva Cells and where new planting beds are being created.
6. All plantings shall be backfilled in three lifts and ALL lifts shall be watered so the planting will be set and free of air pockets- NO EXCEPTIONS.
7. An earth berm shall be placed around the perimeter of the planting hole except where curbed planting beds or pits are being used.
8. 2"-3" of mulch shall be placed over the planting area.
9. At the time the planting is complete the planting shall receive additional water to ensure complete hydration of the roots, backfill material and mulch layer.
10. Stakes and guys shall be used where appropriate and/or necessary. Guy material shall be nondamaging to the tree.
11. All planting stock shall be specimen quality, free of defects, and disease or injury. The City of Portsmouth, NH reserves the right to refuse/reject any plant material or planting action that fails to meet the standards set forth in the ANSI A300 Part 6 Standard Practices for Planting and Transplanting and/or The City of Portsmouth, NH Planting Requirements.

LANDSCAPE NOTES:

1. THE CONTRACTOR SHALL LOCATE AND VERIFY THE EXISTENCE OF ALL UTILITIES PRIOR TO STARTING WORK.
2. THE CONTRACTOR SHALL SUPPLY ALL PLANT MATERIALS IN QUANTITIES SUFFICIENT TO COMPLETE THE PLANTINGS SHOWN ON THE DRAWINGS.
3. ALL MATERIAL SHALL CONFORM TO THE GUIDELINES ESTABLISHED BY THE CURRENT AMERICAN STANDARD FOR NURSERY STOCK PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN.
4. ALL PLANT SUBSTITUTIONS MUST BE APPROVED BY THE LANDSCAPE ARCHITECT.
5. ALL PLANT MATERIALS SHALL BE EXACTLY AS SPECIFIED BY THE LANDSCAPE ARCHITECT. IF PLANT SPECIES CULTIVARS ARE FOUND TO VARY FROM THAT SPECIFIED AT ANY TIME DURING THE GUARANTEE PERIOD, THE LANDSCAPE ARCHITECT RESERVES THE RIGHT TO HAVE THE CONTRACTOR REPLACE THAT PLANT MATERIAL. THE LANDSCAPE ARCHITECT RESERVES THE RIGHT TO REJECT ANY PLANT DELIVERED TO THE SITE FOR AESTHETIC REASONS BEFORE PLANTING. THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR THE QUALITY FOR ALL THE PLANTS.
6. PLANTS SHALL BE SUBJECT TO INSPECTION AND APPROVAL AT THE PLACE OF GROWTH, UPON DELIVERY OR AT THE JOB SITE WHILE WORK IS ON-GOING TO CONFORMITY TO SPECIFIED QUALITY, SIZE AND VARIETY.
7. PLANTS FURNISHED IN CONTAINERS SHALL HAVE THE ROOTS WELL ESTABLISHED IN THE SOIL MASS AND SHALL HAVE AT LEAST ONE (1) GROWING SEASON. ROOT-BOUND PLANTS OR INADEQUATELY SIZED CONTAINERS TO SUPPORT THE PLANT MAY BE DEEMED UNACCEPTABLE.
8. NO PLANT SHALL BE PUT IN THE GROUND BEFORE GRADING HAS BEEN FINISHED AND APPROVED BY THE LANDSCAPE ARCHITECT.
9. ALL PLANTS SHALL BE INSTALLED AND DETAILED PER PROJECT SPECIFICATIONS.
10. ALL PLANTS SHALL BE WATERED THOROUGHLY TWICE DURING THE FIRST 24-HOUR PERIOD AFTER PLANTING. ALL PLANTS SHALL BE WATERED WEEKLY, OR MORE OFTEN IF NECESSARY, DURING THE FIRST GROWING SEASON.
11. ALL PLANTS SHALL BE GUARANTEED BY THE CONTRACTOR FOR NOT LESS THAN ONE FULL YEAR FROM THE TIME OF PROVISIONAL ACCEPTANCE. DURING THIS TIME, THE OWNER SHALL MAINTAIN ALL PLANT MATERIALS IN THE ABOVE MANNER. IT IS THE CONTRACTOR'S RESPONSIBILITY TO INSPECT THE PLANTS TO ENSURE PROPER CARE. IF THE CONTRACTOR IS DISSATISFIED WITH THE CARE GIVEN, HE SHALL IMMEDIATELY, AND IN SUFFICIENT TIME TO PERMIT THE CONDITION TO BE RECTIFIED, NOTIFY THE LANDSCAPE ARCHITECT IN WRITING OR OTHERWISE FORFEIT HIS CLAIM. LANDSCAPE CONTRACTOR SHALL PRUNE PLANTINGS OF DEAD LIMBS OR TWIGS DURING THE FIRST YEAR OF GROWTH.
12. FINAL ACCEPTANCE BY THE LANDSCAPE ARCHITECT WILL BE MADE UPON THE CONTRACTOR'S REQUEST AFTER ALL CORRECTIVE WORK HAS BEEN COMPLETED.
13. LANDSCAPE CONTRACTOR SHOULD REPLACE DEAD PLANTINGS IMMEDIATELY UPON OWNER DIRECTION WITHIN THE WARRANTY PERIOD AND AGAIN AT THE END OF THE GUARANTEE PERIOD, THE CONTRACTOR SHALL HAVE REPLACED ANY PLANT MATERIAL THAT IS MISSING, NOT TRUE TO SIZE AS SPECIFIED, THAT HAVE DIED, THAT HAVE LOST THEIR NATURAL SHAPE DUE TO DEAD BRANCHES, EXCESSIVE PRUNING OR INADEQUATE OR IMPROPER CARE, OR THAT ARE, IN THE OPINION OF THE LANDSCAPE ARCHITECT, IN UNHEALTHY OR UNSIGHTLY CONDITION.
14. ALL LANDSCAPE AREAS TO BE GRASS COMMON TO REGION EXCEPT FOR INTERIOR LANDSCAPED ISLANDS OR WHERE OTHER PLANT MATERIAL IS CALLED FOR.
15. ALL TREES AND SHRUBS TO BE PLANTED IN MULCH BEDS WITH DEFINED AND CUT EDGES TO SEPARATE TURF GRASS AREAS.
16. FOR ANY LANDSCAPE AREA SO DESIGNATED TO REMAIN, WHETHER ON OR OFF-SITE, REMOVE WEEDS, ROCKS, CONSTRUCTION ITEMS, ETC., THEN APPLY GRASS SEED OR PINE BARK MULCH AS DEPICTED ON PLANS.
17. LANDSCAPE CONTRACTOR SHALL FEED AND PRUNE EX. TREES, ON OR JUST OFF SITE, THAT HAVE EXPERIENCED ROOT BASE INTRUSION OR DAMAGE DURING CONSTRUCTION IMMEDIATELY AND FOR THE DURATION OF THE WARRANTY PERIOD AT THE DIRECTION OF THE LANDSCAPE ARCHITECT.
18. EXISTING TREES TO REMAIN SHALL BE PROTECTED WITH TEMPORARY SNOW FENCING AT THE EDGE OF THE EX. TREE CANOPY THE CONTRACTOR SHALL NOT STORE VEHICLES OR MATERIALS WITHIN THE LANDSCAPED AREAS. ANY DAMAGE TO EXISTING TREES, SHRUBS OR LAWN SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
19. ALL MULCH AREAS SHALL RECEIVE A 3" LAYER OF SHREDDED PINE BARK MULCH.
20. ALL WORK SHALL BE DONE IN STRICT ACCORDANCE WITH PROJECT SPECIFICATIONS.

NOTE: 6 INCHES OF TOPSOIL WITH 25% COMPOST TO BE ADDED TO ALL PLANTING AREAS

PLEASE NOTE: SHEET SIZE IS SCALED FOR ASME D . DO NOT REDUCE OR ENLARGE.

terra firma
landscape architecture

165.a Court Street Portsmouth, NH 03801
603.551.9109 | terrence@terrafirmalandarch.com

Project Title
134 PLEASANT
134 PLEASANT STREET
PORTSMOUTH, NH

TERRENCE PARKER — 00254 — NEW HAMPSHIRE — REGISTERED LANDSCAPE ARCHITECT

Landscape Architect

Scale: SEE DETAILS

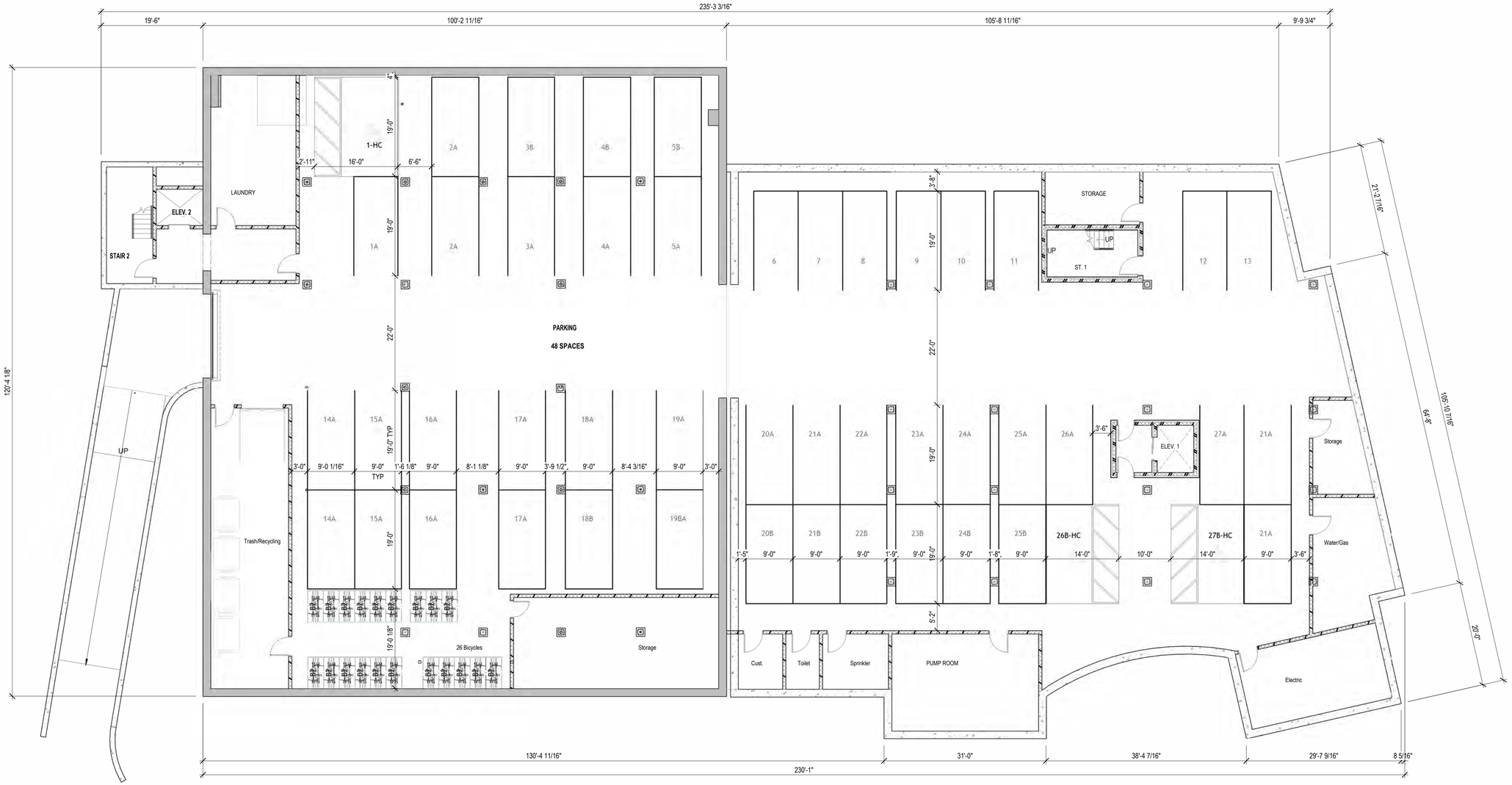
REV.	DATE	DESCRIPTION

NO.	DATE	ISSUE NOTE

Project Manager	Drawn By
Date: 9/18/2025	Reviewed By
Project ID: 134 PLEASANT	

Sheet Title: LANDSCAPE PLAN

Sheet No.: L-2



1 **PB BASEMENT**
3/32" = 1'-0"

ARCOVE ARCHITECTS
767 ISLINGTON ST, STE 2A
PORTSMOUTH NH 03801
603.988.0042
www.ARCove.com

134 PLEASANT STREET

PORTSMOUTH, NH 03801

PROJECT NO: 1028

OWNER

Double Mc, LLC
134 Pleasant Street
Portsmouth, New Hampshire 03801

CIVIL ENGINEERING

Haley Ward
200 Griffin Road, Unit
Portsmouth, NH 03801
(603) 430-9282

LANDSCAPE ARCHITECT

Terra Firma Landscape Architecture
163a Court St
Portsmouth, NH 03801
(603) 531-9109

BASEMENT FLOOR PLAN

SCALE: 3/32" = 1'-0"
DATE: 12/22/2025
DRAWN: HA
CHECKED: TK

PB0.1

1 **PB LEVEL 1**
3/32" = 1'-0"

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LEVEL 1 FLOOR PLAN

SCALE: 3/32" = 1'-0"
DATE: 12/22/2025
DRAWN: HA
CHECKED: TK

PB0.2

1 **PB LEVEL 2A**
3/32" = 1'-0"

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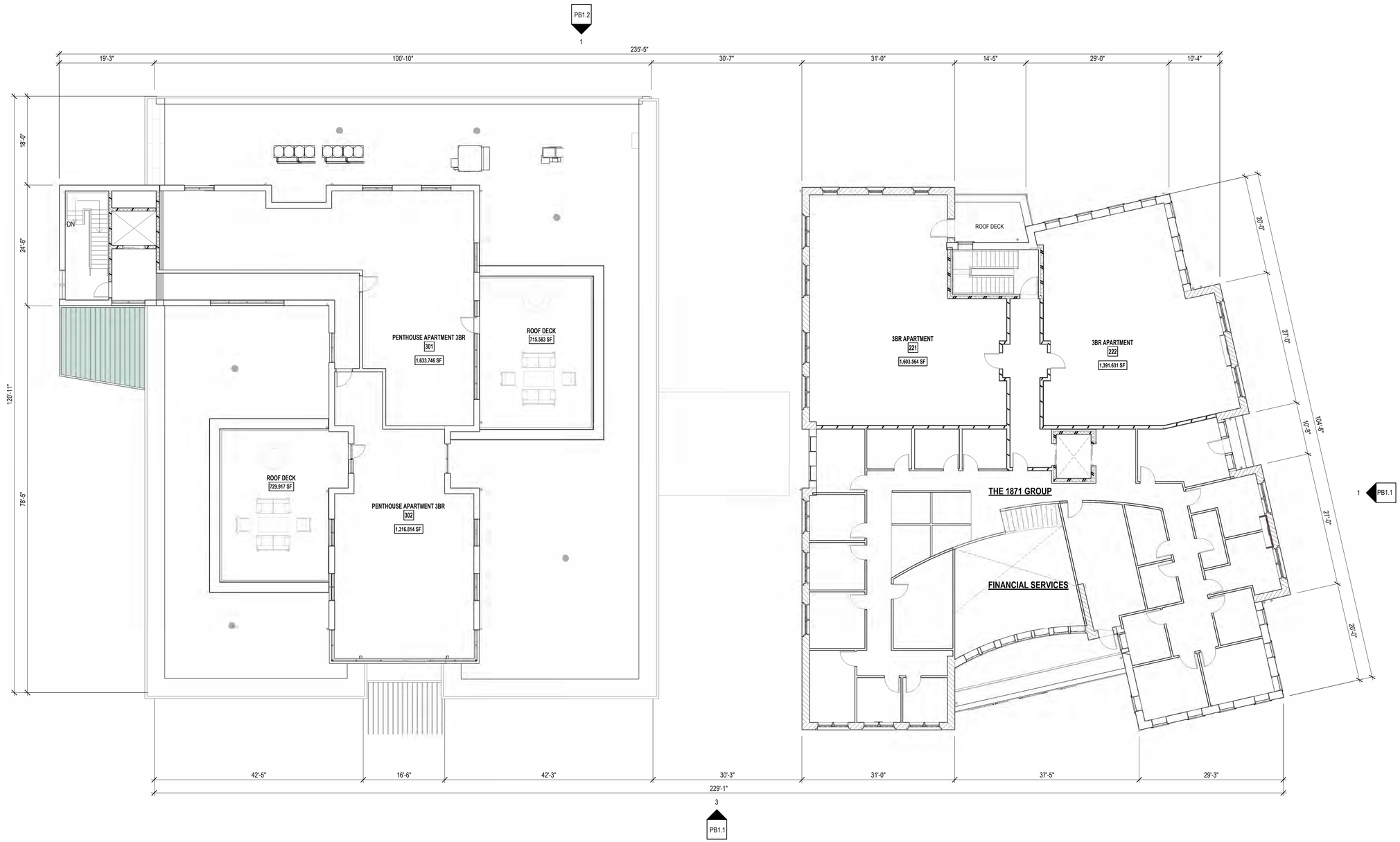
LEVEL 2A FLOOR PLAN

SCALE: 3/32" = 1'-0"
DATE: 12/22/2025
DRAWN: HA
CHECKED: TK

PB0.3



1 **PB LEVEL 2B**
3/32" = 1'-0"

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CIVIL ENGINEERING

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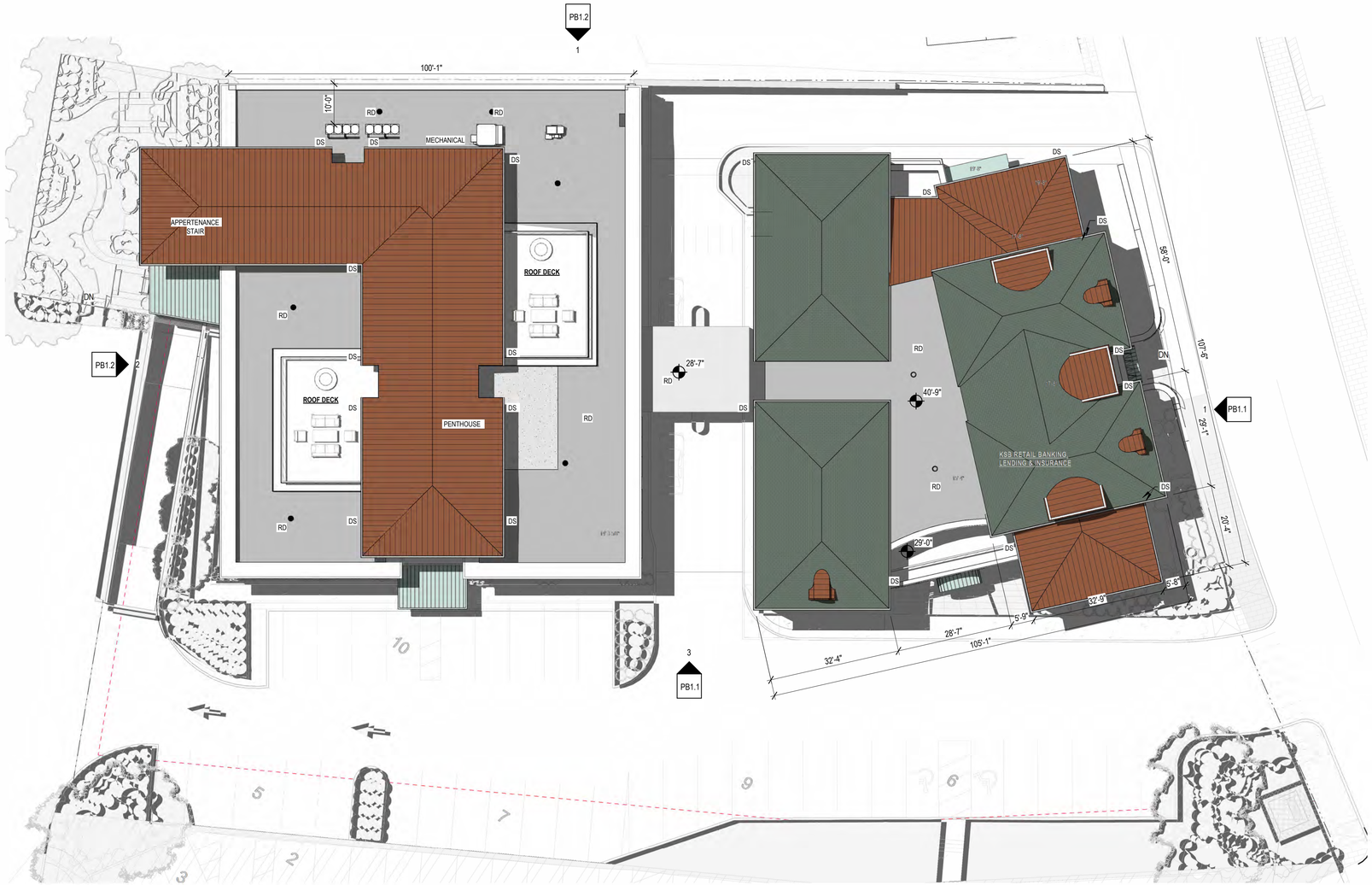
LEVEL 2B FLOOR PLAN

SCALE: 3/32" = 1'-0"
DATE: 12/22/2025
DRAWN: HA
CHECKED: TK

PB0.4



1 **PB ROOF PLAN**
1/16" = 1'-0"

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134 PLEASANT STREET

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ROOF PLAN

SCALE: 1/16" = 1'-0"
DATE: 12/22/2025
DRAWN: HA
CHECKED: TSK

PB0.5

1 **PB EAST ELEVATION**
3/32" = 1'-0"

3 **PB SOUTH ELEVATION**
3/32" = 1'-0"

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LANDSCAPE ARCHITECT

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SOUTH - EAST ELEVATIONS

SCALE: 3/32" = 1'-0"
DATE: 12/22/2025
DRAWN: HA
CHECKED: TK

PB1.1

1 **PB NORTH ELEVATION**
3/32" = 1'-0"

2 **PB WEST ELEVATION**
3/32" = 1'-0"

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134 PLEASANT STREET

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NORTH - WEST ELEVATIONS

SCALE: 3/32" = 1'-0"
DATE: 12/22/2025
DRAWN: HA
CHECKED: TK

PB1.2

VIEW FROM LANGDON HOUSE PB

VIEW FROM PLEASANT STREET AT TREADWELL HOUSE PB

VIEW FROM JUNKINS AVENUE AT CITY HALL PB

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VIGNETTES

SCALE:

DATE: 12/22/25
DRAWN: HA
CHECKED: TK

PB2.1

VIEW FROM PLEASANT STREET

3D View SW

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134 PLEASANT STREET

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LANDSCAPE ARCHITECT

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Portsmouth, NH 03801
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VIGNETTES

0' 4' 8' 16'
SCALE:

DATE: 12/22/2025
DRAWN: HA
CHECKED: Checker

PB2.2

VIEW FROM JUNKINS AVENUE AT CITY HALL PB

VIEW FROM JUNKINS AVENUE AT CITY HALL PB

2 LONGITUDINAL SECTION EW
3/32" = 1'-0"

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134 PLEASANT STREET

PORTSMOUTH, NH 03801

PROJECT NO: 1028

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(603) 430-9282

LANDSCAPE ARCHITECT

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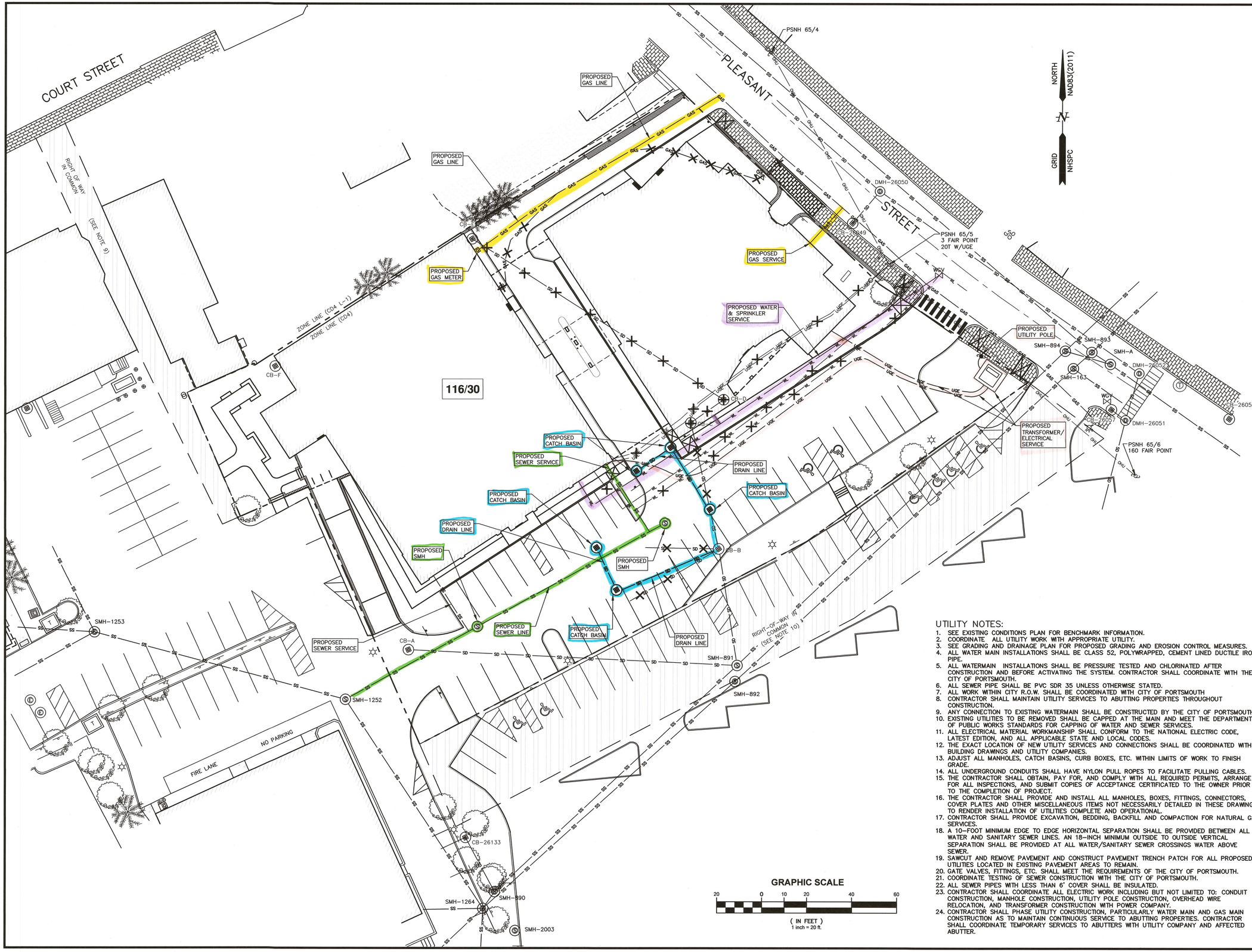
163a Court St
Portsmouth, NH 03801
(603) 531-9109

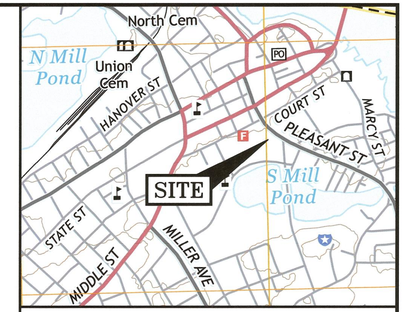
BUILDING SECTION & VIGNETTES

0' 4' 8' 16'
SCALE: 3/32" = 1'-0"

DATE: 12/22/2025
DRAWN: HA
CHECKED: TK

PB3.1





LOCATION MAP SCALE: 1" = 1,000'

NOTES:

1) THE CONTRACTOR SHALL NOTIFY DIG SAFE AT 1-888-DIG-SAFE (1-888-344-7233) AT LEAST 72 HOURS PRIOR TO COMMENCING ANY EXCAVATION ON PUBLIC OR PRIVATE PROPERTY WITHIN 100 FEET OF UNDERGROUND UTILITIES. THE EXCAVATOR IS RESPONSIBLE TO MAINTAIN MARKS. DIG SAFE TICKETS EXPIRE IN THIRTY DAYS.

2) UNDERGROUND UTILITY LOCATIONS ARE BASED UPON BEST AVAILABLE EVIDENCE AND ARE NOT FIELD VERIFIED. LOCATING AND PROTECTING ANY ABOVEGROUND OR UNDERGROUND UTILITIES IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND/OR THE OWNER. UTILITY CONFLICTS SHOULD BE REPORTED AT ONCE TO THE DESIGN ENGINEER.

3) CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE "NEW HAMPSHIRE STORMWATER MANUAL, VOLUME 3, EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION. (NHDES DECEMBER 2008).

4) PROVIDE TEMPORARY WATER AND SEWER SERVICES TO ALL IMPACTED PROPERTIES DURING NEW WATER AND SEWER MAIN INSTALLATION.

5) PROPOSED SEWER FLOW:

HEALTH CLUB: 11,000 S.F.
100 PATRONS X 5 GPD/PATRON
= 500 GPD

BANK AND BANK OFFICE: 12,500 S.F. X 2.5 GPD/100 S.F.
= 312 GPD

18 APARTMENTS X 216 GPD/APARTMENT
= 3,888 GPD

TOTAL FLOW 4,700 GPD.

6) APPLICANT, AND AUTHORIZED AGENTS, SHALL WORK WITH THE CITY OF PORTSMOUTH AND UTILITY PROVIDERS TO CONSTRUCT NEW, PERMANENT SERVICE CONNECTIONS TO PROPOSED WATER, SEWER, STORMDRAIN, POWER , AND COMMUNICATIONS.

UTILITY NOTES:

1. SEE EXISTING CONDITIONS PLAN FOR BENCHMARK INFORMATION.
2. COORDINATE ALL UTILITY WORK WITH APPROPRIATE UTILITY.
3. SEE GRADING AND DRAINAGE PLAN FOR PROPOSED GRADING AND EROSION CONTROL MEASURES.
4. ALL WATER MAIN INSTALLATIONS SHALL BE CLASS 52, POLYWRAPPED, CEMENT LINED DUCTILE IRON PIPE.
5. ALL WATERMAIN INSTALLATIONS SHALL BE PRESSURE TESTED AND CHLORINATED AFTER CONSTRUCTION AND BEFORE ACTIVATING THE SYSTEM. CONTRACTOR SHALL COORDINATE WITH THE CITY OF PORTSMOUTH.
6. ALL SEWER PIPE SHALL BE PVC SDR 35 UNLESS OTHERWISE STATED.
7. ALL WORK WITHIN CITY R.O.W. SHALL BE COORDINATED WITH CITY OF PORTSMOUTH
8. CONTRACTOR SHALL MAINTAIN UTILITY SERVICES TO ABUTTING PROPERTIES THROUGHOUT CONSTRUCTION.
9. ANY CONNECTION TO EXISTING WATERMAIN SHALL BE CONSTRUCTED BY THE CITY OF PORTSMOUTH.
10. EXISTING UTILITIES TO BE REMOVED SHALL BE CAPPED AT THE MAIN AND MEET THE DEPARTMENT OF PUBLIC WORKS STANDARDS FOR CAPPING OF WATER AND SEWER SERVICES.
11. ALL ELECTRICAL MATERIAL WORKMANSHIP SHALL CONFORM TO THE NATIONAL ELECTRIC CODE, LATEST EDITION, AND ALL APPLICABLE STATE AND LOCAL CODES.
12. THE EXACT LOCATION OF NEW UTILITY SERVICES AND CONNECTIONS SHALL BE COORDINATED WITH BUILDING DRAWINGS AND UTILITY COMPANIES.
13. ADJUST ALL MANHOLES, CATCH BASINS, CURB BOXES, ETC. WITHIN LIMITS OF WORK TO FINISH GRADE.
14. ALL UNDERGROUND CONDUITS SHALL HAVE NYLON PULL ROPES TO FACILITATE PULLING CABLES.
15. THE CONTRACTOR SHALL OBTAIN, PAY FOR, AND COMPLY WITH ALL REQUIRED PERMITS, ARRANGE FOR ALL INSPECTIONS, AND SUBMIT COPIES OF ACCEPTANCE CERTIFICATED TO THE OWNER PRIOR TO THE COMPLETION OF PROJECT.
16. THE CONTRACTOR SHALL PROVIDE AND INSTALL ALL MANHOLES, BOXES, FITTINGS, CONNECTORS, COVER PLATES AND OTHER MISCELLANEOUS ITEMS NOT NECESSARILY DETAILED IN THESE DRAWING TO RENDER INSTALLATION OF UTILITIES COMPLETE AND OPERATIONAL.
17. CONTRACTOR SHALL PROVIDE EXCAVATION, BEDDING, BACKFILL AND COMPACTION FOR NATURAL GAS SERVICES.
18. A 10-FOOT MINIMUM EDGE TO EDGE HORIZONTAL SEPARATION SHALL BE PROVIDED BETWEEN ALL WATER AND SANITARY SEWER LINES. AN 18-INCH MINIMUM OUTSIDE TO OUTSIDE VERTICAL SEPARATION SHALL BE PROVIDED AT ALL WATER/SANITARY SEWER CROSSINGS WATER ABOVE SEWER.
19. SAWCUT AND REMOVE PAVEMENT AND CONSTRUCT PAVEMENT TRENCH PATCH FOR ALL PROPOSED UTILITIES LOCATED IN EXISTING PAVEMENT AREAS TO REMAIN.
20. GATE VALVES, FITTINGS, ETC. SHALL MEET THE REQUIREMENTS OF THE CITY OF PORTSMOUTH.
21. COORDINATE TESTING OF SEWER CONSTRUCTION WITH THE CITY OF PORTSMOUTH.
22. ALL SEWER PIPES WITH LESS THAN 6' COVER SHALL BE INSULATED.
23. CONTRACTOR SHALL COORDINATE ALL ELECTRIC WORK INCLUDING BUT NOT LIMITED TO: CONDUIT CONSTRUCTION, MANHOLE CONSTRUCTION, UTILITY POLE CONSTRUCTION, OVERHEAD WIRE RELOCATION, AND TRANSFORMER CONSTRUCTION WITH POWER COMPANY.
24. CONTRACTOR SHALL PHASE UTILITY CONSTRUCTION, PARTICULARLY WATER MAIN AND GAS MAIN CONSTRUCTION AS TO MAINTAIN CONTINUOUS SERVICE TO ABUTTING PROPERTIES. CONTRACTOR SHALL COORDINATE TEMPORARY SERVICES TO ABUTTERS WITH UTILITY COMPANY AND AFFECTED ABUTTER.

REV.	DATE	DESCRIPTION	BY	CHK
1	12-22-2025	ISSUED FOR APPROVAL	SJR	JRC
0	09-05-2025	ISSUED FOR COMMENT	SJR	JRC

DRAWING ISSUE STATUS

PERMIT PLANS

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Road, Unit 14
Portsmouth, NH 03801
603-430-9282
WWW.HALEYWARD.COM

PROJECT

DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE

UTILITY PLAN

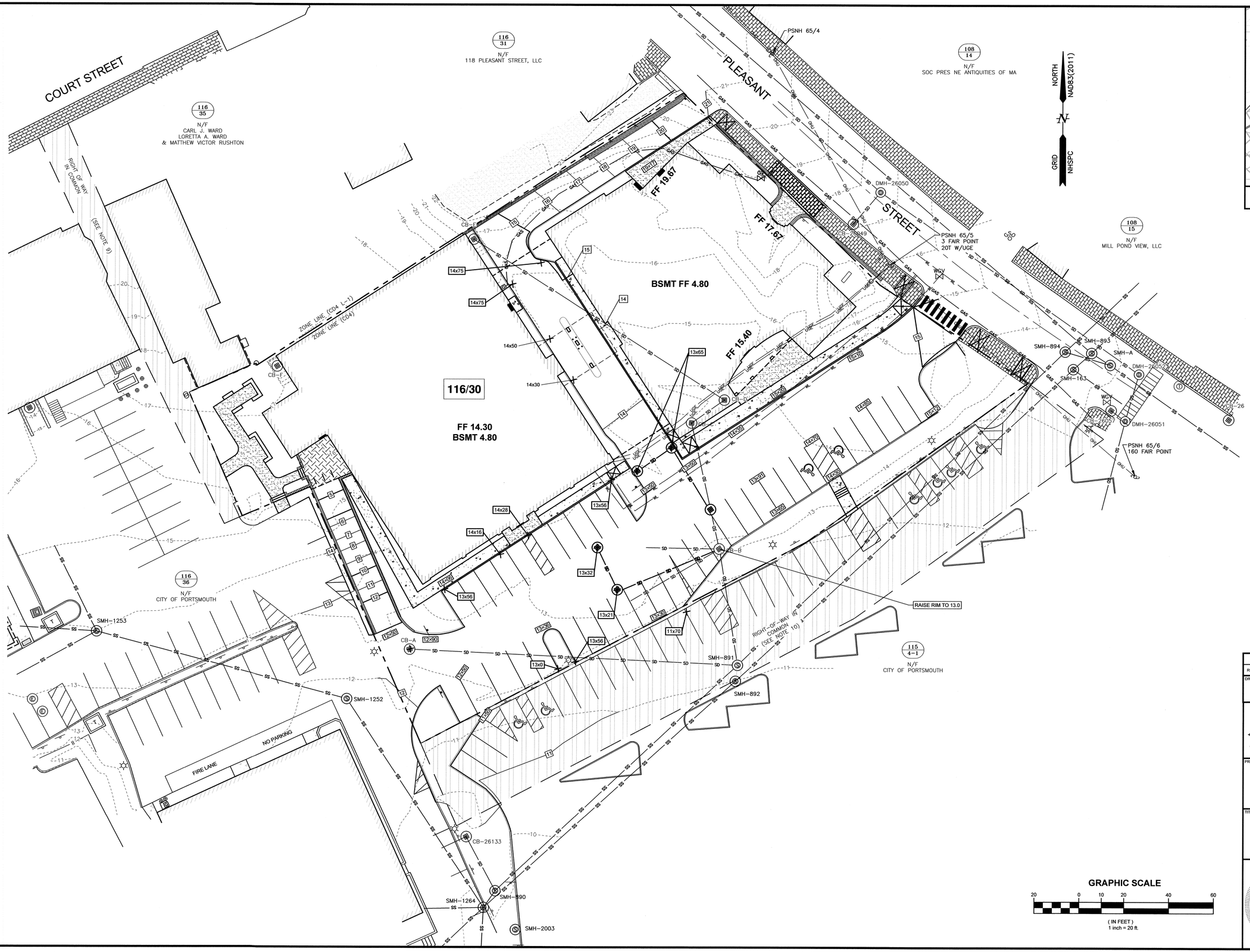
JOHN R. CHAGNON NO. 9651 LICENSED PROFESSIONAL ENGINEER STATE OF NEW HAMPSHIRE

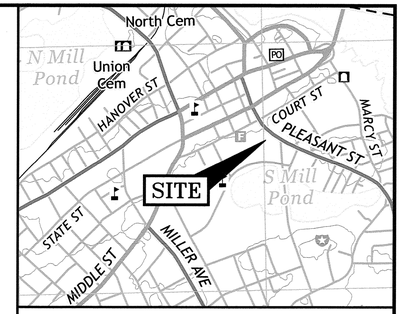
DATE	SCALE
JULY 2025	1"=20'

DRAWN BY	DESIGNED BY	CHECKED BY
SJR	---	JRC

PROJECT No. 5010156.1532 | FIELD BOOK / PAGE FB 276 PG 60

DRAWING No. **C103** | REV. **2**





LOCATION MAP SCALE: 1" = 1,000'

NOTES:

1) THE CONTRACTOR SHALL NOTIFY DIG SAFE AT 1-888-DIG-SAFE (1-888-344-7233) AT LEAST 72 HOURS PRIOR TO COMMENCING ANY EXCAVATION ON PUBLIC OR PRIVATE PROPERTY WITHIN 100 FEET OF UNDERGROUND UTILITIES. THE EXCAVATOR IS RESPONSIBLE TO MAINTAIN MARKS. DIG SAFE TICKETS EXPIRE IN THIRTY DAYS.

2) UNDERGROUND UTILITY LOCATIONS ARE BASED UPON BEST AVAILABLE EVIDENCE AND ARE NOT FIELD VERIFIED. LOCATING AND PROTECTING ANY ABOVEGROUND OR UNDERGROUND UTILITIES IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND/OR THE OWNER. UTILITY CONFLICTS SHOULD BE REPORTED AT ONCE TO THE DESIGN ENGINEER.

3) CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE "NEW HAMPSHIRE STORMWATER MANUAL, VOLUME 3, EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION. (NHDES DECEMBER 2008).

4) OWNER/DEVELOPER SHALL MAINTAIN & CLEAR SNOW FROM PRIVATE ROADWAYS.

REV.	DATE	DESCRIPTION	BY	CHK
0	12-22-25	ISSUED FOR APPROVAL	SJR	JRC

DRAWING ISSUE STATUS

PERMIT PLANS

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WWW.HALEYWARD.COM
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Portsmouth, NH 03801
603-430-9282

PROJECT

DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE

GRADING PLAN

STATE OF NEW HAMPSHIRE – JOHN R. CHAGNON – NO. 9651 – LICENSED PROFESSIONAL ENGINEER

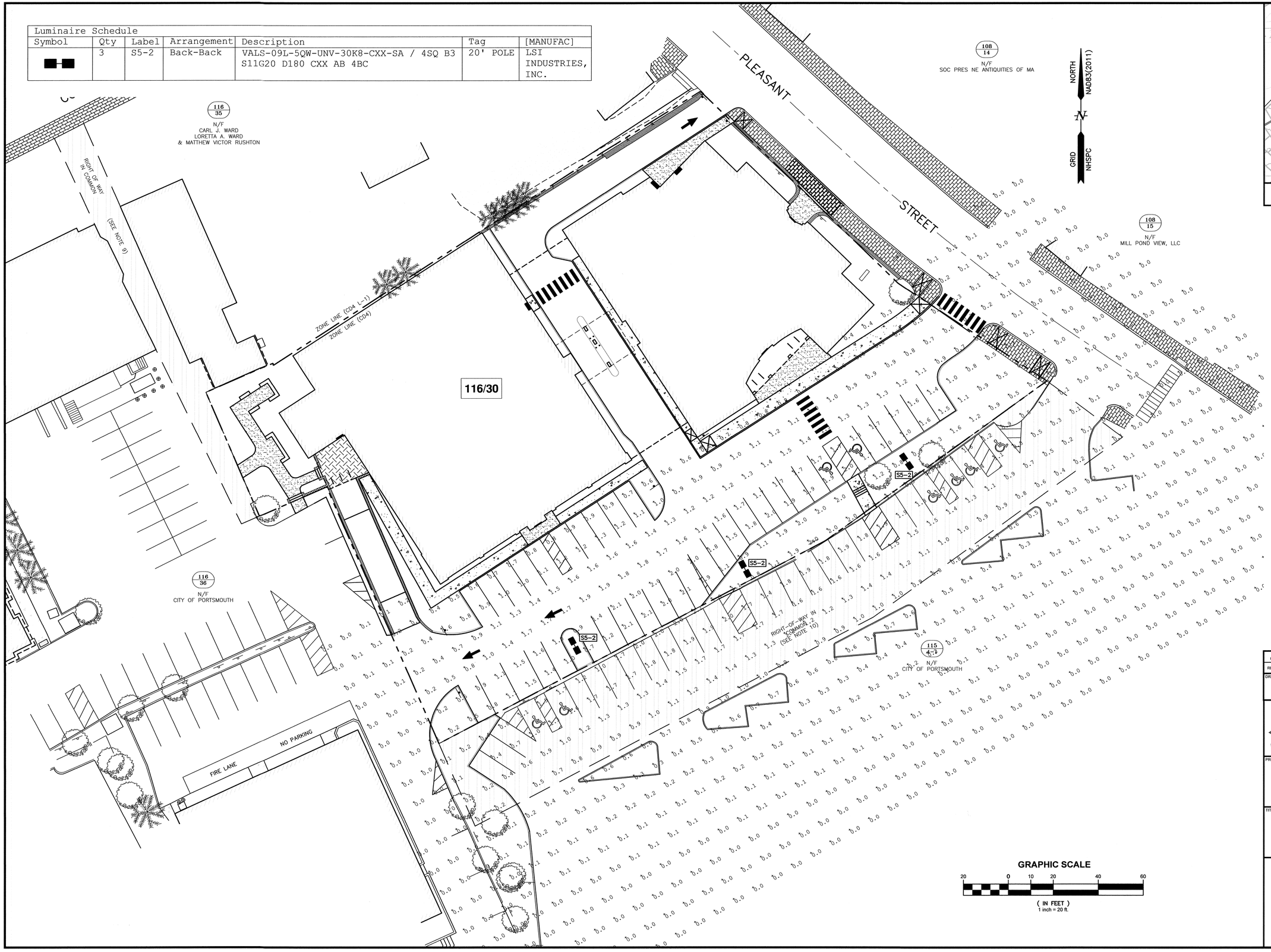
DATE	SCALE
JULY 2025	1"=20'

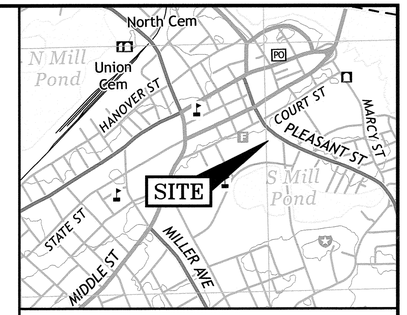
DRAWN BY	DESIGNED BY	CHECKED BY
SJR	---	JRC

PROJECT No.	FIELD BOOK / PAGE
5010156.1532	FB 276 PG 60

DRAWING No.	REV
C104	1

Luminaire Schedule						
Symbol	Qty	Label	Arrangement	Description	Tag	[MANUFAC]
■-■	3	S5-2	Back-Back	VALS-09L-5QW-UNV-30K8-CXX-SA / 4SQ B3 S11G20 D180 CXX AB 4BC	20' POLE	LSI INDUSTRIES, INC.





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NOTES:

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2) UNDERGROUND UTILITY LOCATIONS ARE BASED UPON BEST AVAILABLE EVIDENCE AND ARE NOT FIELD VERIFIED. LOCATING AND PROTECTING ANY ABOVEGROUND OR UNDERGROUND UTILITIES IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND/OR THE OWNER. UTILITY CONFLICTS SHOULD BE REPORTED AT ONCE TO THE DESIGN ENGINEER.

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4) ALL LIGHTING SHALL BE SHIELDED TO MINIMIZE LIGHT TRESPASS AND DIRECT GLARE BEYOND THE PROPERTY.

5) ALL LIGHTS SHALL BE DARK SKY COMPLIANT AND DIRECTED DOWNWARD.

6) LIGHTING LAYOUT & FIXTURE SPECIFICATIONS DESIGNED/PROVIDED BY: EXPOSURE 2 LIGHTING. CONTACT: KEN SWEENEY 603-601-8080.

7) POLE MOUNTED LIGHTS SHALL HAVE A MAXIMUM FIXTURE HEIGHT OF 20'.

8) LIGHTS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL REGULATIONS.

9) NUMBERS ON PLAN REPRESENT INITIAL FOOT CANDLE VALUES AT 3 FEET ABOVE GRADE.

GRAPHIC SCALE

20 0 10 20 40 60

(IN FEET)
1 inch = 20 ft.

REV.	DATE	DESCRIPTION	BY	CHK
0	12-22-25	ISSUED FOR COMMENT	SJR	JRC

DRAWING ISSUE STATUS

PERMIT PLANS

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Road, Unit 14
Portsmouth, NH 03801
603-430-9282
WWW.HALEYWARD.COM

PROJECT

DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE

LIGHTING PLAN

DATE: JULY 2025		SCALE: 1"=20'
DRAWN BY: SJR	DESIGNED BY: ---	CHECKED BY: JRC
PROJECT No.: 5010156.1532	FIELD BOOK / PAGE: FB 276 PG 60	
DRAWING No.: **C105**		REV.: **0**

ARCOVE
ARCHITECTS

767 ISLINGTON ST, STE 2A
PORTSMOUTH NH 03801
603.988.0042
www.ARCove.com

134 PLEASANT STREET

PORTSMOUTH, NH 03801

PROJECT NO: 1028

OWNER

Double Mc, LLC
134 Pleasant Street
Portsmouth, New Hampshire 03801

CIVIL ENGINEERING

HALEY WARD
Haley Ward
200 Griffin Road, Unit
Portsmouth, NH 03801
(603) 430-9282

LANDSCAPE ARCHITECT

Terra Firma Landscape Architecture
163a Court St
Portsmouth, NH 03801
(603) 531-9109

NIGHTIME LIGHTING VIEWS

0' 4' 8' 16'
SCALE:

DATE: 12/22/2025
DRAWN: HA
CHECKED: Checker

C105A

EROSION CONTROL NOTES

CONSTRUCTION SEQUENCE

DO NOT BEGIN CONSTRUCTION UNTIL ALL LOCAL, STATE AND FEDERAL PERMITS HAVE BEEN APPLIED FOR AND RECEIVED.

THE FOLLOWING REPRESENTS THE GENERAL OBSERVATION AND REPORTING PRACTICES THAT SHALL BE FOLLOWED AS PART OF THIS PROJECT:

1. OBSERVATIONS OF THE PROJECT FOR COMPLIANCE WITH THE CITY OF PORTSMOUTH CMMP SHALL BE MADE BY THE CONTRACTOR AT LEAST ONCE A WEEK OR WITHIN 24 HOURS OF A STORM 0.25 INCHES OR GREATER;
2. AN OBSERVATION REPORT SHALL BE MADE AFTER EACH OBSERVATION AND DISTRIBUTED TO THE ENGINEER, THE OWNER, AND THE CONTRACTOR;
3. A REPRESENTATIVE OF THE SITE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE AND REPAIR ACTIVITIES;
4. IF A REPAIR IS NECESSARY, IT SHALL BE INITIATED WITHIN 24 HOURS OF REPORT.

INSTALL PERIMETER CONTROLS, i.e., SILTSOXX AND CATCH BASIN PROTECTION AROUND THE LIMITS OF DISTURBANCE BEFORE ANY EARTH MOVING OPERATIONS. THE USE OF HAYBALES IS NOT ALLOWED.

THE CONTRACTOR SHALL CONSTRUCT STABILIZED CONSTRUCTION ENTRANCE(S) PRIOR TO ANY EXCAVATION ACTIVITIES. PLACE FODS AS NEEDED.

CUT AND GRUB ALL TREES, SHRUBS, SAPLINGS, BRUSH, VINES AND REMOVE OTHER DEBRIS AND RUBBISH AS REQUIRED. DEMOLISH BUILDINGS AND FENCES AS NEEDED. CONSTRUCT FOUNDATIONS.

ROUGH GRADE SITE.

LAYOUT AND INSTALL ALL BURIED UTILITIES TO THE PROPOSED BUILDING FOUNDATIONS. CAP AND MARK TERMINATIONS OR LOG SWING TIES.

CONSTRUCT BUILDING.

CONNECT UTILITIES.

PLACE BINDER LAYER OF PAVEMENT FOR SIDEWALKS.

AFTER BUILDINGS ARE COMPLETED, FINISH ALL REMAINING SIDEWALK WORK.

REMOVE TRAPPED SEDIMENTS FROM COLLECTION DEVICES AS APPROPRIATE, AND THEN REMOVE TEMPORARY EROSION CONTROL MEASURES UPON COMPLETION OF FINAL STABILIZATION OF THE SITE.

PROJECT DESCRIPTION

THE PROJECT CONSISTS OF A BUILDING REDEVELOPMENT WITH ASSOCIATED UTILITIES AND PARKING.

THE TOTAL AREA TO BE DISTURBED IS APPROXIMATELY 0.94 ACRES.

BASED ON THE USCS WEB SOIL SURVEY THE SOILS ON SITE CONSIST OF URBAN LAND WHICH HAS AN UNSPECIFIED HYDROLOGIC SOIL GROUP RATING, ASSUMED D.

THE STORMWATER RUNOFF FROM THE SITE WILL BE DISCHARGED VIA A CLOSED DRAINAGE SYSTEM TO THE CITY OF PORTSMOUTH CLOSED DRAINAGE SYSTEM WHICH ULTIMATELY FLOWS TO THE PISCATAQUA RIVER.

GENERAL CONSTRUCTION NOTES

THE EROSION CONTROL PROCEDURES SHALL CONFORM TO SECTION 645 OF THE "STANDARD SPECIFICATION FOR ROAD AND BRIDGE CONSTRUCTION" OF THE NHDOT, AND "STORM WATER MANAGEMENT AND EROSION AND SEDIMENT CONTROL HANDBOOK FOR URBAN AND DEVELOPING AREAS IN NEW HAMPSHIRE". THE PROJECT IS TO BE MANAGED IN A MANNER THAT MEETS THE REQUIREMENTS AND INTENT OF RSA 430:53 AND CHAPTER AGR 3800 RELATIVE TO INVASIVE SPECIES.

DURING CONSTRUCTION AND THEREAFTER, EROSION CONTROL MEASURES ARE TO BE IMPLEMENTED AS NOTED. THE SMALLEST PRACTICAL AREA OF LAND SHOULD BE EXPOSED AT ANY ONE TIME DURING DEVELOPMENT. NO DISTURBED AREA SHALL BE LEFT UNSTABILIZED FOR MORE THAN 45 DAYS.

ANY DISTURBED AREAS WHICH ARE TO BE LEFT TEMPORARILY, AND WHICH WILL BE REGRADED LATER DURING CONSTRUCTION SHALL BE MACHINE HAY MULCHED AND SEEDED WITH RYE GRASS TO PREVENT EROSION.

THE PROJECT IS TO BE MANAGED IN A MANNER THAT MEETS THE REQUIREMENTS AND INTENT OF RSA 430:53 AND CHAPTER AGR 3800 RELATIVE TO INVASIVE SPECIES.

DUST CONTROL: DUST CONTROL MEASURES SHALL INCLUDE BUT ARE NOT LIMITED TO SPRINKLING WATER ON EXPOSED AREAS, COVERING LOADED DUMP TRUCKS LEAVING THE SITE, AND TEMPORARY MULCHING.
DUST CONTROL MEASURES SHALL BE UTILIZED SO AS TO PREVENT THE MIGRATION OF DUST FROM THE SITE TO ABUTTING AREAS.
IF TEMPORARY STABILIZATION PRACTICES, SUCH AS TEMPORARY VEGETATION AND MULCHING, DO NOT ADEQUATELY REDUCE DUST GENERATION, APPLICATION OF WATER OR CALCIUM CHLORIDE SHALL BE APPLIED IN ACCORDANCE WITH BEST MANAGEMENT PRACTICES.

SILTSOXX SHALL BE PERIODICALLY INSPECTED DURING THE LIFE OF THE PROJECT AND AFTER EACH STORM. ALL DAMAGED SILTSOXX SHALL BE REPAIRED. SEDIMENT DEPOSITS SHALL PERIODICALLY BE REMOVED AND DISPOSED IN A SECURED LOCATION.

ALL FILLS SHALL BE PLACED AND COMPACTED TO REDUCE EROSION, SLIPPAGE, SETTLEMENT, SUBSIDENCE OR OTHER RELATED PROBLEMS.

ALL NON-STRUCTURAL, SITE-FILL SHALL BE PLACED AND COMPACTED TO 90% MODIFIED PROCTOR DENSITY IN LAYERS NOT EXCEEDING 18 INCHES IN THICKNESS UNLESS OTHERWISE NOTED.

FROZEN MATERIAL OR SOFT, MUCKY OR HIGHLY COMPRESSIBLE MATERIAL, TRASH, WOODY DEBRIS, LEAVES, BRUSH OR ANY DELETERIOUS MATTER SHALL NOT BE INCORPORATED INTO FILLS.

FILL MATERIAL SHALL NOT BE PLACED ON FROZEN FOUNDATION SUBGRADE.

DURING CONSTRUCTION AND UNTIL ALL DEVELOPED AREAS ARE FULLY STABILIZED, ALL EROSION CONTROL MEASURES SHALL BE INSPECTED WEEKLY AND AFTER EACH ONE HALF INCH OF RAINFALL.

THE CONTRACTOR SHALL MODIFY OR ADD EROSION CONTROL MEASURES AS NECESSARY TO ACCOMMODATE PROJECT CONSTRUCTION.

AN AREA SHALL BE CONSIDERED STABLE IF ONE OF THE FOLLOWING HAS OCCURRED:

- BASE COURSE GRAVELS HAVE BEEN INSTALLED ON AREAS TO BE PAVED
- A MINIMUM OF 85% VEGETATED GROWTH HAS BEEN ESTABLISHED
- A MINIMUM OF 3 INCHES OF NON-EROSIVE MATERIAL SUCH AS STONE OR RIPRAP HAS BEEN INSTALLED
- EROSION CONTROL BLANKETS HAVE BEEN INSTALLED.
- IN AREAS TO BE PAVED, "STABLE" MEANS THAT BASE COURSE GRAVELS MEETING THE REQUIREMENTS OF NHDOT STANDARD FOR ROAD AND BRIDGE CONSTRUCTION, 2016, ITEM 304.2 HAVE BEEN INSTALLED.

STABILIZATION SHALL BE INITIATED ON ALL STOCKPILES, AND DISTURBED AREAS, WHERE CONSTRUCTION ACTIVITY SHALL NOT OCCUR FOR MORE THAN TWENTY-ONE (21) CALENDAR DAYS BY THE FOURTEENTH (14TH) DAY AFTER CONSTRUCTION ACTIVITY HAS PERMANENTLY OR TEMPORARILY CEASED IN THAT AREA.

STABILIZATION MEASURES TO BE USED INCLUDE:

- TEMPORARY SEEDING;
- MULCHING.

MAINTENANCE AND PROTECTION

THE SILTSOXX BARRIER AND CATCH BASIN FILTERS SHALL BE CHECKED AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL.

THE CATCH BASIN INLET BASKET SHALL BE INSPECTED WITHIN 24 HOURS AFTER EACH RAINFALL OR DAILY DURING EXTENDED PERIODS OF PRECIPITATION. REPAIRS SHALL BE MADE IMMEDIATELY, AS NECESSARY, TO PREVENT PARTICLES FROM REACHING THE DRAINAGE SYSTEM AND/OR CAUSING SURFACE FLOODING, SEDIMENT DEPOSITS SHALL BE REMOVED AFTER EACH STORM EVENT, OR MORE OFTEN IF THE FABRIC BECOMES CLOGGED.

WINTER NOTES

ALL PROPOSED VEGETATED AREAS THAT DO NOT EXHIBIT A MINIMUM OF 85% VEGETATED GROWTH BY OCTOBER 15, OR WHICH ARE DISTURBED AFTER OCTOBER 15, SHALL BE STABILIZED BY SEEDING AND INSTALLING EROSION CONTROL BLANKETS ON SLOPES GREATER THAN 3:1, AND SEEDING AND PLACING 3 TO 4 TONS OF MULCH PER ACRE, SECURED WITH ANCHORED NETTING, ELSEWHERE. THE INSTALLATION OF EROSION CONTROL BLANKETS OR MULCH AND NETTING SHALL NOT OCCUR OVER ACCUMULATED SNOW OR ON FROZEN GROUND AND SHALL BE COMPLETED IN ADVANCE OF THAW OR SPRING MELT EVENTS.

ALL DITCHES OR SWALES WHICH DO NOT EXHIBIT A MINIMUM OF 85 PERCENT VEGETATIVE GROWTH BY OCTOBER 15, OR WHICH ARE DISTURBED AFTER OCTOBER 15, SHALL BE STABILIZED TEMPORARILY WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW CONDITIONS;

AFTER OCTOBER 15, INCOMPLETE ROAD OR PARKING SURFACES, WHERE WORK HAS STOPPED FOR THE WINTER SEASON, SHALL BE PROTECTED WITH A MINIMUM OF 3 INCHES OF CRUSHED GRAVEL PER NHDOT ITEM 304.3, OR IF CONSTRUCTION IS TO CONTINUE THROUGH THE WINTER SEASON BE CLEARED OF ANY ACCUMULATED SNOW AFTER EACH STORM EVENT;

STOCKPILES

1. LOCATE STOCKPILES A MINIMUM OF 50 FEET AWAY FROM CATCH BASINS, SWALES, AND CULVERTS,
2. ALL STOCKPILES SHOULD BE SURROUNDED WITH TEMPORARY EROSION CONTROL MEASURES PRIOR TO THE ONSET OF PRECIPITATION.
3. PERIMETER BARRIERS SHOULD BE MAINTAINED AT ALL TIMES, AND ADJUSTED AS NEEDED TO ACCOMMODATE THE DELIVERY AND REMOVAL OF MATERIALS FROM THE STOCKPILE. THE INTEGRITY OF THE BARRIER SHOULD BE INSPECTED AT THE END OF EACH WORKING DAY.
4. PROTECT ALL STOCKPILES FROM STORMWATER RUN-OFF USING TEMPORARY EROSION CONTROL MEASURES SUCH AS BERMS, SILT SOCK, OR OTHER APPROVED PRACTICE TO PREVENT MIGRATION OF MATERIAL BEYOND THE IMMEDIATE CONFINES OF THE STOCKPILES.

CONCRETE WASHOUT AREA

THE FOLLOWING ARE THE ONLY NON-STORMWATER DISCHARGES ALLOWED. ALL OTHER NON-STORMWATER DISCHARGES ARE PROHIBITED ON SITE:

1. THE CONCRETE DELIVERY TRUCKS SHALL, WHENEVER POSSIBLE, USE WASHOUT FACILITIES AT THEIR OWN PLANT OR DISPATCH FAILITY;
2. IF IT IS NECESSARY, SITE CONTRACTOR SHALL DESIGNATE SPECIFIC WASHOUT AREAS AND DESIGN FACILITIES TO HANDLE ANTICIPATED WASHOUT WATER;
3. CONTRACTOR SHALL LOCATE WASHOUT AREAS AT LEAST 150 FEET AWAY FROM STORM DRAINS, SWALES AND SURFACE WATERS OR DELINEATED WETLANDS;
4. INSPECT WASHOUT FACILITIES DAILY TO DETECT LEAKS OR TEARS AND TO IDENTIFY WHEN MATERIALS NEED TO BE REMOVED.

ALLOWABLE NON-STORMWATER DISCHARGES

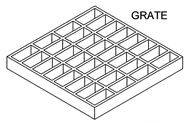
1. FIRE-FIGHTING ACTIVITIES;
2. FIRE HYDRANT FLUSHING;
3. WATERS USED TO WASH VEHICLES WHERE DETERGENTS ARE NOT USED;
4. WATER USED TO CONTROL DUST;
5. POTABLE WATER INCLUDING UNCONTAMINATED WATER LINE FLUSHING;
6. ROUTINE EXTERNAL BUILDING WASH DOWN WHERE DETERGENTS ARE NOT USED;
7. PAVEMENT WASH WATERS WHERE DETERGENTS ARE NOT USED;
8. UNCONTAMINATED AIR CONDITIONING/COMPRESSOR CONDENSATION;
9. UNCONTAMINATED GROUND WATER OR SPRING WATER;
10. FOUNDATION OR FOOTING DRAINS WHICH ARE UNCONTAMINATED;
11. UNCONTAMINATED EXCAVATION DEWATERING;
12. LANDSCAPE IRRIGATION.

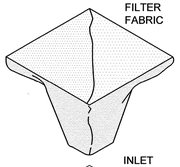
WASTE DISPOSAL

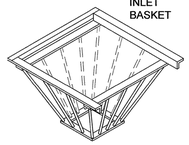
1. WASTE MATERIAL
   - ALL WASTE MATERIALS SHALL BE COLLECTED AND STORED IN SECURELY LIDDED RECEPTACLES. ALL TRASH AND CONSTRUCTION DEBRIS FROM THE SITE SHALL BE DEPOSITED IN A DUMPSTER;
   - NO CONSTRUCTION WASTE MATERIALS SHALL BE BURIED ON SITE;
   - ALL PERSONNEL SHALL BE INSTRUCTED REGARDING THE CORRECT PROCEDURE FOR WASTE DISPOSAL BY THE SUPERINTENDENT.
2. HAZARDOUS WASTE
   - ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN THE MANNER SPECIFIED BY LOCAL OR STATE REGULATION OR BY THE MANUFACTURER;
   - SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES BY THE SUPERINTENDENT.
3. SANITARY WASTE
   - ALL SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS A MINIMUM OF ONCE PER WEEK BY A LICENSED SANITARY WASTE MANAGEMENT CONTRACTOR.

BLASTING NOTES

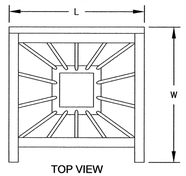
1. CONTRACTOR SHALL CONTACT THE NHDES AND/OR LOCAL JURISDICTION PRIOR TO
2. COMMENCING ANY BLASTING ACTIVITIES.
3. FOR ANY PROJECT FOR WHICH BLASTING OF BEDROCK IS ANTICIPATED, THE APPLICANT
4. SHALL SUBMIT A BLASTING PLAN THAT IDENTIFIES:
   - WHERE THE BLASTING ACTIVITIES ARE ANTICIPATED TO OCCUR;
   - THE ESTIMATED QUANTITY OF BLAST ROCK IN CUBIC YARDS; AND
   - SITE-SPECIFIC BLASTING BEST MANAGEMENT PRACTICES.



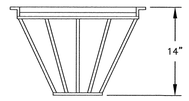




LENGTH (L) & WIDTH (W) AS REQUIRED TO FIT NHDOT TYPE GRATE & FRAME.



TOP VIEW



SIDE VIEW

1) INLET BASKETS SHALL BE INSTALLED IMMEDIATELY AFTER CATCH BASIN CONSTRUCTION IS COMPLETE AND SHALL REMAIN IN PLACE AND BE MAINTAINED UNTIL PAVEMENT BINDER COURSE IS COMPLETE.

2) FILTER FABRIC SHALL BE PUSHED DOWN AND FORMED TO THE SHAPE OF THE BASKET. THE SHEET OF FABRIC SHALL BE LARGE ENOUGH TO BE SUPPORTED BY THE BASKET FRAME WHEN HOLDING SEDIMENT AND, SHALL EXTEND AT LEAST 6" PAST THE FRAME. THE INLET GRATE SHALL BE PLACED OVER THE BASKET/FRAME AND WILL SERVE AS THE FABRIC ANCHOR.

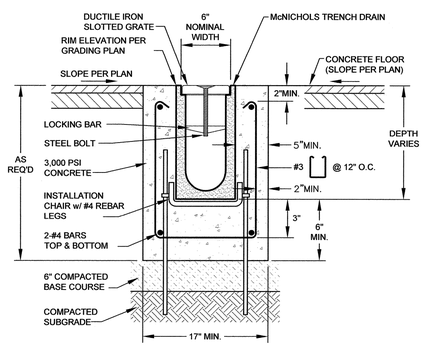
3) THE FILTER FABRIC SHALL BE A GEOTEXTILE FABRIC; POLYESTER, POLYPROPYLENE, STABILIZED NYLON, POLYETHYLENE, OR POLYVINYLIDENE CHLORIDE MEETING THE FOLLOWING SPECIFICATIONS:
-RAB STRENGTH: 45 LB. MIN. IN ANY PRINCIPAL DIRECTION (ASTM D1682)
-MULLEN BURST STRENGTH: MIN. 60 psi (ASTM D774)

4) THE FABRIC SHALL HAVE AN OPENING NO GREATER THAN A NUMBER 20 U.S. STANDARD SIEVE AND A MINIMUM PERMEABILITY OF 120 gpm/s.f. (MULTIPLY THE PERMITTIVITY IN SEC.-1 FROM ASTM 54491-85 CONSTANT HEAD TEST USING THE CONVERSION FACTOR OF 74.)

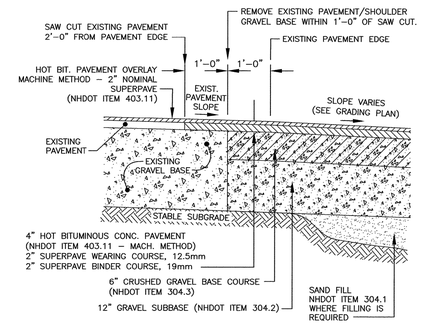
5) THE INLET BASKET SHALL BE INSPECTED WITHIN 24 HOURS AFTER EACH RAINFALL OR DAILY DURING EXTENDED PERIODS OF PRECIPITATION. REPAIRS SHALL BE MADE IMMEDIATELY, AS NECESSARY, TO PREVENT PARTICLES FROM REACHING THE DRAINAGE SYSTEM AND/OR CAUSING SURFACE FLOODING.

6) SEDIMENT DEPOSITS SHALL BE REMOVED AFTER EACH STORM EVENT, OR MORE OFTEN IF THE FABRIC BECOMES CLOGGED.

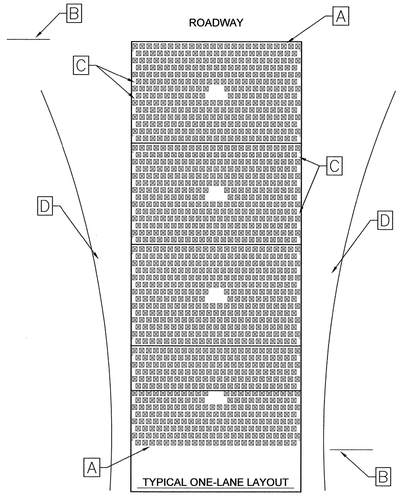
A C201 CATCH BASIN INLET BASKET
NTS



B C201 EVAPORATOR TRENCH DETAIL
COORDINATE WITH BUILDING PLANS
NTS



D C201 TYPICAL PAVEMENT CROSS SECTION
OFF SITE REPAIR AS NEEDED
NTS



INSTALLATION:
THE PURPOSE AND DESIGN OF THE FODS TRACKOUT CONTROL SYSTEM IS TO EFFECTIVELY REMOVE MOST SEDIMENT FROM VEHICLE TIRES AS THEY EXIT A DISTURBED LAND AREA ONTO A PAVED STREET. THIS MANUAL IS A PLATFORM FROM WHICH TO INSTALL A FODS TRACKOUT CONTROL SYSTEM. (NOTE: THIS IS NOT A ONE SIZE FITS ALL GUIDE.) THE INSTALLATION MAY NEED TO BE MODIFIED TO MEET THE EXISTING CONDITIONS, EXPECTATIONS, OR DEMANDS OF A PARTICULAR SITE. THIS IS A GUIDELINE. ULTIMATELY THE FODS TRACKOUT CONTROL SYSTEM SHOULD BE INSTALLED SAFELY WITH PROPER ANCHORING AND SIGNS PLACED AT THE ENTRANCE AND EXIT TO CAUTION USERS AND OTHERS.

KEY NOTES:

A. FODS TRACKOUT CONTROL SYSTEM MAT.
B. FODS SAFETY SIGN.
C. ANCHOR POINT.
D. SILT OR ORANGE CONSTRUCTION FENCE.

INSTALLATION:

1. THE SITE WHERE THE FODS TRACKOUT CONTROL SYSTEM IS TO BE PLACED SHOULD CORRESPOND TO BEST MANAGEMENT PRACTICES AS MUCH AS POSSIBLE. THE SITE WHERE FODS TRACKOUT CONTROL SYSTEM IS PLACED SHOULD ALSO MEET OR EXCEED THE LOCAL JURISDICTION OR STORM WATER POLLUTION PREVENTION PLAN (SWPPP) REQUIREMENTS.
2. CALL FOR UTILITY LOCATES 3 BUSINESS DAYS IN ADVANCE OF THE OF FODS TRACKOUT CONTROL SYSTEM INSTALLATION FOR THE MARKING OF UNDERGROUND UTILITIES. CALL THE UTILITY NOTIFICATION CENTER AT 811.
3. ONCE THE SITE IS ESTABLISHED WHERE FODS TRACKOUT CONTROL SYSTEM IS TO BE PLACED, ANY EXCESSIVE UNEVEN TERRAIN SHOULD BE LEVELED OUT OR REMOVED SUCH AS LARGE ROCKS, LANDSCAPING MATERIALS, OR SUDDEN ABRUPT CHANGES IN ELEVATION.
4. THE INDIVIDUAL MATS CAN START TO BE PLACED INTO POSITION. THE FIRST MAT SHOULD BE PLACED NEXT TO THE CLOSEST POINT OF EGRESS. THIS WILL ENSURE THAT THE VEHICLE WILL EXIT STRAIGHT FROM THE SITE ONTO THE PAVED SURFACE.
8. AFTER THE FIRST MAT IS PLACED DOWN IN THE PROPER LOCATION, MATS SHOULD BE ANCHORED TO PREVENT THE POTENTIAL MOVEMENT WHILE THE ADJOINING MATS ARE INSTALLED. ANCHORS SHOULD BE PLACED AT EVERY ANCHOR POINT (IF FEASIBLE) TO HELP MAINTAIN THE MAT IN ITS CURRENT POSITION.
9. AFTER THE FIRST MAT IS ANCHORED IN ITS PROPER PLACE, AN H BRACKET SHOULD BE PLACED AT THE END OF THE FIRST MAT BEFORE ANOTHER MAT IS PLACED ADJACENT TO THE FIRST MAT.
10. ONCE THE SECOND MAT IS PLACED ADJACENT TO THE FIRST MAT, MAKE SURE THE H BRACKET IS CORRECTLY SITUATED BETWEEN THE TWO MATS, AND SLIDE MATS TOGETHER.
11. NEXT THE CONNECTOR STRAPS SHOULD BE INSTALLED TO CONNECT THE TWO MATS TOGETHER.
12. UPON PLACEMENT OF EACH NEW MAT IN THE SYSTEM, THAT MAT SHOULD BE ANCHORED AT EVERY ANCHOR POINT TO HELP STABILIZE THE MAT AND ENSURE THE SYSTEM IS CONTINUOUS WITH NO GAPS IN BETWEEN THE MATS.
13. SUCCESSIVE MATS CAN THEN BE PLACED TO CREATE THE FODS TRACKOUT CONTROL SYSTEM REPEATING THE ABOVE STEPS.

USE AND MAINTENANCE

1. VEHICLES SHOULD TRAVEL DOWN THE LENGTH OF THE TRACKOUT CONTROL SYSTEM AND NOT CUT ACROSS THE MATS.
2. DRIVERS SHOULD TURN THE WHEEL OF THEIR VEHICLES SUCH THAT THE VEHICLE WILL MAKE A SHALLOW S-TURN ROUTE DOWN THE LENGTH OF THE FODS TRACKOUT CONTROL SYSTEM.
3. MATS SHOULD BE CLEANED ONCE THE VOIDS BETWEEN THE PYRAMIDS BECOME FULL OF SEDIMENT. TYPICALLY THIS WILL NEED TO BE PERFORMED WITHIN TWO WEEKS AFTER A STORM EVENT. BRUSHING IS THE PREFERRED METHOD OF CLEANING, EITHER MANUALLY OR MECHANICALLY.
4. THE USE OF ICE MELT, ROCK SALT, SNOW MELT, DE-ICER, ETC. SHOULD BE UTILIZED AS NECESSARY DURING THE WINTER MONTHS AND AFTER A SNOW EVENT TO PREVENT ICE BUILDUP.

REMOVAL

1. REMOVAL OF FODS TRACKOUT CONTROL SYSTEM IS REVERSE ORDER OF INSTALLATION.
2. STARTING WITH THE LAST MAT, THE MAT THAT IS PLACED AT THE INNERMOST POINT OF THE SITE OR THE MAT FURTHEST FROM THE EXIT OR PAVED SURFACE SHOULD BE REMOVED FIRST.
3. THE ANCHORS SHOULD BE REMOVED.
4. THE CONNECTOR STRAPS SHOULD BE UNBOLTED AT ALL LOCATIONS IN THE FODS TRACKOUT CONTROL SYSTEM.
5. STARTING WITH THE LAST MAT IN THE SYSTEM, EACH SUCCESSIVE MAT SHOULD THEN BE MOVED AND STACKED FOR LOADING BY FORKLIFT OR EXCAVATOR ONTO A TRUCK FOR REMOVAL FROM THE SITE.

C C201 FODS SEDIMENT TRACKING SYSTEM
NTS

NOTES:

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REV	DATE	DESCRIPTION	BY	CHK
0	10/21/25	ISSUED FOR COMMENT	SJR	JRC

DRAWING ISSUE STATUS

PERMIT PLANS

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Road, Unit 14
Portsmouth, NH 03801
603-430-9282
WWW.HALEYWARD.COM

PROJECT

DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE

DETAILS

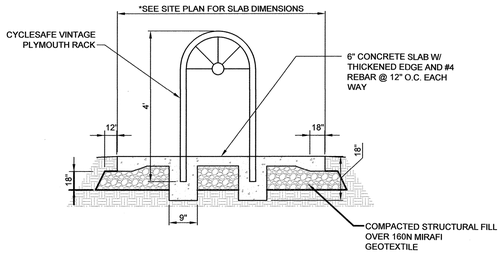
JOHN R. CHANNON NO. 7851 LICENSED PROFESSIONAL ENGINEER - STATE OF NEW HAMPSHIRE

DATE	SCALE
SEPTEMBER 2025	1"=20'

DRAWN BY	DESIGNED BY	CHECKED BY
SJR	—	JRC

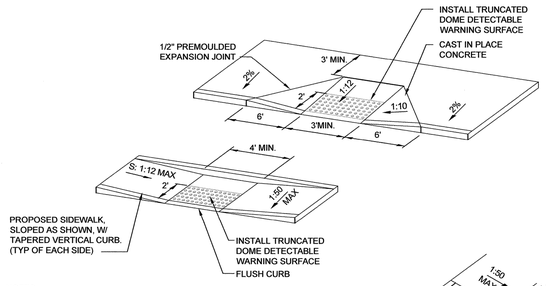
PROJECT No.	FIELD BOOK / PAGE
5010156.1532	FB 276 PG 60

DRAWING No. **C501** REV. **0**



E C201 BIKE RACK DETAIL

BIKE RACK SUBSTITUTIONS UPON APPROVAL NTS

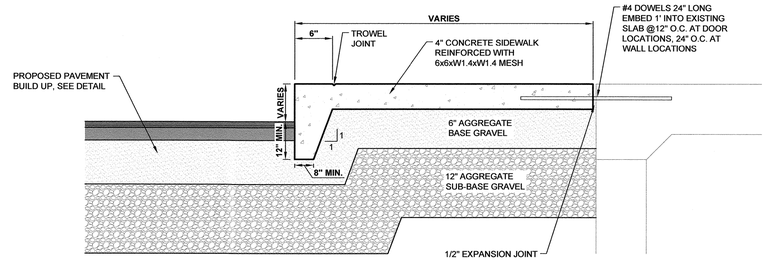


NOTES:

1. THE TEXTURE OF THE WARNING FEATURE MUST CONTRAST WITH THE SURROUNDING SURFACES (EITHER LIGHT ON DARK, OR DARK ON LIGHT).
2. SIZE OF DETECTABLE SURFACE TO BE 24" MIN. HEIGHT x THE FULL WIDTH OF THE RAMP / WALK.
3. CAST IRON DETECTABLE WARNING PAVER SURFACE SHALL BE EQUAL TO NEENAH FOUNDRY, UN-PAINTED.
4. PAVERS SHALL BE INSTALLED IN ACCORDANCE WITH ADA GUIDELINES, AND PER MANUFACTURER'S WRITTEN INSTRUCTIONS.

H C201 TYPICAL HANDICAP CURB RAMP DETAILS

NTS



NOTE:
EXPANSION JOINTS SHALL BE INSTALLED EVERY 20' WITH SCORE JOINTS EVERY 5'. EXPANSION JOINTS SHALL ALSO BE PLACED WHERE THE SIDEWALK ABUTS THE BUILDING, BETWEEN ADJACENT SLABS, CURBS OR AT PENETRATION STRUCTURES. EXPANSION JOINTS ARE TO BE 1/2" WIDE.

K C201 CONCRETE SIDEWALK W/ INTEGRAL CURB DETAIL

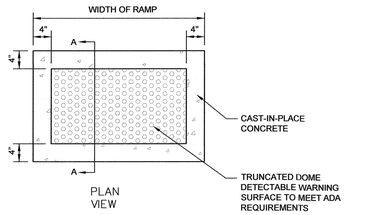
NTS

NOTES:

1) THE CONTRACTOR SHALL NOTIFY DIG SAFE AT 1-888-DIG-SAFE (1-888-344-7233) AT LEAST 72 HOURS PRIOR TO COMMENCING ANY EXCAVATION ON PUBLIC OR PRIVATE PROPERTY WITHIN 100 FEET OF UNDERGROUND UTILITIES. THE EXCAVATOR IS RESPONSIBLE TO MAINTAIN MARKS. DIG SAFE TICKETS EXPIRE IN THIRTY DAYS.

2) UNDERGROUND UTILITY LOCATIONS ARE BASED UPON BEST AVAILABLE EVIDENCE AND ARE NOT FIELD VERIFIED. LOCATING AND PROTECTING ANY ABOVEGROUND OR UNDERGROUND UTILITIES IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND/OR THE OWNER. UTILITY CONFLICTS SHOULD BE REPORTED AT ONCE TO THE DESIGN ENGINEER.

3) CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE "NEW HAMPSHIRE STORMWATER MANUAL, VOLUME 3, EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION. (NHDES DECEMBER 2008).

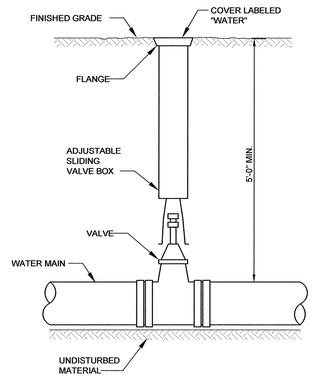


NOTES:

1. DETECTABLE WARNINGS SHALL BE AN INTEGRAL PART OF THE RAMP AND COMPLY WITH ALL SECTIONS OF THE ADA ACCESSIBILITY GUIDELINES AND ALL SECTIONS OF THE ADA STANDARDS FOR ACCESSIBLE DESIGN.
2. ALL DETECTABLE WARNING AREAS SHALL START 6-10" FROM THE FLOW LINE OF THE CURB, BE 24" IN DEPTH AND COVER THE COMPLETE WIDTH OF THE RAMP AREA ONLY. MATCH ROADWAY RADIUS, TYP.

F C201 TYPICAL DETECTABLE WARNING SURFACE DETAIL

NTS



NOTES:
1. GATE VALVE TO BE LOCATED WITHIN ROADWAY PAVEMENT WHERE POSSIBLE.

2. PROPER SIZE VALVE BOX SHALL BE INSTALLED WHERE GATE VALVES ARE SHOWN ON PLANS.

I C201 TYPICAL VALVE AND BOX DETAIL

NTS

BRICK PAVEMENT NOTES

SCOPE OF WORK:

1) THE WORK SHALL CONSIST OF CONSTRUCTING/RECONSTRUCTING THE SUB-BASE AND CONSTRUCTING A NEW BRICK SIDEWALK AS DIRECTED IN THE FIELD BY THE ENGINEER AND COORDINATED WITH PORTSMOUTH DPW.
2) REVEAL SHALL BE COORDINATED WITH PORTSMOUTH DPW.

METHODS OF CONSTRUCTION:

A) ALL LABOR AND MATERIALS SHALL CONFORM TO THE STATE OF NEW HAMPSHIRE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 608, AND CITY OF PORTSMOUTH SPECIFICATIONS FOR NEW BRICK SIDEWALK, SECTION 6.

B) EXCAVATION FOR SIDEWALKS SHALL BE AT A DEPTH OF 10 INCHES BELOW FINISH GRADE. IN AREAS NOT BUTTING CURBING OR BUILDINGS, THE EXCAVATION SHALL BE 6 INCHES WIDER THAN THE FINISHED SIDEWALK WIDTH. AT ALL DRIVE CROSSINGS, THE DEPTH OF EXCAVATION SHALL BE INCREASED ACCORDINGLY. THE CONTRACTOR SHALL PROVIDE NEAT AND SQUARE CUTTING OF EXISTING ASPHALT ROAD SURFACE AS NEEDED. ALL UNSUITABLE MATERIAL SHALL BE REMOVED AND DISPOSED OF OFF-SITE AT THE CONTRACTOR'S OWN EXPENSE.

C) THE BASE MATERIAL SHALL CONSIST OF A MIXTURE OF STONES OR ROCK FRAGMENTS AND PARTICLES WITH 100% PASSING THE 3 INCH SIEVE, 95% TO 100% PASSING THE 2 INCH SIEVE, 55% TO 85% PASSING THE 1 INCH SIEVE, AND 27% TO 52% PASSING THE NO. 4 SIEVE. AT LEAST 50% OF THE MATERIALS RETAINED ON THE 1 INCH SIEVE SHALL HAVE A FRACTURED FACE. THE BASE MATERIAL SHALL BE THOROUGHLY COMPACTED TO THE DEPTH SPECIFIED OR DIRECTED. IN THE WAY OF ALL DRIVE CROSSINGS THE BASE WILL BE INCREASED TO A COMPACTED DEPTH OF 12 INCHES. GRAVEL REQUIREMENTS FOR RECONSTRUCTION WILL BE AS DIRECTED, BASED ON SITE CONDITIONS. THE WORK INCLUDES BACKING UP ANY AND ALL CURB BEING INSTALLED BY OTHERS ON BOTH SIDES.

D) THE CLAY BRICK PAVERS SHALL BE LAID IN A 1 INCH BED OF A SAND MIXTURE COMPRISED OF: 3 PARTS SAND MIXED WITH 1 PART PORTLAND CEMENT.

E) THE CONTRACTOR SHALL LAY THE BRICKS SO THAT APPROXIMATELY 4.5 BRICKS SHALL COVER ONE SQUARE FOOT.

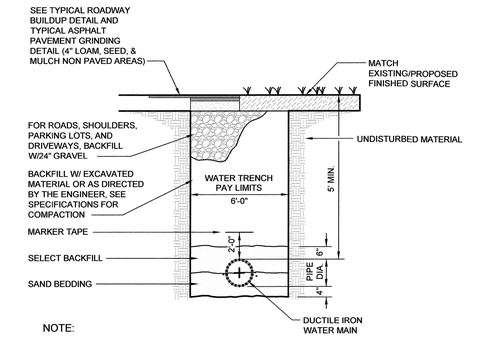
F) THE SIDEWALK SHALL PITCH TOWARDS THE STREET AS SHOWN ON THE GRADING PLAN.

G) IN AREAS WHERE THE FRONT OF THE BRICK SIDEWALK IS NOT ADJACENT TO GRANITE CURBING, THE CONTRACTOR SHALL INSTALL EDGING TO HOLD THE BRICKS IN PLACE. SUCH EDGING SHALL BE INSTALLED PER THE MANUFACTURER'S RECOMMENDATIONS.

H) THE CONTRACTOR SHALL SUBMIT A SAMPLE OF THE BRICKS FOR APPROVAL BY THE CITY BEFORE BRICKS ARE INSTALLED.

CONSTRUCTION NOTE:

EXISTING GRANITE CURB DISTURBED BY CONSTRUCTION SHALL BE REUSED AND ANY MISSING CURB SHALL BE REPLACED WITH NEW CURB MATCHING EXISTING CURB SIZE. NO CURB LESS THAN 3' IN LENGTH WILL BE ALLOWED.



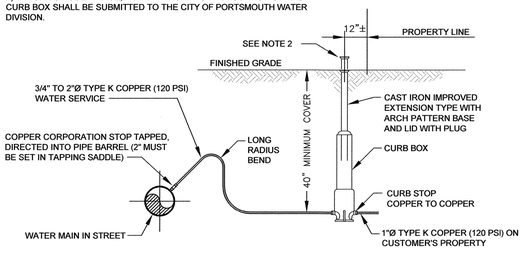
NOTE:

1. MATCH EXISTING SURFACE FINISH, EXCEPT WHERE NOTED. IN LAWN AREAS INSTALL 4" OF LOAM AND SEED AND MULCH.

G C201 TYPICAL TRENCH DETAIL-DUCTILE IRON WATER MAIN

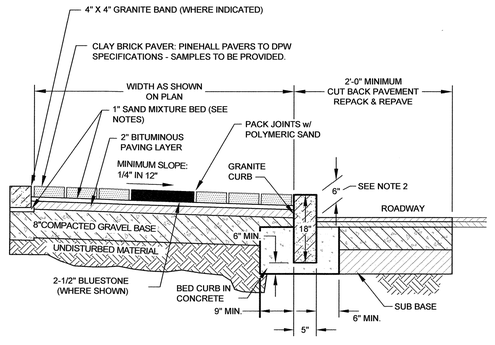
NTS

NOTE:
1) INSTALLATION OF WATER MAIN TAP & CURB STOP & BOX SHALL ONLY BE PERFORMED BY THOSE AUTHORIZED BY THE PUBLIC WORKS DEPARTMENT.
2) IN AREAS OF HEAVY GROWTH THE CURB BOX COVER SHALL BE SET 6" ABOVE FINISH GRADE AND A WITNESS STAKE SET.
3) CURB BOX SHALL BE SET APPROXIMATELY 12" OUTSIDE PROPERTY LINE AS SHOWN.
4) PRIOR TO ACCEPTANCE, A PLAN INDICATING THE LOCATION OF THE CURB BOX SHALL BE SUBMITTED TO THE CITY OF PORTSMOUTH WATER DIVISION.



J C201 TYPICAL WATER SERVICE CONNECTION DETAIL

NTS



L C201 BRICK SIDEWALK W/ VERTICAL GRANITE CURB DETAIL

(STONE DUST BEDDING OVER BITUMINOUS PAVING) NTS

REV.	DATE	DESCRIPTION	BY	CHK
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PROJECT

DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE

DETAILS

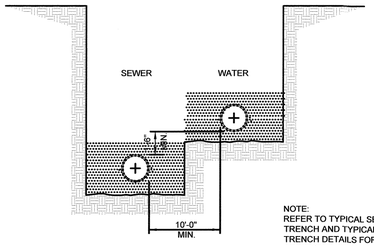
STATE OF NEW HAMPSHIRE — JOHN R. CHAMPION No. 15591 LICENSED PROFESSIONAL ENGINEER

DATE	SCALE
SEPTEMBER 2025	1"=20'

DRAWN BY	DESIGNED BY	CHECKED BY
SJR	---	JRC

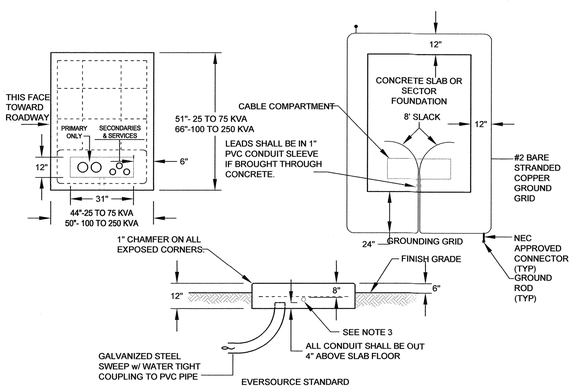
PROJECT No.	FIELD BOOK / PAGE
5010156.1532	FB 276 PG 60

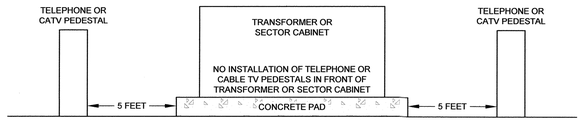
DRAWING No. **C502** REV. **0**



NOTE:
REFER TO TYPICAL SEWER TRENCH AND TYPICAL WATER TRENCH DETAILS FOR ADDITIONAL INFORMATION.

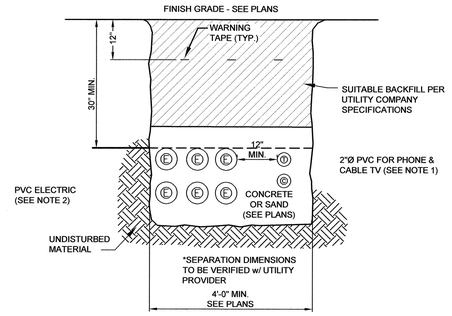
M C201 TYPICAL SEWER/WATER SEPARATION DETAIL
NTS





NOTES
1. ALL REINFORCING TO BE #6 BARS.
2. 1" PVC CONDUIT SLEEVE FOR GROUND GRID LEADS.
3. THE GROUND GRID SHALL BE SUPPLIED AND INSTALLED BY THE CONTRACTOR AND IS TO BE BURIED AT LEAST 12" BELOW GRADE. EIGHT FEET OF EXTRA WIRE FOR EACH GROUND GRID LEG SHALL BE LEFT EXPOSED IN THE CABLE COMPARTMENT TO ALLOW FOR THE CONNECTION TO THE TRANSFOMER. THE TWO 8' GROUND RODS MAY BE EITHER GALVANIZED STEEL OR COPPERWELD AND THEY SHALL BE CONNECTED TO THE GRID WITH NEC APPROVED CONNECTORS.
4. NO SHRUBS, FENCES, OR PERMANENT STRUCTURES CAN BE PLACED WITHIN 10 FEET OF THE FRONT AND 5 FEET OF THE SIDES AND BACK OF PAD-MOUNTED EQUIPMENT. THE COMPANY HAS THE RIGHT TO REMOVE THESE OBJECTS WITHOUT NOTICE TO THE OWNER.

N C201 TRANSFORMER PAD DETAIL
COORDINATE WITH EVERSOURCE
NTS



NOTES:
1) ALL CONDUIT TO BE U.L. LISTED, SCH. 80 UNDER ALL TRAVEL WAYS, & SCHED. 40 FOR THE REMAINDER.

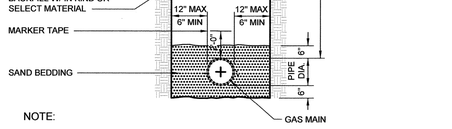
2) NORMAL CONDUIT SIZES FOR PSNH ARE 3 INCH FOR SINGLE PHASE PRIMARY AND SECONDARY VOLTAGE CABLES, 4 INCH FOR THREE PHASE SECONDARY, AND 5 INCH FOR THREE PHASE PRIMARY.

3) ALL WORK TO CONFORM TO THE NATIONAL ELECTRICAL CODE (LATEST REVISION)

4) INSTALL A 200# PULL ROPE FOR EACH CONDUIT

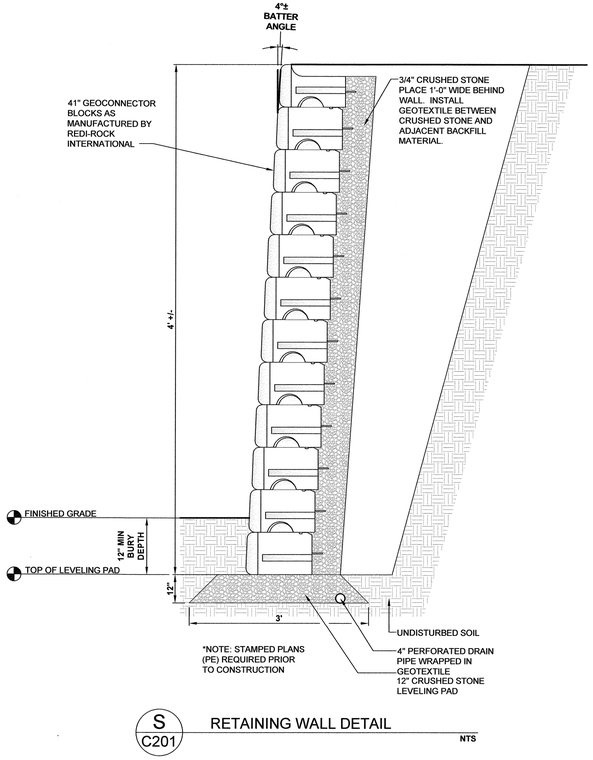
5) VERIFY ALL CONDUIT SPECIFICATIONS WITH UTILITY COMPANY'S PRIOR TO ANY CONSTRUCTION.

P C201 TYPICAL UNDERGROUND UTILITY TRENCH DETAIL
NTS

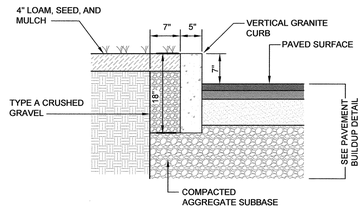


NOTE:
1. COMPACT ALL GRANULAR MATERIAL AND BACKFILL TO 95%.
2. SIDE CLEARANCE APPLICABLE TO BACKHOE OPERATIONS.

Q C201 TYPICAL GAS TRENCH DETAIL
NTS



S C201 RETAINING WALL DETAIL
NTS



T C201 TYPICAL GRANITE CURB DETAIL
NTS

NOTES:
1) THE CONTRACTOR SHALL NOTIFY DIG SAFE AT 1-888-DIG-SAFE (1-888-344-7233) AT LEAST 72 HOURS PRIOR TO COMMENCING ANY EXCAVATION ON PUBLIC OR PRIVATE PROPERTY WITHIN 100 FEET OF UNDERGROUND UTILITIES. THE EXCAVATOR IS RESPONSIBLE TO MAINTAIN MARKS. DIG SAFE TICKETS EXPIRE IN THIRTY DAYS.

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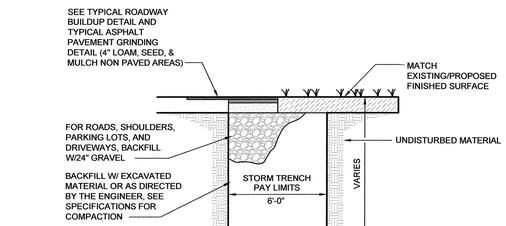
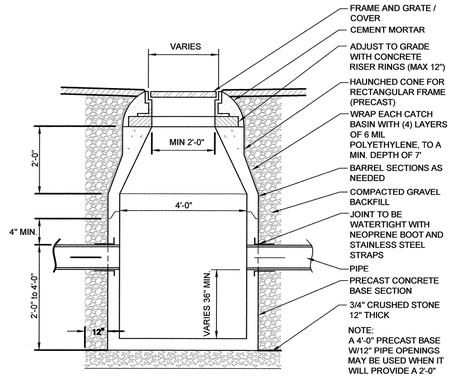


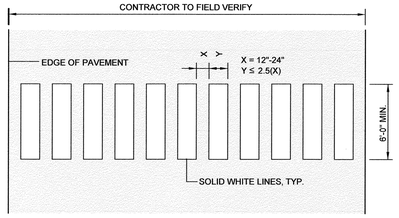
NOTE:
1. MATCH EXISTING SURFACE FINISH, EXCEPT WHERE NOTED. IN LAWN AREAS INSTALL 4" OF LOAM AND SEED AND MULCH.

O C201 TYPICAL STORM DRAIN TRENCH DETAIL
NTS



NOTE:
A 4'-0" PRECAST BASE W/12" PIPE OPENINGS MAY BE USED WHEN IT WILL PROVIDE A 2'-0" SUMP AND 12"s RING ABOVE ALL OPENINGS INTO THE BASE.

R C201 TYPICAL CATCH BASIN DETAIL
NTS



NOTE:
1. ALL PAVEMENT MARKINGS SHALL CONFORM TO THE CURRENT EDITION OF THE MUTCD

U C201 TYPICAL CROSSWALK DETAIL
NTS

REV.	DATE	DESCRIPTION	BY	CHK.
0	10/21/25	ISSUED FOR COMMENT	SJR	JRC

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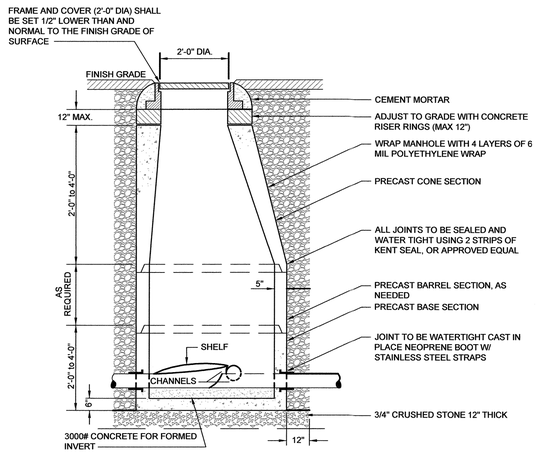
PROJECT
DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE
DETAILS

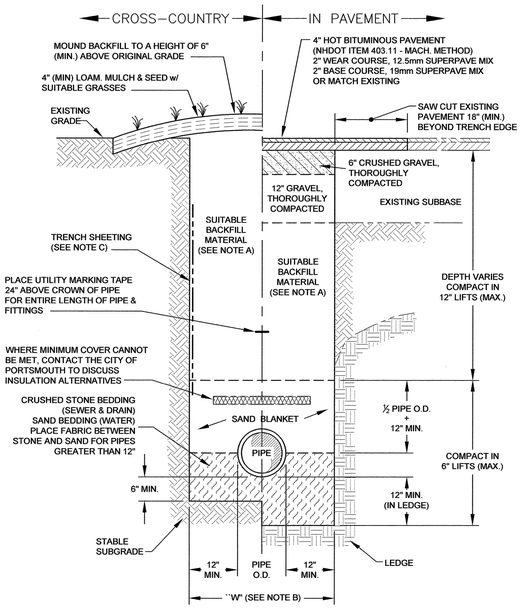
JOHN R. CHAGNON No. 7651 LICENSED PROFESSIONAL ENGINEER – STATE OF NEW HAMPSHIRE

DATE	SCALE
SEPTEMBER 2025	1"=20'
DRAWN BY: SJR	DESIGNED BY: ---	CHECKED BY: JRC
PROJECT No.: 5010156.1532	FIELD BOOK / PAGE: FB 276 PG 60
DRAWING No.: C503	REV.: 0



V C201 TYPICAL SEWER MANHOLE DETAIL

NTS



TRENCH NOTES:
A) TRENCH BACKFILL:
- IN PAVED AREAS, SUITABLE MATERIAL FOR TRENCH BACKFILL SHALL BE THE NATURAL MATERIAL EXCAVATED DURING CONSTRUCTION, BUT SHALL EXCLUDE DEBRIS, PIECES OF PAVEMENT, ORGANIC MATTER, TOP SOIL, ALL WET OR SOFT MUCK, PEAT OR CLAY, ALL EXCAVATED LEDGE MATERIAL, AND ALL ROCKS OVER SIX INCHES IN LARGEST DIMENSION, OR ANY MATERIALS DEEMED TO BE UNACCEPTABLE BY THE ENGINEER.

- IN CROSS-COUNTRY CONSTRUCTION, SUITABLE MATERIAL SHALL BE AS DESCRIBED ABOVE, EXCEPT THAT THE ENGINEER MAY PERMIT THE USE OF TOP SOIL, LOAM, MUCK OR PEAT, IF HE IS SATISFIED THAT THE COMPLETED CONSTRUCTION WILL BE ENTIRELY STABLE.

B) "W" = MAXIMUM ALLOWABLE TRENCH WIDTH TO A PLANE 12 INCHES ABOVE THE PIPE. FOR PIPES 15 INCHES NOMINAL DIAMETER OR LESS, W SHALL BE NO MORE THAN 36 INCHES. FOR PIPES GREATER THAN 15 INCHES NOMINAL DIAMETER, W SHALL BE 24 INCHES PLUS PIPE O.D.

C) TRENCH SHEETING:
IF REQUIRED. WHERE SHEETING IS PLACED ALONGSIDE THE PIPE AND EXTENDS BELOW MID-DIAMETER, IT SHALL BE CUT OFF AND LEFT IN PLACE TO AN ELEVATION NOT LESS THAN 1 FOOT ABOVE THE TOP OF THE PIPE. WHERE SHEETING IS ORDERED BY THE ENGINEER TO BE LEFT IN PLACE, IT SHALL BE CUT OFF AT LEAST 3 FEET BELOW FINISHED GRADE, BUT NOT LESS THAN 1 FOOT ABOVE THE TOP OF THE PIPE.

D) MINIMUM PIPE COVER FOR UTILITY MAINS (UNLESS GOVERNED BY OTHER CODES):
6' MINIMUM FOR SEWER (IN PAVEMENT)
4' MINIMUM FOR SEWER (CROSS COUNTRY)
3' MINIMUM FOR STORMWATER DRAINS
5' MINIMUM FOR WATER MAINS

E) ALL PAVEMENT CUTS SHALL BE REPAIRED BY THE INFRARED HEAT METHOD.

W C201 TYPICAL PIPE TRENCH DETAIL

NTS

SEWER UTILITY GENERAL NOTES:

1) MINIMUM PIPE SIZE FOR COMMERCIAL SERVICE SHALL BE SIX INCHES.

2) PIPE AND JOINT MATERIALS:
A. PLASTIC SEWER PIPE

1. PIPE AND FITTINGS SHALL CONFORM TO THE FOLLOWING ASTM STANDARDS:

ASTM STANDARDS	GENERIC PIPE MATERIAL	SIZES APPROVED
D3034	*PVC (SOLID WALL)	8" THROUGH 15" (SDR 35)
F679	PVC (SOLID WALL)	18" THROUGH 27" (T-1 & T-2)
F789	PVC (SOLID WALL)	4" THROUGH 18" (T-1 To T-3)
F794	PVC (RIBBED WALL)	8" THROUGH 36"
AWWA C900	PVC (SOLID WALL)	8" THROUGH 18"

*PVC: POLYVINYL CHLORIDE

2. JOINT SEALS FOR PVC PIPE SHALL BE OIL RESISTANT COMPRESSION RINGS OF ELASTOMERIC MATERIAL CONFORMING TO ASTM D-3212 AND SHALL BE PUSH-ON BELL AND SPIGOT TYPE.

B. DUCTILE IRON PIPE, FITTINGS AND JOINTS.

1. DUCTILE IRON PIPE AND FITTINGS FOR SEWERS SHALL CONFORM TO THE FOLLOWING STANDARDS OF THE UNITED STATES OF AMERICA STANDARDS INSTITUTE:

A21.50 THICKNESS DESIGN OF DUCTILE IRON PIPE AND WITH ASTM A-536 DUCTILE IRON CASTINGS.

A21.51 DUCTILE IRON PIPE, CENTRIFUGALLY CAST IN METAL MOULDS OR SAND LINED MOULDS FOR SEWER APPLICATIONS.

2. JOINTS SHALL BE OF THE MECHANICAL OR PUSH ON TYPE. JOINTS AND GASKETS SHALL CONFORM TO:

A21.11 RUBBER GASKET JOINTS FOR CAST IRON PRESSURE PIPE & FITTINGS.

3) DAMAGED PIPE SHALL BE REJECTED AND REMOVED FROM THE JOB SITE.

4) JOINTS SHALL BE DEPENDENT UPON A NEOPRENE OR ELASTOMERIC GASKET FOR WATER TIGHTNESS. ALL JOINTS SHALL BE PROPERLY MATCHED WITH THE PIPE MATERIALS USED. WHERE DIFFERING MATERIALS ARE TO BE CONNECTED, AS AT THE STREET SEWER WYE OR AT THE FOUNDATION WALL, APPROPRIATE MANUFACTURED ADAPTERS SHALL BE USED.

5) TEES AND WYES: WHERE A TEE OR WYE IS NOT AVAILABLE IN THE EXISTING STREET SEWER, AN APPROPRIATE CONNECTION SHALL BE MADE DEPENDING ON THE PIPE ENCOUNTERED, FOR PVC PIPE, OR CUT IN A SANITARY TEE. FOR CLAY PIPE, USE INSERT-A-TEE OR CUT IN A SANITARY TEE. ALL WORK TO BE APPROVED BY GOVERNING BODY.

6) HOUSE SEWER INSTALLATION: THE PIPE SHALL BE HANDLED, PLACED AND JOINTED IN ACCORDANCE WITH INSTALLATION GUIDES OF THE APPROPRIATE MANUFACTURER. IT SHALL BE CAREFULLY BEDDED ON A 4 INCH LAYER OF CRUSHED STONE AND/OR GRAVEL AS SPECIFIED IN NOTE 10. BEDDING AND REFILL FOR DEPTH OF 12 INCHES ABOVE THE TOP OF THE PIPE SHALL BE CAREFULLY AND THOROUGHLY TAMPED BY HAND OR WITH APPROPRIATE MECHANICAL DEVICES.

THE PIPE SHALL BE LAID AT A CONTINUOUS AND CONSTANT GRADE FROM THE STREET SEWER CONNECTION TO THE FOUNDATION AT A GRADE OF NOT LESS THAN 1/4 INCH PER FOOT. PIPE JOINTS MUST BE MADE UNDER DRY CONDITIONS. IF WATER IS PRESENT, ALL NECESSARY STEPS SHALL BE TAKEN TO DEWATER THE TRENCH.

7) TESTING: WHEN REQUIRED BY THE GOVERNING AUTHORITY, TESTING SHALL CONFORM TO ENV-WQ 704.07.

8) ILLEGAL CONNECTIONS: NOTHING BUT SANITARY WASTE FLOW FROM DWELLING TOILETS, SINKS, LAUNDRY ETC. SHALL BE PERMITTED. ROOF LEADERS, FOOTING DRAINS, SUMP PUMPS OR OTHER SIMILAR CONNECTIONS CARRYING RAIN WATER, DRAINAGE OR GROUND WATER SHALL NOT BE PERMITTED.

9) WATER SERVICE SHALL NOT BE LAID IN SAME TRENCH AS SEWER SERVICE, UNLESS IT IS ON A SHELF 12" HIGHER, AND 18" APART.

10) BEDDING: SCREENED GRAVEL AND/OR CRUSHED STONE, FREE FROM CLAY, LOAM, ORGANIC MATTER AND MEETING ASTM C33 STONE SIZE NO. 67.

100% PASSING	1 INCH SCREEN
90%-100% PASSING	3/4 INCH SCREEN
20%- 55% PASSING	3/8 INCH SCREEN
0%- 10% PASSING	#4 SIEVE
0%- 5% PASSING	#8 SIEVE

WHERE ORDERED BY THE ENGINEER TO STABILIZE THE TRENCH BASE, GRADED SCREENED GRAVEL OR CRUSHED STONE 1/2 INCH TO 1-1/2 INCH SHALL BE USED.

11) LOCATION: THE LOCATION OF THE TEE OR WYE SHALL BE RECORDED AND FILED IN THE MUNICIPAL RECORDS. IN ADDITION, A FERROUS METAL ROD OR PIPE SHALL BE PLACED OVER THE TEE OR WYE AS DESCRIBED IN THE TYPICAL "CHIMNEY" DETAIL, TO AID IN LOCATING THE BURIED PIPE WITH A DIP NEEDLE OR PIPE FINDER.

12) CAST-IN-PLACE CONCRETE: SHALL CONFORM TO THE REQUIREMENTS FOR CLASS A (3000 PSI) CONCRETE OF THE NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS AS FOLLOWS:
CEMENT: 6.0 BAGS PER CUBIC YARD
WATER: 5.75 GALLONS PER BAG OF CEMENT
MAXIMUM AGGREGATE SIZE: 3/4 INCH

13) CHIMNEYS: IF VERTICAL DROP INTO SEWER IS GREATER THAN 4 FEET, A CHIMNEY SHALL BE CONSTRUCTED FOR THE HOUSE CONNECTION. CHIMNEY INSTALLATION AS RECOMMENDED BY THE PIPE MANUFACTURER MAY BE USED IF APPROVED BY THE ENGINEER.

14) BACKFILL UP TO SUBBASE GRAVEL SHALL BE WITH EXCAVATED SOIL FROM TRENCHING OPERATIONS. COMPACT IN 8" LIFTS WITH VIBRATORY PLATE COMPACTORS TO 90% OF MODIFIED PROCTOR DENSITY. IF FINE-GRAINED, COMPACT WITH POGO STICKS OR SHEEPSFOOT ROLLERS. PLACE NO LARGE ROCKS WITHIN 24" OF PIPE. TRENCHES THAT ARE NOT ADEQUATELY COMPACTED SHALL BE RE-EXCAVATED AND BACKFILLED UNDER THE SUPERVISION OF THE DESIGN ENGINEER OR GOVERNING BODY. UNSUITABLE BACKFILL MATERIAL INCLUDES CHUNKS OF PAVEMENT, TOPSOIL, ROCKS OVER 6" IN SIZE, MUCK, PEAT OR PIECES OF PAVEMENT.

15) THE CONTRACTOR IS SOLELY RESPONSIBLE FOR JOB-SITE SAFETY AND COMPLIANCE WITH GOVERNING REGULATIONS.

16) ORDERED EXCAVATION OF UNSUITABLE MATERIAL BELOW GRADE. REFILL WITH BEDDING MATERIAL. FOR TRENCH WIDTH SEE TRENCH DETAIL.

17) SAND BLANKET: CLEAN SAND, FREE FROM ORGANIC MATTER, SO GRADED THAT 90% - 100% PASSES A 1/2 INCH SIEVE AND NOT MORE THAN 15% WILL PASS A #200 SIEVE. BLANKET MAY BE OMITTED FOR DUCTILE IRON AND REINFORCED CONCRETE PIPE PROVIDED THAT NO STONE LARGER THAN 2 INCHES IS IN CONTACT WITH THE PIPE.

18) BASE COURSE GRAVEL, IF ORDERED BY THE ENGINEER, SHALL MEET THE REQUIREMENTS OF DIVISION 300 OF THE LATEST EDITION OF THE:
"STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION OF THE STATE OF NEW HAMPSHIRE, DEPARTMENT OF TRANSPORTATION".

19) FOR CROSS COUNTRY CONSTRUCTION, BACKFILL OR FILL SHALL BE MOUNDED TO A HEIGHT OF 6 INCHES ABOVE THE ORIGINAL GROUND SURFACE.

20) IF FULL ENCASEMENT IS UTILIZED, DEPTH OF CONCRETE BELOW PIPE SHALL BE 1/4 I.D. (4" MIN.) BLOCK SUPPORT SHALL BE SOLID CONCRETE BLOCKS.

21) CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE "NEW HAMPSHIRE STORMWATER MANUAL, VOLUME 3, EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION. (NHDES DECEMBER 2008).

22) THE CONTRACTOR SHALL NOTIFY DIG SAFE AT 1-888-DIG-SAFE (1-888-344-7233) AT LEAST 72 HOURS PRIOR TO COMMENCING ANY EXCAVATION.

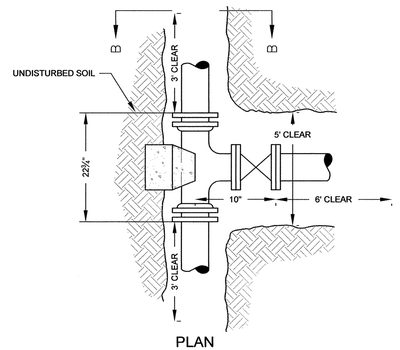
23) THE PURPOSE OF THIS PLAN IS TO SHOW STANDARDS FOR SEWER CONSTRUCTION.

24) ALL WORK SHALL BE IN COMPLIANCE WITH NHDES CODE OF ADMINISTRATIVE RULES PART ENV-WQ 704 DESIGN OF SEWERS.

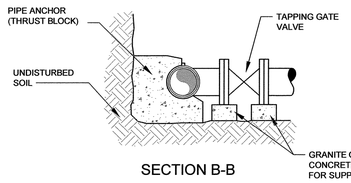
NOTES:
1) THE CONTRACTOR SHALL NOTIFY DIG SAFE AT 1-888-DIG-SAFE (1-888-344-7233) AT LEAST 72 HOURS PRIOR TO COMMENCING ANY EXCAVATION ON PUBLIC OR PRIVATE PROPERTY WITHIN 100 FEET OF UNDERGROUND UTILITIES. THE EXCAVATOR IS RESPONSIBLE TO MAINTAIN MARKS. DIG SAFE TICKETS EXPIRE IN THIRTY DAYS.

2) UNDERGROUND UTILITY LOCATIONS ARE BASED UPON BEST AVAILABLE EVIDENCE AND ARE NOT FIELD VERIFIED. LOCATING AND PROTECTING ANY ABOVEGROUND OR UNDERGROUND UTILITIES IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND/OR THE OWNER. UTILITY CONFLICTS SHOULD BE REPORTED AT ONCE TO THE DESIGN ENGINEER.

3) CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE "NEW HAMPSHIRE STORMWATER MANUAL, VOLUME 3, EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION. (NHDES DECEMBER 2008).



PLAN

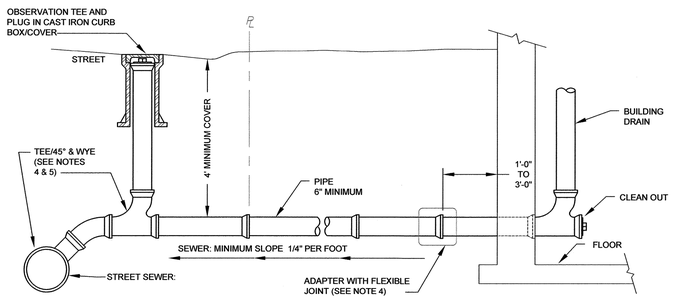


SECTION B-B

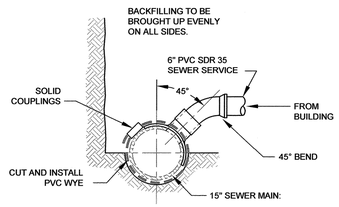
NOTES:
1) ALL MATERIALS SHALL BE APPROVED BY THE PORTSMOUTH WATER DEPARTMENT PRIOR TO INSTALLATION AND USE.
2) ALL JOINTS SHALL BE MECHANICAL.
3) "CLEAR" DIMENSIONS SHOWN ATE REQUIRED FOR WORKSPACE. NO JOINTS ON PIPE BEING TAPPED WITHIN "CLEAR" AREA.
4) FORD TYPE STAINLESS STEEL TAPPING SADDLES OR APPROVED EQUAL ARE ALSO ACCEPTABLE.

X C201 TAPPING SLEEVE & GATE DETAILS
INSTALL PER PORTSMOUTH REQUIREMENTS

NTS

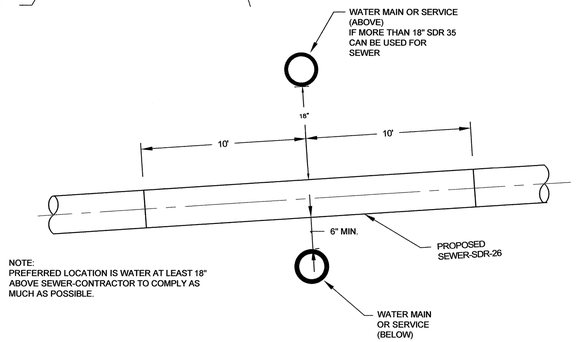


SEWER SERVICE



SECTION

SEWER SERVICE CONNECTION



NOTE:
PREFERRED LOCATION IS WATER AT LEAST 18" ABOVE SEWER-CONTRACTOR TO COMPLY AS MUCH AS POSSIBLE.

WATER CROSSING

Y C201 SEWER SERVICE DETAILS
INSTALL PER PORTSMOUTH REQUIREMENTS

NTS

REV	DATE	DESCRIPTION	BY	CHK
0	10/21/25	ISSUED FOR COMMENT	SJR	JRC

DRAWING ISSUE STATUS

PERMIT PLANS

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
WWW.HALEYWARD.COM
200 Griffin Road, Unit 14
Portsmouth, NH 03801
603-430-9282

PROJECT

DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

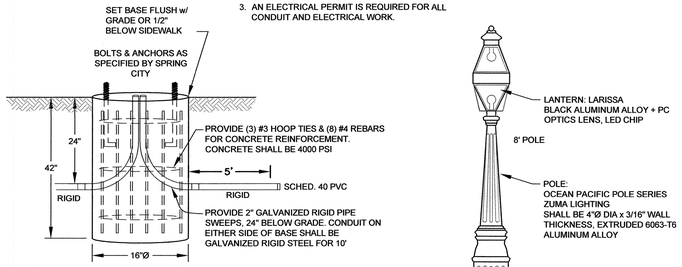
TITLE

DETAILS

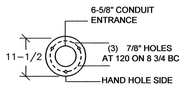
JOHN R. CHAGNON NH 7681 LICENSED PROFESSIONAL ENGINEER - STATE OF NEW HAMPSHIRE

DATE	SEPTEMBER 2025
SCALE	1"=20'
DRAWN BY	SJR
DESIGNED BY	---
CHECKED BY	JRC
PROJECT No.	5010156.1532
FIELD BOOK / PAGE	FB 276 PG 60
DRAWING No.	C504
REV.	0

NOTES:
1. ELECTRICAL INSTALLATION SHALL BE BY A LICENSED ELECTRICIAN.
2. LIGHTS SHALL BE WIRED IN ACCORDANCE WITH NEC AND CITY OR PORTSMOUTH ORDINANCES
3. AN ELECTRICAL PERMIT IS REQUIRED FOR ALL CONDUIT AND ELECTRICAL WORK.



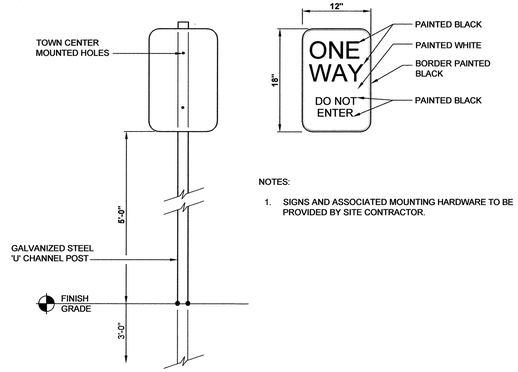
LIGHT POLE BASE DETAIL



BOLT PATTERN

LIGHT POLE DETAIL

Z / C201 — CITY STREET LIGHT DETAIL
IF NEEDED
NTS



NOTES:
1. SIGNS AND ASSOCIATED MOUNTING HARDWARE TO BE PROVIDED BY SITE CONTRACTOR.

AA / C201 — "EXIT ONLY" & "ONE-WAY" SIGN DETAIL
NTS

NOTES:
1) THE CONTRACTOR SHALL NOTIFY DIG SAFE AT 1-888-DIG-SAFE (1-888-344-7233) AT LEAST 72 HOURS PRIOR TO COMMENCING ANY EXCAVATION ON PUBLIC OR PRIVATE PROPERTY WITHIN 100 FEET OF UNDERGROUND UTILITIES. THE EXCAVATOR IS RESPONSIBLE TO MAINTAIN MARKS. DIG SAFE TICKETS EXPIRE IN THIRTY DAYS.

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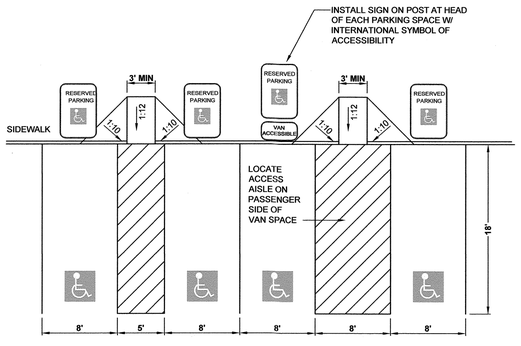
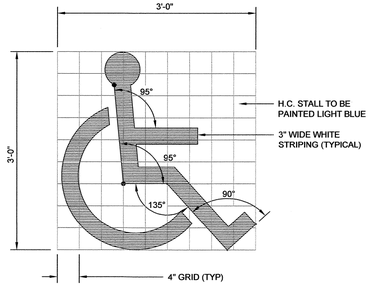


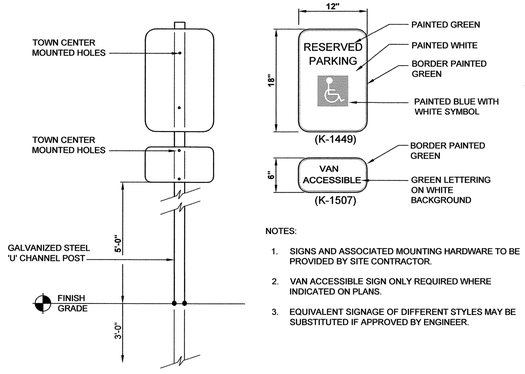
NOTES:
1. CAR PARKING SPACE: 8' WIDE MINIMUM W/ ACCESS AISLE 5' WIDE MINIMUM.
2. VAN PARKING SPACE 8' WIDE MINIMUM W/ ACCESS AISLE 8' WIDE MINIMUM.
3. SLOPES ACROSS PARKING SPACES IN EACH DIRECTION SHALL BE 2% MAX.

TYPICAL HANDICAP PARKING STALL LAYOUT



NOTE: CENTER SYMBOL WITHIN PARKING STALL

TYPICAL HANDICAP PARKING STALL SYMBOL



NOTES:
1. SIGNS AND ASSOCIATED MOUNTING HARDWARE TO BE PROVIDED BY SITE CONTRACTOR.
2. VAN ACCESSIBLE SIGN ONLY REQUIRED WHERE INDICATED ON PLANS.
3. EQUIVALENT SIGNAGE OF DIFFERENT STYLES MAY BE SUBSTITUTED IF APPROVED BY ENGINEER.

TYPICAL HANDICAP PARKING SIGNS

BB / C201 — TYPICAL HANDICAP PARKING DETAILS
NTS

REV.	DATE	DESCRIPTION	BY	CHK.
0	10/21/25	ISSUED FOR COMMENT	SJR	JRC

DRAWING ISSUE STATUS
PERMIT PLANS

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Road, Unit 14
Portsmouth, NH 03801
603-430-9282
WWW.HALEYWARD.COM

PROJECT
DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE
DETAILS

STATE OF NEW HAMPSHIRE — JOHN R. CHAGNON 19651 — LICENSED PROFESSIONAL ENGINEER

DATE	SCALE
SEPTEMBER 2025	1"=20'
DRAWN BY: SJR	DESIGNED BY: —	CHECKED BY: JRC
PROJECT No.: 5010156.1532	FIELD BOOK / PAGE: FB 276 PG 60
DRAWING No.: C505	REV.: 0

OVERVIEW

The Bio Clean Downspout Filter is the industry's leading solution for treatment of roof runoff. This technology is used to treat commercial and industrial rooftops along with highrise buildings, parking structures, and residential buildings.

Available in 3 sizes, this filter can easily adapt to downspouts 2" to 12" in diameter. The filter comes standard with rubber boots that allow for easy installation to the downspout.

Proven since 2003, the Downspout Filter has been used on hundreds of installations throughout the United States. All internal components are constructed of stainless steel.

The sleek in-line design allows the filter to be used in tight spaces. Approved by the IAPMO, this filter can meet all your needs.

PERFORMANCE

93% REMOVAL OF TSS

87% REMOVAL OF HYDROCARBONS

- EFFECTIVE AT REMOVING METALS, NUTRIENTS, AND BACTERIA (MEDIA TYPE)

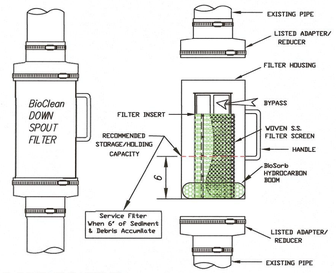
ADVANTAGES

- 1-YEAR WARRANTY
- NO NETS OR GEOFABRICS
- SLEEK IN-LINE DESIGN
- HIGH TREATMENT FLOW RATE
- HIGH BYPASS FLOW RATE
- LOW COST

SERVICE MANUAL

(Cleaning Procedures)

Bio Clean DOWNSPOUT FILTER
Screen Type With Hydrocarbon Boom



DETAIL OF PARTS

TOOLS AND EQUIPMENT NEEDED:

1. **Medium size flat scred driver**
2. **BioSorb hydrocarbon boom. 25-1/2" X 2" dia. (Call Bio Clean to order)**
3. **Trash container or bag**
4. **Wooden dowel approx. 3' x 1/2' dia.**

Bio Clean
A Forterra Company

P.O. BOX 869, Oceanside, Ca. 92049
(760) 433-7640 Fax (760) 433-3176
www.biocleanenvironmental.net

REMOVING FILTER

Bio Clean
A Forterra Company

P.O. BOX 869, Oceanside, Ca. 92049
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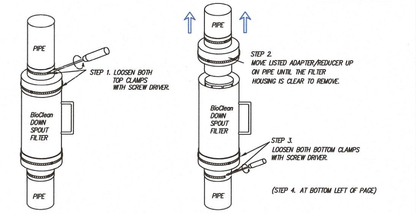
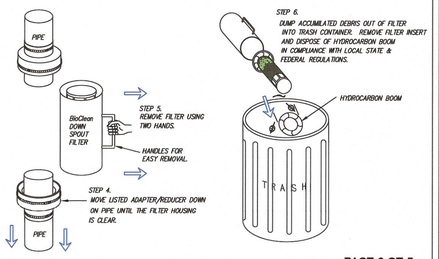


(STEP 4. AT BOTTOM LEFT OF PAGE)

CLEANING FILTER



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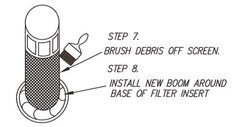
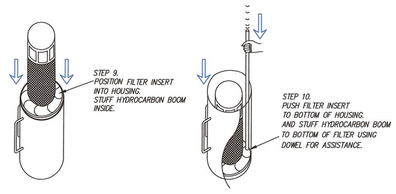


REPLACING FILTER INSERT



REPLACING FILTER

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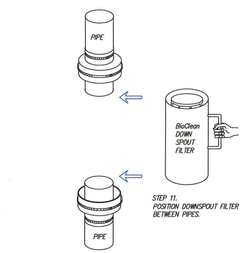
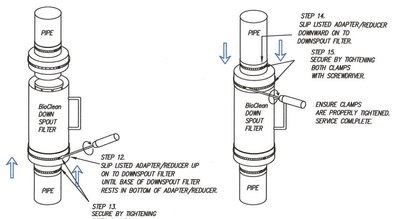
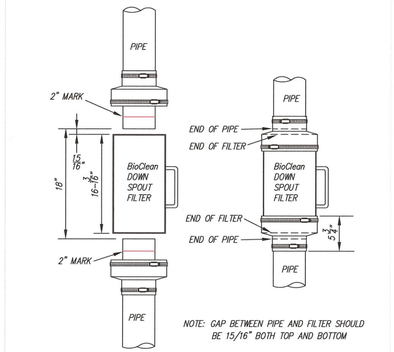




APPROPRIATE INSTALLATION

FILTER CENTERED BETWEEN PIPES WITH EVEN GAPS ON TOP AND BOTTOM



NOTE: GAP BETWEEN PIPE AND FILTER SHOULD BE 15/16" BOTH TOP AND BOTTOM

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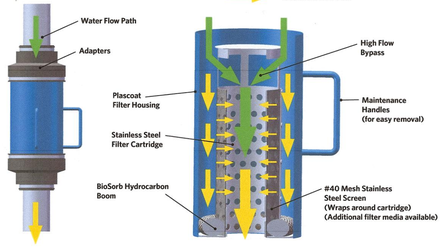
OPERATION



APPROVALS

IAPMO Testing & Approval Listing

SPECIFICATIONS

MODEL #	INLET ID (dia., in.)	FILTER OD (dia., in.)	STORAGE CAP. (cu. ft.)	FILTERED FLOW (gpm)	BYPASS FLOW (gpm)
BC-DF4	4	6.625	0.09	249	566
BC-DF6	6	8.625	0.21	509	1006
BC-DF8	8	8.625	0.21	509	1006
BC-DF10	10	12.75	0.77	1145	2264
BC-DF12	12	12.75	0.77	1145	2264

DOWNSPOUT FILTER

MAINTENANCE:

THE FILTER IS DESIGNED TO ALLOW FOR THE USE OF MANUAL OR VACUUM REMOVAL OF CAPTURED MATERIALS IN THE FILTER

STRUCTURE. FILTERS CAN BE CLEANED EASILY BY SIMPLY LOOSENING THE METAL CLAMPS AND REMOVING THE FILTER. THE HYDROCARBON

ADSORBENT MEDIA THEN IS REMOVED AND THE TRASH AND DEBRIS CAN BE REMOVED FROM THE STRUCTURE. AT EACH CLEANING, NEW

HYDROCARBON ADSORBENT MEDIA SHOULD BE REINSTALLED.

MAINTENANCE NOTES:

1. BIO CLEAN ENVIRONMENTAL SERVICES, INC. RECOMMENDS CLEANING AND DEBRIS REMOVAL MAINTENANCE A MINIMUM OF TWO TO FOUR TIMES PER YEAR, AND REPLACEMENT OF MEDIA BOOMS A MINIMUM OF TWICE A YEAR.
2. THE DOWNSPOUT FILTER CAN BE CLEANED BY LOOSING THE METAL CLAMPS AT BOTTOM AND TOP OF RUBBER BOOTS. REMOVE THE FILTER BY GRASPING THE HANDLES, SLIDE DOWN THE BOTTOM BOOT OVER THE OUTFLOW PIPE AND SLIDE UP THE TOP BOOT OVER INFLOW PIPE. PLACE THE FILTER ON THE GROUND. DISPOSE OF ANY TRASH AND SEDIMENTS COLLECTED IN FILTER.
3. ONCE THE FILTER IS FREE, REMOVE THE INTERIOR INSERT. REMOVE THE HYDROCARBON ADSORBENT MEDIA BY UNWRAPPING IT FROM THE INTERIOR INSERT AND REPLACING WITH A NEW MEDIA, WRAPPING IT THE SAME WAY.
4. PLACE THE INTERIOR INSERT BACK INTO THE FILTER.
5. PLACE THE FILTER BACK IN LINE WITH THE PIPE AND SLIDE BACK THE TOP AND BOTTOM BOOTS IN PLACE AND TIGHTEN THE METAL CLAMPS SECURELY.
6. EVALUATION OF THE HYDROCARBON MEDIA SHALL BE PERFORMED AT EACH CLEANING. IF THE MEDIA IS FILLED WITH HYDROCARBONS AND OILS IT SHOULD BE REPLACED.
7. TRANSPORT ALL DEBRIS, TRASH, ORGANICS AND SEDIMENTS TO APPROVED FACILITY FOR DISPOSAL IN ACCORDANCE WITH LOCAL AND STATE REQUIREMENTS.
8. THE HYDROCARBON MEDIA WITH ABSORBED HYDROCARBONS IS CONSIDERED HAZARDOUS WASTE AND NEEDS TO BE HANDLED AND DISPOSED OF AS HAZARDOUS MATERIAL. PLEASE REFER TO STATE AND LOCAL REGULATIONS FOR THE PROPER DISPOSAL OF USED MOTOR OIL/FILTERS.
9. FOLLOWING MAINTENANCE AND/OR INSPECTION, THE MAINTENANCE OPERATOR SHALL PREPARE A MAINTENANCE/INSPECTION RECORD. THE RECORD SHALL INCLUDE ANY MAINTENANCE ACTIVITIES PERFORMED, AMOUNT AND DESCRIPTION OF DEBRIS COLLECTED, AND CONDITION OF FILTER.
10. THE OWNER SHALL RETAIN THE MAINTENANCE/INSPECTION RECORD FOR A MINIMUM OF FIVE YEARS FROM THE DATE OF MAINTENANCE. THESE RECORDS SHALL BE MADE AVAILABLE TO THE GOVERNING MUNICIPALITY FOR INSPECTION UPON REQUEST AT ANY TIME.
11. ANY TOXIC SUBSTANCE OR ITEM FOUND IN THE FILTER IS CONSIDERED AS HAZARDOUS MATERIAL AND CAN ONLY BE HANDLED BY A CERTIFIED HAZARDOUS WASTE TRAINED PERSON (MINIMUM 24-HOUR HAZWOPER).

NOTES:

1) THE CONTRACTOR SHALL NOTIFY DIG SAFE AT 1-888-DIG-SAFE (1-888-344-7233) AT LEAST 72 HOURS PRIOR TO COMMENCING ANY EXCAVATION ON PUBLIC OR PRIVATE PROPERTY WITHIN 100 FEET OF UNDERGROUND UTILITIES. THE EXCAVATOR IS RESPONSIBLE TO MAINTAIN MARKS. DIG SAFE TICKETS EXPIRE IN THIRTY DAYS.

2) UNDERGROUND UTILITY LOCATIONS ARE BASED UPON BEST AVAILABLE EVIDENCE AND ARE NOT FIELD VERIFIED. LOCATING AND PROTECTING ANY ABOVEGROUND OR UNDERGROUND UTILITIES IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND/OR THE OWNER. UTILITY CONFLICTS SHOULD BE REPORTED AT ONCE TO THE DESIGN ENGINEER.

3) CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE "NEW HAMPSHIRE STORMWATER MANUAL, VOLUME 3, EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION. (NHDES DECEMBER 2008).

REV.	DATE	DESCRIPTION	BY	CHK
0	10/21/25	ISSUED FOR COMMENT	SJR	JRC

DRAWING ISSUE STATUS

PERMIT PLANS

HALEY WARD
ENGINEERING | ENVIRONMENTAL | SURVEYING
200 Griffin Road, Unit 14
Portsmouth, NH 03801
603-430-9282
WWW.HALEYWARD.COM

PROJECT

DOUBLE MC, LLC
134 PLEASANT STREET PORTSMOUTH, N.H.

TITLE

DETAILS

JOHN R. CHAGNON No. 9551 LICENSED PROFESSIONAL ENGINEER - STATE OF NEW HAMPSHIRE -

DATE	SCALE
SEPTEMBER 2025	1"=20'

DRAWN BY	DESIGNED BY	CHECKED BY
SJR	---	JRC

PROJECT No. 5010156.1532 | FIELD BOOK / PAGE FB 276 PG 60

DRAWING No. **C506** | REV. **0**

ARCOVE

134 Pleasant Street

12/23/2025 revised

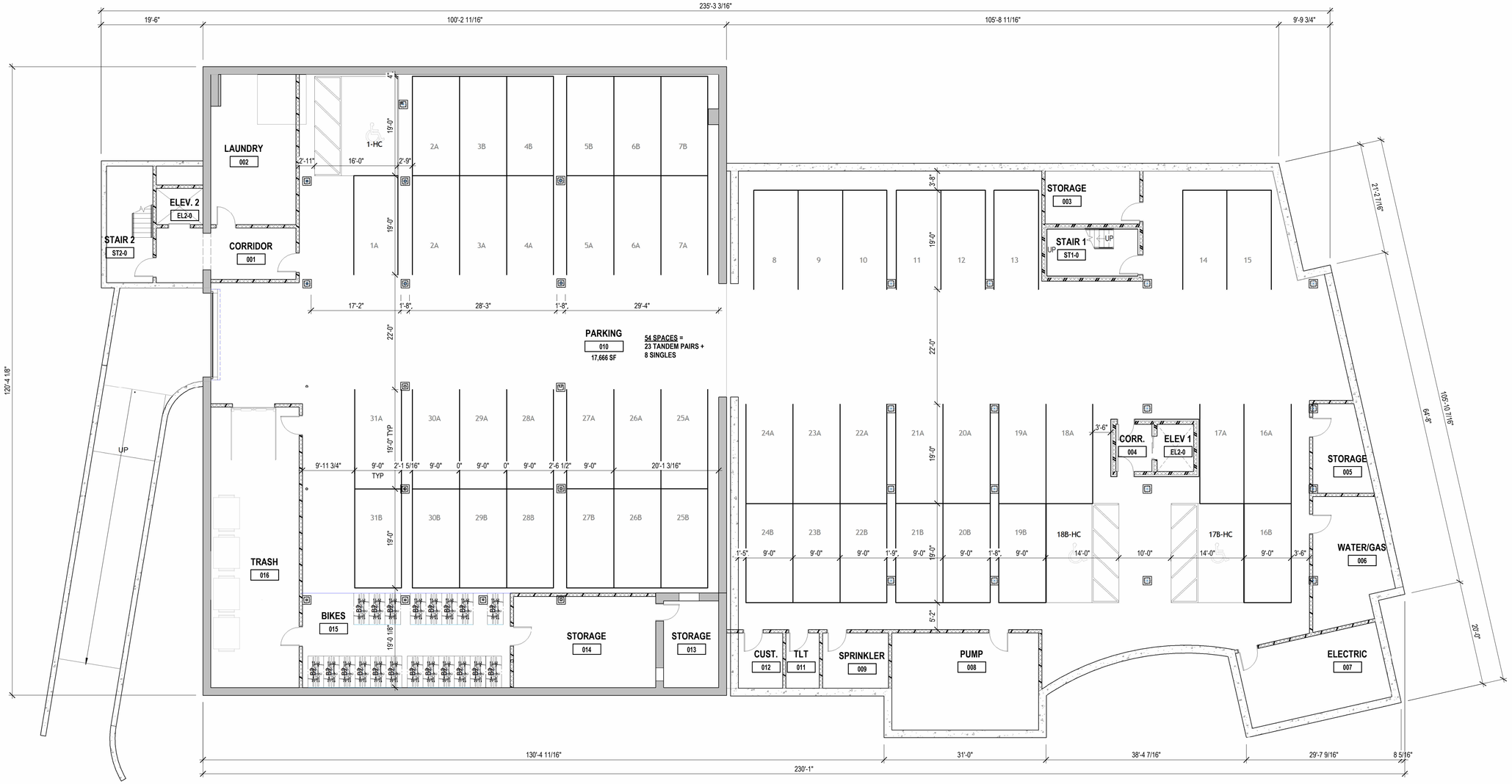
Parking Demand Analysis

Residential - dwelling units				parking		
Level	**UNIT No.**	**QTY bedrooms**	**Area (sf)**	**spaces/unit**	**TOTAL**	
LEVEL 2 A	201	2	735	1	1	
LEVEL 2 A	202	1	666	1	1	
LEVEL 2 A	203	1	753	1	1	
LEVEL 2 A	204	1	718	1	1	
LEVEL 2 A	205	2	751	1	1	
LEVEL 2 A	206	2	772	1	1	
LEVEL 2 A	207	2	741	1	1	
LEVEL 2 A	208	1	604	1	1	
LEVEL 2 A	209	1	666	1	1	
LEVEL 2 A	210	1	666	1	1	
LEVEL 2 A	211	2	761	1	1	
LEVEL 2 A	212	1	565	1	1	
LEVEL 2 A	213	1	540	1	1	
LEVEL 2 A	214	1	583	1	1	
LEVEL 2 B	221	3	1,604	1	1	
LEVEL 2 B	222	3	1,392	1	1	
PENTHOUSE APARTMENT 3BR	301	3	1,634	1	1	
PENTHOUSE APARTMENT 3BR	302	3	1,317	1	1	
subtotal, gross	**18**	**31**	**15,466**		**18**	
Visitor Parking - 1 space per every 5 dwelling units (apartments) rounded up					4	
total, gross					**22**	
Bicycle Parking , Residential						
use	spaces required per use			TOTAL		
multifamily dwellings	1 bicycle per 5 dwelling units where > 4 dwelli			4		
bicycle parking provided	outside			5		
bicycle parking provided	inside			20		
total bike parking provided - Residential				25		
Car parking deduction = 1 car space for 6 excess bikes provided, max 5%:						
max deduction 5% allowed =	1.1	parking spaces deduction		Rounded dn	(1)	
Net Total Residential parking required (round up)					**21**	

Commercial						
level	**Suite No.**	**Name**	**Area (sf)**	**sf/parking space**	**TOTAL spaces**	
LEVEL 1 B	103	RETAIL BANKING	8,323	350	24	
LEVEL 2 B	200	FINANCIAL SERVICES	4,087	350	12	
LEVEL 1 A	102	HEALTH CLUB / PHYSICAL THERAPY	11,144	250	45	
subtotal, gross					**81**	
Bicycle Parking , Commercial						
use	spaces required per use			TOTAL		
Commercial Use	1 bicycle per 10 spaces required			9		
bicycle parking provided	outside			5		
bicycle parking provided	inside			20		
total bike parking provided - Commercial				25		
Car parking deduction = 1 car space for 6 excess bikes provided, max 5%:						
max deduction 5% allowed =	4.1	parking spaces deduction		Rounded dn	(4)	
Net Total Commercial parking required (round up)					**77**	

Shared Parking 10.1112.60										
Total Parking Required	**Weekday**				**Weekend**				**Nighttime**		
Land Use	**daytime (8am-5pm)**	**spaces required**	**evening (6-12pm)**	**spaces required**	**daytime (8am-5pm)**	**spaces required**	**evening (6-midnight)**	**spaces required**	**(midnight 6am)**	**spaces required**	**Maximum TOTAL Required**
Apartments (Residential)	60%	12.6	100%	21.0	80%	16.8	100%	21.0	100%	21.0	
Retail Banking (Service)	60%	14.4	90%	21.6	100%	24.0	70%	16.8	5%	1.2	
Financial Services (Office)	100%	12.0	20%	2.4	10%	1.2	5%	0.6	5%	0.6	
Health Club / PT (Service)	60%	27.0	90%	40.5	100%	45.0	70%	31.5	5%	2.3	
Shared Parking TOTAL Required		66.0		85.50		87.0		69.9		25.1	**86**
PARKING EASEMENT - 93 PLEASANT ST (HOTEL)											**5**
NET TOTAL PARKING REQUIRED (residential + commercial)											**91**

SUMMARY						
parking proposed - outside						37
parking proposed - inside		**23 tandems (*2) + 8 singles = 54**				**54**
Total parking proposed						**91**
EXCESS beyond Required *(Defecit)*						**0**
Existing total available onsite today						38
Proposed Net **Increase** (reduction) in parking						**53**



1 **PB BASEMENT**
3/32" = 1'-0"

ARCOVE ARCHITECTS
767 ISLINGTON ST, STE 2A
PORTSMOUTH NH 03801
603.988.0042
www.ARCove.com

134 PLEASANT STREET

PORTSMOUTH, NH 03801

PROJECT NO: 1028

OWNER

Double Mc, LLC
134 Pleasant Street
Portsmouth, New Hampshire 03801

CIVIL ENGINEERING

Haley Ward
200 Griffin Road, Unit
Portsmouth, NH 03801
(603) 430-9282

LANDSCAPE ARCHITECT

Terra Firma Landscape Architecture
163a Court St
Portsmouth, NH 03801
(603) 531-9109

BASEMENT FLOOR PLAN

SCALE: 3/32" = 1'-0"
DATE: 12/23/2025
DRAWN: HA
CHECKED: TK

PB0.1