



City of Portsmouth, New Hampshire

Site Plan Application Checklist

This site plan application checklist is a tool designed to assist the applicant in the planning process and for preparing the application for Planning Board review. The checklist is required to be completed and uploaded to the Site Plan application in the City's online permitting system. A pre-application conference with a member of the planning department is strongly encouraged as additional project information may be required depending on the size and scope. The applicant is cautioned that this checklist is only a guide and is not intended to be a complete list of all site plan review requirements. Please refer to the Site Plan review regulations for full details.

Applicant Responsibilities (Section 2.5.2): Applicable fees are due upon application submittal along with required attachments. The application shall be complete as submitted and provide adequate information for evaluation of the proposed site development. Waiver requests must be submitted in writing with appropriate justification.

Name of Applicant: North Mill Pond Holdings, LLC Date Submitted: March 22, 2021

Application # (in City's online permitting): LU 21-

Site Address: Raynes Avenue Map: 123 Lot: 10, 12, 13 & 14

Application Requirements			
<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page or Plan Sheet/Note #)	Waiver Requested
<input checked="" type="checkbox"/>	Complete application form submitted via the City's web-based permitting program (2.5.2.1(2.5.2.3A))	Enclosed	N/A
<input checked="" type="checkbox"/>	All application documents, plans, supporting documentation and other materials uploaded to the application form in viewpoint in digital Portable Document Format (PDF). One hard copy of all plans and materials shall be submitted to the Planning Department by the published deadline. (2.5.2.8)	Enclosed	N/A

Site Plan Review Application Required Information			
<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page/line or Plan Sheet/Note #)	Waiver Requested
<input checked="" type="checkbox"/>	Statement that lists and describes "green" building components and systems. (2.5.3.1B)	Enclosed	
<input checked="" type="checkbox"/>	Existing and proposed gross floor area and dimensions of all buildings and statement of uses and floor area for each floor. (2.5.3.1C)	Site Plan Sheet C-102	N/A
<input checked="" type="checkbox"/>	Tax map and lot number, and current zoning of all parcels under Site Plan Review. (2.5.3.1D)	Site Plan Sheet C-102	N/A

Site Plan Review Application Required Information			
<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page/line or Plan Sheet/Note #)	Waiver Requested
<input checked="" type="checkbox"/>	Owner's name, address, telephone number, and signature. Name, address, and telephone number of applicant if different from owner. (2.5.3.1E)	Enclosed Existing Conditions Plan Sheet 1 of 3	N/A
<input checked="" type="checkbox"/>	Names and addresses (including Tax Map and Lot number and zoning districts) of all direct abutting property owners (including properties located across abutting streets) and holders of existing conservation, preservation or agricultural preservation restrictions affecting the subject property. (2.5.3.1F)	Existing Conditions Plan Sheets	N/A
<input checked="" type="checkbox"/>	Names, addresses and telephone numbers of all professionals involved in the site plan design. (2.5.3.1G)	Cover Sheet	N/A
<input checked="" type="checkbox"/>	List of reference plans. (2.5.3.1H)	Existing Conditions Plan Sheet 1 of 3	N/A
<input checked="" type="checkbox"/>	List of names and contact information of all public or private utilities servicing the site. (2.5.3.1I)	Utilities Plan Sheet C-104	N/A

Site Plan Specifications			
<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page/line or Plan Sheet/Note #)	Waiver Requested
<input checked="" type="checkbox"/>	Full size plans shall not be larger than 22 inches by 34 inches with match lines as required, unless approved by the Planning Director.. (2.5.4.1A)	Required on all plan sheets	N/A
<input checked="" type="checkbox"/>	Scale: Not less than 1 inch = 60 feet and a graphic bar scale shall be included on all plans. (2.5.4.1B)	Required on all plan sheets	N/A
<input checked="" type="checkbox"/>	GIS data should be referenced to the coordinate system New Hampshire State Plane, NAD83 (1996), with units in feet. (2.5.4.1C)	Existing Conditions Plan Sheet C-101	N/A
<input checked="" type="checkbox"/>	Plans shall be drawn to scale and stamped by a NH licensed civil engineer. (2.5.4.1D)	Required on all plan sheets	N/A
<input checked="" type="checkbox"/>	Wetlands shall be delineated by a NH certified wetlands scientist and so stamped. (2.5.4.1E)	Existing Conditions Plan Sheet C-101	N/A
<input checked="" type="checkbox"/>	Title (name of development project), north point, scale, legend. (2.5.4.2A)	Required on all plan sheets	N/A
<input checked="" type="checkbox"/>	Date plans first submitted, date and explanation of revisions. (2.5.4.2B)	Required on all plan sheets	N/A
<input checked="" type="checkbox"/>	Individual plan sheet title that clearly describes the information that is displayed. (2.5.4.2C)	Required on all plan sheets	N/A
<input checked="" type="checkbox"/>	Source and date of data displayed on the plan. (2.5.4.2D)	Required on all plan sheets	N/A

Site Plan Specifications – Required Exhibits and Data

<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page/line or Plan Sheet/Note #)	Waiver Requested
<input checked="" type="checkbox"/>	<p>1. Existing Conditions: (2.5.4.3A)</p> <ul style="list-style-type: none"> • Surveyed plan of site showing existing natural and built features; • Existing building footprints and gross floor area; • Existing parking areas and number of parking spaces provided; • Zoning district boundaries; • Existing, required, and proposed dimensional zoning requirements including building and open space coverage, yards and/or setbacks, and dwelling units per acre; • Existing impervious and disturbed areas; • Limits and type of existing vegetation; • Wetland delineation, wetland function and value assessment (including vernal pools); • SFHA, 100-year flood elevation line and BFE data, as required. 	Existing Conditions Plan Sheets	
<input checked="" type="checkbox"/>	<p>2. Buildings and Structures: (2.5.4.3B)</p> <ul style="list-style-type: none"> • Plan view: Use, size, dimensions, footings, overhangs, 1st fl. elevation; • Elevations: Height, massing, placement, materials, lighting, façade treatments; • Total Floor Area; • Number of Usable Floors; • Gross floor area by floor and use. 	Site Plan Sheets C-102, C-102.1	
<input checked="" type="checkbox"/>	<p>3. Access and Circulation: (2.5.4.3C)</p> <ul style="list-style-type: none"> • Location/width of access ways within site; • Location of curbing, right of ways, edge of pavement and sidewalks; • Location, type, size and design of traffic signing (pavement markings); • Names/layout of existing abutting streets; • Driveway curb cuts for abutting prop. and public roads; • If subdivision; Names of all roads, right of way lines and easements noted; • AASHTO truck turning templates, description of minimum vehicle allowed being a WB-50 (unless otherwise approved by TAC). 	Site Plan Sheets C-102, C-102.1	
<input checked="" type="checkbox"/>	<p>4. Parking and Loading: (2.5.4.3D)</p> <ul style="list-style-type: none"> • Location of off street parking/loading areas, landscaped areas/buffers; • Parking Calculations (# required and the # provided). 	Site Plan Sheets C-102, C-102.1	
<input checked="" type="checkbox"/>	<p>5. Water Infrastructure: (2.5.4.3E)</p> <ul style="list-style-type: none"> • Size, type and location of water mains, shut-offs, hydrants & Engineering data; • Location of wells and monitoring wells (include protective radii). 	Utilities Plan Sheet C-104	
<input checked="" type="checkbox"/>	<p>6. Sewer Infrastructure: (2.5.4.3F)</p> <ul style="list-style-type: none"> • Size, type and location of sanitary sewage facilities & Engineering data, including any onsite temporary facilities during construction period. 	Utilities Plan Sheet C-104	


<input checked="" type="checkbox"/>	7. Utilities: (2.5.4.3G) <ul style="list-style-type: none"> The size, type and location of all above & below ground utilities; Size type and location of generator pads, transformers and other fixtures. 	Utilities Plan Sheet C-104	
<input checked="" type="checkbox"/>	8. Solid Waste Facilities: (2.5.4.3H)	Site Plan Sheet C-102.1	
	<ul style="list-style-type: none"> The size, type and location of solid waste facilities. 	Site Plan Sheet C-102.1	
<input checked="" type="checkbox"/>	9. Storm water Management: (2.5.4.3I) <ul style="list-style-type: none"> The location, elevation and layout of all storm-water drainage. The location of onsite snow storage areas and/or proposed off-site snow removal provisions. Location and containment measures for any salt storage facilities Location of proposed temporary and permanent material storage locations and distance from wetlands, water bodies, and stormwater structures. 	Grading and Drainage Plan Sheet C-103	
<input checked="" type="checkbox"/>	10. Outdoor Lighting: (2.5.4.3J) <ul style="list-style-type: none"> Type and placement of all lighting (exterior of building, parking lot and any other areas of the site) and photometric plan. 	Photometrics Plan	
<input checked="" type="checkbox"/>	11. Indicate where dark sky friendly lighting measures have been implemented. (10.1)	Photometrics Plan	
<input checked="" type="checkbox"/>	12. Landscaping: (2.5.4.3K) <ul style="list-style-type: none"> Identify all undisturbed area, existing vegetation and that which is to be retained; Location of any irrigation system and water source. 	Landscaping Plan Sheets L-101	
<input checked="" type="checkbox"/>	13. Contours and Elevation: (2.5.4.3L) <ul style="list-style-type: none"> Existing/Proposed contours (2 foot minimum) and finished grade elevations. 	Grading and Drainage Plan Sheet C-103	
<input checked="" type="checkbox"/>	14. Open Space: (2.5.4.3M) <ul style="list-style-type: none"> Type, extent and location of all existing/proposed open space. 	Site Plan Sheet C-102	
<input checked="" type="checkbox"/>	15. All easements, deed restrictions and non-public rights of ways. (2.5.4.3N)	Existing Conditions Plan Sheets	
<input checked="" type="checkbox"/>	16. Character/Civic District (All following information shall be included): (2.5.4.3P) <ul style="list-style-type: none"> Applicable Building Height (10.5A21.20 & 10.5A43.30); Applicable Special Requirements (10.5A21.30); Proposed building form/type (10.5A43); Proposed community space (10.5A46). 	Site Plan Sheet C-102	
<input checked="" type="checkbox"/>	17. Special Flood Hazard Areas (2.5.4.3Q) <ul style="list-style-type: none"> The proposed development is consistent with the need to minimize flood damage; All public utilities and facilities are located and construction to minimize or eliminate flood damage; Adequate drainage is provided so as to reduce exposure to flood hazards. 	N/A	

Other Required Information			
<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page/line or Plan Sheet/Note #)	Waiver Requested
<input checked="" type="checkbox"/>	Traffic Impact Study or Trip Generation Report, as required. (3.2.1-2)	Enclosed	
<input checked="" type="checkbox"/>	Indicate where Low Impact Development Design practices have been incorporated. (7.1)	Grading and Drainage Plan Sheet C-103	
<input checked="" type="checkbox"/>	Indicate whether the proposed development is located in a wellhead protection or aquifer protection area. Such determination shall be approved by the Director of the Dept. of Public Works. (7.3.1)	N/A	
<input checked="" type="checkbox"/>	Stormwater Management and Erosion Control Plan. (7.4)	Enclosed	
<input checked="" type="checkbox"/>	Inspection and Maintenance Plan (7.6.5)	Enclosed	

Final Site Plan Approval Required Information			
<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page/line or Plan Sheet/Note #)	Waiver Requested
<input checked="" type="checkbox"/>	All local approvals, permits, easements and licenses required, including but not limited to: <ul style="list-style-type: none"> • Waivers; • Driveway permits; • Special exceptions; • Variances granted; • Easements; • Licenses. (2.5.3.2A)	Cover Sheet	
<input checked="" type="checkbox"/>	Exhibits, data, reports or studies that may have been required as part of the approval process, including but not limited to: <ul style="list-style-type: none"> • Calculations relating to stormwater runoff; • Information on composition and quantity of water demand and wastewater generated; • Information on air, water or land pollutants to be discharged, including standards, quantity, treatment and/or controls; • Estimates of traffic generation and counts pre- and post-construction; • Estimates of noise generation; • A Stormwater Management and Erosion Control Plan; • Endangered species and archaeological / historical studies; • Wetland and water body (coastal and inland) delineations; • Environmental impact studies. (2.5.3.2B)	Enclosed	
<input checked="" type="checkbox"/>	A document from each of the required private utility service providers indicating approval of the proposed site plan and indicating an ability to provide all required private utilities to the site. (2.5.3.2D)	Enclosed	

Final Site Plan Approval Required Information

<input checked="" type="checkbox"/>	Required Items for Submittal	Item Location (e.g. Page/line or Plan Sheet/Note #)	Waiver Requested
<input checked="" type="checkbox"/>	A list of any required state and federal permit applications required for the project and the status of same. (2.5.3.2E)	Cover Sheet	
<input checked="" type="checkbox"/>	A note shall be provided on the Site Plan stating: "All conditions on this Plan shall remain in effect in perpetuity pursuant to the requirements of the Site Plan Review Regulations." (2.5.4.2E)	Site Plan Sheets C-102 & C-102.1	N/A
<input checked="" type="checkbox"/>	For site plans that involve land designated as "Special Flood Hazard Areas" (SFHA) by the National Flood Insurance Program (NFIP) confirmation that all necessary permits have been received from those governmental agencies from which approval is required by Federal or State law, including Section 404 of the Federal Water Pollution Control Act Amendments of 1972, 33 U.S.C. 1334. (2.5.4.2F)	N/A	
<input checked="" type="checkbox"/>	Plan sheets submitted for recording shall include the following notes: a. "This Site Plan shall be recorded in the Rockingham County Registry of Deeds." b. "All improvements shown on this Site Plan shall be constructed and maintained in accordance with the Plan by the property owner and all future property owners. No changes shall be made to this Site Plan without the express approval of the Portsmouth Planning Director." (2.13.3)	Site Plan Sheets C-102.1 & C-102.2	N/A

Applicant's Signature:  Date: 3/22/21

P0595-005
March 22, 2021

Ms. Juliet Walker, Planning Director
City of Portsmouth Planning Department
1 Junkins Avenue
Portsmouth, New Hampshire 03801

**Re: Site Review Permit Application
Proposed Mixed Use Development, Raynes Avenue, Portsmouth, NH**

Dear Juliet:

On behalf of One Raynes Ave, LLC, 31 Raynes Ave, LLC & 203 Maplewood Ave, LLC (owners), and North Mill Pond Holdings, LLC (applicant), we are pleased to submit the following information to support a request for a Site Review Permit for the above referenced project:

- One (1) copy of the Parking Conditional Use Permit Request, dated March 22, 2021;
- One (1) full size & one (1) half size copy of the Site Plan Set, dated March 22, 2021;
- One (1) copy of the Owner Authorization, dated January 11, 2021;
- One (1) copy of the Applicant Authorization, dated January 11, 2021;
- One (1) copy of the Site Review Checklist, dated March 22, 2021;
- One (1) copy of the Drainage Analysis, dated March 22, 2021;
- One (1) copy of the Grade Plane Exhibit, dated March 22, 2021;
- One (1) copy of the Colored Landscape Plan, dated March 18, 2021;
- One (1) copy of the Wetland and Buffer Report, dated January 6, 2020;
- One (1) copy of the Wetland Buffer Impervious Surface Exhibit, dated March 22, 2021;
- One (1) copy of the Community Space Exhibit, dated March 22, 2021;
- One (1) copy of the Truck Turning Exhibit, dated March 22, 2021;
- One (1) copy of the Impervious Surface Reduction Exhibit, dated March 22, 2021;
- One (1) copy of the Traffic Impact Study, dated March 22, 2021;
- One (1) copy of the Unutil Will Service Letter, dated February 22, 2021;
- One (1) copy of the Green Building Statement, dated March 16, 2021;
- One (1) copy of the Application fee calculation form for the Site Review Permit;
- One (1) Site Review Application fee check in the amount of \$5,800.00;
- One (1) Shared Parking Conditional Use Permit fee check in the amount of \$200

The proposed project is located at 1 Raynes Avenue, 31 Raynes Avenue & 203 Maplewood Avenue on properties identified as Map 123 Lots 10, 12, 13 & 14 on the City of Portsmouth Tax Maps. The existing parcels are bound by Raynes Avenue to south, Maplewood Avenue to the west, North Mill Pond to the north and the municipal land to the east, which is the future site of the North Mill Pond community park.

The project will include a voluntary merger of Map 123 Lots 10, 12, 13 & 14 as shown in the enclosed Site Plan. The proposed voluntary merger will create a single development lot with an area of 2.53 acres. The project is also currently under review by the Historic District Commission (HDC).

The proposed project will include the construction of two (2) 5-story buildings. The first is a mixed-use residential building that has a first-floor residential lobby and two (2) commercial spaces, and 60 upper floor residential units. The second is a hotel building with 128 rooms at the corner of Raynes Ave and Vaughan Street. The project will include associated site



improvements such as paving, utilities, lighting, landscaping and community space. The proposed project is providing 22,342 SF of community spaces (20.2% of the total lot area) which meets the 20% of total lot area required to receive the incentive bonus for one additional story (10 ft) above the maximum height requirement on the buildings, with the 5th story on the mixed-use building stepped back 50ft from the street. The community space calculation is depicted in the enclosed Community Space Exhibit. A Conditional Use Permit for Wetland Buffer Impact will be required for the project.

To date the applicant has attending the following meetings with the local land-use boards related to the Site Plan:

- December 8, 2020 – Technical Advisory Committee Work Session
- December 9, 2020 – Conservation Commission Work Session
- December 17, 2020 – Planning Board Conceptual Consultation

The enclosed information which has been prepared to address comments and feedback received to date from these land-use boards.

We respectfully request to be placed on the TAC meeting agenda for April 6, 2021. If you have any questions or need any additional information, please contact Patrick Crimmins by phone at (603) 433-8818 or by email at pmcrimmins@tighebond.com.

Sincerely,

TIGHE & BOND, INC.



Patrick M. Crimmins, PE
Senior Project Manager



Neil A. Hansen, PE
Project Engineer

Cc: North Mill Pond Holdings, LLC (via e-mail)

P0595-007
March 22, 2021

Mr. Dexter Legg, Chairman
City of Portsmouth Planning Department
1 Junkins Avenue
Portsmouth, New Hampshire 03801

Re: **Conditional Use Permit Request for Reduced Off-Street Parking
& Shared Parking on Separate Lots,
Proposed Mixed-Use Development, Raynes Avenue, Portsmouth, NH**

Dear Chairman Legg:

On behalf of One Raynes Ave, LLC, 31 Raynes Ave, LLC & 203 Maplewood Ave, LLC (owners), and North Mill Pond Holdings, LLC (applicant), this letter is to request that a Conditional Use Permit be granted by the Planning Board to allow for reduced off-street parking and parking on a separate lot as allowed by Section 10.1112.14 and 10.1112.62 of the Zoning Ordinance.

The total parking provided on the proposed site plan is 111 spaces. The City Zoning Ordinance requires 159 parking spaces for the proposed mixed-use development. The project team has prepared a parking demand analysis for this mixed-used development based on Institute of Transportation Engineers (ITE) Parking Generation manual, 5th edition (latest).

For this project, parking demand was reviewed for the proposed multi-family and hotel uses using ITE land use code 221 and land use code 310 respectively. We excluded the 1st floor commercial uses for this analysis since parking is not required for these uses in the Downtown Overlay District (DOD) parking regulations. With the project being in the DOD, we studied the ITE parking generation assuming a dense multi-use urban setting, with no nearby rail transit. For a 128-key hotel in this setting, peak parking demand based on ITE Parking Generation is 97 parking spaces. For the proposed 60 dwelling unit multi-family use, we calculated peak parking demand based on the 70 total bedrooms that will be provided in these units. Based on ITE Parking Generation, 70 bedrooms generates an average peak parking demand in this dense multi-use setting of 34 spaces. Thus, based on ITE Parking Generation, the total average peak parking demand for the project is 131 spaces. The applicant will have the ability to share private parking with the office building that is currently being constructed across the street at 145 Maplewood Avenue. With the off-setting peaks of these complimentary uses, the project anticipates it will be able to share up 25 spaces with the office building, a use that does not have a parking requirement in the DOD and the use of which is complimentary to the hotel, providing a total of 136 spaces for the project which exceeds the ITE peak parking demand of 131 spaces.

In addition to seeking the Conditional Use Permit for reduced parking, the applicant has explored creative parking solutions to achieve the City's parking requirements. The applicant has designed the mixed-use building such that parking lift systems can be installed in the locations of the tandem spaces that are covered by the multi-family units above in the mixed-use building. The lift systems would provide an additional 23 parking spaces on top of the 136 that the project is providing, bringing the total parking provided to 159 spaces. The applicant does not anticipate these will ever be needed but to show that the project could meet the City's parking requirements, the applicant proposes to include these lift systems as "reserve spaces" that could be constructed in the future if the applicant deems that this additional parking is in fact needed to support the developments building program.



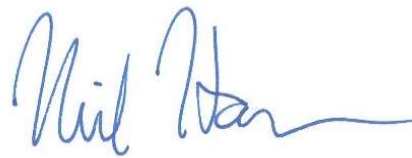
The applicant respectfully requests a Conditional Use Permit for Reduced Off-Street Parking & Shared Parking on Separate Lots be granted. If you have any questions or need any additional information, please contact Patrick Crimmins by phone at (603) 433-8818 or by email at pmcrimmins@tighebond.com.

Sincerely,

TIGHE & BOND, INC.



Patrick M. Crimmins, PE
Senior Project Manager



Neil A. Hansen, PE
Project Engineer

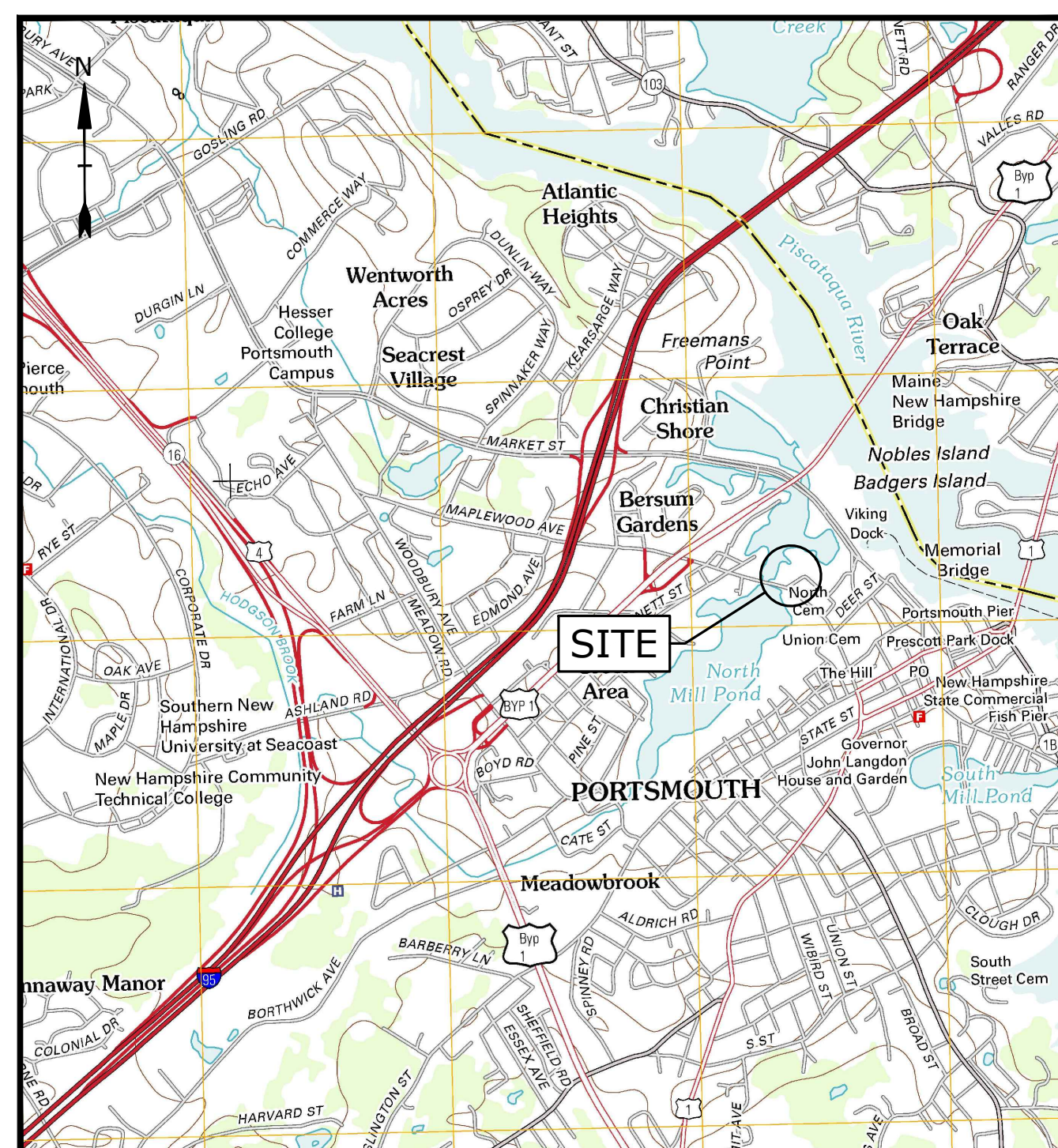
Copy: North Mill Pond Holdings, LLC

PROPOSED MIXED USE DEVELOPMENT

RAYNES AVENUE
PORTSMOUTH, NEW HAMPSHIRE

MARCH 22, 2021

LIST OF DRAWINGS		
SHEET NO.	SHEET TITLE	LAST REVISED
	COVER SHEET	3/22/2021
G-100	GENERAL NOTES AND LEGEND	3/22/2021
1 OF 3	EXISTING CONDITIONS PLAN	6/17/2020
2 OF 3	EXISTING CONDITIONS PLAN	6/17/2020
3 OF 3	EXISTING CONDITIONS PLAN	6/17/2020
C-101	DEMOLITION PLAN	3/22/2021
C-102	OVERALL SITE PLAN	3/22/2021
C-102.1	SITE PLAN	3/22/2021
C-103	GRADING, DRAINAGE AND EROSION CONTROL PLAN	3/22/2021
C-104	UTILITIES PLAN	3/22/2021
C-201	EASEMENT PLAN	3/22/2021
L-100	LANDSCAPE MATERIAL PLAN LEGEND AND NOTES	3/22/2021
L-101	LANDSCAPE PLANTING PLAN	3/22/2021
C-501	EROSION CONTROL NOTES AND DETAILS SHEET	3/22/2021
C-502	DETAILS SHEET	3/22/2021
C-503	DETAILS SHEET	3/22/2021
C-504	DETAILS SHEET	3/22/2021
C-505	DETAILS SHEET	3/22/2021
C-506	DETAILS SHEET	3/22/2021
C-507	DETAILS SHEET	3/22/2021
A3.00	EXTERIOR ELEVATIONS	



LOCATION MAP
SCALE: 1" = 2,000'

PREPARED BY:
Tighe & Bond
177 CORPORATE DRIVE
PORTSMOUTH, NEW HAMPSHIRE 03801
603-433-8818

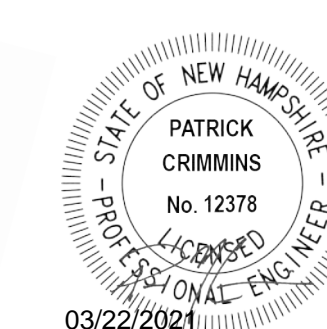
APPLICANT:
NORTH MILL POND HOLDINGS LLC
1359 HOOKSETT ROAD
HOOKSETT, NEW HAMPSHIRE 03106

OWNERS:
TAX MAP 123, LOT 10 & 13
31 RAYNES LLC C/O
PORTSMOUTH CHEVROLET
549 ROUTE 1 BYPASS
PORTSMOUTH, NEW HAMPSHIRE 03801

SURVEYOR:
DOUCET SURVEY, LLC
102 KENT PLACE
NEWMARKET, NH 03857

TAX MAP 123, LOT 12
203 MAPLEWOOD AVENUE LLC
549 HIGHWAY 1 BYPASS
PORTSMOUTH, NH 03801

TAX MAP 123, LOT 14
ONE RAYNES AVE LLC
1359 HOOKSETT RD
HOOKSETT, NEW HAMPSHIRE 03106



LIST OF PERMITS		
LOCAL	STATUS	DATE
SITE PLAN REVIEW PERMIT		
CONDITIONAL USE PERMIT- WETLAND BUFFER		
CONDITIONAL USE PERMIT- PARKING		
STATE		
NHDES - ALTERATION OF TERRAIN PERMIT		
NHDES - WETLAND PERMIT		
NHDES - SHORELAND PERMIT		
NHDES - SEWER CONNECTION PERMIT		

- NOTES:**
- REFERENCE:
 - TAX MAP 123, LOT 10
 - TAX MAP 123, LOT 12
 - TAX MAP 123, LOT 13
 - TAX MAP 123, LOT 14
 - RAYNES AVENUE & MAPLEWOOD AVENUE
 - PORTSMOUTH, NEW HAMPSHIRE
 - D.S. PROJECT NO. 6082
 - TOTAL PARCEL AREA: 71,149 SQ. FT. OR 1.633 AC. (COMBINED LOTS 10, 12 & 13) 39,459 SQ. FT. OR 0.906 AC. (LOT 14)
 - OWNER OF RECORD:

TAX MAP 123, LOTS 10 & 13 31 RAYNES LLC C/O PORTSMOUTH CHEVROLET 549 ROUTE 1 BYPASS PORTSMOUTH, NH 03801 R.C.R.D. BOOK 4676, PAGE 654 R.C.R.D. BOOK 4676, PAGE 657	TAX MAP 123 LOT 12 203 MAPLEWOOD AVENUE LLC C/O PORTSMOUTH CHEVROLET 549 ROUTE 1 BYPASS PORTSMOUTH, NH 03801 R.C.R.D. BOOK 5621, PAGE 420 R.C.R.D. BOOK 5621, PAGE 420
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- ZONE: CD4
 - OVERLAY DISTRICTS
 - DOWNTOWN OVERLAY DISTRICT
 - HISTORIC DISTRICT
- ZONING DISTRICTS BASED ON THE CITY OF PORTSMOUTH ZONING MAP DATED 11/12/15 AS AVAILABLE ON THE CITY WEBSITE ON 11/18/19. SEE CITY OF PORTSMOUTH ZONING ORDINANCE ARTICLE 5A, SECTION 10.5440 FOR DIMENSIONAL REGULATIONS. THE LAND OWNER IS RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE MUNICIPAL, STATE AND FEDERAL REGULATIONS.

THE SITE IS SUBJECT TO THE STATE OF NH SHORELAND WATER QUALITY PROTECTION ACT. SEE NHDES WEBSITE FOR SPECIFIC DIMENSIONAL REQUIREMENT.
- FIELD SURVEY PERFORMED BY D.C.B. & K.J.L. DURING NOVEMBER 2019 & BY G.M.E. & J.P.E. DURING JUNE 2020 USING A TRIMBLE S7 TOTAL STATION AND A TRIMBLE R8 SURVEY GRADE GPS WITH A TRIMBLE 13C3 DATA COLLECTOR. TRIMBLE DINI DIGITAL LEVEL. TRAVERSE ADJUSTMENT BASED ON LEAST SQUARE ANALYSIS.

FIELD SURVEY PERFORMED BY M.J.C. ON OCTOBER 2019 USING A LEICA HDS SCANNER. REGISTRATION ADJUSTMENT BASED ON LEAST SQUARE ANALYSIS.
- JURISDICTIONAL WETLANDS DELINEATED BY TIGHE & BOND, DURING OCTOBER 2019 IN ACCORDANCE WITH 1987 CORPS OF ENGINEERS WETLANDS DELINEATION MANUAL, TECHNICAL REPORT Y-87-1 AND THE INTERIM REGIONAL SUPPLEMENT TO THE CORPS OF ENGINEERS WETLAND DELINEATION MANUAL: NORTH CENTRAL AND NORTHEAST REGION (OCTOBER, 2009).
- VERTICAL DATUM IS BASED ON NGVD29 PER DISK B2 1923.
- HORIZONTAL DATUM BASED ON NEW HAMPSHIRE STATE PLANE(2800) NAD83(2011) DERIVED FROM REDUNDANT GPS OBSERVATIONS UTILIZING THE KEYNET GPS VRS NETWORK.
- PROPER FIELD PROCEDURES WERE FOLLOWED IN ORDER TO GENERATE CONTOURS AT 2' INTERVALS. ANY MODIFICATION OF THIS INTERVAL WILL DIMINISH THE INTEGRITY OF THE DATA, AND DOUCET SURVEY, INC. WILL NOT BE RESPONSIBLE FOR ANY SUCH ALTERATION PERFORMED BY THE USER.
- UNDERGROUND UTILITIES SHOWN HEREON ARE BASED ON OBSERVABLE PHYSICAL EVIDENCE AND PAINT MARKS FOUND ON-SITE.
- THE ACCURACY OF MEASURED UTILITY INVERTS AND PIPE SIZES/TYPES IS SUBJECT TO NUMEROUS FIELD CONDITIONS, INCLUDING: THE ABILITY TO MAKE VISUAL OBSERVATIONS, DIRECT ACCESS TO THE VARIOUS ELEMENTS, MANHOLE CONFIGURATION, ETC.
- WATER BOUNDARIES ARE DYNAMIC IN NATURE AND ARE SUBJECT TO CHANGE DUE TO NATURAL CAUSES SUCH AS EROSION OR ACCRETION.
- MEAN HIGH WATER (EL. 3.0' NGVD1929) AND HIGHEST OBSERVABLE TIDE (EL. 4.3' NGVD1929) ELEVATIONS PER "MAPLEWOOD AVENUE CULVERT REPLACEMENT AND NORTH MILL POND RESTORATION, WATERFRONT/STRUCTURAL BASIS OF DESIGN, BY WATERFRONT ENGINEERS, LLC, DATED DECEMBER 30, 2009", PROVIDED BY TIGHE & BOND ON 11-30-15.
- THE INTENT OF THIS PLAN IS TO SHOW THE LOCATION OF BOUNDARIES IN ACCORDANCE WITH AND IN RELATION TO THE CURRENT LEGAL DESCRIPTION, AND IS NOT AN ATTEMPT TO DEFINE UNWRITTEN RIGHTS, DETERMINE THE EXTENT OF OWNERSHIP, OR DEFINE THE LIMITS OF TITLE.
- DUE TO THE COMPLEXITY OF RESEARCHING ROAD RECORDS AS A RESULT OF INCOMPLETE, UNORGANIZED, INCONCLUSIVE, OBLITERATED, OR LOST DOCUMENTS, THERE IS AN INHERENT UNCERTAINTY INVOLVED WHEN ATTEMPTING TO DETERMINE THE LOCATION AND WIDTH OF A ROADWAY RIGHT OF WAY. THE EXTENT OF GREEN STREET AS DEPICTED HEREON IS/ARE BASED ON RESEARCH CONDUCTED AT THE CITY OF PORTSMOUTH CITY HALL, THE CITY OF PORTSMOUTH DEPARTMENT OF PUBLIC WORKS & THE ROCKINGHAM COUNTY REGISTRY OF DEEDS.

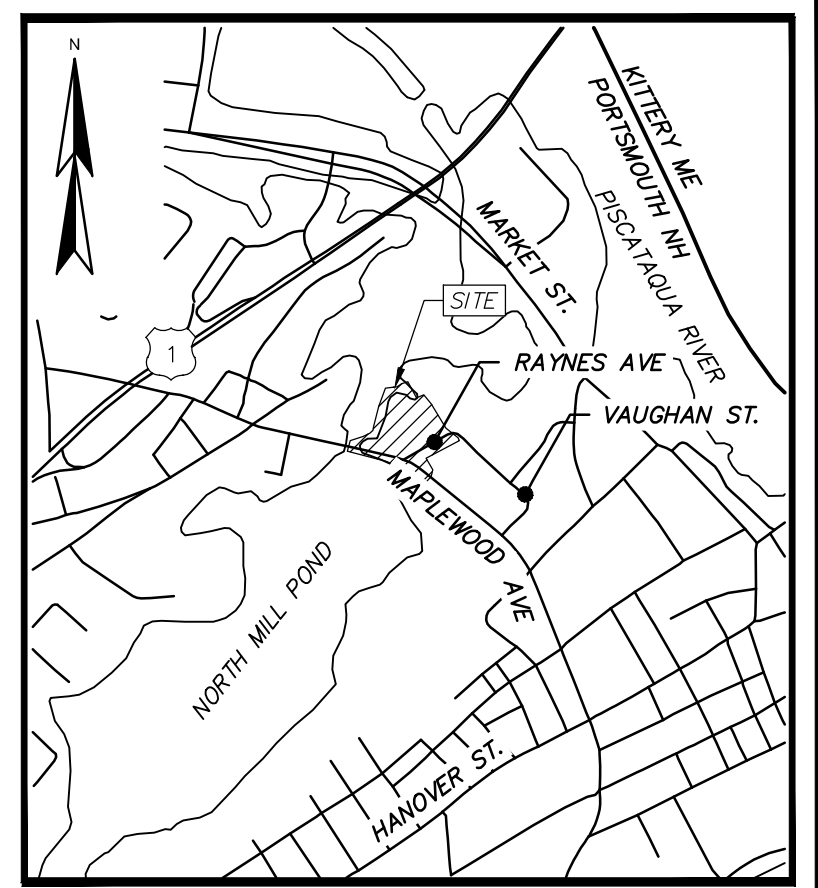
EDGE OF RIGHT OF WAY BASED ON HOLDING 52 FOOT WIDE RIGHT OF WAY ALONG RAYNES AVENUE PER REFERENCE PLANS #10 & #11. THE GEOMETRY FROM REFERENCE PLAN #11 WAS THEN ALIGNED TO THE REBAR SHOWN ON THE NORTHERLY SIDE OF MAPLEWOOD AVENUE.
- ALL UNDERGROUND UTILITIES (ELECTRIC, GAS, TEL, WATER, SEWER DRAIN SERVICES) ARE SHOWN IN SCHEMATIC FASHION. THEIR LOCATIONS ARE NOT PRECISE OR NECESSARILY ACCURATE. NO WORK WHATSOEVER SHALL BE UNDERTAKEN USING THIS PLAN TO LOCATE THE ABOVE SERVICES. CONSULT WITH THE PROPER AUTHORITIES CONCERNED WITH THE SUBJECT SERVICE LOCATIONS FOR INFORMATION REGARDING SUCH. CALL DIG-SAFE AT 1-888-DIG-SAFE.
- TAX MAP 123, LOTS 10, 12, 13 & 14 IS/ARE EITHER SUBJECT TO OR IN BENEFIT OF, BUT NOT LIMITED TO, THE FOLLOWING EASEMENTS/RIGHTS OF RECORD:
 - 12' WIDE RIGHT OF WAY, SEE R.C.R.D. BOOK 4676, PAGE 657 AND REFERENCE PLAN #11.
 - RIGHT OF WAY, SEE R.C.R.D. BOOK 4676, PAGE 657 & BOOK 5621, PAGE 420.
 - SEWER RIGHTS, SEE R.C.R.D. BOOK 4676, PAGE 657 (LOCATION UNKNOWN).
 - 15' WIDE WALKWAY & LANDSCAPE EASEMENT, SEE R.C.R.D. BOOK 4676, PAGE 657.
 - ELECTRIC EASEMENT, SEE R.C.R.D. BOOK 3205, PAGE 1449.
 - TAX MAP 123, LOT 14 IS SUBJECT TO LEASEHOLD RIGHTS AS LISTED IN R.C.R.D. BOOK 6088, PAGE 1267.

REFERENCE PLANS:

- "STANDARD BOUNDARY SURVEY, TAX MAP 123 - LOT 15 & TAX MAP 124 LOT 10" DATED JULY 2008, REVISED 4/25/13 BY AMBIT ENGINEERING, INC. R.C.R.D. PLAN #D-37722.
- "PROPERTY STAKEOUT SKETCH, PORTSMOUTH PROPERTY TRUST, PE SPAULDING REVOCABLE TRUST", BY AMBIT ENGINEERING, INC., DATED JANUARY 30, 2007, NOT RECORDED.
- "VAUGHAN STREET URBAN RENEWAL PROJECT N.H. R-10 PORTSMOUTH, NH, CONDEMNATION MAP", BY ANDERSON-NICHOLS & CO., INC., DATED FEBRUARY 1971, R.C.R.D. PLAN D-2425.
- "STANDARD BOUNDARY SURVEY, TAX MAP 123, LOTS 10 & 13 FOR RAYNES, LLC", BY AMBIT ENGINEERING, INC., NOT RECORDED.
- "EASEMENT PLAN, EGRESS EASEMENT TO 319 VAUGHAN STREET CENTER, LLC, TAX MAP 124, LOT 9 & TAX MAP 123, LOT 15, PROPERTY OF 299 VAUGHAN STREET, LLC C/O CATHARTES PRIVATE INVESTMENTS", BY AMBIT ENGINEERING, INC., DATED MARCH 2014, R.C.R.D. PLAN #D-38358.
- "EASEMENT PLAN SIDEWALK EASEMENT TO CITY OF PORTSMOUTH, TAX MAP 124, LOT 9 PROPERTY OF 319 VAUGHAN STREET CENTER, LLC", BY AMBIT ENGINEERING, INC., DATED FEBRUARY 2014, R.C.R.D. PLAN #D-38315.
- "PLAN OF LAND PORTSMOUTH, NH FOR WILLIAM A. HYDER", BY JOHN W. DURGIN, DATED JUNE 1955, ON FILE AT JAMES VERRA & ASSOCIATES.
- "STANDARD PROPERTY SURVEY FOR PROPERTY AT 111 MAPLEWOOD AVENUE", BY EASTERLY SURVEYING, INC., DATED 1/31/06, R.C.R.D. PLAN #D-33786.
- "VAUGHAN STREET URBAN RENEWAL PROJECT N.H. R-10 PORTSMOUTH, NH, DISPOSITION PLAN PARCEL 3", BY ANDERSON-NICHOLS & CO., INC., DATED JUNE 1973, R.C.R.D. PLAN D-4019.
- "VAUGHAN STREET URBAN RENEWAL PROJECT N.H. R-10 PORTSMOUTH, NH, DISPOSITION MAP", BY ANDERSON-NICHOLS & CO., INC., DATED NOVEMBER 1969, R.C.R.D. PLAN D-2408.
- "LAND OF HEIRS OF JOHN AUGUST HETT", BY JOHN W. DURGIN, DATED APRIL 1938, ON FILE AT JAMES VERRA AND ASSOCIATES.
- "LAND IN PORTSMOUTH, NH OWNED BY ARMOUR & CO.", BY JOHN W. DURGIN DATED OCTOBER 1938, ON FILE AT JAMES VERRA AND ASSOCIATES.
- "LAND ON VAUGHAN STREET PORTSMOUTH, NH ESTATE OF CARRIE HAM TO LAWRENCE V. REGAN", BY JOHN W. DURGIN, DATED AUGUST 1937, ON FILE AT JAMES VERRA AND ASSOCIATES.
- "SKETCH TO RALPH SPINNEY", DATED APRIL 23, 1936, ON FILE AT JAMES VERRA AND ASSOCIATES.
- "PLOT PLAN OF LAND PORTSMOUTH, NH FOR JOHN R. AND WINFIELD R. WELCH", BY JOHN W. DURGIN, DATED APRIL 1973, ON FILE AT JAMES VERRA AND ASSOCIATES.
- "PLAN OF PROPERTY IN PORTSMOUTH, NH OWNED BY R.I. SUGDEN", BY WM A. GROVER, DATED APRIL 15, 1919, ON FILE AT JAMES VERRA AND ASSOCIATES.
- "PLAN OF LAND PORTSMOUTH, NH FOR WILLIAM A. HYDER", BY JOHN W. DURGIN, DATED JUNE 1955, ON FILE AT JAMES VERRA AND ASSOCIATES.
- "PROPERTY OF ELDRD V. AND BARBARA J. STRAW", BY C.RE. LAWSON, DATED JUNE 1971, R.C.R.D. PLAN C-3277.
- "SUBDIVISION PLAN OF TAX MAP 123, LOT 15 FOR 299 VAUGHAN STREET, LLC", BY DOUCET SURVEY, INC., DATED MAY 19, 2017, R.C.R.D. PLAN D-40759.
- "LICENSE, EASEMENT & LAND TRANSFER PLAN FOR 299 VAUGHAN STREET, LLC & VAUGHAN STREET HOTEL, LLC", BY DOUCET SURVEY INC., DATED AUGUST 2017, R.C.R.D. PLAN D-40760.

LEGEND

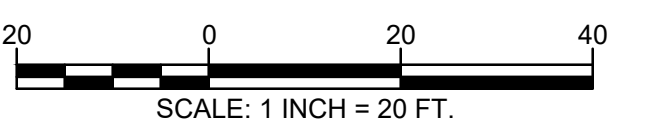
- APPROXIMATE ABUTTERS LOT LINE
- ○ CHAIN LINK FENCE
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- G GAS LINE
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- CONC.
- EDGE OF PAVEMENT
- VCC VERTICAL GRANITE CURB
- VCC VERTICAL CONCRETE CURB
- SWL SINGLE WHITE LINE
- EM ELECTRIC METER
- GM GAS METER
- PM PARKING METER
- 5/8" REBAR W/D CAP TO BE SET



I CERTIFY THAT THIS SURVEY PLAT IS NOT A SUBDIVISION PURSUANT TO THIS TITLE (NHRSA TITLE LXIV) AND THAT THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW WAYS ARE SHOWN. I CERTIFY THAT THIS SURVEY AND PLAN WERE PREPARED BY ME OR BY THOSE UNDER MY DIRECT SUPERVISION AND FALLS UNDER THE URBAN SURVEY CLASSIFICATION OF THE NH CODE OF ADMINISTRATIVE RULES OF THE BOARD OF LICENSURE FOR LAND SURVEYORS. I CERTIFY THAT THIS SURVEY WAS MADE ON THE GROUND AND IS CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. RANDOM TRAVERSE SURVEY BY TOTAL STATION, WITH A PRECISION GREATER THAN 1:15,000.

____ L.L.S. #989
____ DATE

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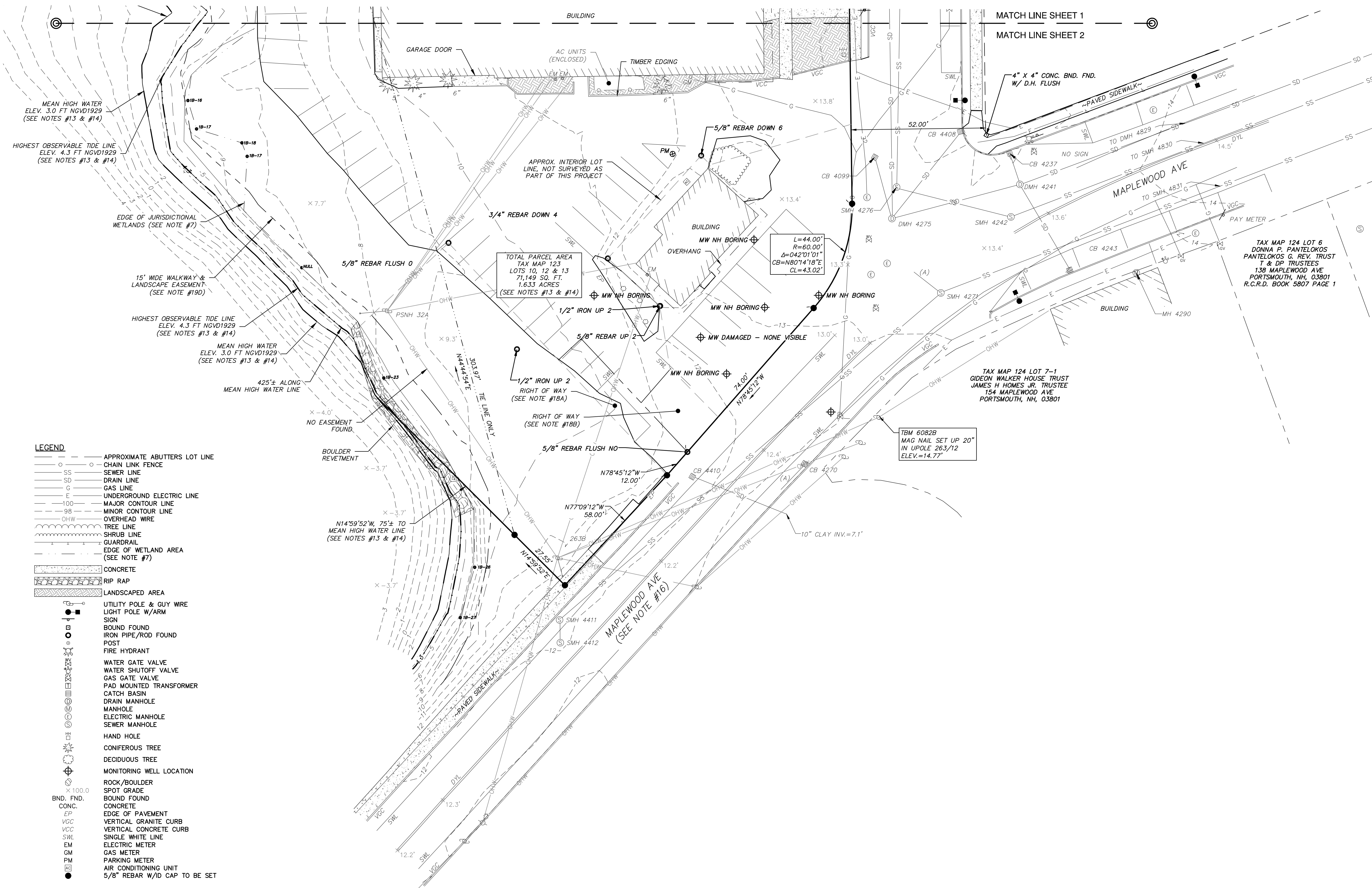
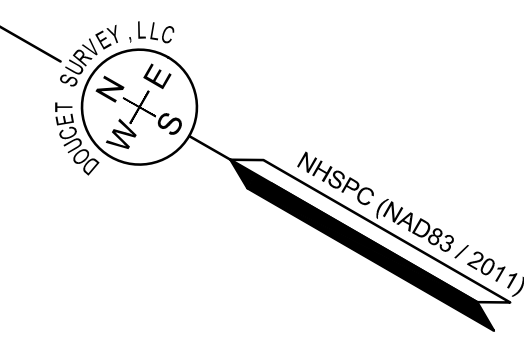
EXISTING CONDITIONS PLAN
FOR
TIGHE & BOND
LAND OF
31 RAYNES LLC
(TAX MAP 123, LOTS 10 & 13)
203 MAPLEWOOD AVENUE LLC
(TAX MAP 123, LOT 12)
&
ONE RAYNES AVENUE LLC
(TAX MAP 123, LOT 14)
MAPLEWOOD AVENUE & RAYNES AVENUE
PORTSMOUTH, NEW HAMPSHIRE

DRAINAGE STRUCTURES		SEWER STRUCTURES	
DMH 1096 RIM ELEV.=8.2' OUTSIDE OF SCOPE	CB 4243 RIM ELEV.=12.9' (4241) 12" CLAY INV.=10.1'	DMH 4827 RIM ELEV.=10.4' (4046) 24" DIP INV.=2.7' (RECESSED) (A) 18" RCP INV.=2.4' (OUTLET NOT FOUND)	SMH 4242 RIM ELEV.=13.4' (4276) 12" DIP INV.=5.1' (4830) 12" DIP INV.=5.1'
DMH 1099 RIM ELEV.=8.2' OUTSIDE OF SCOPE	CB 4270 RIM ELEV.=11.7' (A) 10" CLAY INV.=8' SUMP ELEV.=6.9'	(B) 18" RCP INV.=2.3' (C) 12" RCP INV.=6.2'	SMH 4271 RIM ELEV.=13.2' (A) 12" CLAY TOP OF PIPE=7.2' (4411) 24" CLAY INV.=3.0' (4831) 24" CLAY INV.=3.0'
MH 4046 RIM ELEV.=11.8' (4275) 12" UNKN INV.=3.2' (4827) 24" DIP INV.=2.5' (4839) 24" DIP INV.=2.3'	DMH 4275 RIM ELEV.=13.4' (4099) 12" CLAY INV.=10.9' (4408) 12" CLAY INV.=9.7' (4046) 12" CLAY INV.=5.6' (4241) 12" CLAY INV.=0.5'	DMH 4829 RIM ELEV.=15.8' (A) 12" CLAY INV.=12' (B) 12" CLAY INV.=11.9' (C) UNKN INV.=9.2' (4241) UNKN INV.=9.2'	SMH 4276 RIM ELEV.=13.3' (5419) 10" PVC INV.=5.5' (4242) 10" CLAY INV.=4.9'
CB 4099 RIM ELEV.=13.3' (4275) 12" RCP INV.=11.1' SUMP ELEV.=10.3'	MH 4290 RIM ELEV.=13.8' NOT OPENED	DMH 4839 RIM ELEV.=9.8' (4046) 24" RCP INV.=1.8' (4840) 24" RCP INV.=1.7'	SMH 4411 RIM ELEV.=12.0' (4412) 24" CLAY INV.=3.5' (4271) 24" CLAY INV.=3.5'
CB 4237 RIM ELEV.=12.9' (4241) 12" CLAY INV.=10.1' SUMP ELEV.=9.3'	CB 4408 RIM ELEV.=12.6' (4275) 12" RCP INV.=10.0' SUMP ELEV.=9.5'	DMH 4840 RIM ELEV.=9.4' (4839) 24" RCP INV.=1.6' (OUTFALL) 24" RCP INV.=1.6'	SMH 4412 RIM ELEV.=12.0' NOT OPENED
DMH 4241 RIM ELEV.=13.3' (4243) 12" CLAY INV.=9.8' (4237) 12" CLAY INV.=9.5' (4275) 12" CLAY INV.=7.0' (4829) 12" CLAY INV.=7.0'	CB 4410 RIM ELEV.=11.6' (4270) 10" CLAY INV.=6.8' SUMP ELEV.=6.1'	CB 5564 RIM ELEV.=10.1' (A) 12" RCP INV.=6.3'	SMH 4830 RIM ELEV.=18.2' (A) 12" DIP INV.=10.4' (4242) 12" DIP INV.=10.2'

NO.	DATE	DESCRIPTION	BY

DRAWN BY: E.D.P.	DATE: JUNE 17, 2020
CHECKED BY: M.W.F.	DRAWING NO. 6082B
JOB NO. 6082	SHEET 1 OF 3

DOUCET SURVEY
Serving Your Professional Surveying & Mapping Needs
102 Kent Place, Newmarket, NH 03857 (603) 659-6560
2 Commerce Drive (Suite 202) Bedford, NH 03110 (603) 614-4060
10 Storer Street (Riverview Suite) Kennebunk, ME (207) 502-7005
http://www.doucetsurvey.com



- LEGEND**
- APPROXIMATE ABUTTERS LOT LINE
 - CHAIN LINK FENCE
 - SS SEWER LINE
 - SD DRAIN LINE
 - G GAS LINE
 - E UNDERGROUND ELECTRIC LINE
 - 100 MAJOR CONTOUR LINE
 - 95 MINOR CONTOUR LINE
 - OHW OVERHEAD WIRE
 - TREE LINE
 - SHRUB LINE
 - GUARDRAIL
 - EDGE OF WETLAND AREA (SEE NOTE #7)
 - CONCRETE
 - RIPP RAP
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 - GAS METER
 - PARKING METER
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 - 5/8" REBAR W/ID CAP TO BE SET

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L.L.S. #989
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TAX MAP 124 LOT 6
DONNA P. PANTELAKOS
PANTELAKOS & REV. TRUST
T & DP TRUSTEES
138 MAPLEWOOD AVE
PORTSMOUTH, NH, 03801
R.C.R.D. BOOK 5807 PAGE 1

TAX MAP 124 LOT 7-1
GIDEON WALKER HOUSE TRUST
JAMES H HOMES JR. TRUSTEE
154 MAPLEWOOD AVE
PORTSMOUTH, NH, 03801



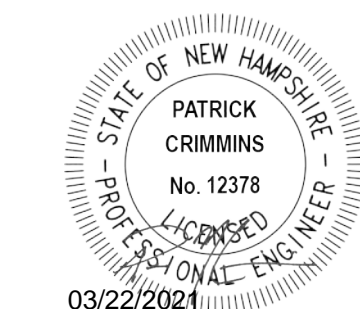
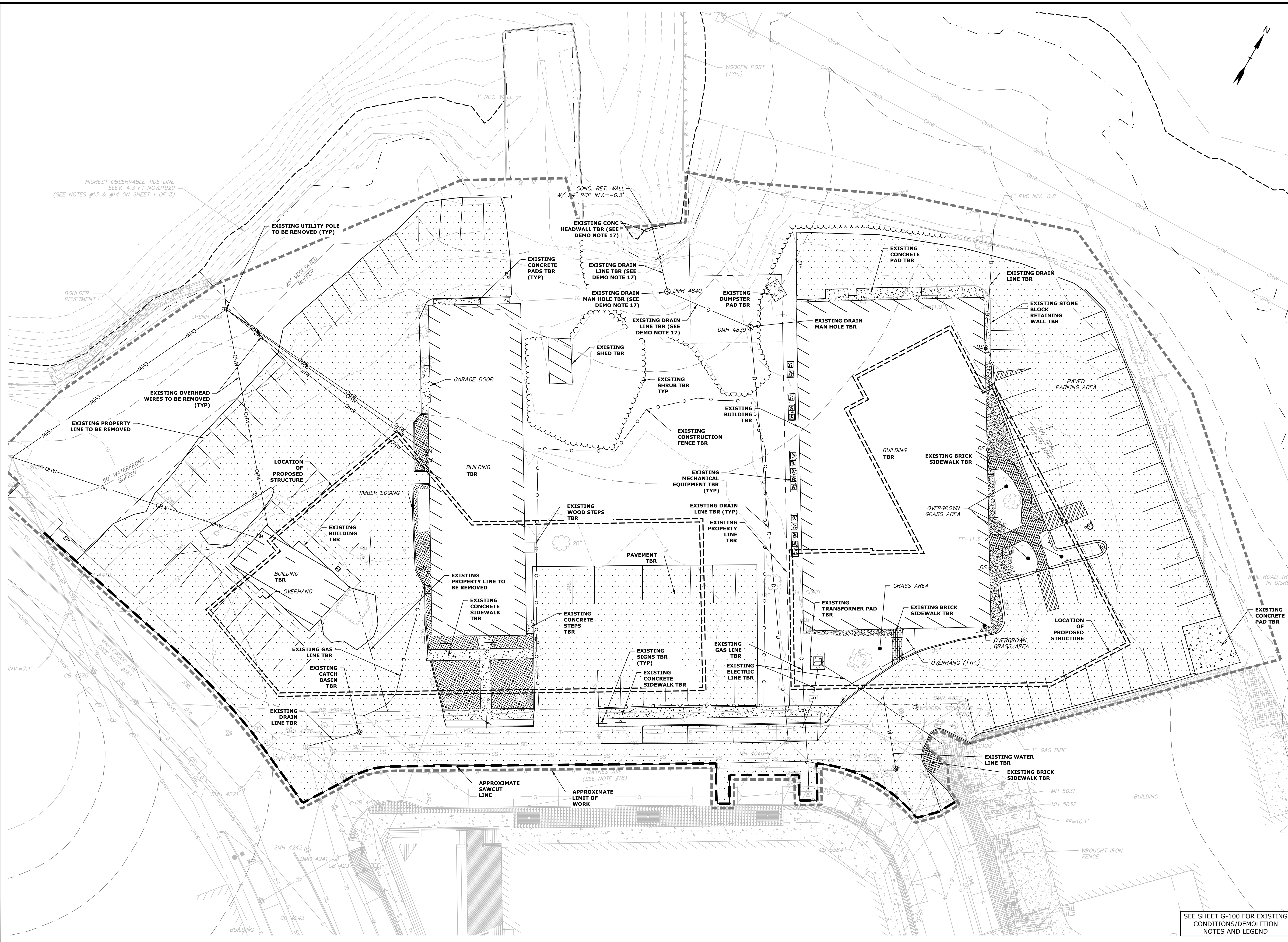
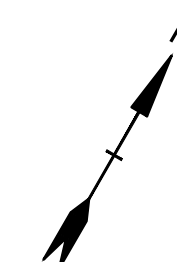
EXISTING CONDITIONS PLAN
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&
ONE RAYNES AVENUE LLC
(TAX MAP 123, LOT 14)
MAPLEWOOD AVENUE & RAYNES AVENUE
PORTSMOUTH, NEW HAMPSHIRE

NO.	DATE	DESCRIPTION	BY

DRAWN BY:	E.D.P.	DATE:	JUNE 17, 2020
CHECKED BY:	M.W.F.	DRAWING NO.	6082B
JOB NO.	6082	SHEET	3 OF 3

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FILE NAME: Y:\PROJECTS\6082\6082-11\6082B.DWG (SHEETED 6/19/2020) PLOTTED: 6/19/2020 10:59:46 AM LAYOUT NAME: TIGHE PLAN (3) PLOTTED: 6/19/2020 10:59:46 AM



Proposed Mixed Use Development

North Mill Pond Holdings, LLC

Portsmouth, New Hampshire

MARK	DATE	DESCRIPTION
C	3/22/2021	TAC Submission
B	3/10/2021	Design Review Resubmission
A	12/1/2020	TAC Work Session

PROJECT NO: P-0595-007
 DATE: December 22, 2020
 FILE: P-0595-007-C-DSGN.DWG
 DRAWN BY: CJK
 CHECKED BY: NAH/PMC
 APPROVED BY: BLM

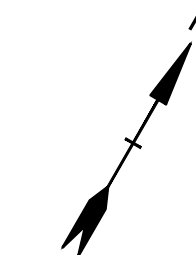
DEMOLITION PLAN

SCALE: AS SHOWN

C-101

SEE SHEET G-100 FOR EXISTING CONDITIONS/DEMOLITION NOTES AND LEGEND

Last Saved: 3/19/2021 1:40pm By: CJK/crk
 Project: 0595-007 - 15 - 2021 - 1 - 40pm By: CJK/crk
 Title: 0595-007-C-DSGN.DWG
 Figure: A:\ucad\Drawings - Figures\Aucad\Sheet\0595-007-C-DSGN.dwg



SITE DATA:

LOCATION: TAX MAP 123, LOT 10
 TAX MAP 123, LOT 12
 TAX MAP 123, LOT 13
 TAX MAP 123, LOT 14
 TAX MAP 123, LOT 12
 RAYNES AVENUE

ZONING DISTRICT: CHARACTER DISTRICT 4 (CD4)
 DOWNTOWN OVERLAY DISTRICT
 NORTH END INCENTIVE OVERLAY DISTRICT
 HISTORIC DISTRICT

PROPOSED USE: MULTI FAMILY DWELLING
 HOTEL
 RETAIL/RESTAURANT

PROPOSED LOT SIZE: ±2.53 ACRES (±110,415 SF)

DEVELOPMENT STANDARDS

BUILDING PLACEMENT (PRINCIPAL BUILDING):	REQUIRED	PROPOSED
MAXIMUM PRINCIPAL FRONT YARD:	15 FT	±16 FT (1)
MAXIMUM SECONDARY FRONT YARD:	12 FT	±5 FT
SIDE YARD:	NR	NR
MINIMUM REAR YARD:	5 FT	N/A
MINIMUM FRONT LOT LINE BUILDOUT:	50%	±68.8%

(1) - INCREASE ABOVE THE MAXIMUM ALLOWED PER 10.5A42.12

BUILDING AND LOT OCCUPATION:	REQUIRED	PROPOSED
MAXIMUM BUILDING BLOCK LENGTH:	200 FT	197 FT
MAXIMUM FACADE MODULATION LENGTH:	80 FT	<80 FT
MAXIMUM ENTRANCE SPACING:	50 FT	<50 FT
MAXIMUM BUILDING COVERAGE:	90%	±47.0%
MAXIMUM BUILDING FOOTPRINT:	30,000 SF (2)	17,565 SF
MINIMUM LOT AREA:	NR	NR
MINIMUM LOT AREA PER DWELLING UNIT:	10%	25.3%
MINIMUM OPEN SPACE:	15,000 SF	8,100 SF

(2) - INCREASE ABOVE 15,000 SF ALLOWED PER 10.5A43.43

BUILDING FORM (PRINCIPAL BUILDING):	REQUIRED	PROVIDED
BUILDING HEIGHT:	5 STORY (3)	5 STORY
MAXIMUM FINISHED FLOOR SURFACE OF GROUND FLOOR ABOVE SIDEWALK GRADE:	60 FT	<60 FT
MINIMUM GROUND STORY HEIGHT:	36 IN	<36"
MINIMUM SECOND STORY HEIGHT:	12 FT	12 FT
MINIMUM SECOND STORY HEIGHT:	10 FT	10 FT
FACADE GLAZING:	70%	70%
ALLOWED ROOF TYPES:	70%	70%
FLAT, GABLE, HIP, GAMBREL, MANSARD	FLAT, GABLE, HIP, GAMBREL, MANSARD	FLAT, GABLE, HIP, GAMBREL, MANSARD

(3) - ADDITIONAL 1 STORY UP TO 10FT ALLOWED FOR PROVIDING AT LEAST 20% OF THE SITE TO BE ASSIGNED AS COMMUNITY SPACE.

COMMUNITY SPACE:	21,274 SF	22,342 SF
	20%	20.2%

OFF-STREET PARKING REQUIREMENTS

PARKING SPACES REQUIRED:	REQUIRED	PROVIDED
DWELLING UNITS:		
0 SF TO 500 SF, 0.5 SPACES PER UNIT	17 UNITS	8.5 SPACES
500 SF TO 750 SF, 1.0 SPACES PER UNIT	33 UNITS	33 SPACES
OVER 750 SF, 1.3 SPACES PER UNIT	10 UNITS	13 SPACES
TOTAL MINIMUM RESIDENTIAL SPACES REQUIRED =		55 SPACES

VISITORS:		
1 SPACES PER 5 DWELLING UNITS	60 UNITS	12 SPACES

HOTEL:		
0.75 SPACES PER GUEST ROOM	128 ROOMS	96 SPACES

DOWNTOWN OVERLAY DISTRICT - 4 SPACES

TOTAL MINIMUM PARKING SPACES REQUIRED = 159 SPACES

TOTAL PARKING SPACES PROVIDED:		
RESERVE SPACES, LIFT SYSTEM (4) =		23 SPACES
SHARED PARKING ON SEPARATE LOT (4) =		25 SPACES
SURFACE PARKING SPACES =		111 SPACES
TOTAL PARKING SPACES PROVIDED =		159 SPACES

SIX (6) ADA ACCESSIBLE SPACES REQUIRED

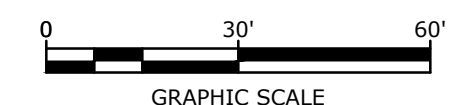
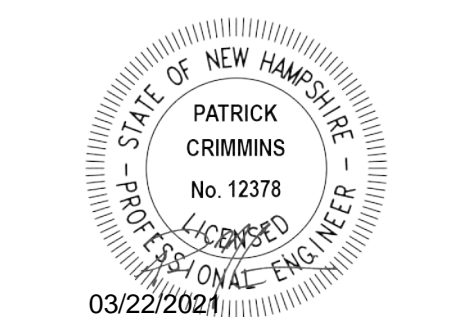
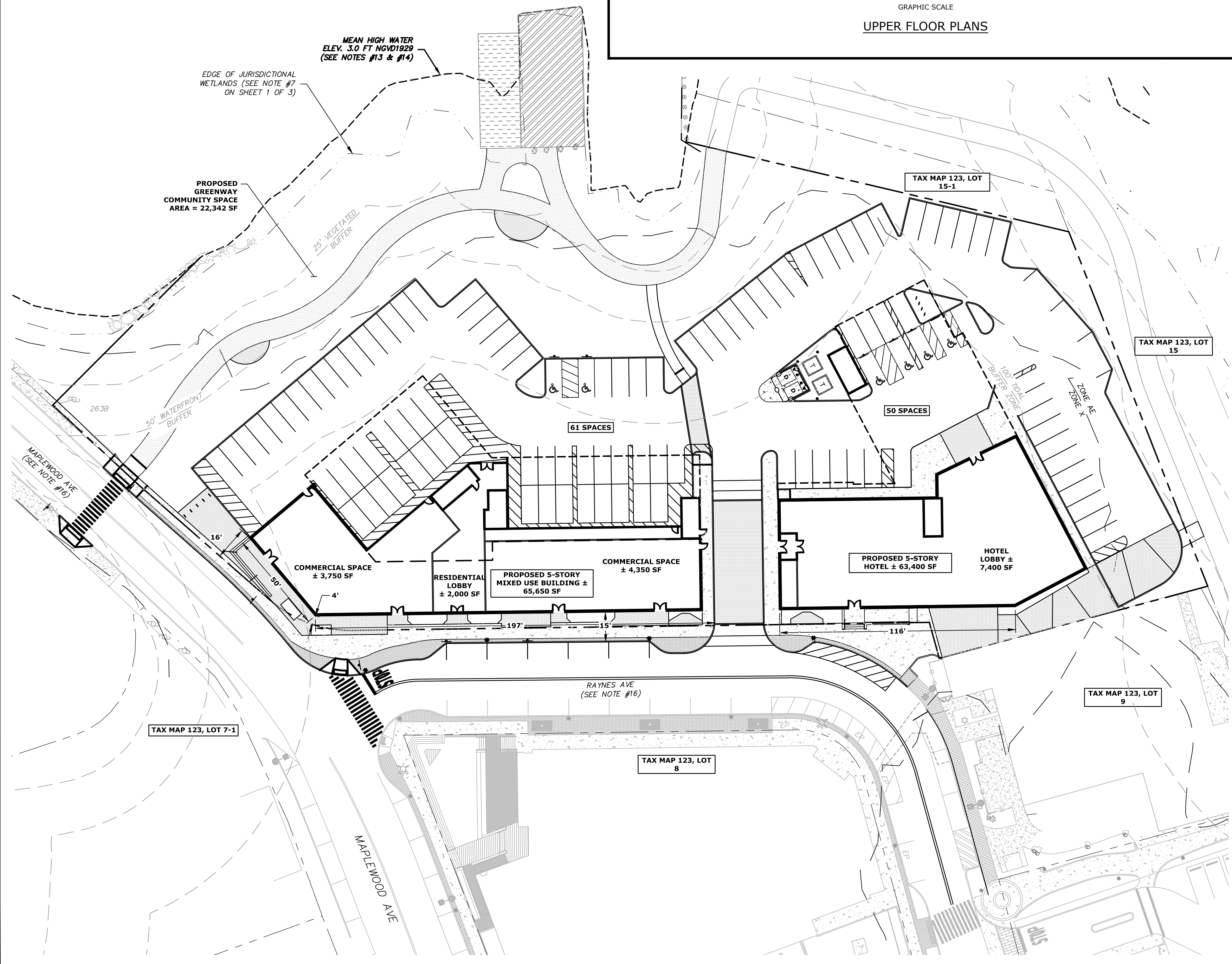
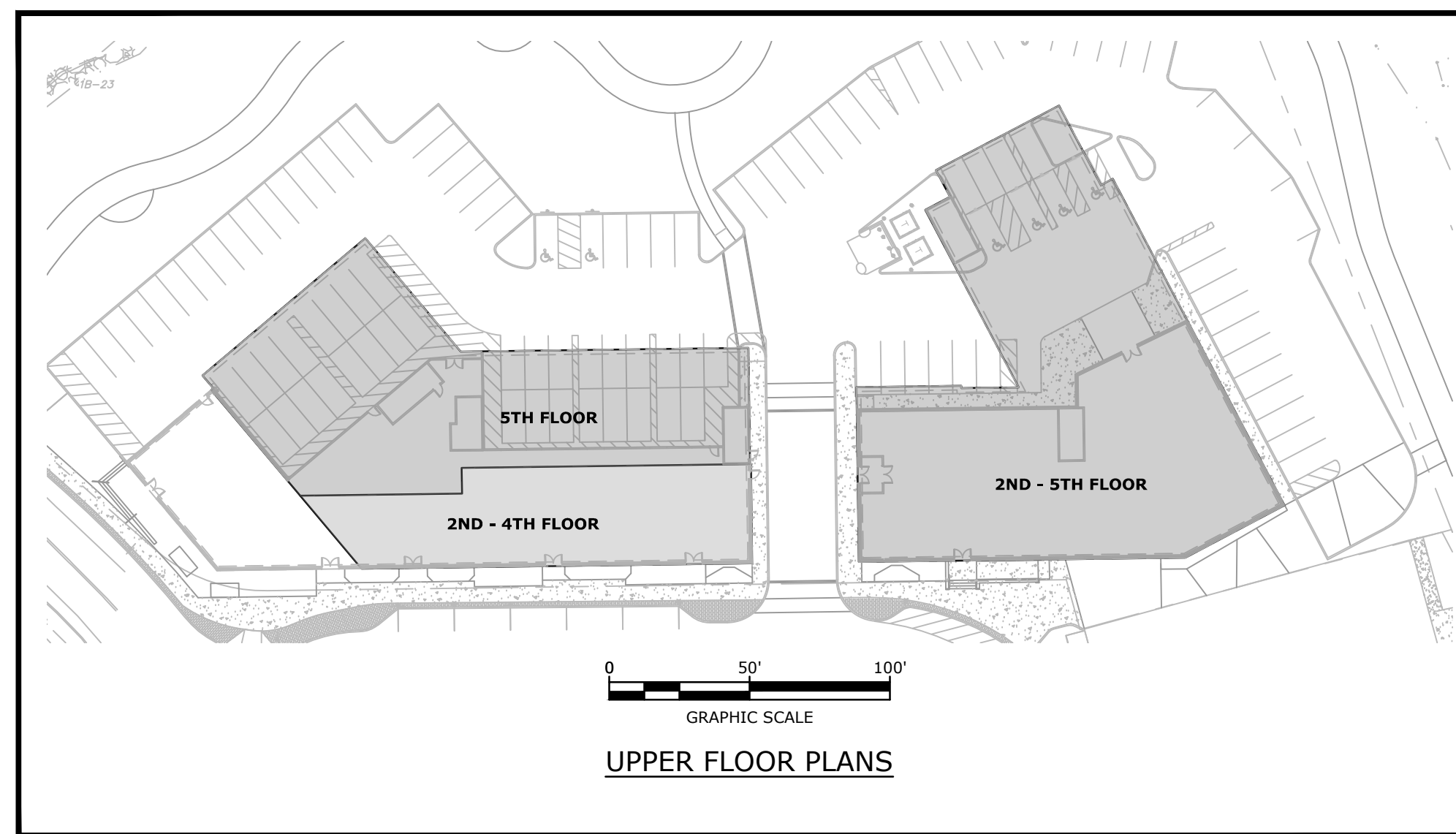
(4) - CONDITIONAL USE PERMIT REQUIRED FOR REDUCTION IN SPACES AND SHARED PARKING ON SEPARATE LOT.

PARKING STALL SIZE:	REQUIRED	PROVIDED
DRIVE AISLE:	8.5' X 19'	8.5' X 19'
	**22'	22'
***ZONING ORDINANCE 10.1114.21 ALLOWS MINIMUM 22' AISLE WIDTH FOR 90 DEGREE PARKING IN A PARKING STRUCTURE		

BIKE SPACES REQUIRED:	REQUIRED	PROVIDED
1 BIKE SPACE / 10 PARKING SPACES	16 SPACES	16 SPACES

PROPOSED MIXED USE GROSS FLOOR AREA				
FLOOR	COMMERCIAL (SF)	LOBBY (SF)	UNITS	TOTAL FLOOR AREA (SF)
FIRST	8,100	2,000	0	10,100
SECOND	0	0	17	15,200
THIRD	0	0	17	15,200
FOURTH	0	0	17	15,200
FIFTH	0	0	9	9,950
TOTAL	8,100	2,000	60	65,650

PROPOSED HOTEL GROSS FLOOR AREA			
FLOOR	LOBBY (SF)	UNITS	TOTAL FLOOR AREA (SF)
FIRST	7,400	0	7,400
SECOND	0	32	14,000
THIRD	0	32	14,000
FOURTH	0	32	14,000
FIFTH	0	32	14,000
TOTAL	7,400	128	63,400



Proposed Mixed Use Development

North Mill Pond Holdings, LLC

Portsmouth, New Hampshire

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A	12/1/2020	TAC Work Session

PROJECT NO:	P-0595-007
DATE:	December 22, 2020
FILE:	P-0595-007-C-DSGN.DWG
DRAWN BY:	CKJ
CHECKED BY:	NAH/PMC
APPROVED BY:	BLM

SITE PLAN

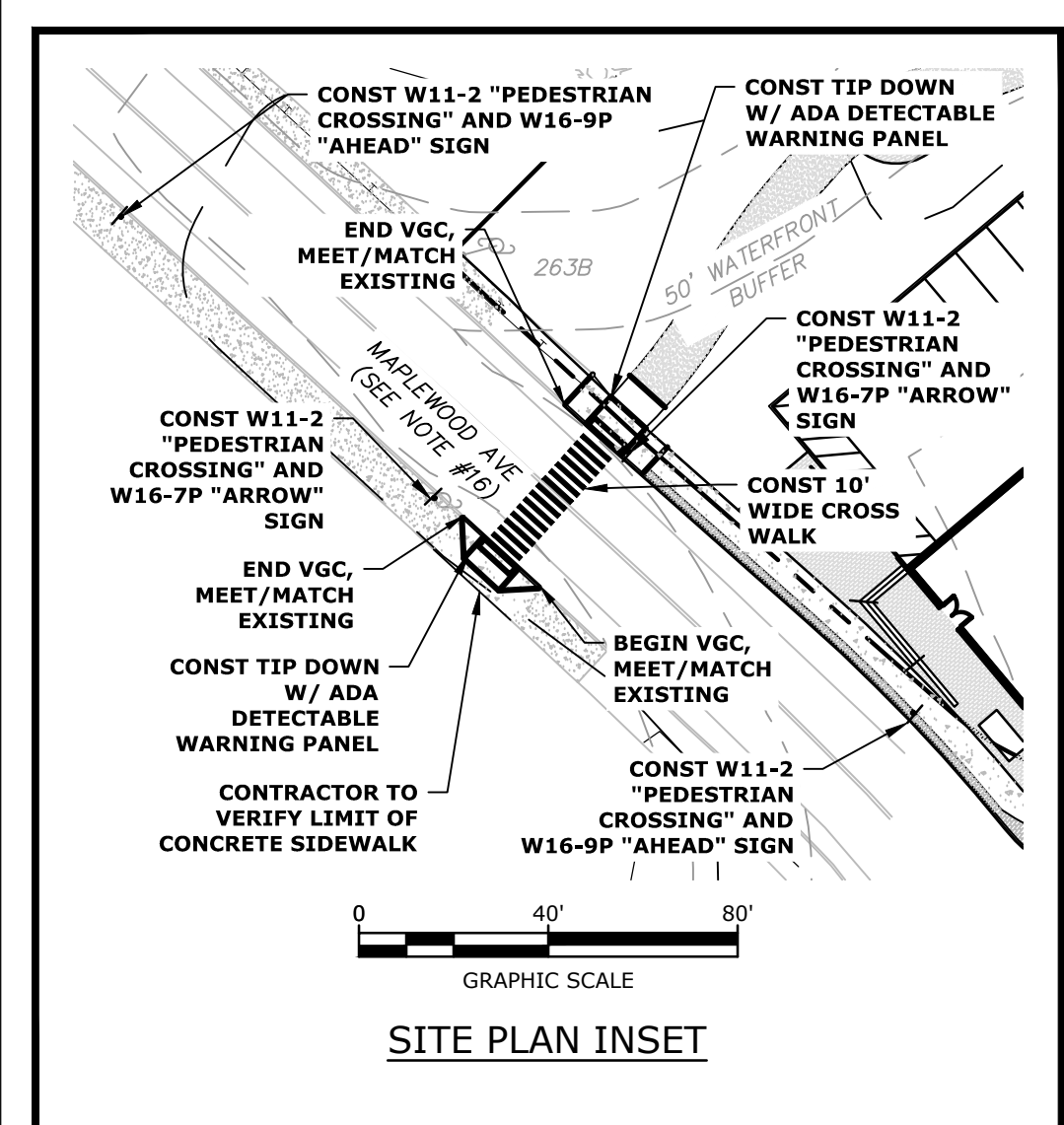
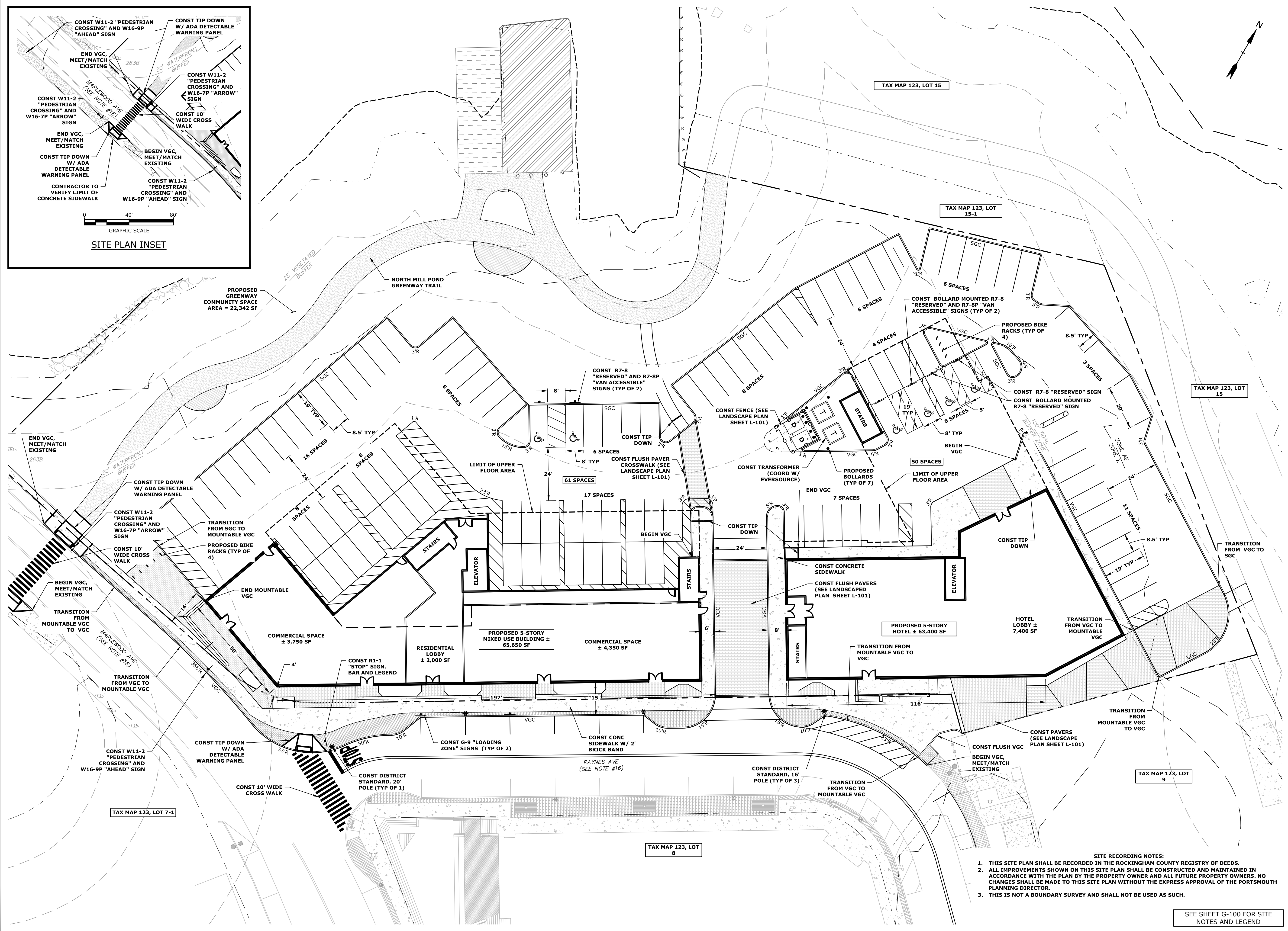
SCALE: AS SHOWN

C-102

SEE SHEET G-100 FOR SITE NOTES AND LEGEND

- SITE RECORDING NOTES:**
- THIS SITE PLAN SHALL BE RECORDED IN THE ROCKINGHAM COUNTY REGISTRY OF DEEDS.
 - ALL IMPROVEMENTS SHOWN ON THIS SITE PLAN SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE PLAN BY THE PROPERTY OWNER AND ALL FUTURE PROPERTY OWNERS. NO CHANGES SHALL BE MADE TO THIS SITE PLAN WITHOUT THE EXPRESS APPROVAL OF THE PORTSMOUTH PLANNING DIRECTOR.
 - THIS IS NOT A BOUNDARY SURVEY AND SHALL NOT BE USED AS SUCH.

Last Saved: 3/22/2021 12:39pm By: Chrzuik
 Plotted On: Mar 22, 2021 12:39pm
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STATE OF NEW HAMPSHIRE
 BRADLEE MEZQUITA
 No. 0595-007
 10/23/2021

STATE OF NEW HAMPSHIRE
 PATRICK CRIMMINS
 No. 12378
 03/23/2024

0 20' 40'
 GRAPHIC SCALE

Proposed Mixed Use Development

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 DRAWN BY: CLK
 CHECKED BY: NAH/PMC
 APPROVED BY: BLM

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 2. ALL IMPROVEMENTS SHOWN ON THIS SITE PLAN SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE PLAN BY THE PROPERTY OWNER AND ALL FUTURE PROPERTY OWNERS. NO CHANGES SHALL BE MADE TO THIS SITE PLAN WITHOUT THE EXPRESS APPROVAL OF THE PORTSMOUTH PLANNING DIRECTOR.
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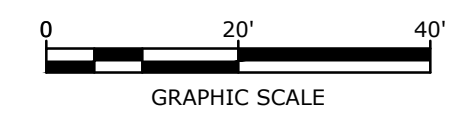
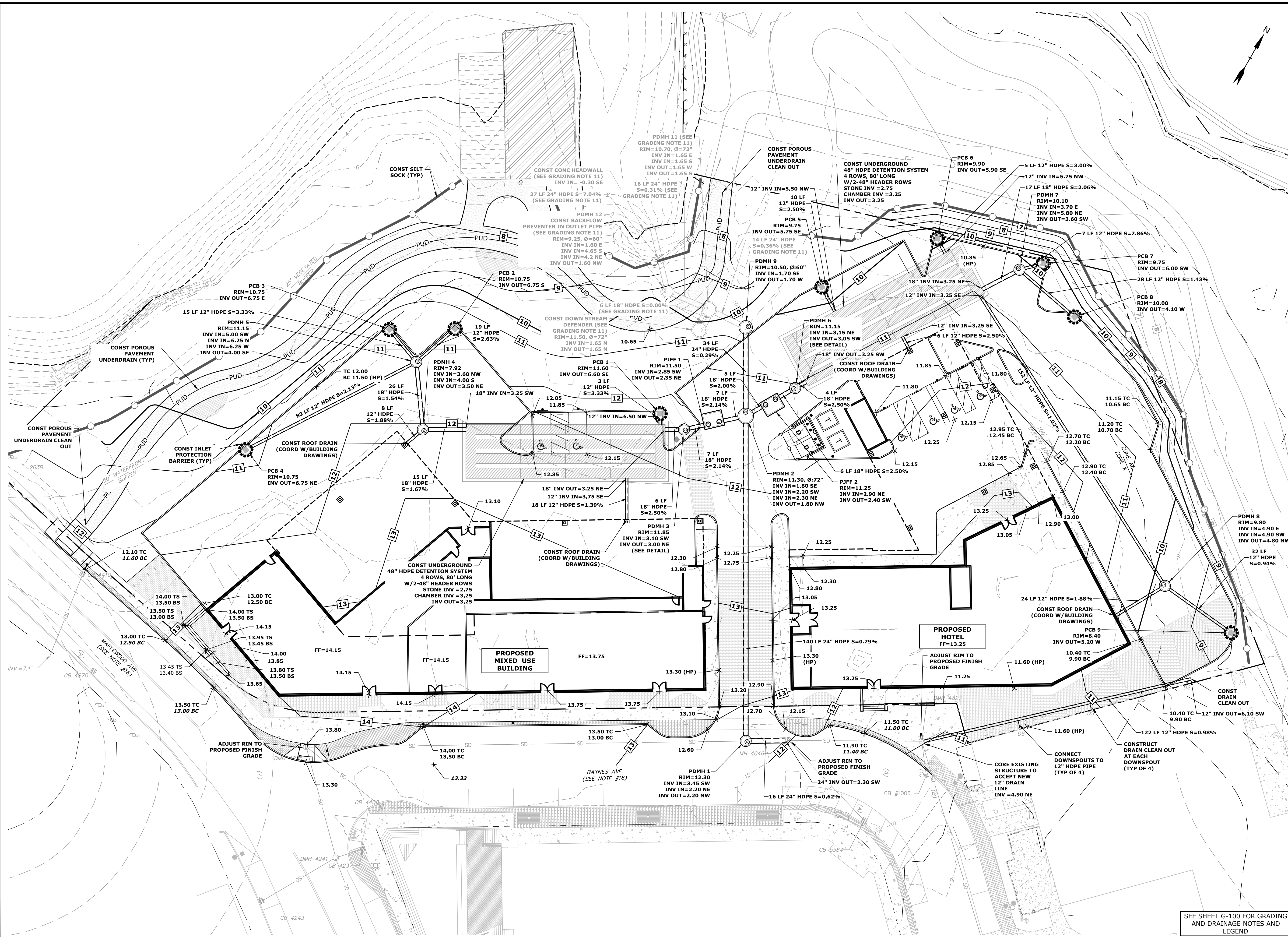
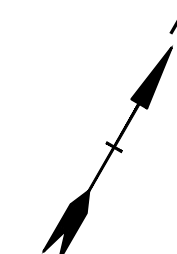
SEE SHEET G-100 FOR SITE NOTES AND LEGEND

SITE PLAN

SCALE: AS SHOWN

C-102.1

Last Saved: 3/22/2021 12:41pm By: Chircauk
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Proposed Mixed Use Development

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Portsmouth, New Hampshire

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DRAWN BY:	CJK
CHECKED BY:	NAH/PMC
APPROVED BY:	BLM

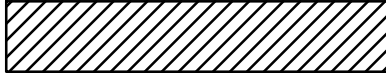
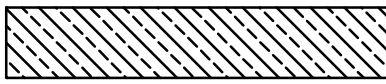
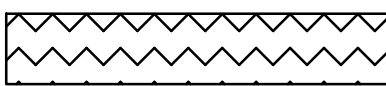

GRADING, DRAINAGE AND EROSION CONTROL PLAN

SCALE: AS SHOWN

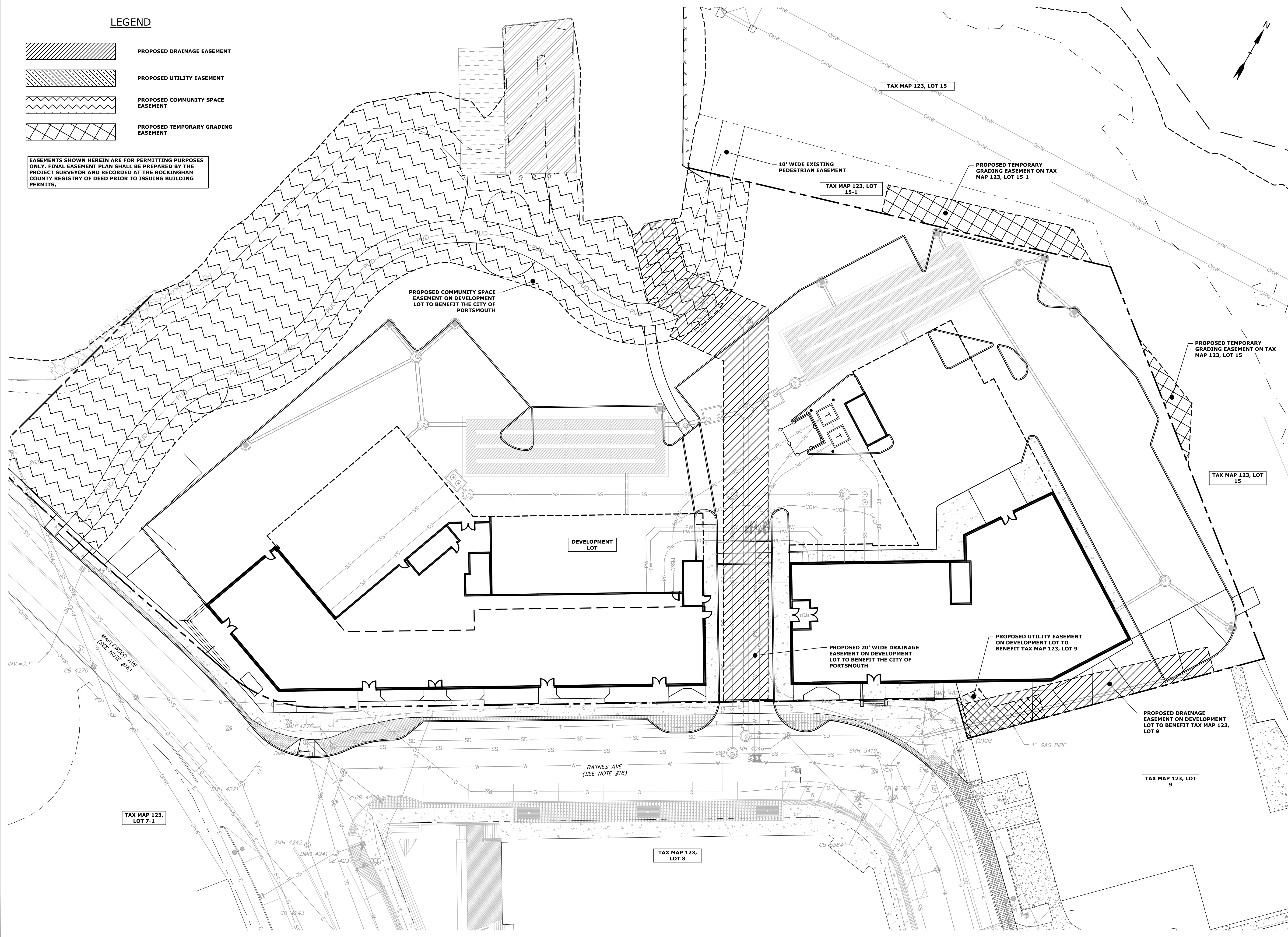
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 Project: North Mill Pond, 2021, 1:44:14pm By: CJKrcnk
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 Figure: Grading, Drainage and Erosion Control Plan (Sheet) P-0595-007-C-DSGN.dwg

SEE SHEET G-100 FOR GRADING AND DRAINAGE NOTES AND LEGEND

LEGEND

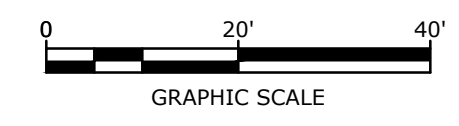
-  PROPOSED DRAINAGE EASEMENT
-  PROPOSED UTILITY EASEMENT
-  PROPOSED COMMUNITY SPACE EASEMENT
-  PROPOSED TEMPORARY GRADING EASEMENT

EASEMENTS SHOWN HEREIN ARE FOR PERMITTING PURPOSES ONLY. FINAL EASEMENT PLAN SHALL BE PREPARED BY THE PROJECT SURVEYOR AND RECORDED AT THE ROCKINGHAM COUNTY REGISTRY OF DEED PRIOR TO ISSUING BUILDING PERMITS.



STATE OF NEW HAMPSHIRE
 BRADLEE MEZQUITA
 No. 05939
 LICENSED PROFESSIONAL ENGINEER

STATE OF NEW HAMPSHIRE
 PATRICK CRIMMINS
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 LICENSED PROFESSIONAL ENGINEER
 03/22/2021



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DRAWN BY:	CJK
CHECKED BY:	NAH/PMC
APPROVED BY:	BLM

EASEMENT PLAN

SCALE: AS SHOWN

C-201

Last Saved: 3/19/2021 1:52pm By: CJKrcuk
 Plotted On: 1/15/2021 11:20am By: CJKrcuk
 Title & Number: P-0595-007 Pro Con General Proposals P-0595-007 Raynes Ave Hotel Drawings Figures/AutocAD/Sheet/P-0595-007-C-DSGN.dwg

PLANT SCHEDULE

Symbol	Quantity	Botanical Name	Common Name	Size	Spacing	Notes
TREES						
AC RU	5	<i>Acer rubrum</i>	Red Maple	4-5" Cal.		B&B; matched
AC KA	2	<i>Acer rubrum 'Karpick'</i>	Karpick Maple	4-5" Cal.		Single-stem, matched
BE AL	3	<i>Betula alleghaniensis</i>	Yellow Birch	4-5" Cal.		Single-stem, matched
CE OC	5	<i>Celtis occidentalis</i>	Hackberry	4-5" Cal.		Single-stem, matched
CH TH	5	<i>Chamaecyparis thyoides</i>	White Cypress	8-10' Ht, B&B		B&B; matched
CH VI	4	<i>Chionanthus virginicus</i>	Fringe Tree	4-5" Cal.		Multi-stem, matched
HA VE	6	<i>Hamamelis vernalis</i>	Vernal Witch Hazel	6-8' Ht, B&B		Multi-stem, matched
JU VI	8	<i>Juniperus virginiana</i>	Eastern Red Cedar	8-10' Ht, B&B		B&B; matched
QU BI	2	<i>Quercus bicolor</i>	Swamp White Oak	4-5" Cal.		B&B; matched
TH OC	6	<i>Thuja occidentalis 'Hetz Wintergreen'</i>	Hetz Wintergreen Arborvitae	6-8' Ht, B&B		B&B; matched
SHRUBS						
Ae Pa		<i>Aesculus parviflora</i>	Bottlebrush Buckeye	#10 Container	72" O.C.	
Co Pe		<i>Comptonia peregrina</i>	Sweet Fern	#3 Container	36" O.C.	
Co Ra		<i>Cornus racemosa</i>	Gray Dogwood	#7 Container	36" O.C.	
Fo Ga		<i>Fothergilla gardenii 'Mount Airy'</i>	Mount Airy Fothergilla	#7 Container	36" O.C.	
Hy Qu		<i>Hydrangea quercifolia</i>	Oakleaf Hydrangea	#7 Container	48" O.C.	
Li Be		<i>Lindera Benzoin</i>	Spice Bush	#7 Container	36" O.C.	
Ix Gl		<i>Ilex glabra 'Shamrock'</i>	Shamrock Inkberry	#7 Container	36" O.C.	
Il Ji		<i>Ilex verticillata 'Jim Dandy'</i>	Jim Dandy Winterberry	#7 Container	48" O.C.	
Il Ve		<i>Ilex verticillata 'Red Sprite'</i>	Red Sprite Winterberry	#7 Container	48" O.C.	
Iv Fr		<i>Iva frutescens</i>	Bigleaf Marsh Elder	#3 Container	36" O.C.	
My Pe		<i>Myrica pensylvanica</i>	Northern Bayberry	#7 Container	48" O.C.	
Rh Gl		<i>Rhus aromatica 'Gro-Low'</i>	Fro-Low Fragrant Sumac	#3 Container	30" O.C.	
Sp To		<i>Spiraea tomentosa</i>	Steeplebush	#3 Container	30" O.C.	
Vi Ca		<i>Viburnum carlesii 'SMVCB'</i>	Spice Baby Viburnum	#7 Container	36" O.C.	
PERENNIALS						
am hu		<i>Amsonia tabernaemontana 'Walter'</i>	Eastern Bluestar	#2 Container	30" O.C.	
as in		<i>Asclepias tuberosa</i>	Butterfly Weed	#2 Container	30" O.C.	
as ob		<i>Aster oblongifolius 'Raydon's Favorite'</i>	Raydon's Favorite Aster	#2 Container	24" O.C.	
ba bi		<i>Baptisia australis</i>	Blue False Indigo	#3 Container	24" O.C.	
de pu		<i>Dennstaedtia punctilobula</i>	Hay Scented Fern	#2 Container	30" O.C.	
ec pu		<i>Echinacea purpurea</i>	Purple Coneflower	#2 Container	24" O.C.	
on se		<i>Onoclea sensibilis</i>	Sensitive Fern	#2 Container	30" O.C.	
sa ma		<i>Salvia 'May Night'</i>	May Night Salvia	#2 Container	30" O.C.	
so ca		<i>Solidago simpervirens</i>	Seaside Goldenrod	#2 Container	24" O.C.	
ti co		<i>Tiarella cordifolia</i>	Foamflower	#2 Container	15" O.C.	
ORNAMENTAL GRASSES						
bo cu		<i>Bouteloua curtipendula</i>	Side Oats Grama	#2 Container	30" O.C.	
ca ac		<i>Calamagrostis acutiflora 'Karl Foerster'</i>	Feather Reed Grass	#3 Container	30" O.C.	
de ce		<i>Deschampsia cespitosa 'Pixie Fountain'</i>	Tufted Hair Grass	#2 Container	30" O.C.	
fe ru		<i>Festuca rubra L.</i>	Coastal Red Fescue	Plug	12" O.C.	
mi si		<i>Miscanthus sinensis 'Adagio'</i>	Dwarf Silver Grass	#2 Container	30" O.C.	
pe al		<i>Pennisetum alopecuroides 'Hamelin'</i>	Hameln Dwarf Fountain Grass	#2 Container	24" O.C.	

PLANTING NOTES

- LANDSCAPE ARCHITECT TO APPROVE PLANT MATERIAL PRIOR TO DELIVERY TO SITE.
- PLANT MATERIAL SHALL CONFORM TO "THE AMERICAN STANDARD FOR NURSERY STOCK", PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN, INC.
- NO SUBSTITUTIONS OF PLANT SPECIES WITHOUT LANDSCAPE ARCHITECT'S WRITTEN APPROVAL.
- SUBSTITUTIONS OF PLANT SPECIES SHALL BE A PLANT OF EQUIVALENT OVERALL FORM, HEIGHT AND BRANCHING HABIT, FLOWER, LEAF AND FRUIT, COLOR AND TIME OF BLOOM, AS APPROVED BY LANDSCAPE ARCHITECT.
- LOCATE AND VERIFY UTILITY LINE LOCATIONS PRIOR TO STAKING AND REPORT CONFLICTS TO LANDSCAPE ARCHITECT.
- PLANTING DEMOLITION DEBRIS, GARBAGE, LUMPS OF CONCRETE, STEEL AND OTHER MATERIALS DELETERIOUS TO PLANT'S HEALTH AS DETERMINED BY LANDSCAPE ARCHITECT SHALL BE REMOVED FROM ALL PLANTING AREAS.
- NO PLANTING TO BE INSTALLED BEFORE ACCEPTANCE OF ROUGH GRADING.
- ALL PROPOSED TREE LOCATIONS SHALL BE STAKED OR LAID OUT IN THEIR APPROXIMATE LOCATION BY THE CONTRACTOR. REFER TO LAYOUT AND PLANTING SHEETS FOR LAYOUT INFORMATION. THE CONTRACTOR SHALL ADJUST THE LOCATIONS AS REQUESTED BY THE LANDSCAPE ARCHITECT TO ACCOUNT FOR SUBSURFACE UTILITIES AND OTHER FIELD CONDITIONS. FINAL LOCATIONS OF ALL PLANTS MUST BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO PLANTING.
- INSTALL PLANTS WITH ROOT FLARES FLUSH WITH FINISHED GRADE. IMMEDIATELY REPLANT PLANTS THAT SETTLE OUT OF PLUMB OR BELOW FINISHED GRADE.
- PLANT UNDER FULL TIME SUPERVISION OF CERTIFIED ARBORIST, NURSERYMAN, OR LICENSED LANDSCAPE ARCHITECT. PROVIDE WRITTEN VERIFICATION OF CERTIFICATION AND/OR LICENSE FOR LANDSCAPE ARCHITECT'S APPROVAL.
- WATER PLANTS THOROUGHLY AFTER INSTALLATION, A MINIMUM OF TWICE WITHIN THE FIRST 24 HOURS.
- REPAIR DAMAGE DUE TO OPERATIONS INSIDE AND OUTSIDE OF LIMIT OF WORK
- SOAK ALL PERENNIALS FOR 24 HOURS PRIOR TO INSTALLATION

ZONING NOTES

10.5A44.40 PARKING LOT LANDSCAPE

10.5A44.42 TREES	
PARKING LOTS SHALL CONTAIN AT LEAST (1) TREE FOR EVERY (7) PARKING SPACES	
TOTAL PARKING LOT SPACES	111
TOTAL REQUIRED PARKING LOT TREES	16
TOTAL PARKING LOT TREES PROPOSED	22

10.5A44.43 LANDSCAPING	
ALL LANDSCAPING REQUIRED PURSUANT TO THIS SECTION SHALL BE LOCATED AND DESIGNED IN A MANNER TO PROTECT VEGETATION FROM VEHICULAR DAMAGE.	YES

10.1130 LANDSCAPING AND SCREENING

10.1132.10 SCREENING OF DUMPSTERS	
NATURAL SCREENING SHALL CONSIST OF EVERGREEN SHRUBS/TREES PLANTED IN A LINE TO FORM A CONTINUOUS SCREEN AND GROWING TO A HEIGHT OF 6 FEET WITHIN 3 YEARS. THE REMAINING PORTION OF THE SCREENING AREA SHALL CONSIST OF LARGE AND SMALL TREES, GRASS, FLOWER BEDS, OR OTHER VEGETATIVE GROUNDCOVER TO FULLY COVER THE GROUND SURFACE OF THE AREA WITHIN 3 YEARS.	YES
10.1132.20 SCREENING OF DUMPSTERS	
A 6-FOOT HIGH FENCE OR MASONRY WALL MAY BE SUBSTITUTED FOR NATURAL SCREENING IF APPROVED.	YES

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DRAWN BY:	OS
CHECKED BY:	RU/PMC
APPROVED BY:	BLM

LANDSCAPE MATERIAL PLAN LEGEND AND NOTES

SCALE: AS SHOWN

GENERAL PROJECT INFORMATION

PROJECT APPLICANT: NORTH MILL POND HOLDINGS, LLC
1359 HOOKSETT ROAD
HOOKSETT, NH 03106
PROJECT NAME: PROPOSED MIXED USE DEVELOPMENT
PROJECT MAP / LOT: MAP 123 / LOTS 10, 12, 13 & 14
PROJECT ADDRESS: 1 RAYNES AVENUE PORTSMOUTH, NH 03801
PROJECT LATITUDE: 42°-04'-48" N
PROJECT LONGITUDE: 70°-45'-50" W

PROJECT DESCRIPTION
THE PROPOSED PROJECT INCLUDES TWO BUILDINGS, A 5 STORY MIXED USE BUILDING AND A 5 STORY 128 ROOM HOTEL. THE PROJECT WILL ALSO CONSIST OF ASSOCIATED SITE IMPROVEMENTS SUCH AS PAVING, STORMWATER MANAGEMENT, UTILITIES AND LIGHTING.

DISTURBED AREA
THE TOTAL AREA TO BE DISTURBED IS APPROXIMATELY 2.40 ACRES.

SOIL CHARACTERISTICS
BASED ON THE USCS SITE SPECIFIC SOIL SURVEY CONDUCTED BY LEONARD LORD, PHD, CSS, CWS OF TIGHE & BOND, INC. THE SOIL SURVEY IDENTIFIES MOSTLY HYDROLOGIC SOIL GROUP C SOILS AND SOME PORTIONS OF HYDROLOGIC SOIL GROUP A SOILS. MUCH OF THE SITE IS COMPRISED OF UDORTHENTS WITH TWO DRAINAGE CLASSIFICATIONS, MODERATELY POORLY DRAINED SOILS AND PORTIONS OF WELL DRAINED SOILS.

NAME OF RECEIVING WATERS
THE STORMWATER RUNOFF FROM THE SITE WILL BE DISCHARGED VIA A CLOSED DRAINAGE SYSTEM ULTIMATELY FLOWS TO NORTH MILL POND THEN TO THE PISCATAQUA RIVER.

CONSTRUCTION SEQUENCE OF MAJOR ACTIVITIES:

- 1. CUT AND CLEAR TREES.
2. CONSTRUCT TEMPORARY AND PERMANENT SEDIMENT, EROSION AND DETENTION CONTROL FACILITIES. EROSION, SEDIMENT AND DETENTION MEASURES SHALL BE INSTALLED PRIOR TO ANY EARTH MOVING OPERATIONS THAT WILL INFLUENCE STORMWATER RUNOFF SUCH AS:
- NEW CONSTRUCTION
- CONTROL OF DUST
- NEARNESS OF CONSTRUCTION SITE TO RECEIVING WATERS
- CONSTRUCTION DURING LATE WINTER AND EARLY SPRING
3. ALL PERMANENT DITCHES, SWALES, DETENTION, RETENTION AND SEDIMENTATION BASINS TO BE STABILIZED USING THE VEGETATIVE AND NON-STRUCTURAL BMPs PRIOR TO DIRECTING RUNOFF TO THEM.
4. CLEAR AND DISPOSE OF DEBRIS.
5. CONSTRUCT TEMPORARY CULVERTS AND DIVERSION CHANNELS AS REQUIRED.
6. GRADE AND GRAVEL ROADWAYS AND PARKING AREAS - ALL ROADS AND PARKING AREA SHALL BE STABILIZED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE.
7. BEGIN PERMANENT AND TEMPORARY SEEDING AND MULCHING. ALL CUT AND FILL SLOPES SHALL BE SEEDDED AND MULCHED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE.
8. DAILY, OR AS REQUIRED, CONSTRUCT TEMPORARY BERMS, DRAINS, DITCHES, PERIMETER EROSION CONTROL MEASURES, SEDIMENT TRAPS, ETC., MULCH AND SEED AS REQUIRED.
9. SEDIMENT TRAPS AND/OR BASINS SHALL BE USED AS NECESSARY TO CONTAIN RUNOFF UNTIL SOILS ARE STABILIZED.
10. FINISH PAVING ALL ROADWAYS AND PARKING LOTS.
11. INSPECT AND MAINTAIN ALL EROSION AND SEDIMENT CONTROL MEASURES.
12. COMPLETE PERMANENT SEEDING AND LANDSCAPING.
13. REMOVE TRAPPED SEDIMENTS FROM COLLECTOR DEVICES AS APPROPRIATE AND THEN REMOVE TEMPORARY EROSION CONTROL MEASURES.

SPECIAL CONSTRUCTION NOTES:

- 1. THE CONSTRUCTION SEQUENCE MUST LIMIT THE DURATION AND AREA OF DISTURBANCE.
2. THE PROJECT IS TO BE MANAGED IN A MANNER THAT MEETS THE REQUIREMENTS AND INTENT OF RSA 430:53 AND CHAPTER AGR 3800 RELATIVE TO INVASIVE SPECIES.

EROSION CONTROL NOTES:

- 1. ALL EROSION CONTROL MEASURES AND PRACTICES SHALL CONFORM TO THE "NEW HAMPSHIRE STORMWATER MANUAL VOLUME 3: EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION" PREPARED BY THE NHDES.
2. PRIOR TO ANY WORK OR SOIL DISTURBANCE, CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR EROSION CONTROL MEASURES AS REQUIRED IN THE PROJECT MANUAL.
3. CONTRACTOR SHALL INSTALL TEMPORARY EROSION CONTROL BARRIERS, INCLUDING HAY BALES, SILT FENCES, MULCH BERMS, SILT SACKS AND SILT SOCKS AS SHOWN IN THESE DRAWINGS AS THE FIRST ORDER OF WORK.
4. SILT SACK INLET PROTECTION SHALL BE INSTALLED IN ALL EXISTING AND PROPOSED CATCH BASIN INLETS WITHIN THE WORK LIMITS AND BE MAINTAINED FOR THE DURATION OF THE PROJECT.
5. PERIMETER CONTROLS INCLUDING SILT FENCES, MULCH BERM, SILT SOCK, AND/OR HAY BALE BARRIERS SHALL BE MAINTAINED FOR THE DURATION OF THE PROJECT UNTIL NON-PAVED AREAS HAVE BEEN STABILIZED.
6. THE CONTRACTOR SHALL REMOVE AND PROPERLY DISPOSE OF ALL TEMPORARY EROSION CONTROL DEVICES UPON COMPLETION OF CONSTRUCTION.
7. ALL DISTURBED AREAS NOT OTHERWISE BEING TREATED SHALL RECEIVE 6" LOAM, SEED AND FERTILIZER.
8. INSPECT ALL INLET PROTECTION AND PERIMETER CONTROLS WEEKLY AND AFTER EACH RAIN STORM OF 0.25 INCH OR GREATER. REPAIR/MODIFY PROTECTION AS NECESSARY TO MAXIMIZE EFFICIENCY OF FILTER. REPLACE ALL FILTERS WHEN SEDIMENT IS 1/3 THE FILTER HEIGHT.
9. CONSTRUCT EROSION CONTROL BLANKETS ON ALL SLOPES STEEPER THAN 3:1.

STABILIZATION:

- 1. AN AREA SHALL BE CONSIDERED STABLE WHEN ONE OF THE FOLLOWING HAS OCCURRED:
A. BASE COURSE GRAVELS HAVE BEEN INSTALLED IN AREAS TO BE PAVED;
B. A MINIMUM OF 85% VEGETATED GROWTH HAS BEEN ESTABLISHED;
C. A MINIMUM OF 3" OF NON-EROSIVE MATERIAL SUCH AS STONE OR RIPRAP HAS BEEN INSTALLED;
D. EROSION CONTROL BLANKETS HAVE BEEN PROPERLY INSTALLED.;
E. IN AREAS TO BE PAVED, "STABLE" MEANS THAT BASE COURSE GRAVELS MEETING THE REQUIREMENTS OF NHDOT STANDARD FOR ROAD AND BRIDGE CONSTRUCTION, 2016, ITEM 304.2 HAVE BEEN INSTALLED.
2. WINTER STABILIZATION PRACTICES:
A. ALL PROPOSED VEGETATED AREAS THAT DO NOT EXHIBIT A MINIMUM OF 85 PERCENT VEGETATIVE GROWTH BY OCTOBER 15, OR WHICH ARE DISTURBED AFTER OCTOBER 15, SHALL BE STABILIZED BY SEEDING AND INSTALLING EROSION CONTROL BLANKETS ON SLOPES GREATER THAN 3:1, AND SEEDING AND PLACING 3 TO 4 TONS OF MULCH PER ACRE, SECURED WITH ANCHORED NETTING, ELSEWHERE. THE INSTALLATION OF EROSION CONTROL BLANKETS OR MULCH AND NETTING SHALL NOT OCCUR OVER ACCUMULATED SNOW OR ON FROZEN GROUND AND SHALL BE COMPLETED IN ADVANCE OF THAW OR SPRING MELT EVENTS;
B. ALL DITCHES OR SWALES WHICH DO NOT EXHIBIT A MINIMUM OF 85 PERCENT VEGETATIVE GROWTH BY OCTOBER 15, OR WHICH ARE DISTURBED AFTER OCTOBER 15, SHALL BE STABILIZED TEMPORARILY WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW CONDITIONS;
C. AFTER OCTOBER 15, INCOMPLETE ROAD OR PARKING SURFACES, WHERE WORK HAS STOPPED FOR THE WINTER SEASON, SHALL BE PROTECTED WITH A MINIMUM OF 3 INCHES OF CRUSHED GRAVEL PER NHDOT ITEM 304.3, OR IF CONSTRUCTION IS TO CONTINUE THROUGH THE WINTER SEASON BE CLEARED OF ANY ACCUMULATED SNOW AFTER EACH STORM EVENT;
3. STABILIZATION SHALL BE INITIATED ON ALL LOAM STOCKPILES, AND DISTURBED AREAS, WHERE CONSTRUCTION ACTIVITY SHALL NOT OCCUR FOR MORE THAN TWENTY-ONE (21) CALENDAR DAYS BY THE FOURTEENTH (14TH) DAY AFTER CONSTRUCTION ACTIVITY HAS PERMANENTLY OR TEMPORARILY CEASED IN THAT AREA. STABILIZATION MEASURES TO BE USED INCLUDE:
A. TEMPORARY SEEDING;
B. MULCHING.
4. ALL AREAS SHALL BE STABILIZED WITHIN 45 DAYS OF INITIAL DISTURBANCE.
5. WHEN CONSTRUCTION ACTIVITY PERMANENTLY OR TEMPORARILY CEASES WITHIN 100 FEET OF NEARBY SURFACE WATERS OR DELINEATED WETLANDS, THE AREA SHALL BE STABILIZED WITHIN SEVEN (7) DAYS OR PRIOR TO A RAIN EVENT. ONCE CONSTRUCTION ACTIVITY CEASES PERMANENTLY IN AN THESE AREAS, SILT FENCES, MULCH BERMS, HAY BALE BARRIERS AND ANY EARTH/DIKES SHALL BE REMOVED ONCE PERMANENT MEASURES ARE ESTABLISHED.
6. DURING CONSTRUCTION, RUNOFF WILL BE DIVERTED AROUND THE SITE WITH EARTH DIKES, PIPING OR STABILIZED CHANNELS WHERE POSSIBLE. SHEET RUNOFF FROM THE SITE WILL BE

FILTERED THROUGH SILT FENCES, MULCH BERMS, HAY BALE BARRIERS, OR SILT SOCKS. ALL STORM DRAIN BASIN INLETS SHALL BE PROVIDED WITH FLARED END SECTIONS AND TRASH RACKS. THE SITE SHALL BE STABILIZED FOR THE WINTER BY OCTOBER 15.

DUST CONTROL:

- 1. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONTROL DUST THROUGHOUT THE CONSTRUCTION PERIOD.
2. DUST CONTROL METHODS SHALL INCLUDE, BUT BE NOT LIMITED TO SPRINKLING WATER ON EXPOSED AREAS, COVERING LOADED DUMP TRUCKS LEAVING THE SITE, AND TEMPORARY MULCHING.
3. DUST CONTROL MEASURES SHALL BE UTILIZED SO AS TO PREVENT THE MIGRATION OF DUST FROM THE SITE TO ABUTTING AREAS.

STOCKPILES:

- 1. LOCATE STOCKPILES A MINIMUM OF 50 FEET AWAY FROM CATCH BASINS, SWALES, AND CULVERTS.
2. ALL STOCKPILES SHOULD BE SURROUNDED WITH TEMPORARY EROSION CONTROL MEASURES PRIOR TO THE ONSET OF PRECIPITATION.
3. PERIMETER BARRIERS SHOULD BE MAINTAINED AT ALL TIMES, AND ADJUSTED AS NEEDED TO ACCOMMODATE THE DELIVERY AND REMOVAL OF MATERIALS FROM THE STOCKPILE. THE INTEGRITY OF THE BARRIER SHOULD BE INSPECTED AT THE END OF EACH WORKING DAY.
4. PROTECT ALL STOCKPILES FROM STORMWATER RUN-OFF USING TEMPORARY EROSION CONTROL MEASURES SUCH AS BERMS, SILT SOCK, OR OTHER APPROVED PRACTICE TO PREVENT MIGRATION OF MATERIAL BEYOND THE IMMEDIATE CONFINES OF THE STOCKPILES.

OFF SITE VEHICLE TRACKING:

- 1. THE CONTRACTOR SHALL CONSTRUCT STABILIZED CONSTRUCTION ENTRANCE(S) PRIOR TO ANY EXCAVATION ACTIVITIES.

VEGETATION:

- 1. TEMPORARY GRASS COVER:
A. SEEDBED PREPARATION:
a. SEE LANDSCAPE PLAN FOR SEEDBED PREPARATION REQUIREMENTS;
B. SEEDING:
a. SEE LANDSCAPE PLAN FOR SEEDING REQUIREMENTS;
C. MAINTENANCE:
a. TEMPORARY SEEDING SHALL BE PERIODICALLY INSPECTED. AT A MINIMUM, 95% OF THE SOIL SURFACE SHOULD BE COVERED BY VEGETATION. IF ANY EVIDENCE OF EROSION OR SEDIMENTATION IS APPARENT, REPAIRS SHALL BE MADE AND OTHER TEMPORARY MEASURES USED IN THE INTERIM (MULCH, FILTER BARRIERS, CHECK DAMS, ETC.).
2. VEGETATIVE PRACTICE:
A. SEE LANDSCAPE PLAN FOR PERMANENT MEASURES AND PLANTINGS:
a. THE CONTRACTOR SHALL PROTECT AND MAINTAIN THE SEEDED AREAS UNTIL ACCEPTED;
b. IN NO CASE SHALL THE WEED CONTENT EXCEED ONE (1) PERCENT BY WEIGHT. ALL SEED SHALL COMPLY WITH STATE AND FEDERAL SEED LAWS. SEEDING SHALL BE DONE NO LATER THAN SEPTEMBER 15. IN NO CASE SHALL SEEDING TAKE PLACE OVER SNOW.
3. DORMANT SEEDING (SEPTEMBER 15 TO FIRST SNOWFALL):
A. FOLLOW PERMANENT MEASURES REQUIREMENTS. APPLY SEED MIXTURE AT TWICE THE INDICATED RATE. APPLY MULCH AS INDICATED FOR PERMANENT MEASURES.

CONCRETE WASHOUT AREA:

- 1. THE FOLLOWING ARE THE ONLY NON-STORMWATER DISCHARGES ALLOWED. ALL OTHER NON-STORMWATER DISCHARGES ARE PROHIBITED ON SITE:
A. THE CONCRETE DELIVERY TRUCKS SHALL, WHENEVER POSSIBLE, USE WASHOUT FACILITIES AT THEIR OWN PLANT OR DISPATCH FACILITY;
B. IF IT IS NECESSARY, SITE CONTRACTOR SHALL DESIGNATE SPECIFIC WASHOUT AREAS AND DESIGN FACILITIES TO HANDLE ANTICIPATED WASHOUT WATER;
C. CONTRACTOR SHALL LOCATE WASHOUT AREAS AT LEAST 150 FEET AWAY FROM STORM DRAINS, SWALES AND SURFACE WATERS OR DELINEATED WETLANDS;
D. INSPECT WASHOUT FACILITIES DAILY TO DETECT LEAKS OR TEARS AND TO IDENTIFY WHEN MATERIALS NEED TO BE REMOVED.

ALLOWABLE NON-STORMWATER DISCHARGES:

- 1. FIRE-FIGHTING ACTIVITIES;
2. FIRE HYDRANT FLUSHING;
3. WATERS USED TO WASH VEHICLES WHERE DETERGENTS ARE NOT USED;
4. WATER USED TO CONTROL DUST;
5. POTABLE WATER INCLUDING UNCONTAMINATED WATER LINE FLUSHING;
6. ROUTINE EXTERNAL BUILDING WASH DOWN WHERE DETERGENTS ARE NOT USED;
7. PAVEMENT WASH WATERS WHERE DETERGENTS ARE NOT USED;
8. UNCONTAMINATED AIR CONDITIONING/COMPRESSOR CONDENSATION;
9. UNCONTAMINATED GROUND WATER OR SPRING WATER;
10. FOUNDATION OR FOOTING DRAINS WHICH ARE UNCONTAMINATED;
11. UNCONTAMINATED EXCAVATION DEWATERING;
12. LANDSCAPE IRRIGATION.

WASTE DISPOSAL:

- 1. WASTE MATERIAL:
A. ALL WASTE MATERIALS SHALL BE COLLECTED AND STORED IN SECURELY LIDDED RECEPTACLES. ALL TRASH AND CONSTRUCTION DEBRIS FROM THE SITE SHALL BE DEPOSITED IN A DUMPSTER;
B. NO CONSTRUCTION WASTE MATERIALS SHALL BE BURIED ON SITE;
C. ALL PERSONNEL SHALL BE INSTRUCTED REGARDING THE CORRECT PROCEDURE FOR WASTE DISPOSAL BY THE SUPERINTENDENT.
2. HAZARDOUS WASTE:
A. ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN THE MANNER SPECIFIED BY LOCAL OR STATE REGULATION OR BY THE MANUFACTURER;
B. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES BY THE SUPERINTENDENT.
3. SANITARY WASTE:
A. ALL SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS A MINIMUM OF ONCE PER WEEK BY A LICENSED SANITARY WASTE MANAGEMENT CONTRACTOR.

SPILL PREVENTION:

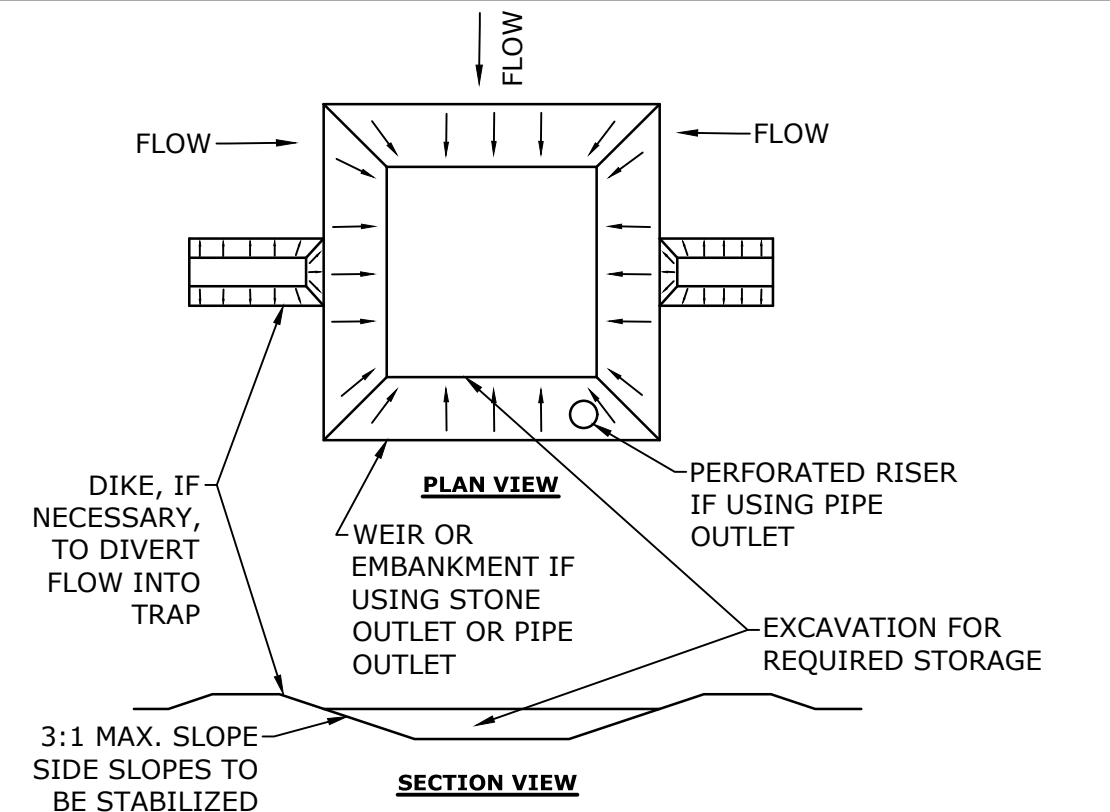
- 1. CONTRACTOR SHALL BE FAMILIAR WITH SPILL PREVENTION MEASURES REQUIRED BY LOCAL, STATE AND FEDERAL AGENCIES. AT A MINIMUM, CONTRACTOR SHALL FOLLOW THE BEST MANAGEMENT SPILL PREVENTION PRACTICES OUTLINED BELOW.
2. THE FOLLOWING ARE THE MATERIAL MANAGEMENT PRACTICES THAT SHALL BE USED TO REDUCE THE RISK OF SPILLS OR OTHER ACCIDENTAL EXPOSURE OF MATERIALS AND SUBSTANCES DURING CONSTRUCTION TO STORMWATER RUNOFF:
A. GOOD HOUSEKEEPING - THE FOLLOWING GOOD HOUSEKEEPING PRACTICE SHALL BE FOLLOWED ON SITE DURING CONSTRUCTION:
a. ONLY SUFFICIENT AMOUNTS OF PRODUCTS TO DO THE JOB SHALL BE STORED ON SITE;
b. ALL REGULATED MATERIALS STORED ON SITE SHALL BE STORED IN A NEAT, ORDERLY MANNER IN THEIR PROPER (ORIGINAL IF POSSIBLE) CONTAINERS AND, IF POSSIBLE, UNDER A ROOF OR OTHER ENCLOSURE, ON AN IMPERVIOUS SURFACE;
c. MANUFACTURER'S RECOMMENDATIONS FOR PROPER USE AND DISPOSAL SHALL BE FOLLOWED;
d. THE SITE SUPERINTENDENT SHALL INSPECT DAILY TO ENSURE PROPER USE AND DISPOSAL OF MATERIALS;
e. SUBSTANCES SHALL NOT BE MIXED WITH ONE ANOTHER UNLESS RECOMMENDED BY THE MANUFACTURER;
f. WHENEVER POSSIBLE ALL OF A PRODUCT SHALL BE USED UP BEFORE DISPOSING OF THE CONTAINER.
g. THE TRAINING OF ON-SITE EMPLOYEES AND THE ON-SITE POSTING OF RELEASE RESPONSE INFORMATION DESCRIBING WHAT TO DO IN THE EVENT OF A SPILL OF REGULATED SUBSTANCES.
B. HAZARDOUS PRODUCTS - THE FOLLOWING PRACTICES SHALL BE USED TO REDUCE THE RISKS ASSOCIATED WITH HAZARDOUS MATERIALS:
a. PRODUCTS SHALL BE KEPT IN THEIR ORIGINAL CONTAINERS UNLESS THEY ARE NOT RESEALABLE;
b. ORIGINAL LABELS AND MATERIAL SAFETY DATA SHALL BE RETAINED FOR IMPORTANT PRODUCT INFORMATION;
c. SURPLUS PRODUCT THAT MUST BE DISPOSED OF SHALL BE DISCARDED ACCORDING TO THE MANUFACTURER'S RECOMMENDED METHODS OF DISPOSAL.
C. PRODUCT SPECIFIC PRACTICES - THE FOLLOWING PRODUCT SPECIFIC PRACTICES SHALL BE FOLLOWED ON SITE:

PETROLEUM PRODUCTS:

- i. ALL ON SITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE LEAKAGE;
ii. PETROLEUM PRODUCTS SHALL BE STORED IN TIGHTLY SEALED CONTAINERS WHICH ARE CLEARLY LABELED. ANY ASPHALT BASED SUBSTANCES USED ON SITE SHALL BE APPLIED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.
iii. SECURE FUEL STORAGE AREAS AGAINST UNAUTHORIZED ENTRY;
iv. INSPECT FUEL STORAGE AREAS WEEKLY;
v. WHEREVER POSSIBLE, KEEP REGULATED CONTAINERS THAT ARE STORED OUTSIDE MORE THAN 50 FEET FROM SURFACE WATER AND STORM DRAINS, 75 FEET FROM PRIVATE WELLS, AND 400 FEET FROM PUBLIC WELLS;
vi. COVER REGULATED CONTAINERS IN OUTSIDE STORAGE AREAS;
vii. SECONDARY CONTAINMENT IS REQUIRED FOR CONTAINERS CONTAINING REGULATED SUBSTANCES STORED OUTSIDE, EXCEPT FOR ON PREMISE USE HEATING FUEL TANKS, OR ABOVEGROUND OR UNDERGROUND STORAGE TANKS OTHERWISE REGULATED.
viii. THE FUEL HANDLING REQUIREMENTS SHALL INCLUDE:
(1) EXCEPT WHEN IN USE, KEEP CONTAINERS CONTAINING REGULATED SUBSTANCES CLOSED AND SEALED;
(2) PLACE DRIP PANS UNDER SPIGOTS, VALVES, AND PUMPS;
(3) HAVE SPILL CONTROL AND CONTAINMENT EQUIPMENT READILY AVAILABLE IN ALL WORK AREAS;
(4) USE FUNNELS AND DRIP PANS WHEN TRANSFERRING REGULATED SUBSTANCES;
(5) PERFORM TRANSFERS OF REGULATED SUBSTANCES OVER AN IMPERVIOUS SURFACE.
ix. FUELING AND MAINTENANCE OF EXCAVATION, EARTHMOVING AND OTHER CONSTRUCTION RELATED EQUIPMENT SHALL COMPLY WITH THE REGULATIONS OF THE NEW HAMPSHIRE DEPARTMENT OF ENVIRONMENTAL SERVICES THESE REQUIREMENTS ARE SUMMARIZED IN WD-DWGB-22-6 BEST MANAGEMENT PRACTICES FOR FUELING AND MAINTENANCE OF EXCAVATION AND EARTHMOVING EQUIPMENT, OR ITS SUCCESSOR DOCUMENT.
HTTPS://WWW.DES.NH.GOV/ORGANIZATION/COMMISSIONER/PIP/FACTSHEETS/DWGB/DOCUMENTS/DWGB-22-6.PDF
b. FERTILIZERS:
i. FERTILIZERS USED SHALL BE APPLIED ONLY IN THE MINIMUM AMOUNTS DIRECTED BY THE SPECIFICATIONS;
ii. ONCE APPLIED FERTILIZER SHALL BE WORKED INTO THE SOIL TO LIMIT EXPOSURE TO STORMWATER;
iii. STORAGE SHALL BE IN A COVERED SHED OR ENCLOSED TRAILERS. THE CONTENTS OF ANY PARTIALLY USED BAGS OF FERTILIZER SHALL BE TRANSFERRED TO A SEALABLE PLASTIC BIN TO AVOID SPILLS.
c. PAINTS:
i. ALL CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE;
ii. EXCESS PAINT SHALL NOT BE DISCHARGED TO THE STORM SEWER SYSTEM;
iii. EXCESS PAINT SHALL BE DISPOSED OF PROPERLY ACCORDING TO MANUFACTURER'S INSTRUCTIONS OR STATE AND LOCAL REGULATIONS.
D. SPILL CONTROL PRACTICES - IN ADDITION TO GOOD HOUSEKEEPING AND MATERIAL MANAGEMENT PRACTICES DISCUSSED IN THE PREVIOUS SECTION, THE FOLLOWING PRACTICES SHALL BE FOLLOWED FOR SPILL PREVENTION AND CLEANUP:
a. MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEANUP SHALL BE CLEARLY POSTED AND SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATION OF THE INFORMATION AND CLEANUP SUPPLIES;
b. MATERIALS AND EQUIPMENT NECESSARY FOR SPILL CLEANUP SHALL BE KEPT IN THE MATERIAL STORAGE AREA ON SITE. EQUIPMENT AND MATERIALS SHALL INCLUDE BUT NOT BE LIMITED TO BROOMS, DUSTPANS, MOPS, RAGS, GLOVES, GOGGLES, KITTY LITTER, SAND, SAWDUST AND PLASTIC OR METAL TRASH CONTAINERS SPECIFICALLY FOR THIS PURPOSE;
c. ALL SPILLS SHALL BE CLEANED UP IMMEDIATELY AFTER DISCOVERY;
d. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL SHALL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE;
e. SPILLS OF TOXIC OR HAZARDOUS MATERIAL SHALL BE REPORTED TO THE APPROPRIATE LOCAL, STATE OR FEDERAL AGENCIES AS REQUIRED;
f. THE SITE SUPERINTENDENT RESPONSIBLE FOR DAY-TO-DAY SITE OPERATIONS SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR.
E. VEHICLE FUELING AND MAINTENANCE PRACTICE:
a. CONTRACTOR SHALL MAKE AN EFFORT TO PERFORM EQUIPMENT/VEHICLE FUELING AND MAINTENANCE AT AN OFF-SITE FACILITY;
b. CONTRACTOR SHALL PROVIDE AN ON-SITE FUELING AND MAINTENANCE AREA THAT IS CLEAN AND DRY;
c. IF POSSIBLE THE CONTRACTOR SHALL KEEP AREA COVERED;
d. CONTRACTOR SHALL KEEP A SPILL KIT AT THE FUELING AND MAINTENANCE AREA;
e. CONTRACTOR SHALL REGULARLY INSPECT VEHICLES FOR LEAKS AND DAMAGE;
f. CONTRACTOR SHALL USE DRIP PANS, DRIP CLOTHS, OR ABSORBENT PADS WHEN REPLACING SPENT FLUID.

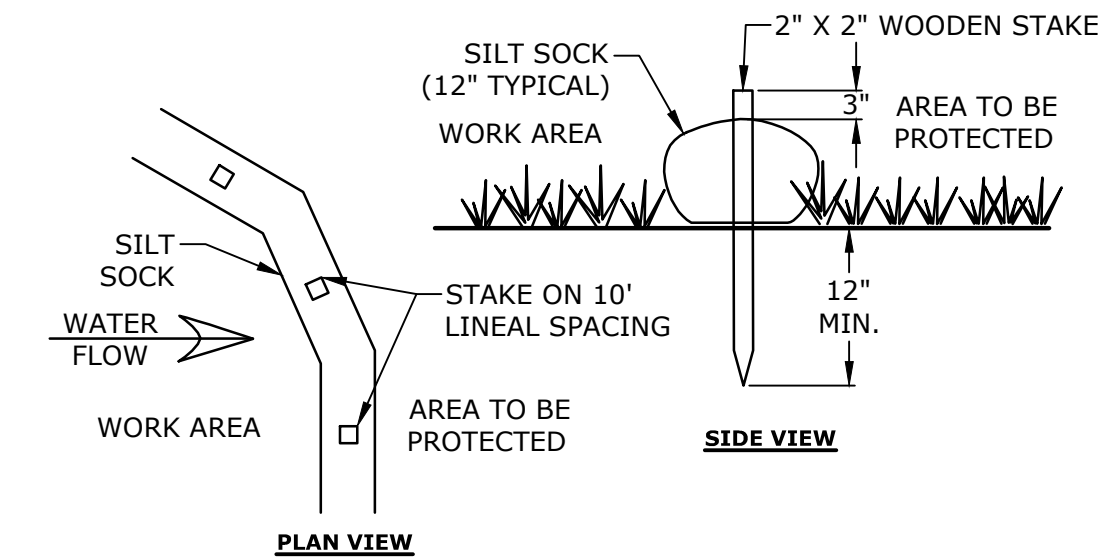
EROSION CONTROL OBSERVATIONS AND MAINTENANCE PRACTICES

- 1. THIS PROJECT EXCEEDS ONE (1) ACRE OF DISTURBANCE AND THUS REQUIRES A SWPPP. THE SWPPP SHALL BE PREPARED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE FAMILIAR WITH THE SWPPP AND KEEP AN UPDATED COPY OF THE SWPPP ONSITE AT ALL TIMES.
2. THE FOLLOWING REPRESENTS THE GENERAL OBSERVATION AND REPORTING PRACTICES THAT SHALL BE FOLLOWED AS PART OF THIS PROJECT:
A. OBSERVATIONS OF THE PROJECT FOR COMPLIANCE WITH THE SWPPP SHALL BE MADE BY THE CONTRACTOR AT LEAST ONCE A WEEK OR WITHIN 24 HOURS OF A STORM 0.25 INCHES OR GREATER;
B. AN OBSERVATION REPORT SHALL BE MADE AFTER EACH OBSERVATION AND DISTRIBUTED TO THE ENGINEER, THE OWNER, AND THE CONTRACTOR;
C. A REPRESENTATIVE OF THE SITE CONTRACTOR, SHALL BE RESPONSIBLE FOR MAINTENANCE AND REPAIR ACTIVITIES;
D. IF A REPAIR IS NECESSARY, IT SHALL BE INITIATED WITHIN 24 HOURS OF REPORT.



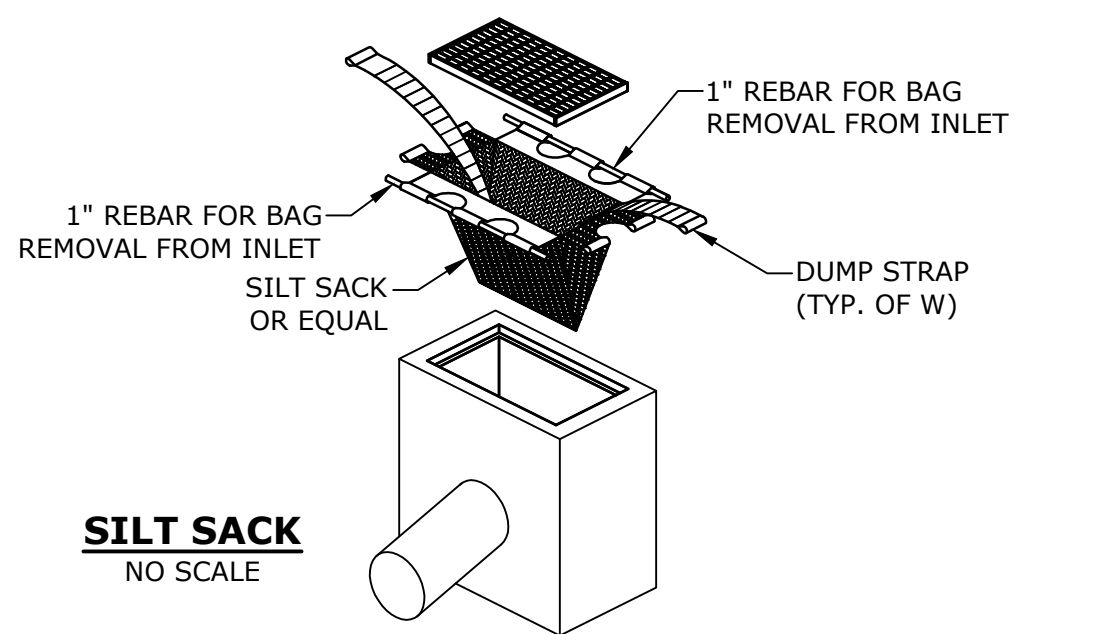
- NOTES:
1. THE TRAP SHALL BE INSTALLED AS CLOSE TO THE DISTURBED AREA AS POSSIBLE.
2. THE MAXIMUM CONTRIBUTING AREA TO A SINGLE TRAP SHALL BE LESS THAN 5 ACRES.
3. THE MINIMUM VOLUME OF THE TRAP SHALL BE 3,600 CUBIC FEET OF STORAGE FOR EACH ACRE OF DRAINAGE AREA.
4. TRAP OUTLET SHALL BE MINIMUM OF ONE FOOT BELOW THE CREST OF THE TRAP.
5. TRAP SHALL DISCHARGE TO A STABILIZED AREA.
6. TRAP SHALL BE CLEANED WHEN 50 PERCENT OF THE ORIGINAL VOLUME IS FILLED.
7. MATERIALS REMOVED FROM THE TRAP SHALL BE PROPERLY DISPOSED OF AND STABILIZED.
8. SEDIMENT TRAPS MUST BE USED AS NEEDED TO CONTAIN RUNOFF UNTIL SOILS ARE STABILIZED.

SEDIMENT TRAP NO SCALE

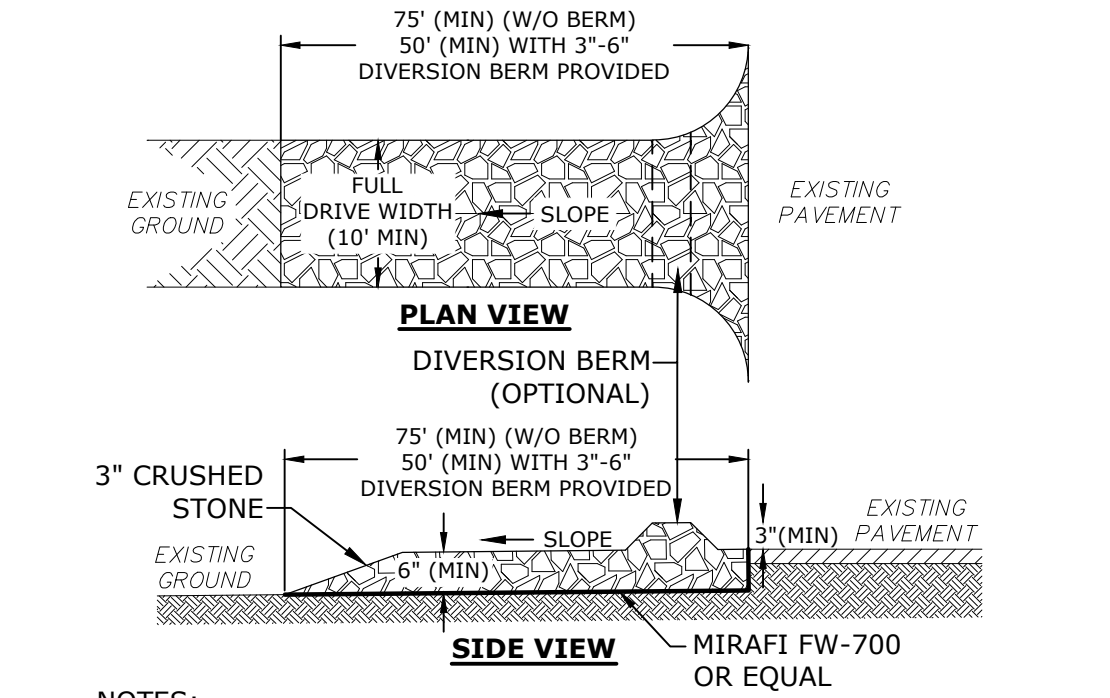


- NOTES:
1. SILT SOCK SHALL BE SILT SOXX BY FILTREXX OR APPROVED EQUAL
2. INSTALL SILT SOCK IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS

SILT SOCK NO SCALE

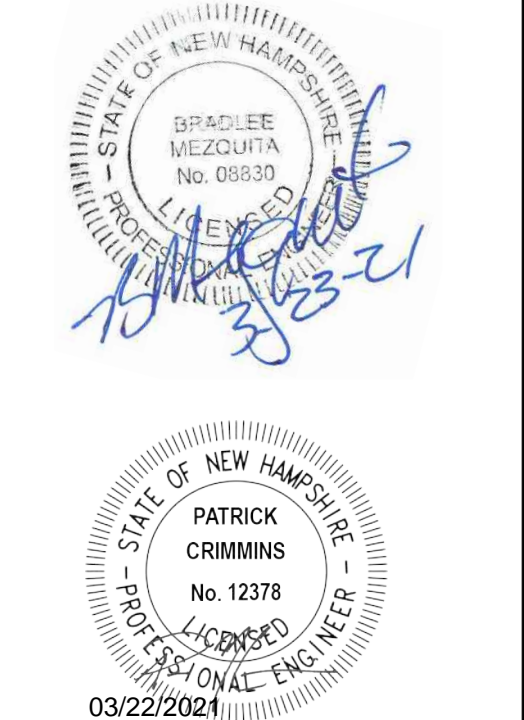


SILT SACK NO SCALE



- NOTES:
1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OF SEDIMENT FROM THE SITE. WHEN WASHING IS REQUIRED, IT SHALL BE DONE SO RUNOFF DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE. ALL SEDIMENT SHALL BE PREVENTED FROM ENTERING STORM DRAINS, DITCHES, OR WATERWAYS

STABILIZED CONSTRUCTION EXIT NO SCALE



Proposed Mixed Use Development

North Mill Pond Holdings, LLC

Portsmouth, New Hampshire

Table with 3 columns: MARK, DATE, DESCRIPTION. Row C: 3/22/2021, TAC Submission. Row B: 3/10/2021, Design Review Resubmission. Row A: 12/1/2020, TAC Work Session.

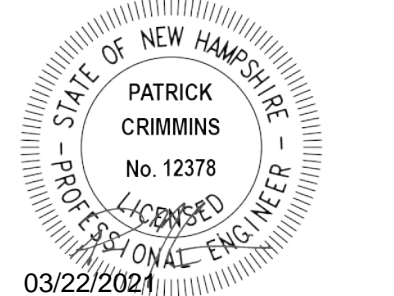
Table with 2 columns: PROJECT NO: P-0595-007, DATE: December 22, 2020, FILE: P-0595-007-DTLS.DWG, DRAWN BY: CLK, CHECKED BY: NAH/PMC, APPROVED BY: BLM.

EROSION CONTROL NOTES AND DETAILS SHEET

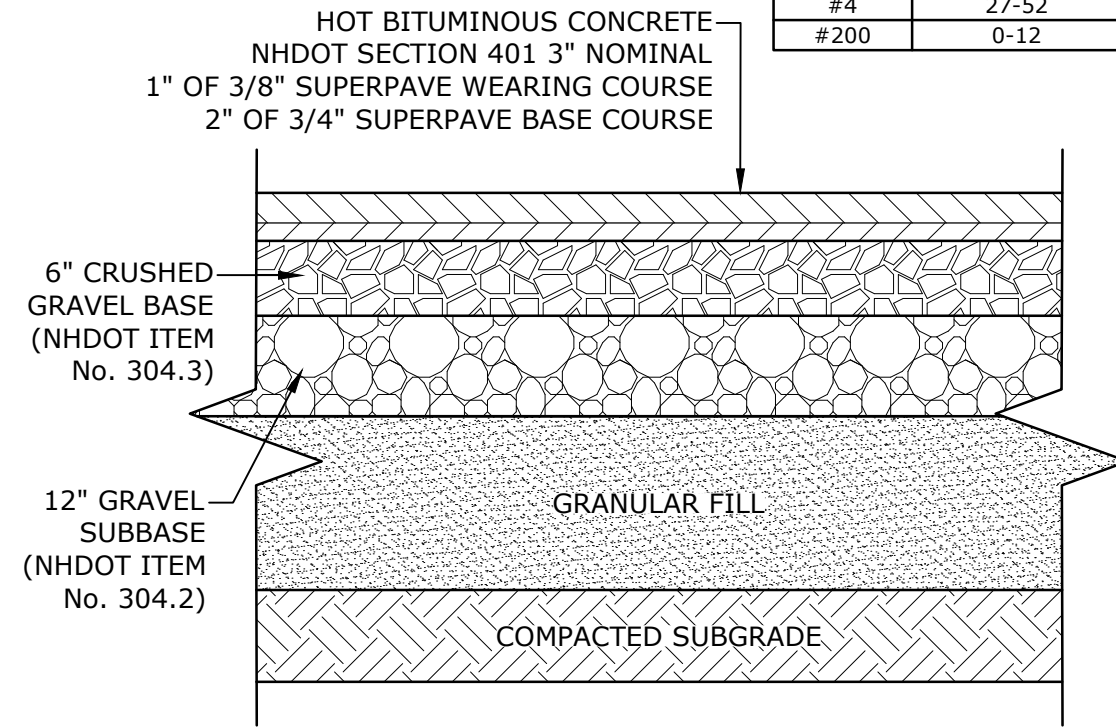
SCALE: AS SHOWN

C-501

Vertical text on the left margin: Last Saved: 3/19/2021, 1:55:41pm By: CLK/CLK, Tighe & Bond 210 W. 905th St., Portsmouth, NH 03801, Project: 0595-007, Drawing: 0595-007-DTLS.dwg, Figures: AlocCAD, Sheet: P-0595-007-DTLS.dwg



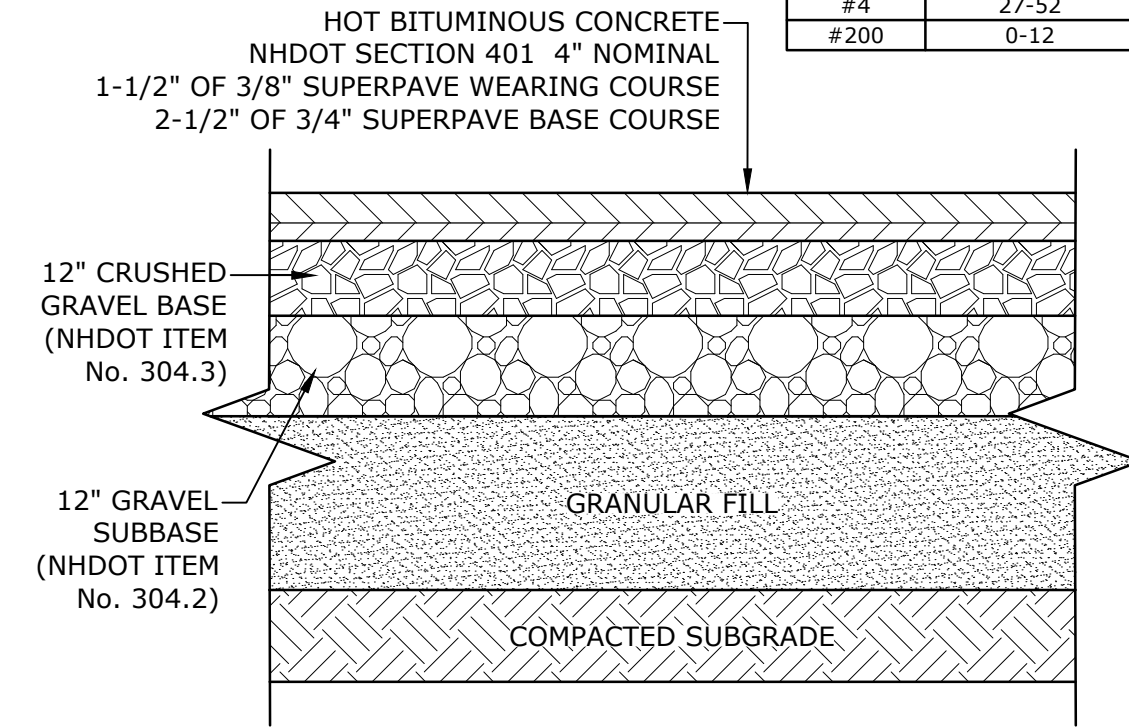
NHDOT ITEM No. 304.2 (GRAVEL)		NHDOT ITEM No. 304.3 (CRUSHED GRAVEL)	
SIEVE SIZE	% PASSING	SIEVE SIZE	% PASSING
6"	100	3"	100
#4	25-70	2"	95-100
#200	0-12	1"	55-85
		#4	27-52
		#200	0-12



- NOTES:**
- SEE SITE PLAN FOR PAVEMENT WIDTH AND LOCATION.
 - SEE GRADING, DRAINAGE AND EROSION CONTROL PLAN FOR PAVEMENT SLOPE AND CROSS-SLOPE.
 - A TACK COAT SHALL BE PLACED ON TOP OF BINDER COURSE PAVEMENT PRIOR TO PLACING WEARING COURSE.
 - REFER TO CITY SPECIFICATIONS FOR ASPHALT MIX DESIGN.

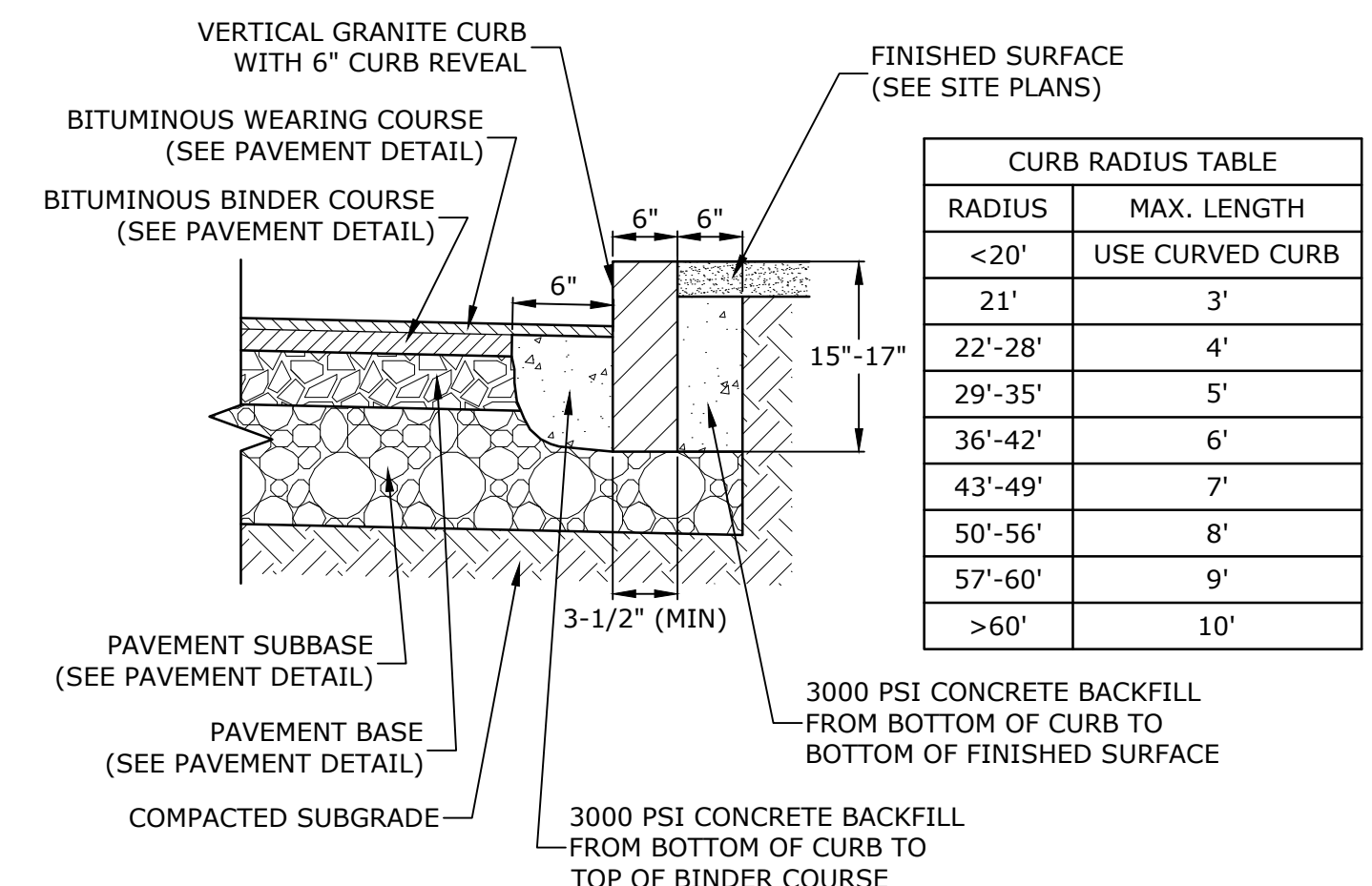
ON-SITE PAVEMENT SECTION
NO SCALE

NHDOT ITEM No. 304.2 (GRAVEL)		NHDOT ITEM No. 304.3 (CRUSHED GRAVEL)	
SIEVE SIZE	% PASSING	SIEVE SIZE	% PASSING
6"	100	3"	100
#4	25-70	2"	95-100
#200	0-12	1"	55-85
		#4	27-52
		#200	0-12



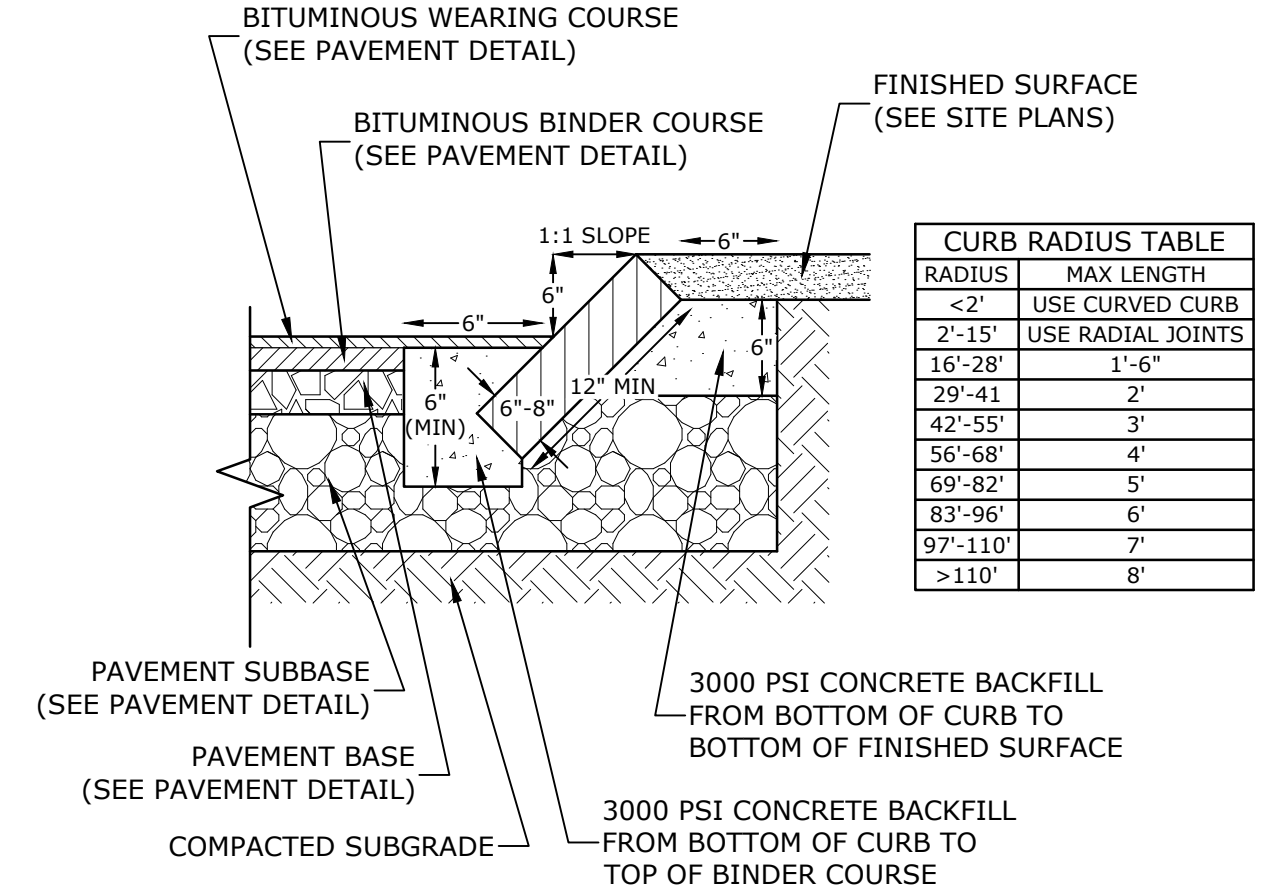
- NOTES:**
- SEE SITE PLAN FOR PAVEMENT WIDTH AND LOCATION.
 - SEE GRADING, DRAINAGE AND EROSION CONTROL PLAN FOR PAVEMENT SLOPE AND CROSS-SLOPE.
 - A TACK COAT SHALL BE PLACED ON TOP OF BINDER COURSE PAVEMENT PRIOR TO PLACING WEARING COURSE.
 - REFER TO CITY SPECIFICATIONS FOR ASPHALT MIX DESIGN.

CITY RIGHT-OF-WAY PAVEMENT SECTION
NO SCALE



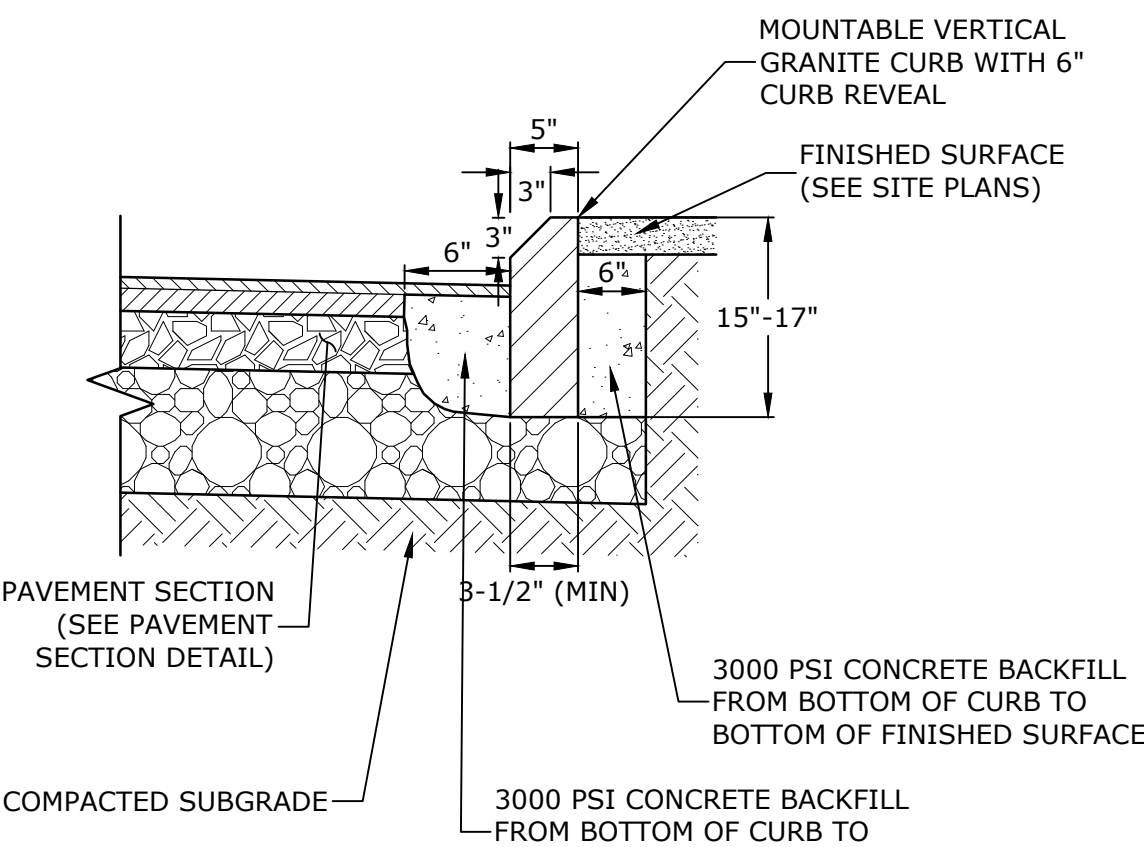
- NOTES:**
- SEE SITE PLAN(S) FOR LIMITS OF VERTICAL GRANITE CURB (VGC).
 - ADJOINING STONES SHALL HAVE THE SAME OR APPROXIMATELY THE SAME LENGTH.
 - MINIMUM LENGTH OF STRAIGHT CURB STONES = 3'
 - MAXIMUM LENGTH OF STRAIGHT CURB STONES = 10'
 - MAXIMUM LENGTH OF STRAIGHT CURB STONES LAID ON CURVES (SEE TABLE).
 - ALL RADII 20 FEET AND SMALLER SHALL BE CONSTRUCTED USING CURVED SECTIONS.
 - JOINTS BETWEEN STONES SHALL HAVE A MAXIMUM SPACING OF 1/2" AND SHALL BE MORTARED.

VERTICAL GRANITE CURB
NO SCALE



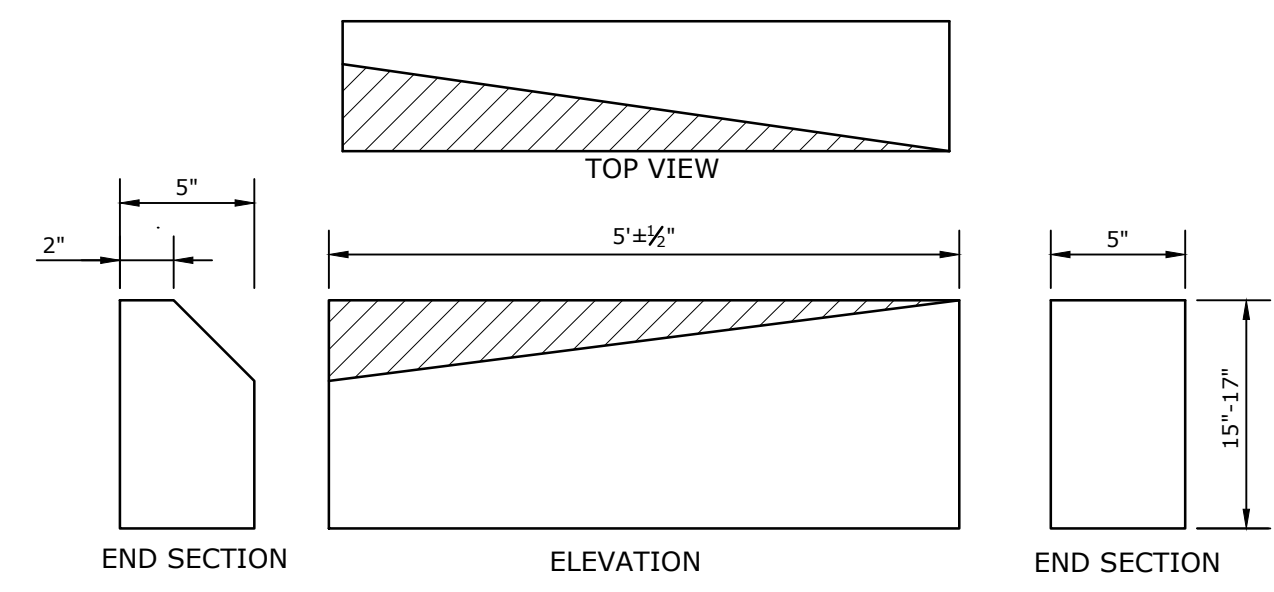
- NOTES:**
- SEE SITE PLAN(S) FOR LIMITS OF SLOPED GRANITE CURB (SGC).
 - ADJOINING STONES SHALL HAVE THE SAME OR APPROXIMATELY THE SAME LENGTH.
 - MINIMUM LENGTH OF STRAIGHT CURB STONES = 18"
 - MAXIMUM LENGTH OF STRAIGHT CURB STONES = 8'
 - MAXIMUM LENGTH OF STRAIGHT CURB STONES LAID ON CURVES (SEE TABLE).
 - JOINTS BETWEEN STONES SHALL HAVE A MAXIMUM SPACING OF 1/2" AND SHALL BE MORTARED.

SLOPED GRANITE CURB
NO SCALE



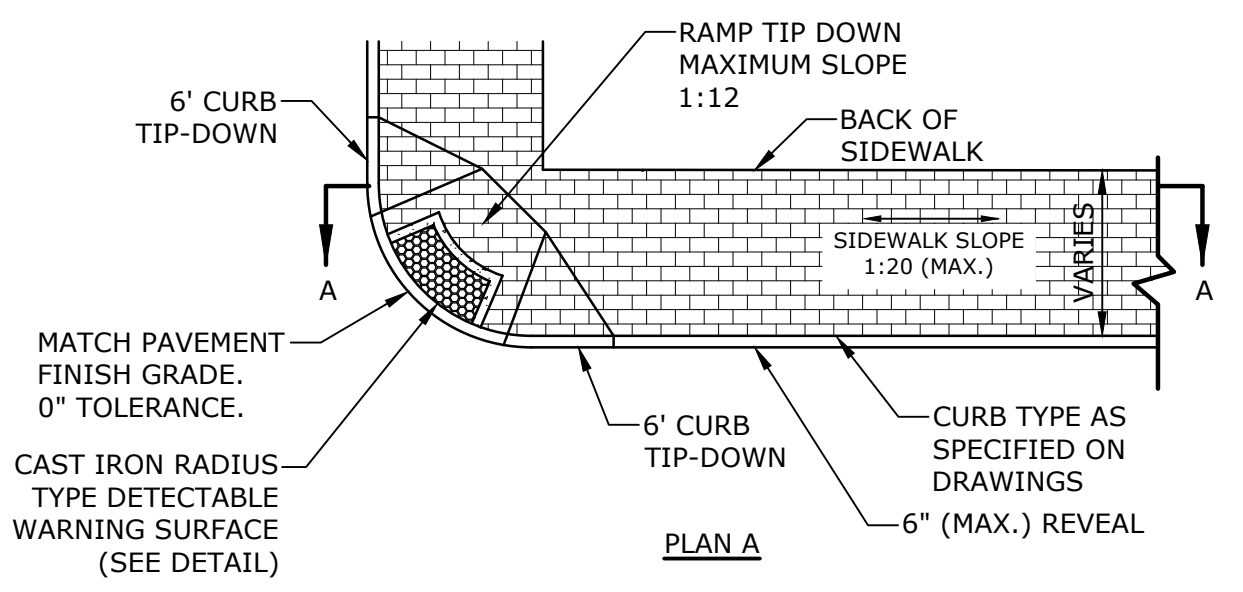
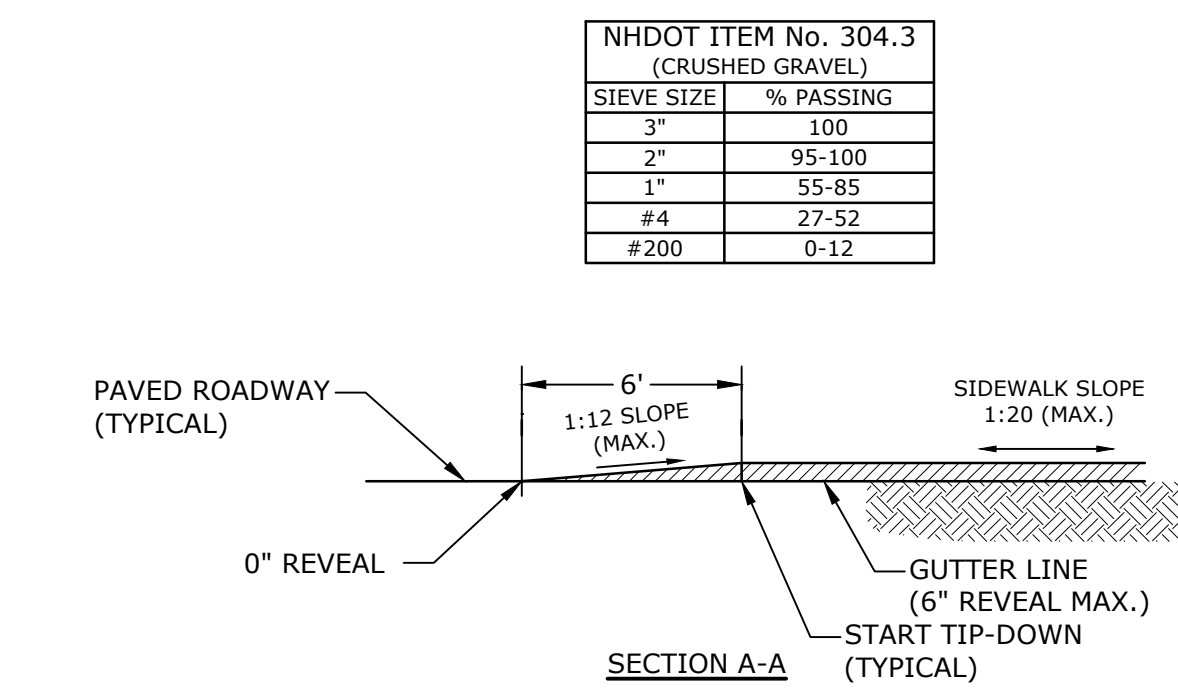
- NOTES:**
- SEE SITE PLAN(S) FOR LIMITS OF MOUNTABLE VERTICAL GRANITE CURB (MVGC).
 - ADJOINING STONES SHALL HAVE THE SAME OR APPROXIMATELY THE SAME LENGTH.
 - MINIMUM LENGTH OF STRAIGHT CURB STONES = 3'
 - MAXIMUM LENGTH OF STRAIGHT CURB STONES = 10'
 - MAXIMUM LENGTH OF STRAIGHT CURB STONES LAID ON CURVES (SEE TABLE).
 - ALL RADII 20 FEET AND SMALLER SHALL BE CONSTRUCTED USING CURVED SECTIONS.
 - JOINTS BETWEEN STONES SHALL HAVE A MAXIMUM SPACING OF 1/2" AND SHALL BE MORTARED.

MOUNTABLE VERTICAL GRANITE CURB
NO SCALE



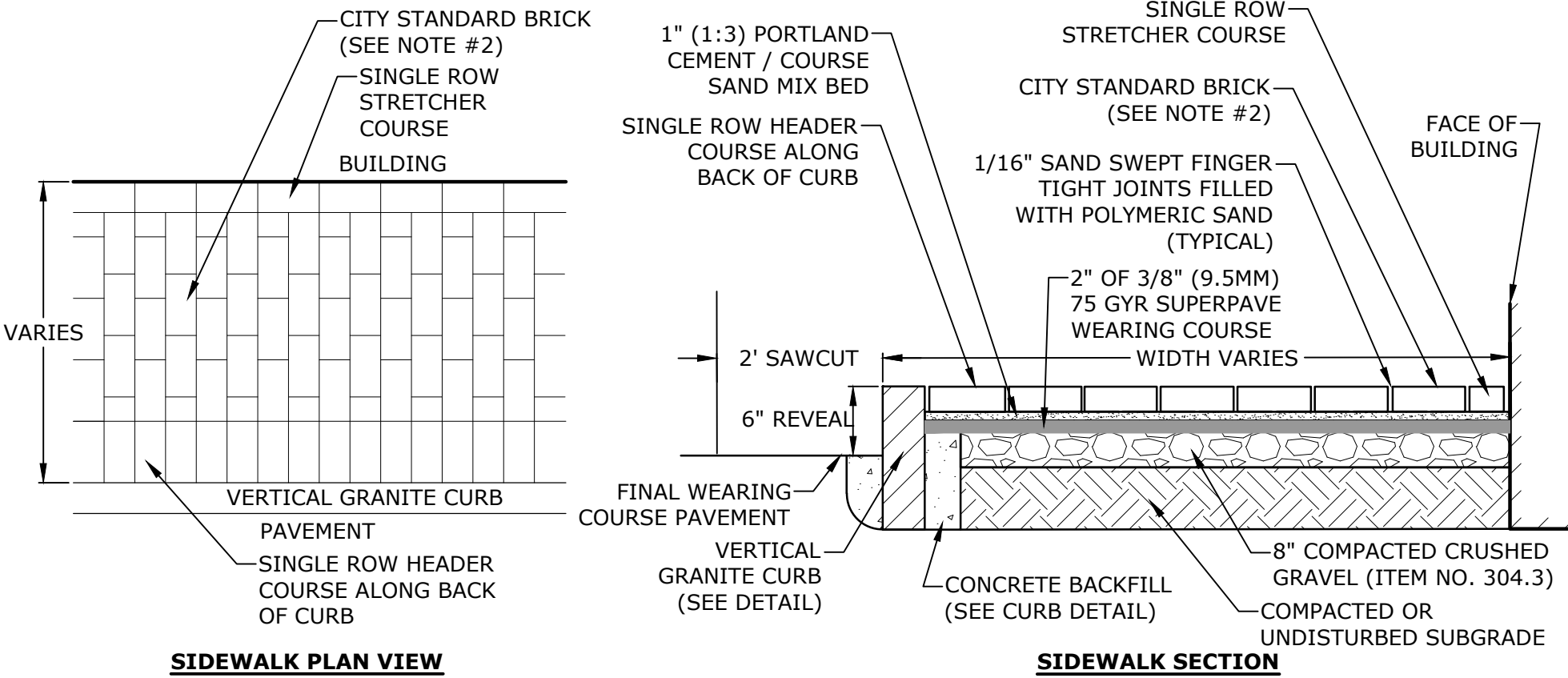
- NOTES:**
- THE INTENT OF THIS ITEM IS TO PROVIDE A SMOOTH TRANSITION BETWEEN VERTICAL GRANITE CURB AND MOUNTABLE VERTICAL GRANITE CURB WITHOUT REQUIRING FIELD CHIPPING DURING INSTALLATION. THE MOUNTABLE VERTICAL GRANITE CURB MAY REQUIRE ADJUSTMENTS TO MEET THE TRANSITION PIECE HEIGHT. TRANSITION SLOPE CURB TO STANDARD REVEAL AS QUICKLY AS POSSIBLE TO PROVIDE FOR THIS SMOOTH TRANSITION.

CURB TRANSITION
NO SCALE



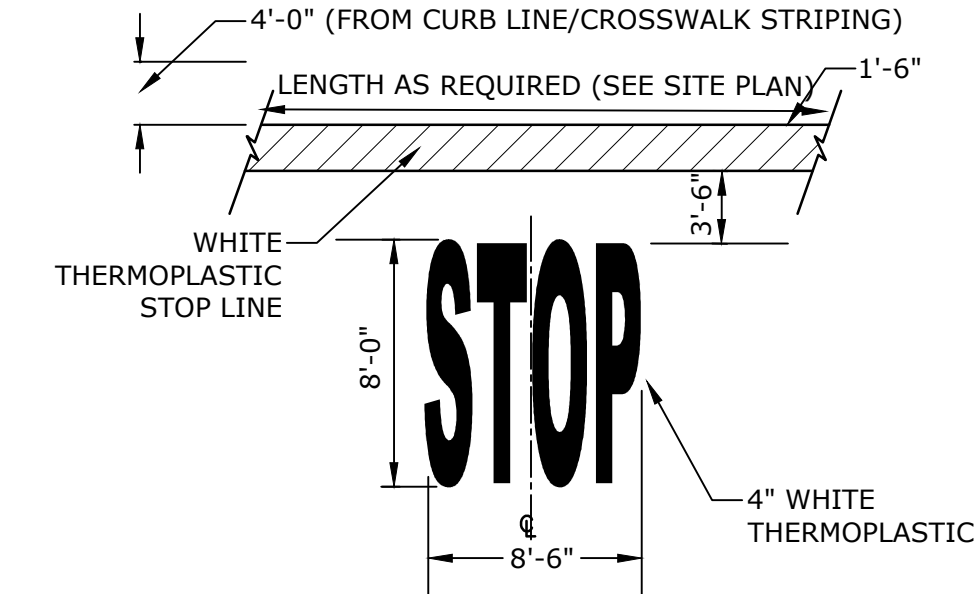
- NOTES:**
- RAMPS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT AND LOCAL AND STATE REQUIREMENTS.
 - A 6" COMPACTED CRUSHED GRAVEL BASE (NHDOT ITEM No. 304.3) SHALL BE PROVIDED BENEATH RAMPS.
 - DETECTABLE WARNING PANEL SHALL BE CAST IRON SET IN CONCRETE (SEE DETAIL.)
 - PROVIDE DETECTABLE WARNING SURFACES ANYTIME THAT A CURB RAMP, BLENDED TRANSITION, OR LANDING CONNECTS TO A STREET.
 - LOCATE THE DETECTABLE WARNING SURFACES AT THE BACK OF THE CURB ALONG THE EDGE OF THE LANDING.
 - THE MAXIMUM RUNNING SLOPE OF ANY SIDEWALK CURB RAMP IS 12:1, THE MAXIMUM CROSS SLOPE IS 2%. THE SLOPE OF THE LANDING SHALL NOT EXCEED 2% IN ANY DIRECTION.
 - TRANSITIONS SHALL BE FLUSH AND FREE OF ABRUPT CHANGES. ROADWAY SHOULDER SLOPES ADJOINING SIDEWALK CURB RAMPS SHALL BE A MAXIMUM OF 5% (FULL WIDTH) FOR A DISTANCE OF 2 FT. FROM THE ROADWAY CURBLINE.
 - THE BOTTOM OF THE SIDEWALK CURB RAMP OR LANDING, EXCLUSIVE OF THE FLARED SIDES, SHALL BE WHOLLY CONTAINED WITHIN THE CROSSWALK MARKINGS.
 - DETECTABLE WARNING PANELS SHALL BE A MINIMUM OF 2 FEET IN DEPTH. THE ROWS OF TRUNCATED DOMES SHALL BE ALIGNED PERPENDICULAR TO THE GRADE BREAK BETWEEN THE RAMP, BLENDED TRANSITION, OR LANDING AND THE STREET.
 - THE TEXTURE OF THE DETECTABLE WARNING FEATURE MUST CONTRAST VISUALLY WITH THE SURROUNDING SURFACES (EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT).

CONCRETE WHEELCHAIR ACCESSIBLE RAMP
NO SCALE



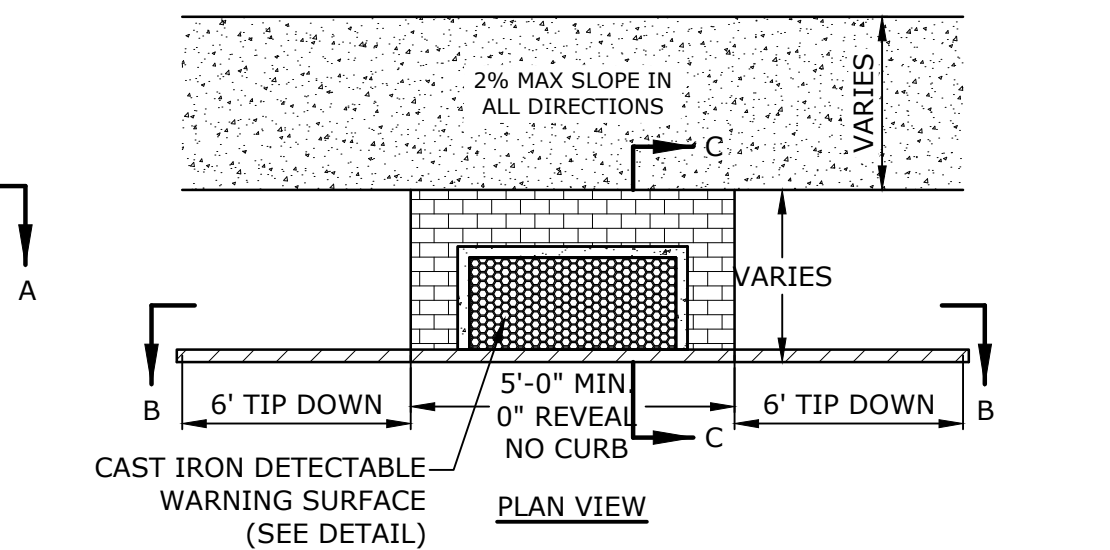
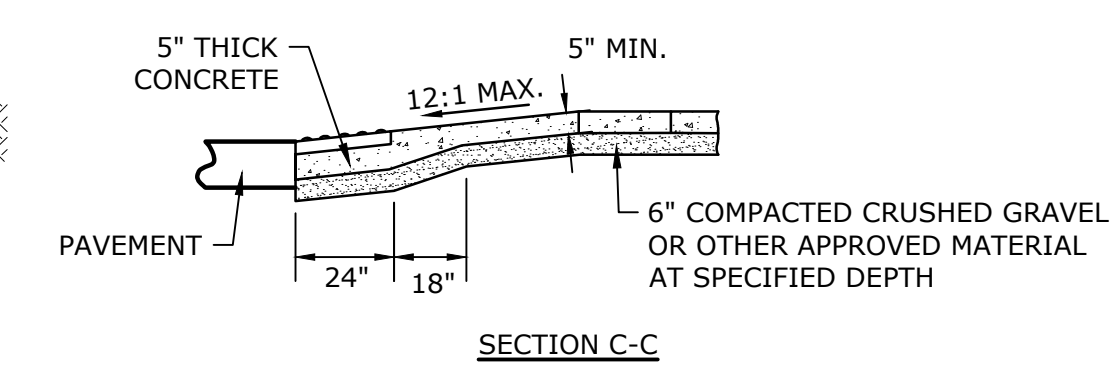
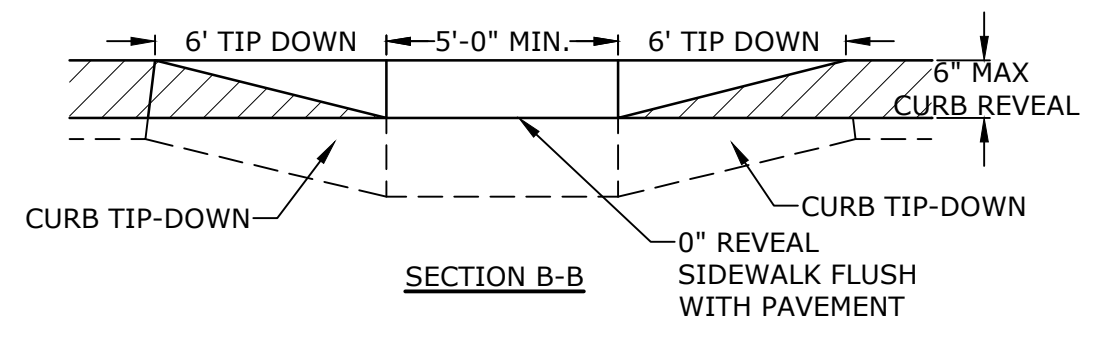
- NOTES:**
- BRICK SIDEWALK SHALL BE INSTALLED AS DETAILED AND PER CITY OF PORTSMOUTH REQUIREMENTS/SPECIFICATIONS AND SHALL INCLUDE A CONTINUOUS APPROVED PAVER EDGE RESTRAINT SYSTEM AT ALL LOCATIONS NOT ADJACENT TO CURB OR BUILDINGS.
 - CITY STANDARD BRICK SHALL BE TRADITIONAL EDGE, PATHWAY, FULL RANGE 2.25"x4"x8" PAVES, BY PINE HALL BRICK, INC. BRICK MATERIAL SAMPLES SHALL BE PROVIDED TO CPW PRIOR TO INSTALLATION FOR REVIEW AND APPROVAL.
 - BEDDING MATERIAL SHALL BE A PORTLAND CEMENT / COURSE SAND MIX THAT IS 1 PART PORTLAND CEMENT AND 3 PARTS COURSE SAND. SAND SHALL CONFORM WITH ASTM C-33 AND CEMENT SHALL BE PORTLAND CEMENT TYPE I/TYPE II.

BRICK SIDEWALK
NO SCALE



- NOTE:**
- PAVEMENT MARKINGS TO BE INSTALLED IN LOCATIONS AS SHOWN ON SITE PLAN.
 - STRIPING SHALL BE CONSTRUCTED USING WHITE THERMO PLASTIC, REFLECTORIZED PAVEMENT MARKING MATERIAL MEETING THE REQUIREMENTS OF ASTM D 4505

STOP BAR AND LEGEND
NO SCALE



Proposed Mixed Use Development

North Mill Pond Holdings, LLC

Portsmouth, New Hampshire

MARK	DATE	DESCRIPTION
C	3/22/2021	TAC Submission
B	3/10/2021	Design Review Resubmission
A	12/1/2020	TAC Work Session

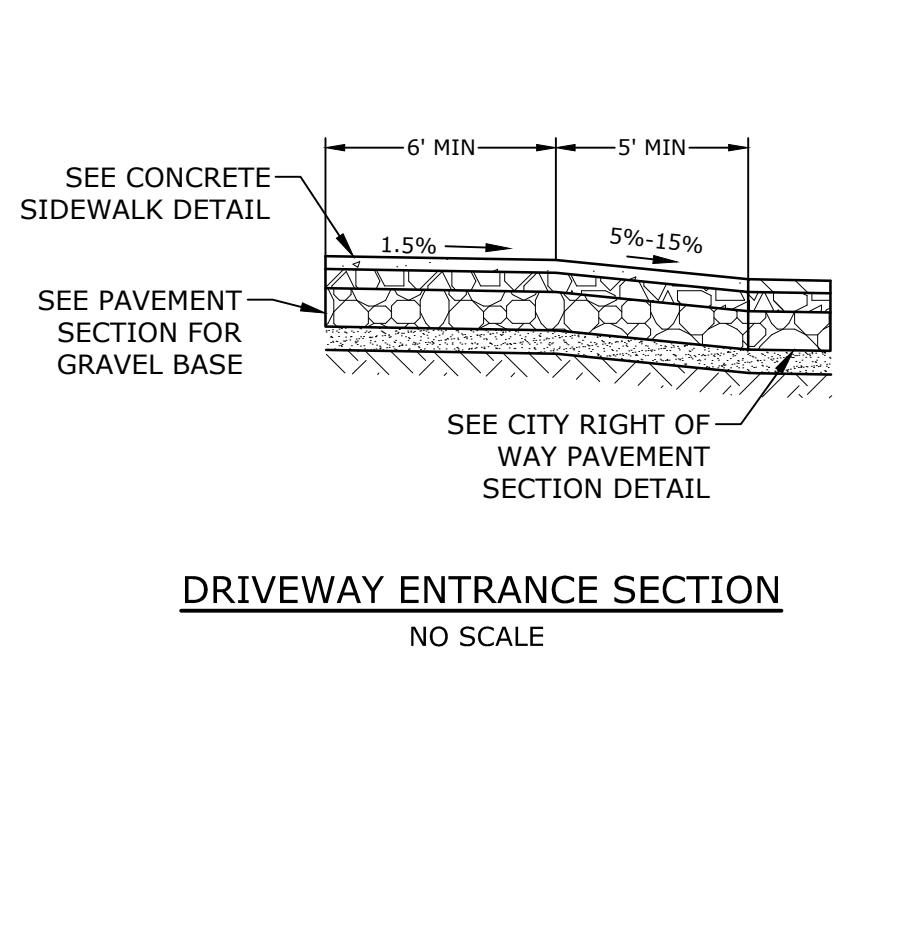
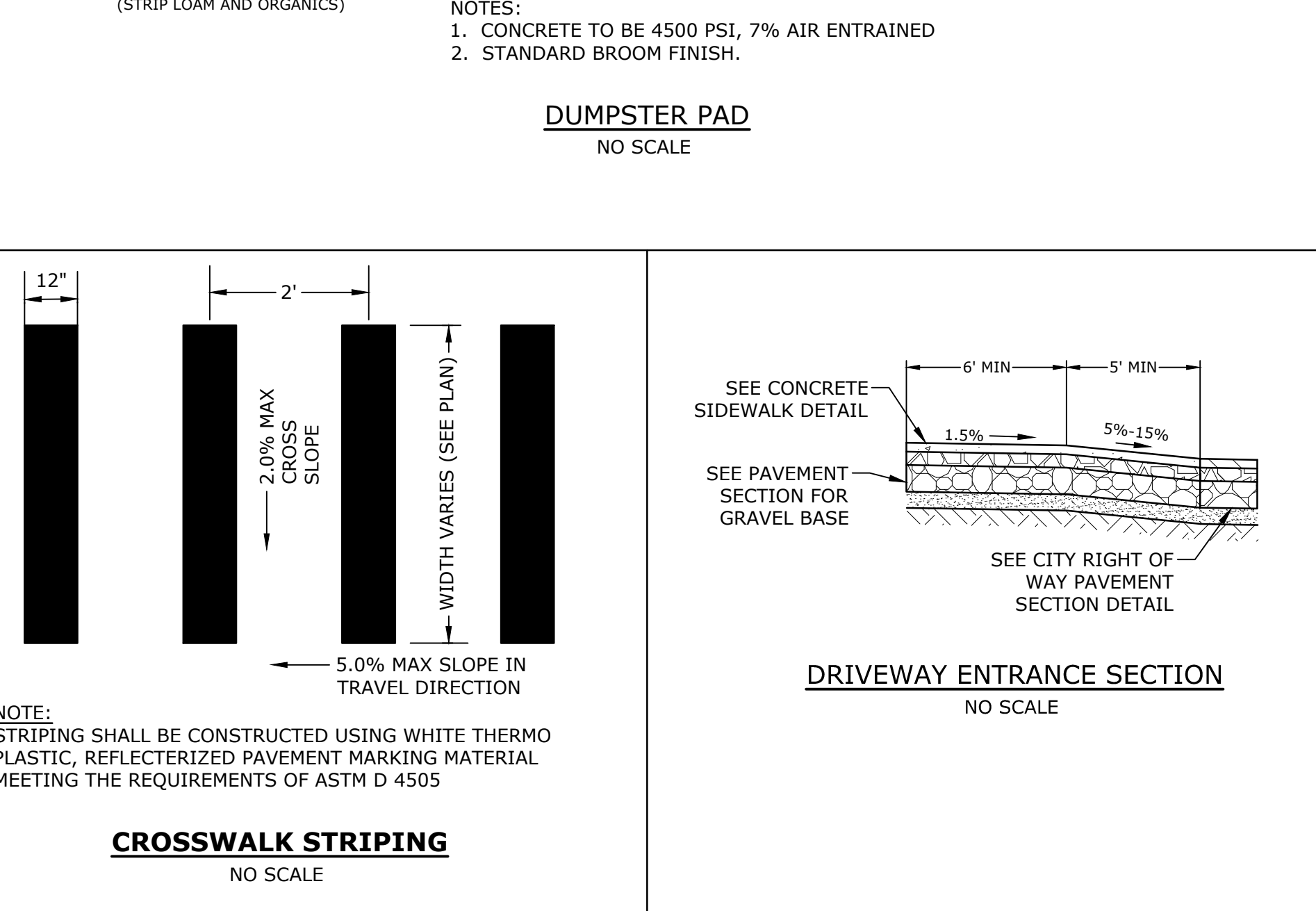
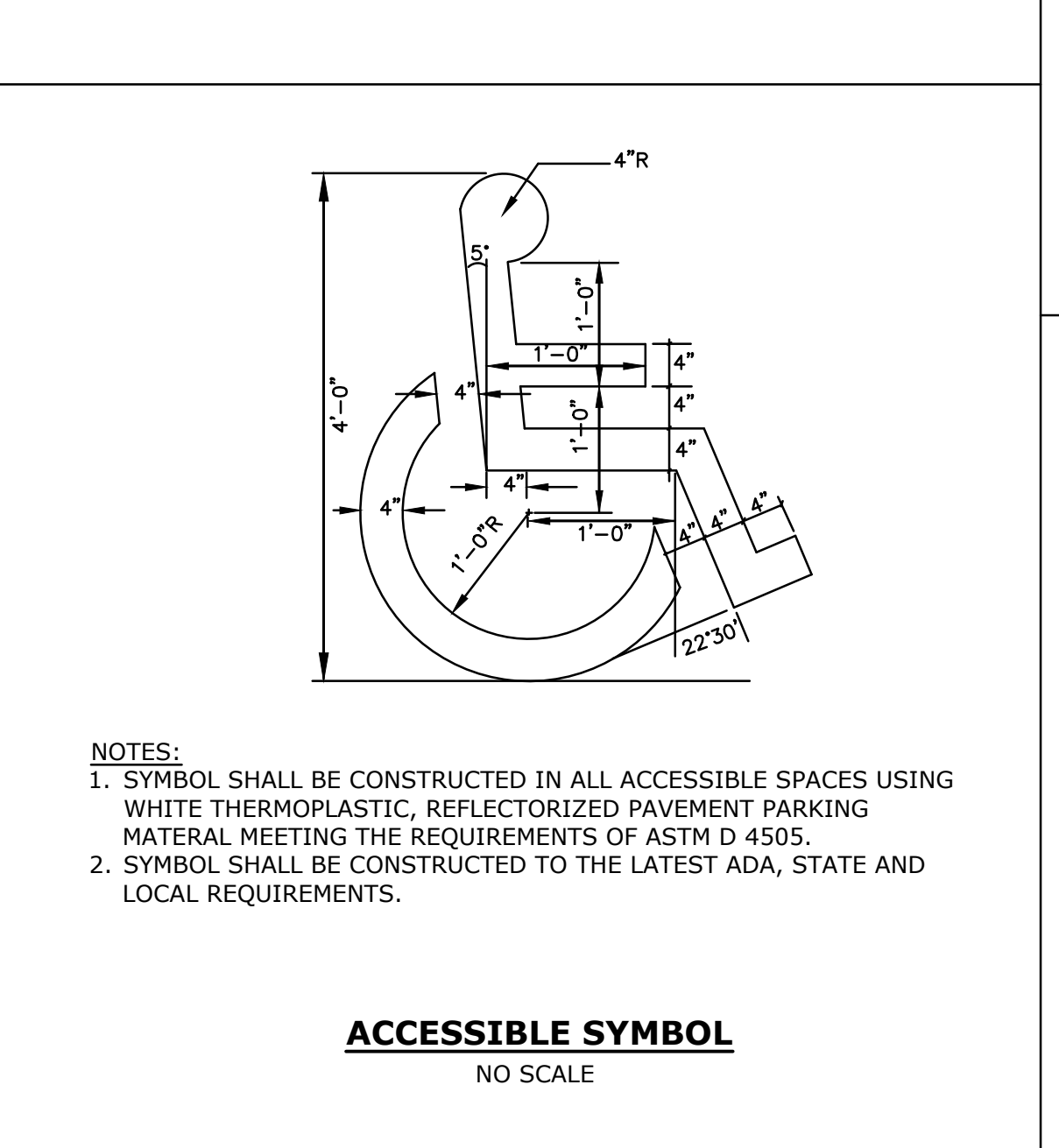
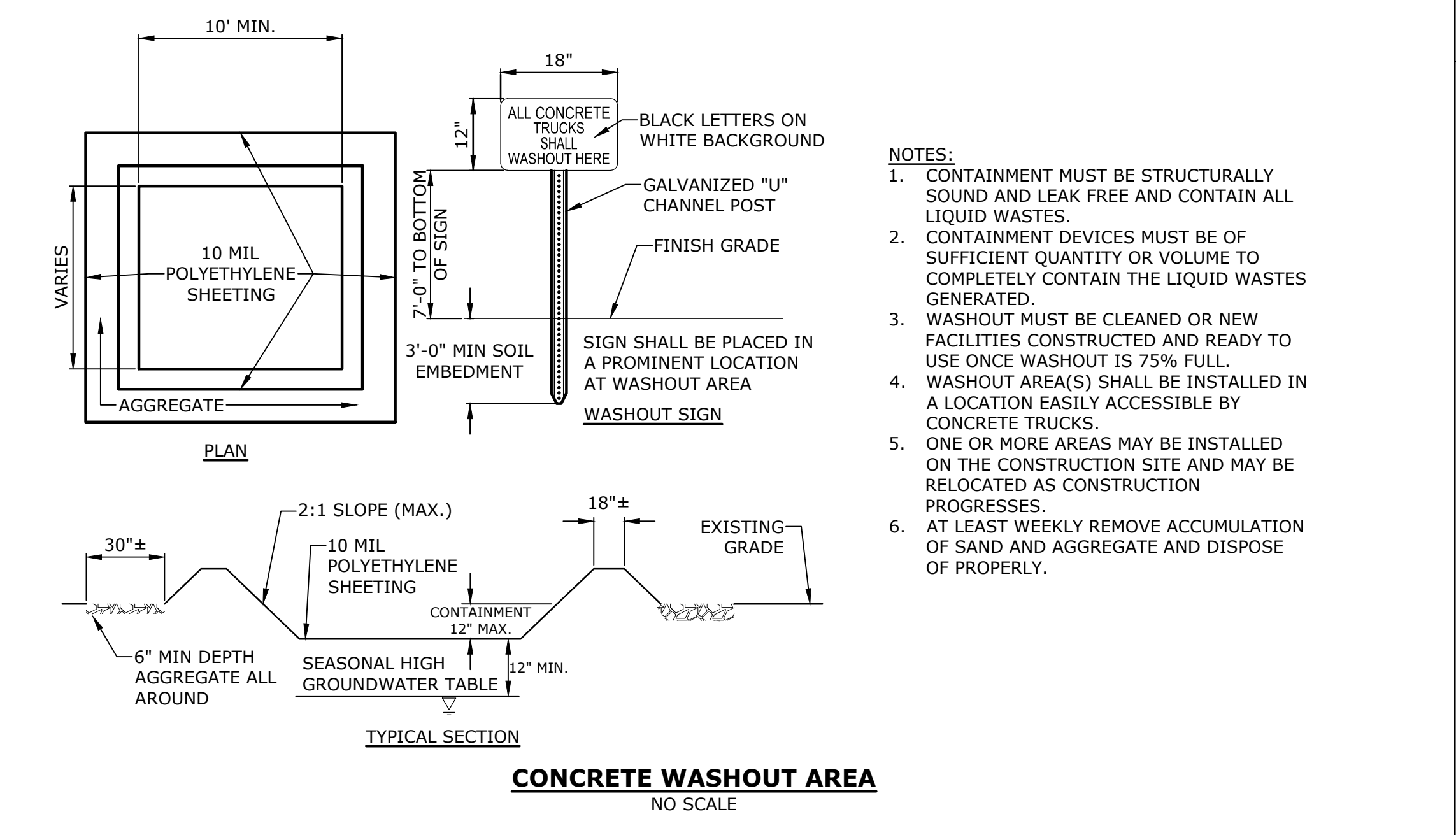
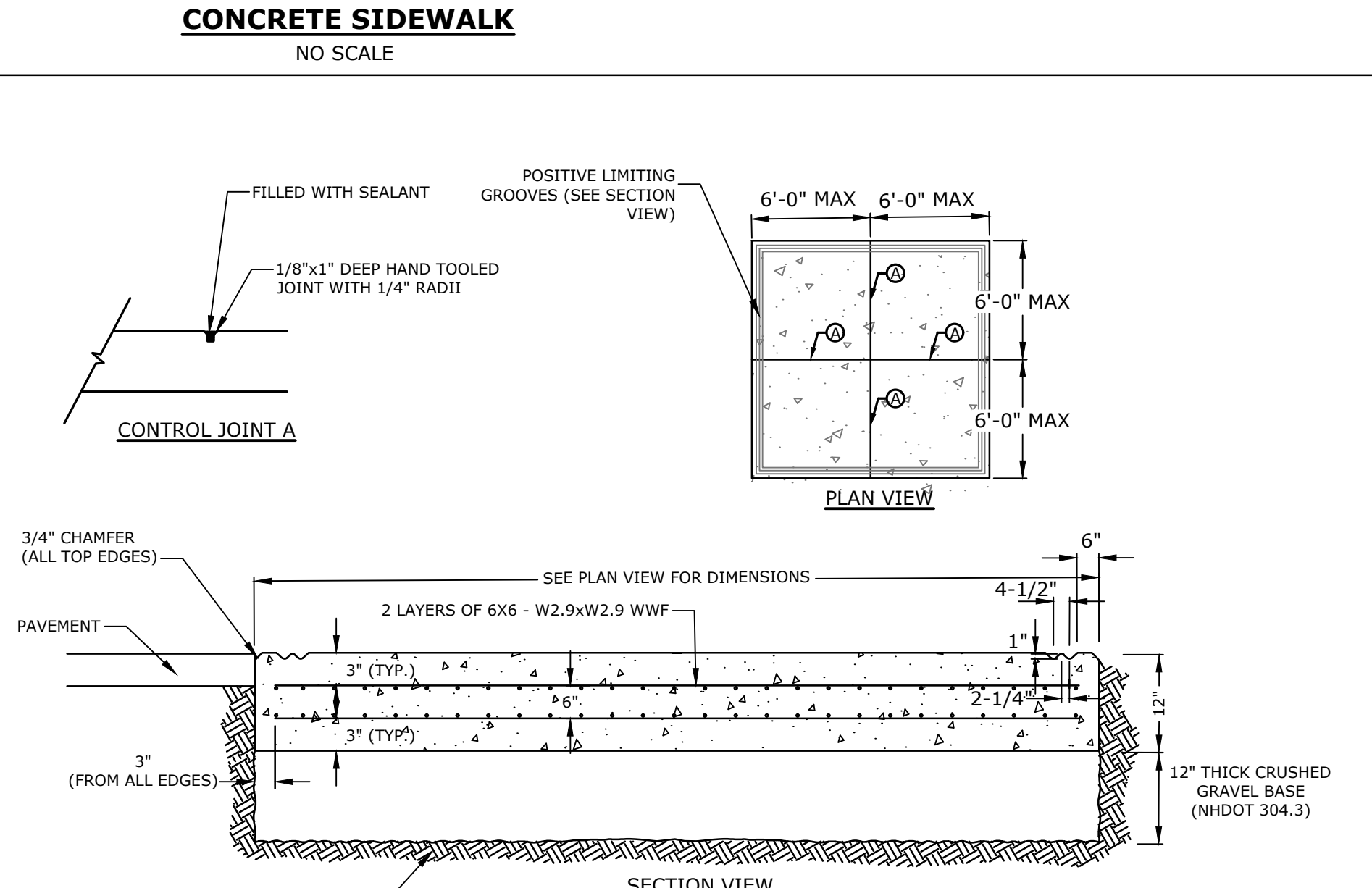
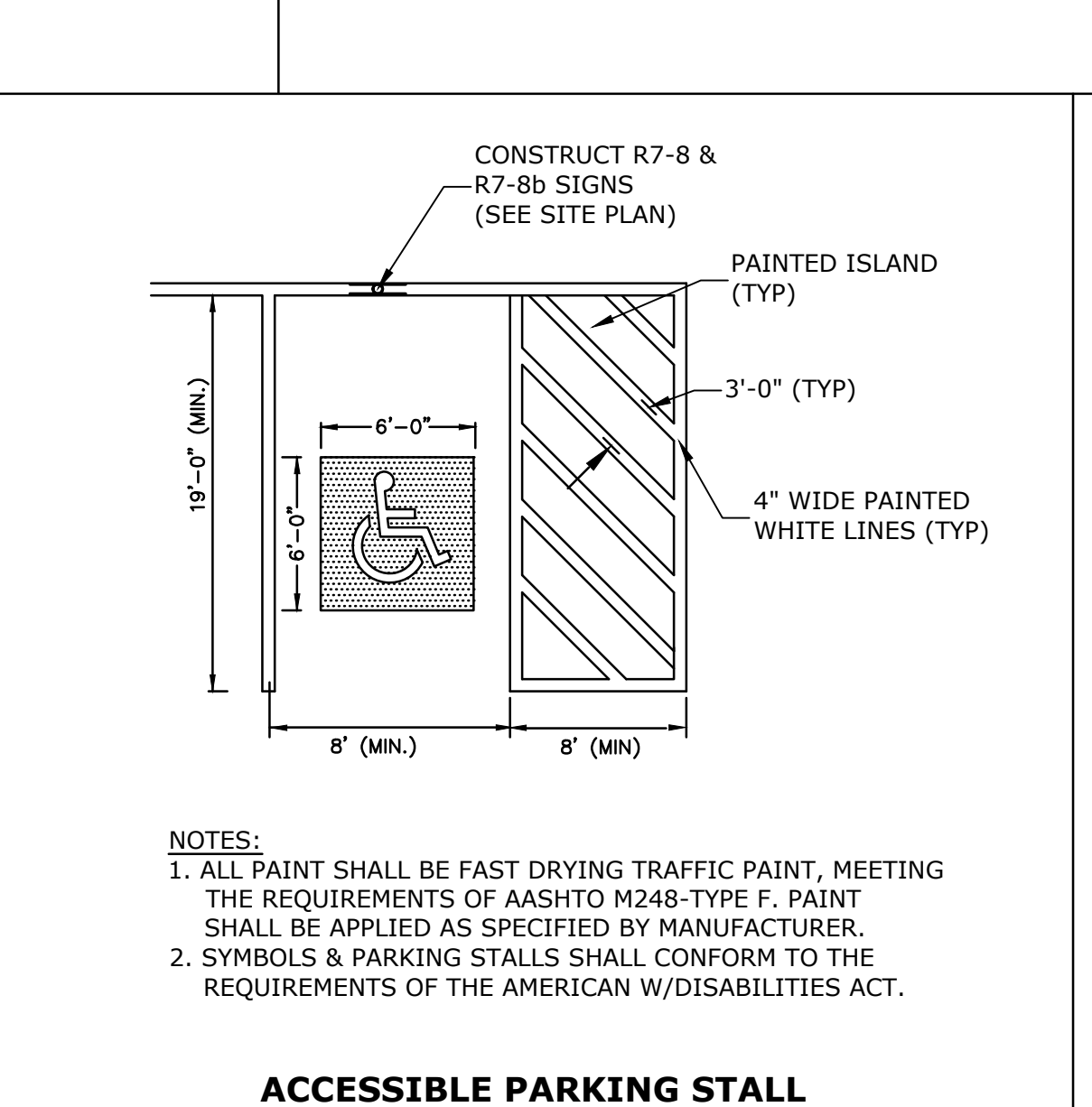
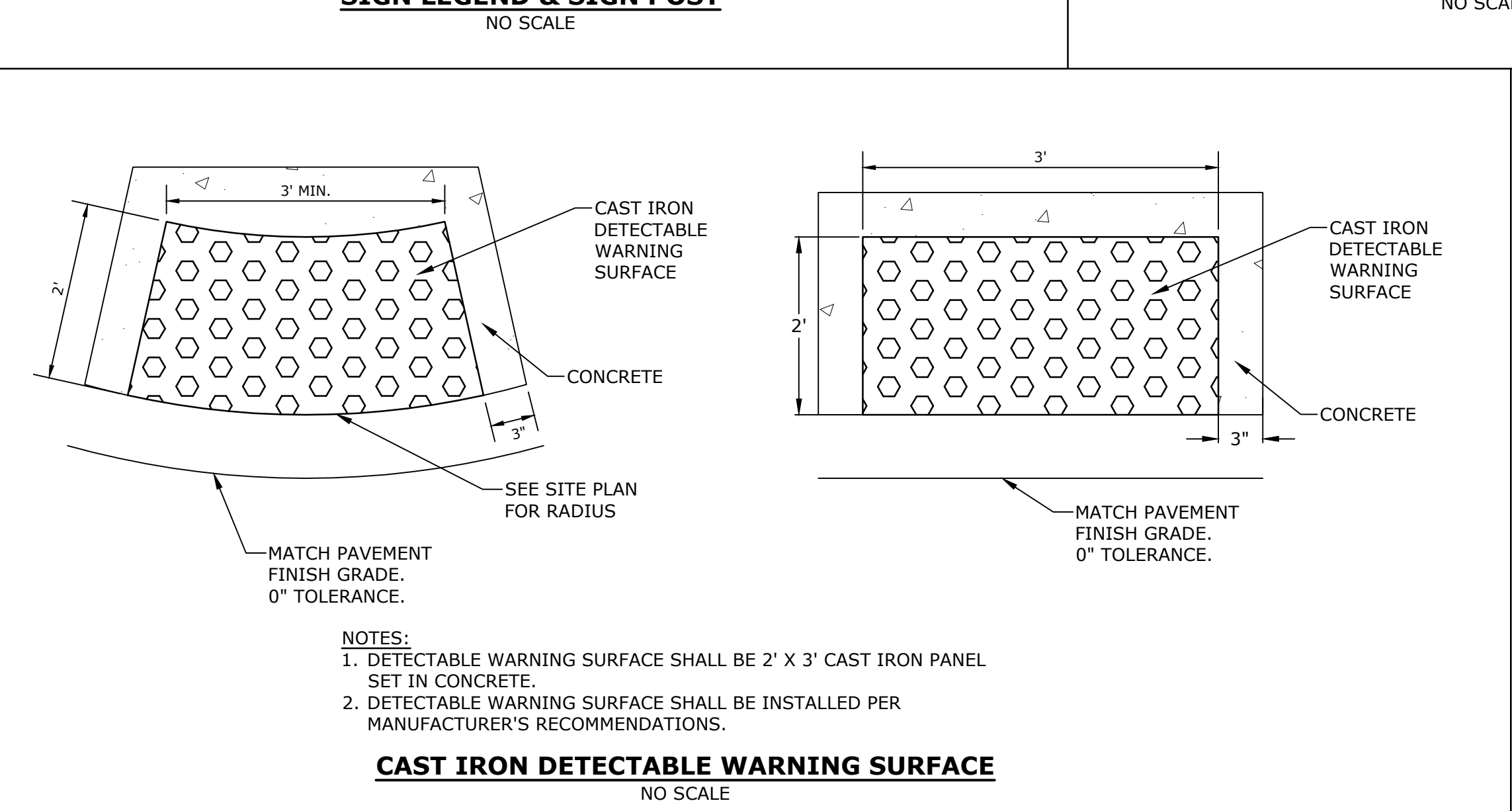
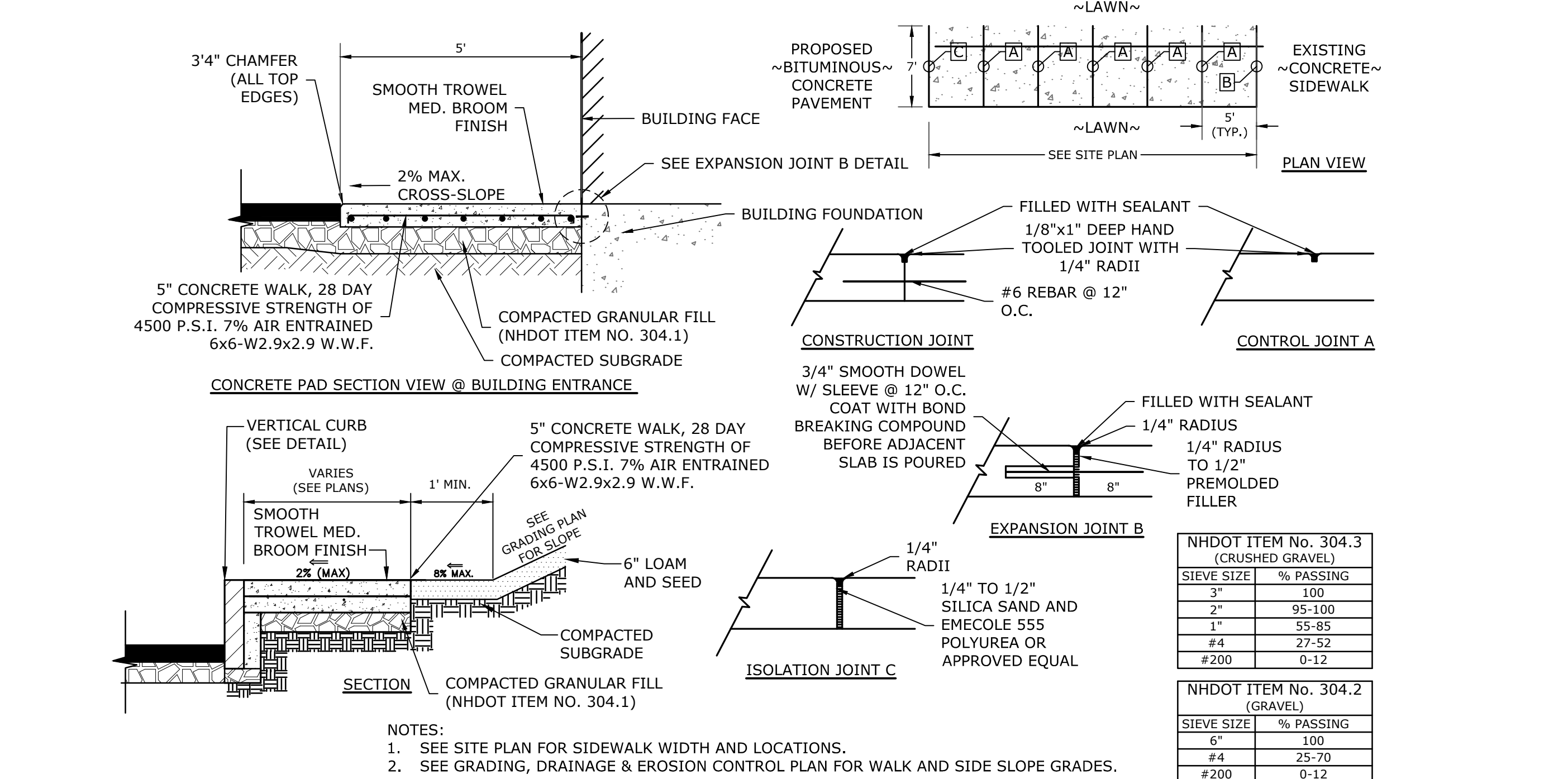
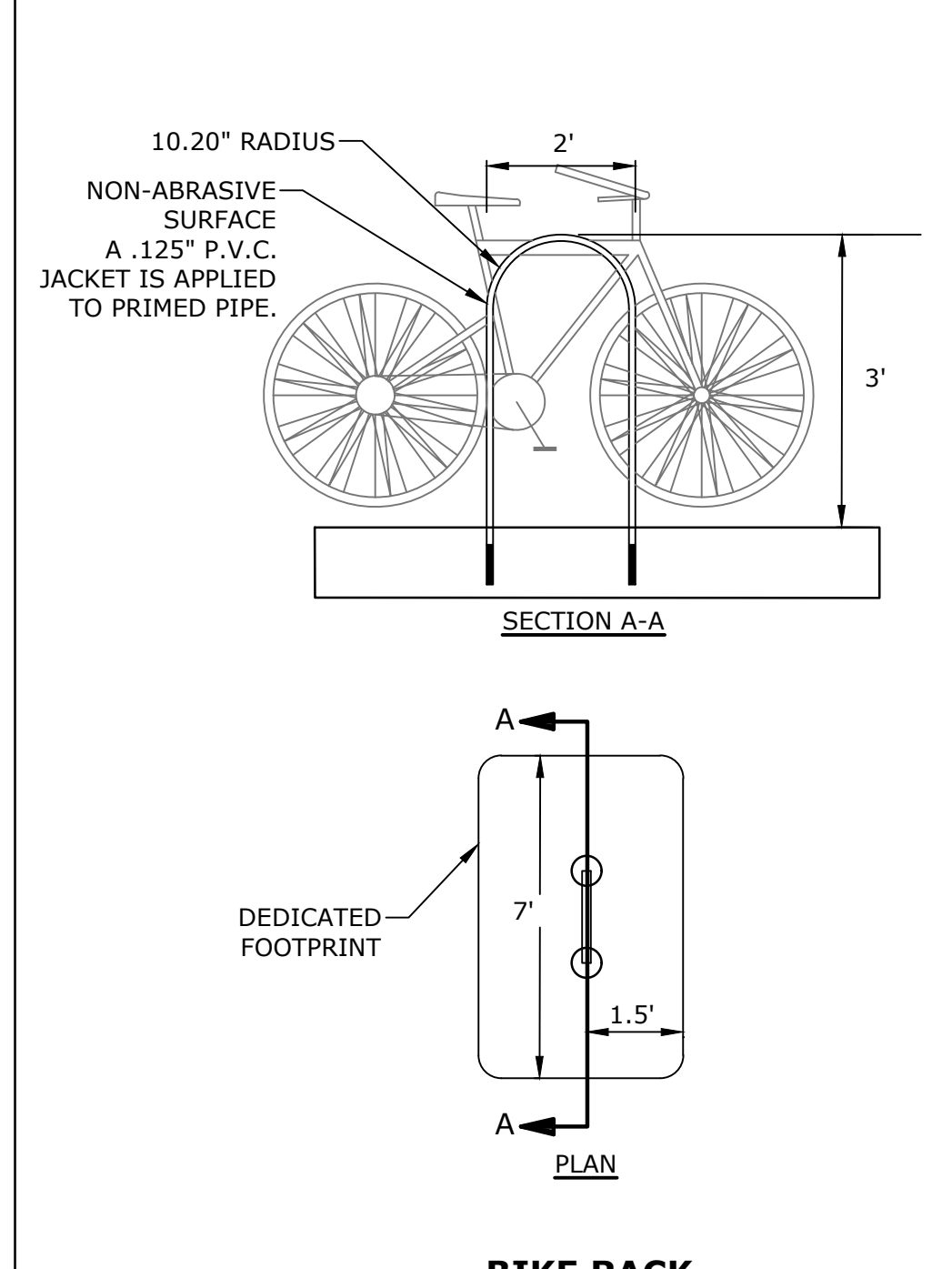
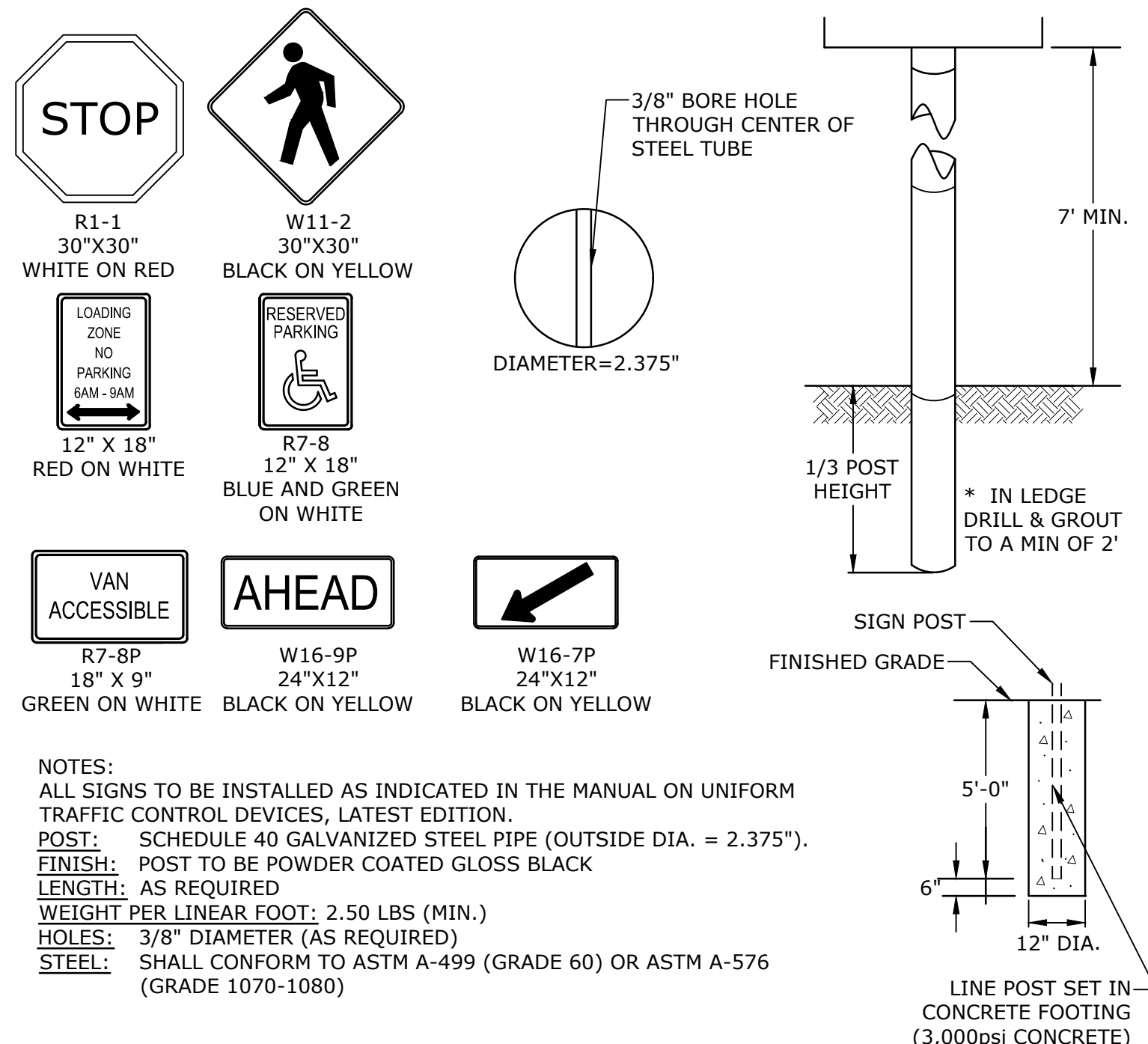
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DATE:	December 22, 2020
FILE:	P-0595-007-DTLS.DWG
DRAWN BY:	CIK
CHECKED BY:	NAH/PMC
APPROVED BY:	BLM

DETAILS SHEET

SCALE: AS SHOWN

C-502

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Tighe & Bond

STATE OF NEW HAMPSHIRE
PROFESSIONAL ENGINEER
BRADLEE MEZQUITA
No. 05939
10/10/2021

STATE OF NEW HAMPSHIRE
PROFESSIONAL ENGINEER
PATRICK CRIMMINS
No. 12378
LICENSED
03/22/2024

Proposed Mixed Use Development

North Mill Pond Holdings, LLC

Portsmouth, New Hampshire

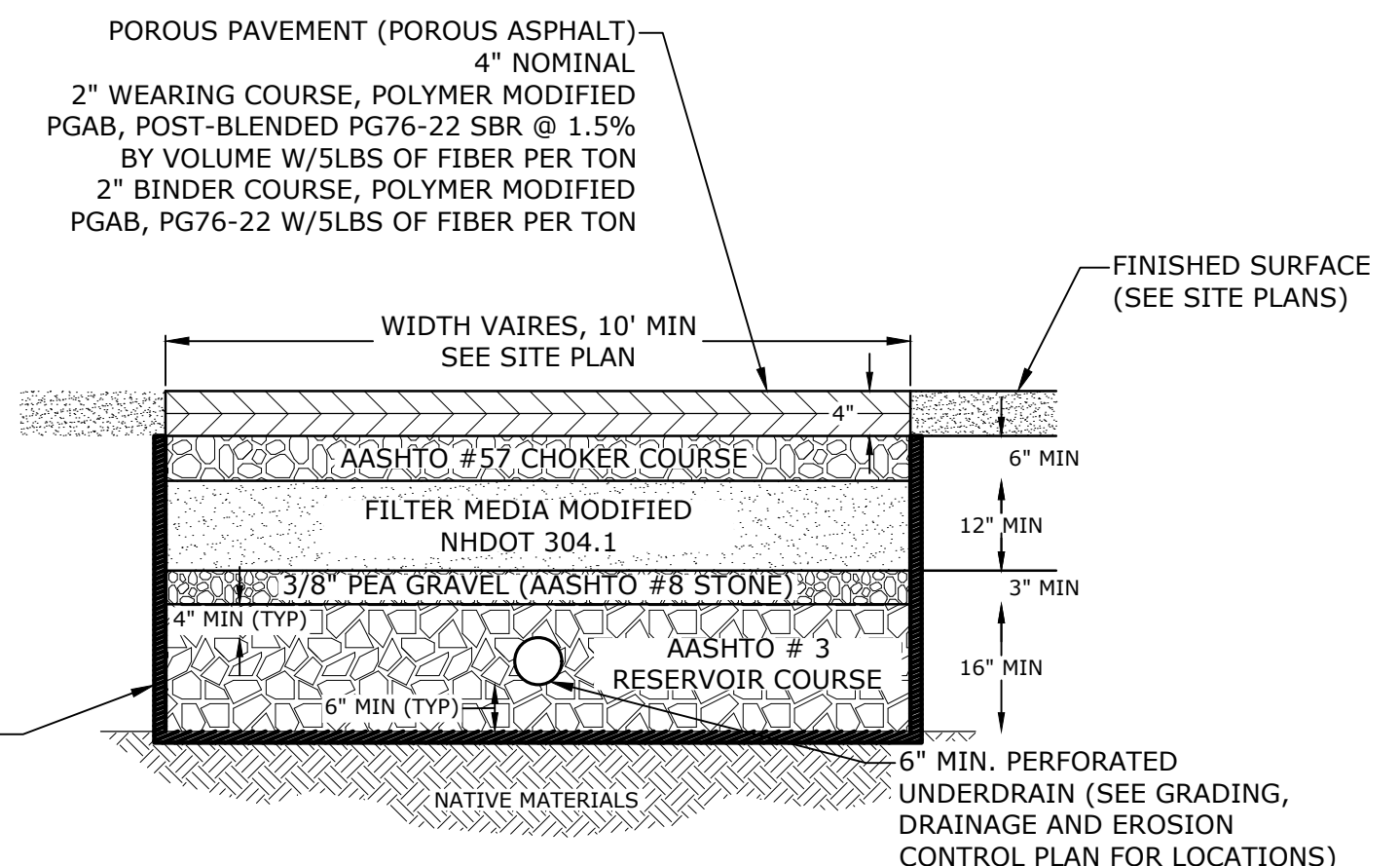
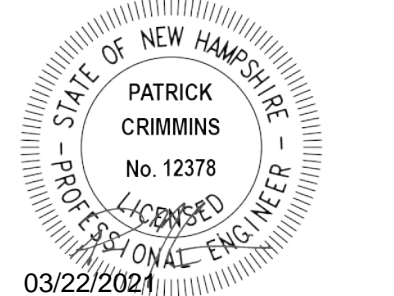
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APPROVED BY:	BLM	

DETAILS SHEET

SCALE: AS SHOWN

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 Portsmouth, NH 03801
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 www.tigheandbond.com

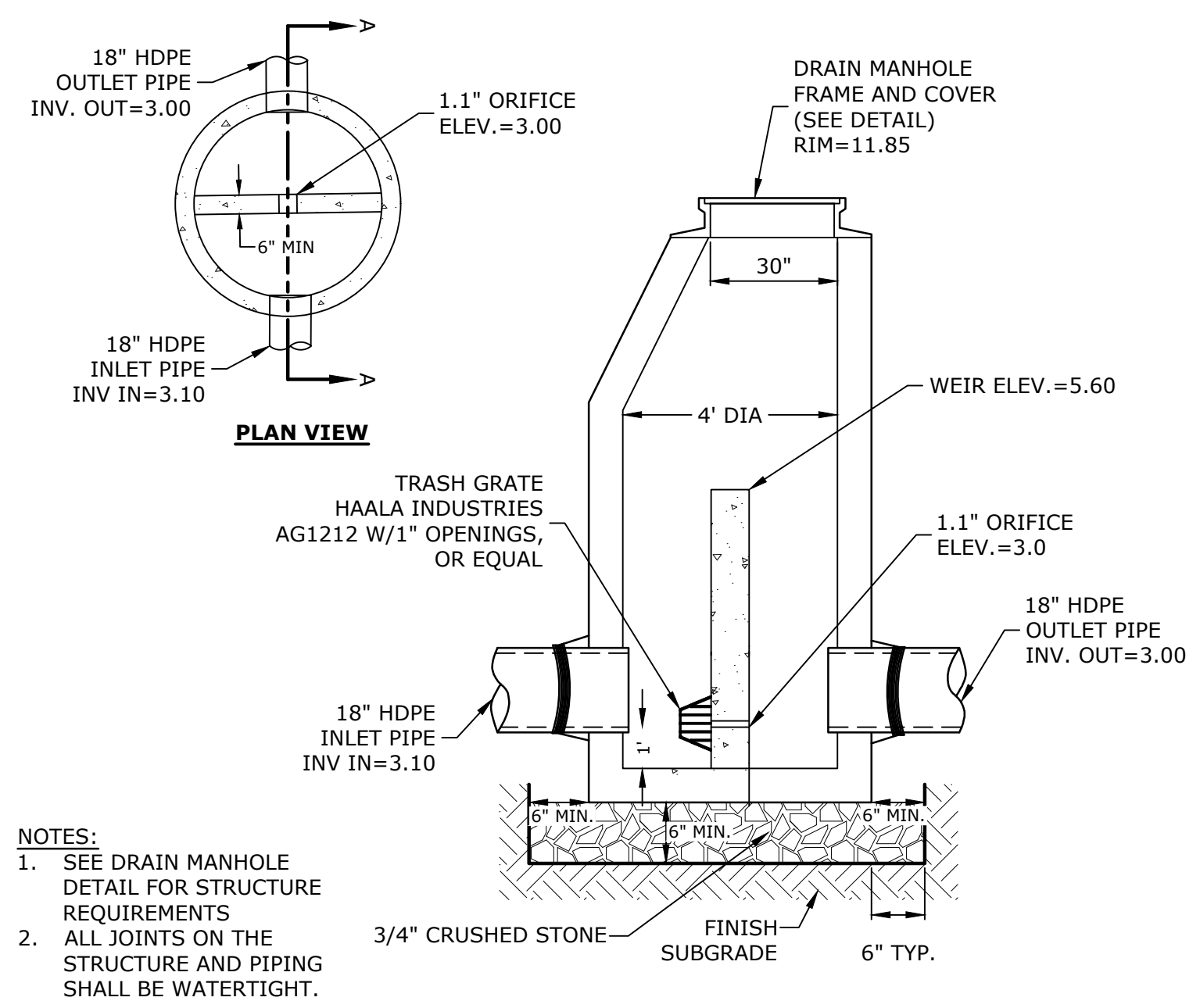


AASHTO #57 STONE (CHOKER COURSE)		MODIFIED NHDOT 304.1		AASHTO #8 STONE (PEA GRAVEL)		AASHTO #3 STONE (RESERVOIR COURSE)	
SIZE	% PASSING	SIZE	% PASSING	SIZE	% PASSING	SIZE	% PASSING
1 1/2"	100	6"	100	3/4"	100	2 1/4"	100
1"	95-100	#4	70-100	3/8"	85-100	2"	90-100
3/4"	25-60	#200	0-6*	#4	10-30	1 1/2"	35-70
#4	0-10			#8	0-10	1"	0-15
#8	0-5			#16	0-5	3/2"	0-5

*PREFERABLY <4%

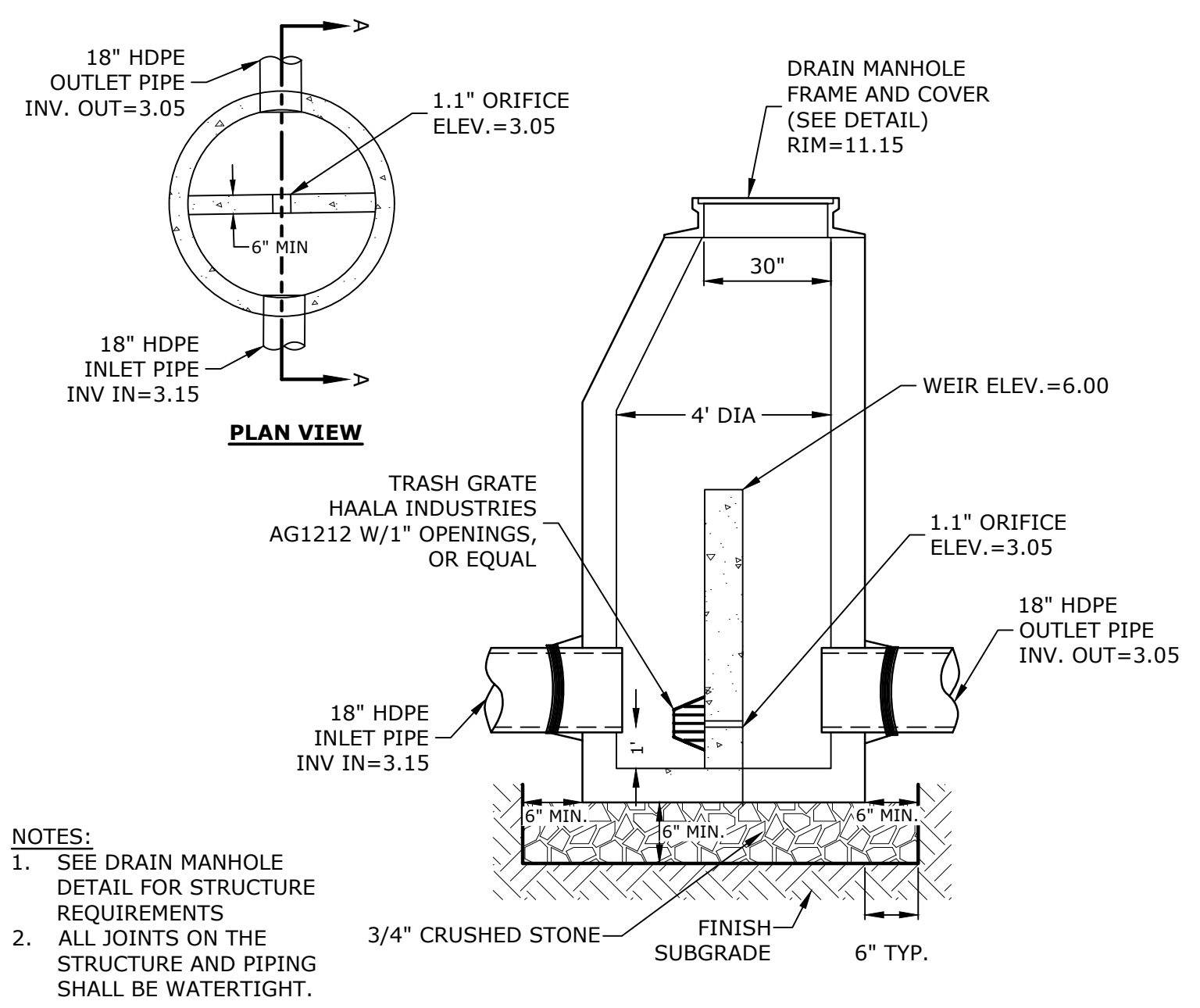
- NOTES:**
- SEE GRADING, DRAINAGE, UTILITIES AND EROSION CONTROL PLAN FOR PAVEMENT SLOPE AND CROSS-SLOPE.
 - POROUS ASPHALT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE LATEST SPECIFICATIONS FROM THE UNH STORMWATER CENTER FOR POROUS ASPHALT.
 - POROUS ASPHALT MIX SPECIFIED IS RECOMMENDED BY THE UNH STORMWATER CENTER FOR SITES ANTICIPATING H-20 LOADING.
 - FILTER COURSE TO BE INCREASED AS NECESSARY TO MEET PROPOSED GRADES.
 - INSTALL FILTER COURSE AGGREGATE IN 8-INCH MAXIMUM LIFTS TO A MAXIMUM OF 95% STANDARD PROCTOR COMPACTION (ASTM D698 / AASHTO T99). INSTALL AGGREGATE TO GRADES INDICATED ON THE DRAWINGS.
 - INSTALL CHOKER, GRAVEL, AND STONE BASE COURSE AGGREGATE TO A MAXIMUM OF 95% COMPACTION STANDARD PROCTOR (ASTM D698 / AASHTO T99). CHOKER SHOULD BE PLACED EVENLY OVER SURFACE OF FILTER COURSE BED, SUFFICIENT TO ALLOW PLACEMENT OF PAVEMENT, AND NOTIFY ENGINEER FOR APPROVAL. CHOKER BASE COURSE THICKNESS SHALL BE SUFFICIENT TO ALLOW FOR EVEN PLACEMENT OF THE POROUS ASPHALT BUT NO LESS THAN 6-INCHES IN DEPTH.
 - THE DENSITY OF SUBBASE COURSES SHALL BE DETERMINED BY AASHTO T 191 (SAND-CONE METHOD), AASHTO T 204 (DRIVE CYLINDER METHOD), OR AASHTO T 238 (NUCLEAR METHODS), OR OTHER APPROVED METHODS AT THE DISCRETION OF THE SUPERVISING ENGINEER.

POROUS ASPHALT SECTION
NO SCALE



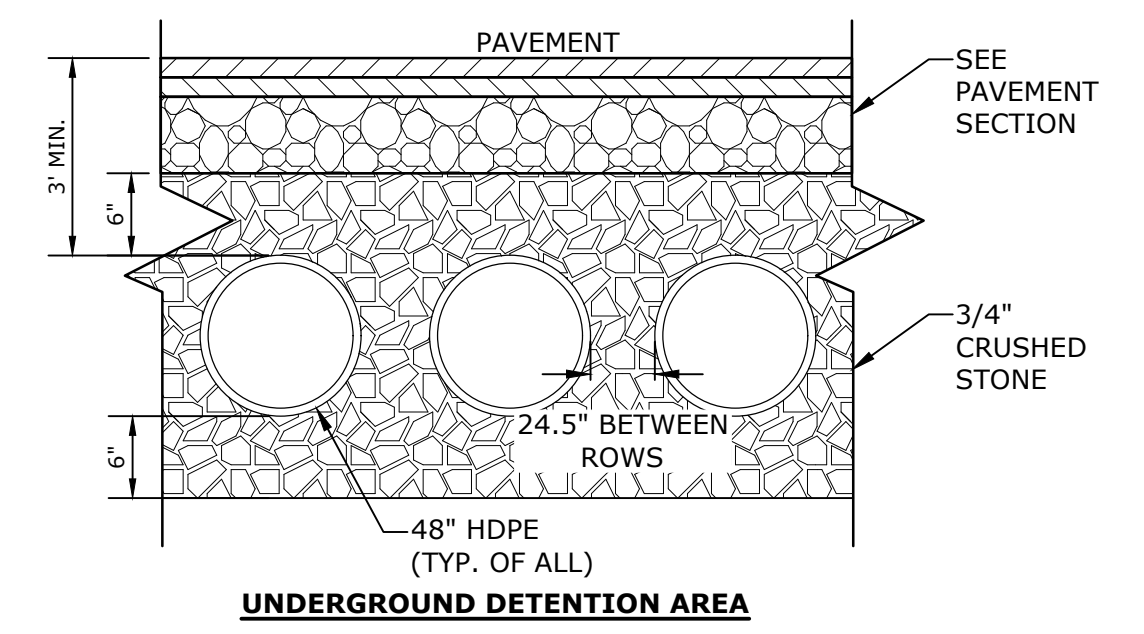
- NOTES:**
- SEE DRAIN MANHOLE DETAIL FOR STRUCTURE REQUIREMENTS
 - ALL JOINTS ON THE STRUCTURE AND PIPING SHALL BE WATERTIGHT.

PDMH3
NO SCALE

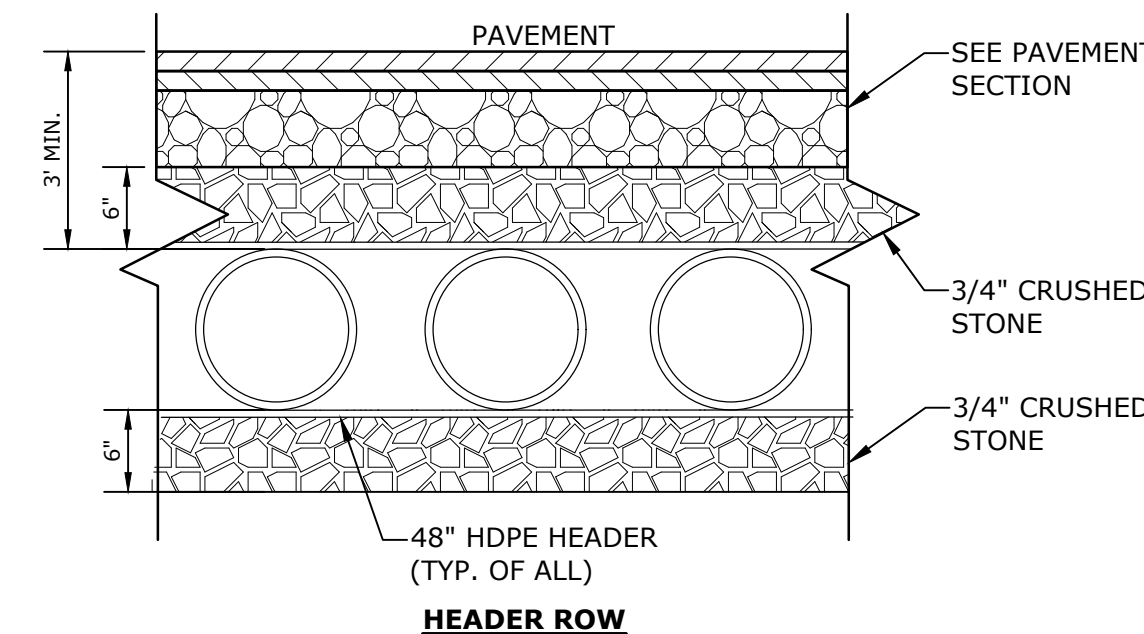


- NOTES:**
- SEE DRAIN MANHOLE DETAIL FOR STRUCTURE REQUIREMENTS
 - ALL JOINTS ON THE STRUCTURE AND PIPING SHALL BE WATERTIGHT.

PDMH6
NO SCALE



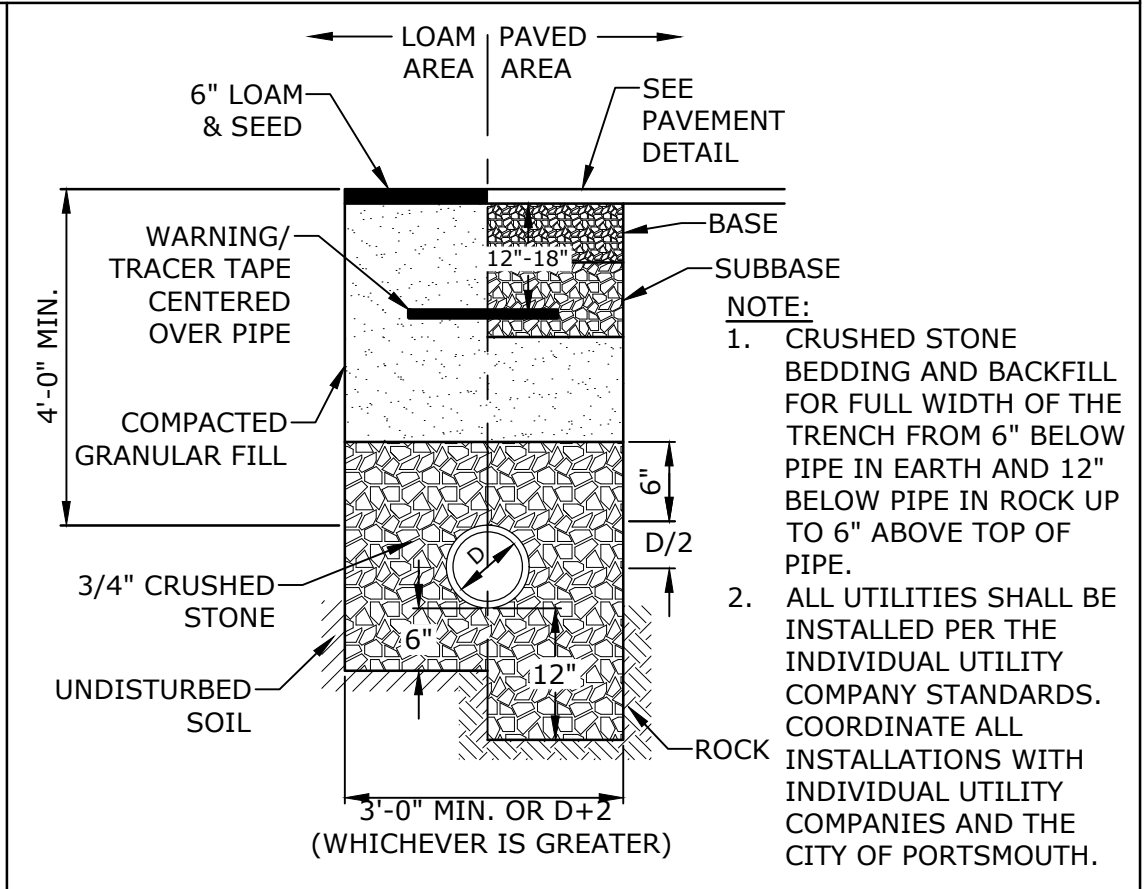
UNDERGROUND DETENTION AREA



HEADER ROW

- NOTES:**
- UNDERGROUND DETENTION SYSTEM TO BE 48" HDPE PIPE DESIGNED FOR H-20 LOADING. CONTRACTOR TO SUBMIT PIPE SPECIFICATIONS AND FINAL MANUFACTURES DESIGN TO ENGINEER FOR APPROVAL.
 - MANUFACTURER TO SUBMIT PLANS STAMPED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF NEW HAMPSHIRE.
 - THE DESIGN ENGINEER SHALL PROVIDE SUFFICIENT INSPECTION TO CERTIFY THAT THE SYSTEM HAS BEEN INSTALLED PER THE APPROVED DESIGN PLAN.
 - REFER TO STANDARD DUTY PAVEMENT SECTION DETAIL FOR PAVEMENT SECTION.

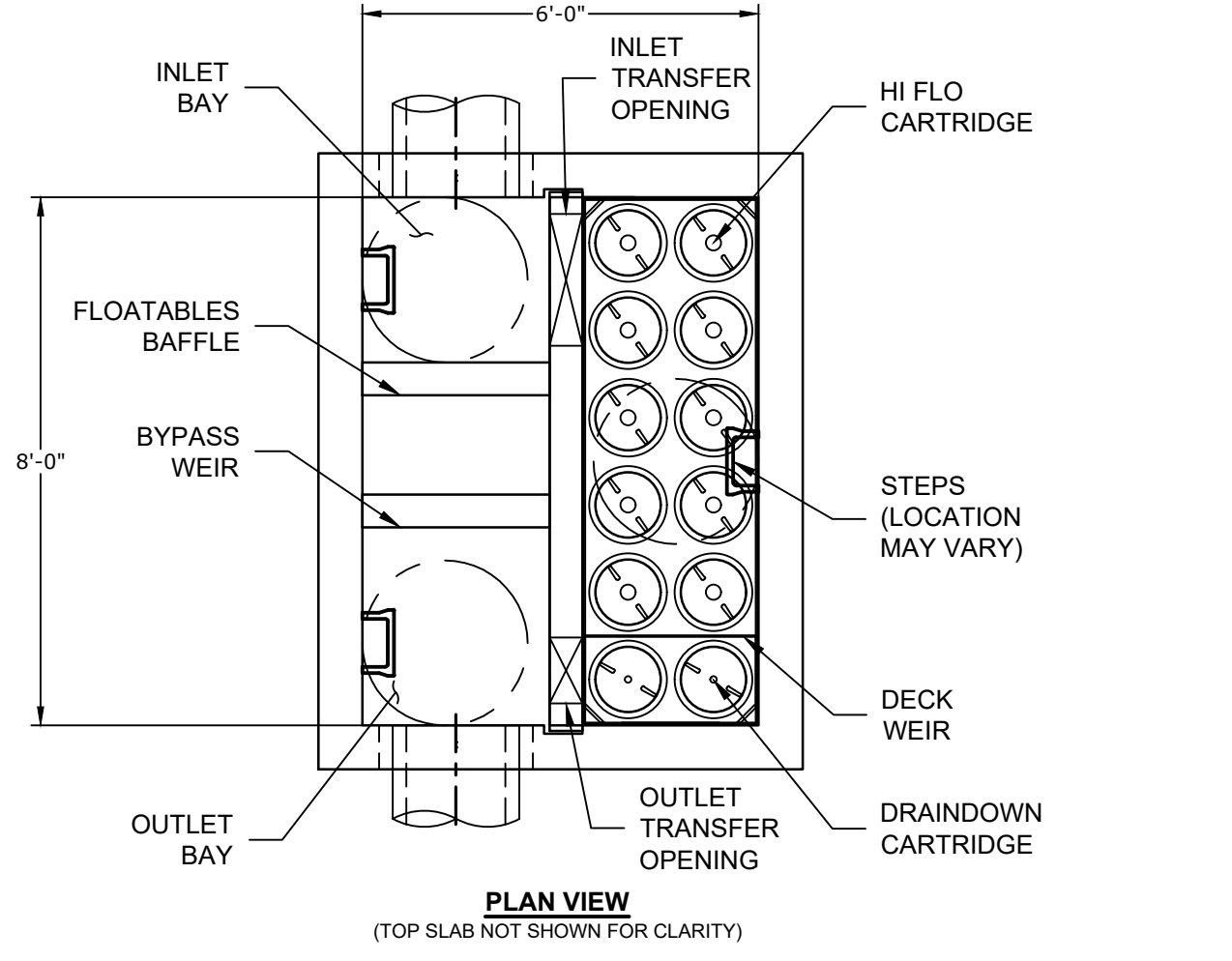
UNDERGROUND DETENTION SYSYTEM DETAIL
NO SCALE



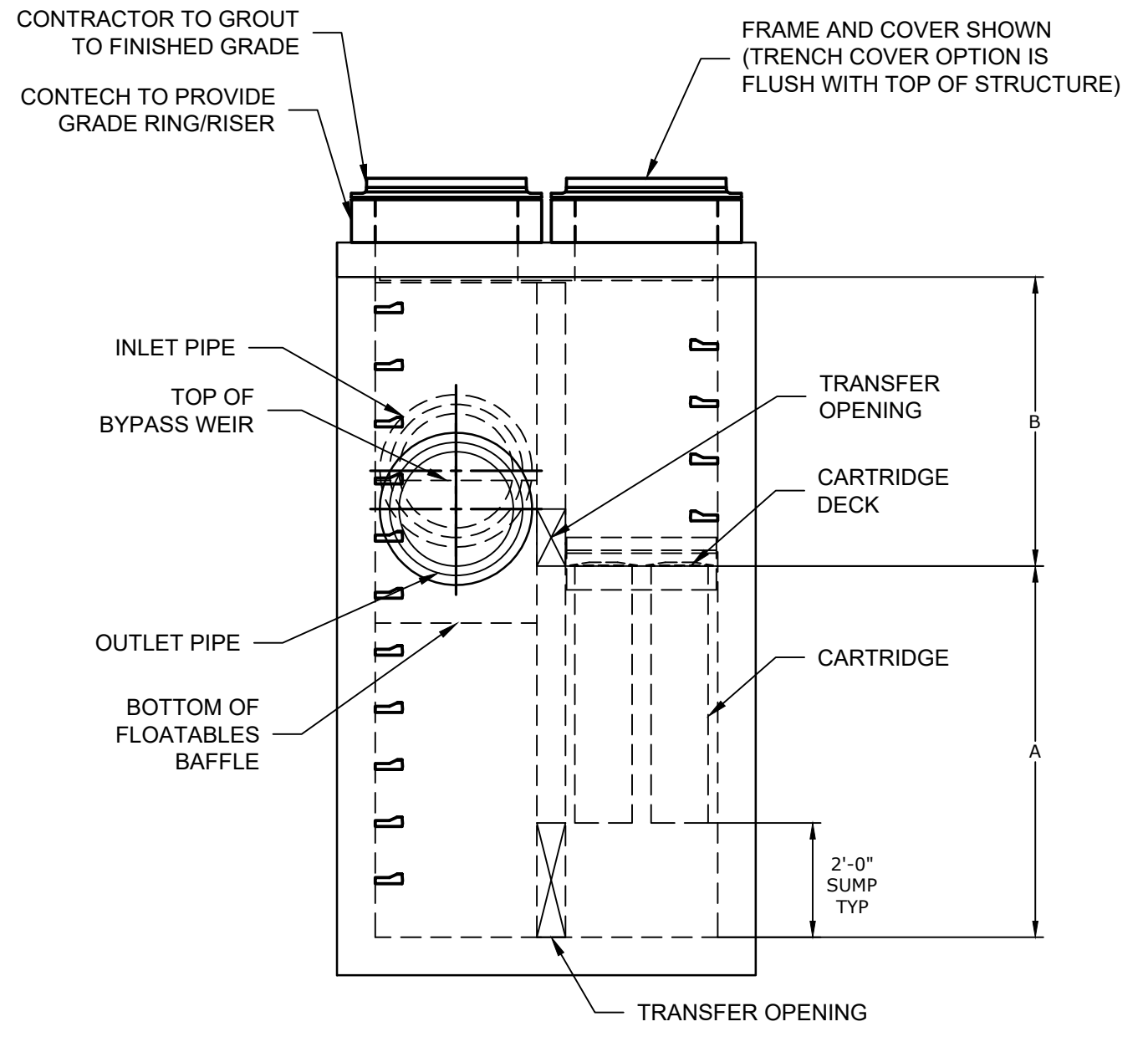
STORM DRAIN TRENCH
NO SCALE

- NOTE:**
- CRUSHED STONE BEDDING AND BACKFILL FOR FULL WIDTH OF THE TRENCH FROM 6" BELOW PIPE IN EARTH AND 12" BELOW PIPE IN ROCK UP TO 6" ABOVE TOP OF PIPE.
 - ALL UTILITIES SHALL BE INSTALLED PER THE INDIVIDUAL UTILITY COMPANY STANDARDS. COORDINATE ALL INSTALLATIONS WITH INDIVIDUAL UTILITY COMPANIES AND THE CITY OF PORTSMOUTH.

	TOP OF STONE	TOP OF PIPE	BOTTOM OF PIPE	BOTTOM OF STONE
UDB 1	8.25'	7.25'	3.50'	2.75'
UDB 2	8.25'	7.25'	3.50'	2.75'



PLAN VIEW
(TOP SLAB NOT SHOWN FOR CLARITY)

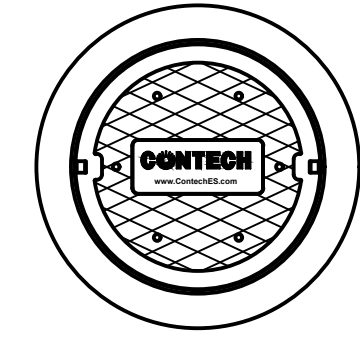


ELEVATION VIEW
JELLYFISH JFPD0806

CONTECH JELLYFISH STORMWATER FILTER
NO SCALE

FIELD ELEVATIONS					
	RIM ELEVATION	INLET ELEVATION	INLET PIPE	OUTLET ELEVATION	OUTLET PIPE
JFF 1	11.85	2.85'	18" HDPE	2.90'	18" HDPE
JFF 2	11.25	2.90'	18" HDPE	2.40'	18" HDPE

JELLYFISH JFPD0806 - DESIGN NOTES				
JELLYFISH TREATMENT CAPACITY IS A FUNCTION OF THE CARTRIDGE LENGTH AND THE NUMBER OF CARTRIDGES. THE STANDARD PEAK DIVERSION STYLE WITH PRECAST TOP SLAB IS SHOWN. ALTERNATE OPENING VOLT AND/OR SHALLOW ORIENTATIONS ARE AVAILABLE. PEAK CONVEYANCE CAPACITY TO BE DETERMINED BY ENGINEER OF RECORD.				
CARTRIDGE SELECTION	54"	40"	27"	15"
CARTRIDGE LENGTH	6'-6"	5'-4"	4'-3"	3'-3"
OUTLET INVERT TO STRUCTURE INVERT (A)	0.178 / 0.089	0.133 / 0.067	0.089 / 0.045	0.049 / 0.025
FLOW RATE HI-FLO / DRAINDOWN (GFS) (PER CART)	1.96	1.47	0.98	0.51
MAX. TREATMENT (GFS)	5.00	4.00	4.00	4.00
DECK TO INSIDE TOP (MIN) (B)				



SITE SPECIFIC DATA REQUIREMENTS			
STRUCTURE ID	JF-1	JF-2	
MODEL SIZE	JFPD0806	JFPD0806	
WATER QUALITY FLOW RATE (cfs)	2.85	0.63	
PEAK FLOW RATE (cfs)	26.54	5.13	
RETURN PERIOD OF PEAK FLOW (yrs)	25	25	
# OF CARTRIDGES REQUIRED (HF / DD)	153	51	
CARTRIDGE SIZE	54"	40"	

- GENERAL NOTES:**
- CONTECH TO PROVIDE ALL MATERIALS UNLESS NOTED OTHERWISE.
 - FOR SITE SPECIFIC DRAWINGS WITH DETAILED STRUCTURE DIMENSIONS AND WEIGHT, PLEASE CONTACT YOUR CONTECH ENGINEERED SOLUTIONS REPRESENTATIVE. www.conteches.com
 - JELLYFISH WATER QUALITY STRUCTURE SHALL BE IN ACCORDANCE WITH ALL DESIGN DATA AND INFORMATION CONTAINED IN THIS DRAWING. CONTRACTOR TO CONFIRM STRUCTURE MEETS REQUIREMENTS OF PROJECT.
 - STRUCTURE SHALL MEET AASHTO HS-20 OR PER APPROVING JURISDICTION REQUIREMENTS, WHICHEVER IS MORE STRINGENT, ASSUMING EARTH COVER OF 0' - 3' AND GROUNDWATER ELEVATION AT OR BELOW THE OUTLET PIPE INVERT ELEVATION. ENGINEER OF RECORD TO CONFIRM ACTUAL GROUNDWATER ELEVATION. CASTINGS SHALL MEET AASHTO HS-20 LOAD RATINGS AND BE CAST WITH THE CONTECH LOGO.
 - STRUCTURE SHALL BE PRECAST CONCRETE CONFORMING TO ASTM C-478 AND AASHTO LOAD FACTOR DESIGN METHOD.
 - OUTLET PIPE INVERT IS EQUAL TO THE CARTRIDGE DECK ELEVATION.
 - THE OUTLET PIPE DIAMETER FOR NEW INSTALLATIONS IS TO BE ONE PIPE SIZE LARGER THAN THE INLET PIPE AT EQUAL OR GREATER SLOPE.
 - NO PRODUCT SUBSTITUTIONS SHALL BE ACCEPTED UNLESS SUBMITTED 10 DAYS PRIOR TO PROJECT BID DATE, OR AS DIRECTED BY THE ENGINEER OF RECORD.

- INSTALLATION NOTES:**
- ANY SUB-BASE, BACKFILL DEPTH, AND/OR ANTI-FLOTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
 - CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE STRUCTURE (LIFTING CLUTCHES PROVIDED).
 - CONTRACTOR WILL INSTALL AND LEVEL THE STRUCTURE, SEALING THE JOINTS, LINE ENTRY AND EXIT POINTS (NON-SHRINK GROUT WITH APPROVED WATERTOP OR FLEXIBLE BOOT).
 - CONTRACTOR TO TAKE APPROPRIATE MEASURES TO PROTECT CARTRIDGES FROM CONSTRUCTION-RELATED EROSION RUNOFF.
 - CARTRIDGE INSTALLATION, BY CONTECH, SHALL OCCUR ONLY AFTER SITE HAS BEEN STABILIZED AND THE JELLYFISH UNIT IS CLEAN AND FREE OF DEBRIS. CONTACT CONTECH TO COORDINATE CARTRIDGE INSTALLATION WITH SITE STABILIZATION AT (866) 740-3318.

Jellyfish Filter
www.conteches.com
9025 Centre Pointe Dr., Suite 400, West Chester, OH 45389
800-338-1122 513-645-7000 513-645-7993 FAX

Proposed Mixed Use Development

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Portsmouth, New Hampshire

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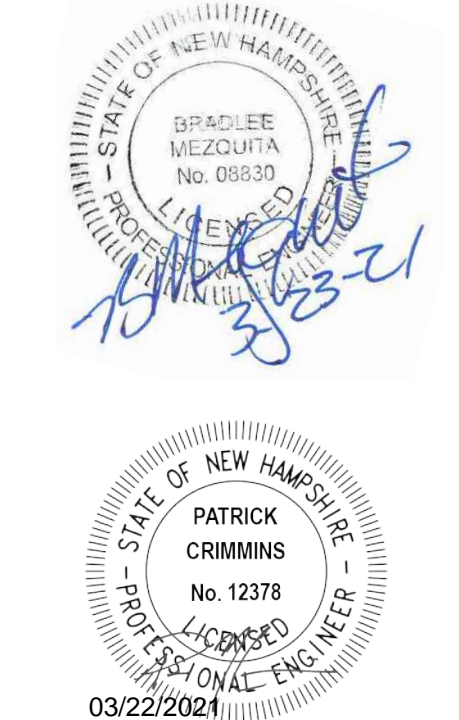
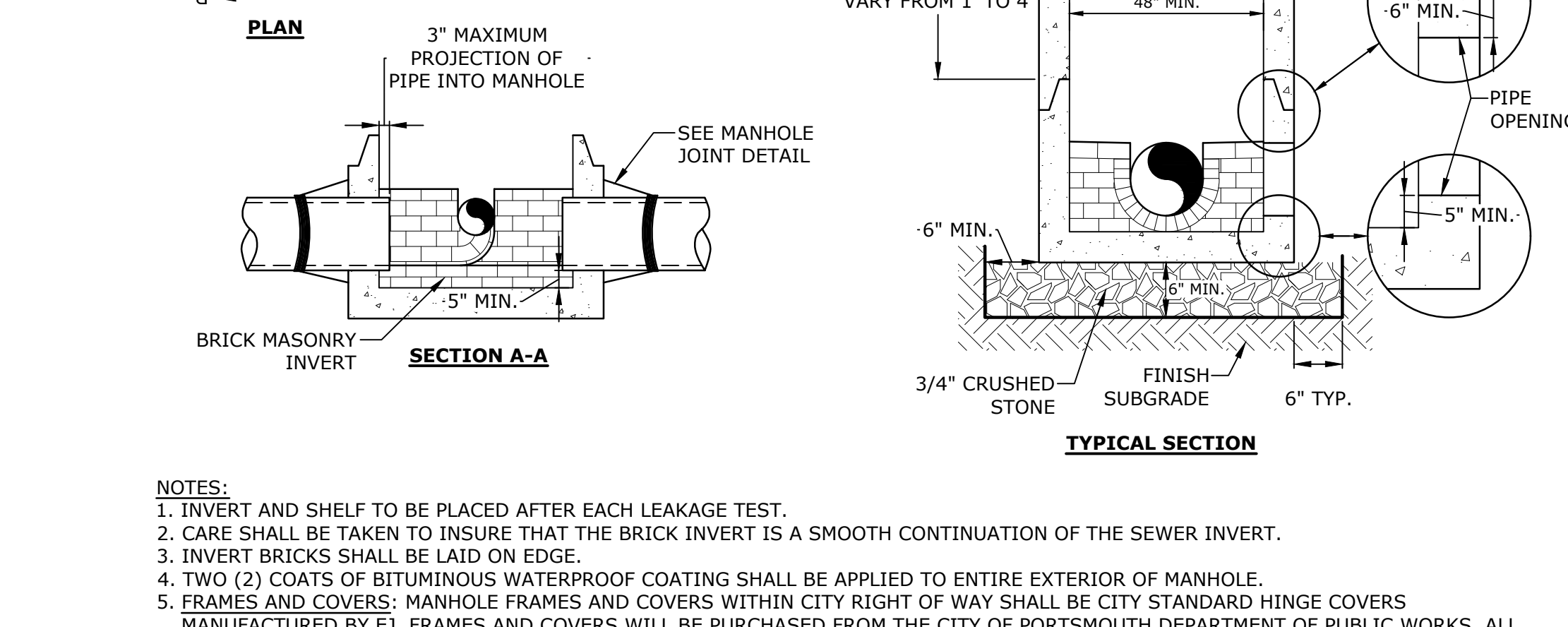
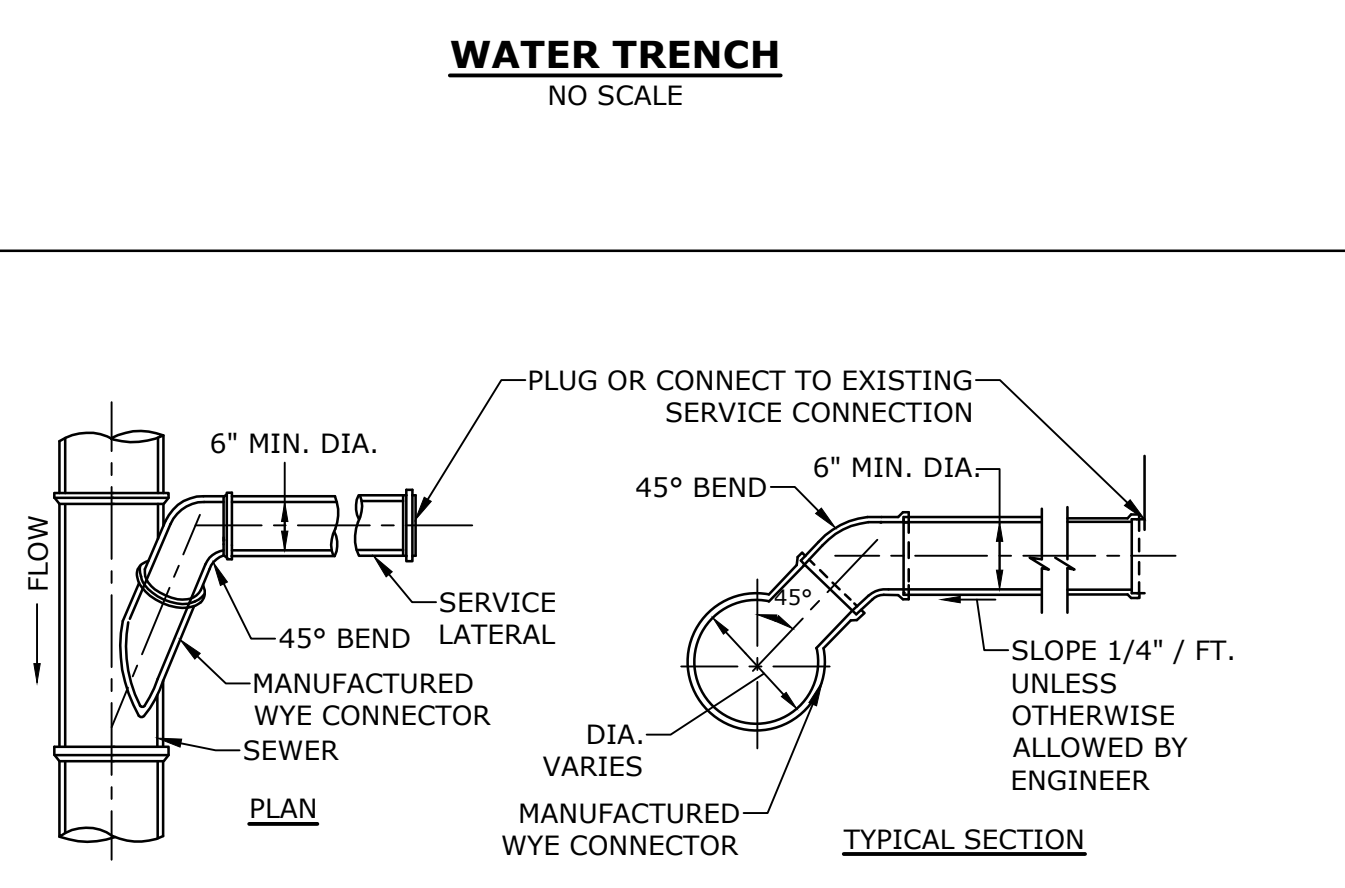
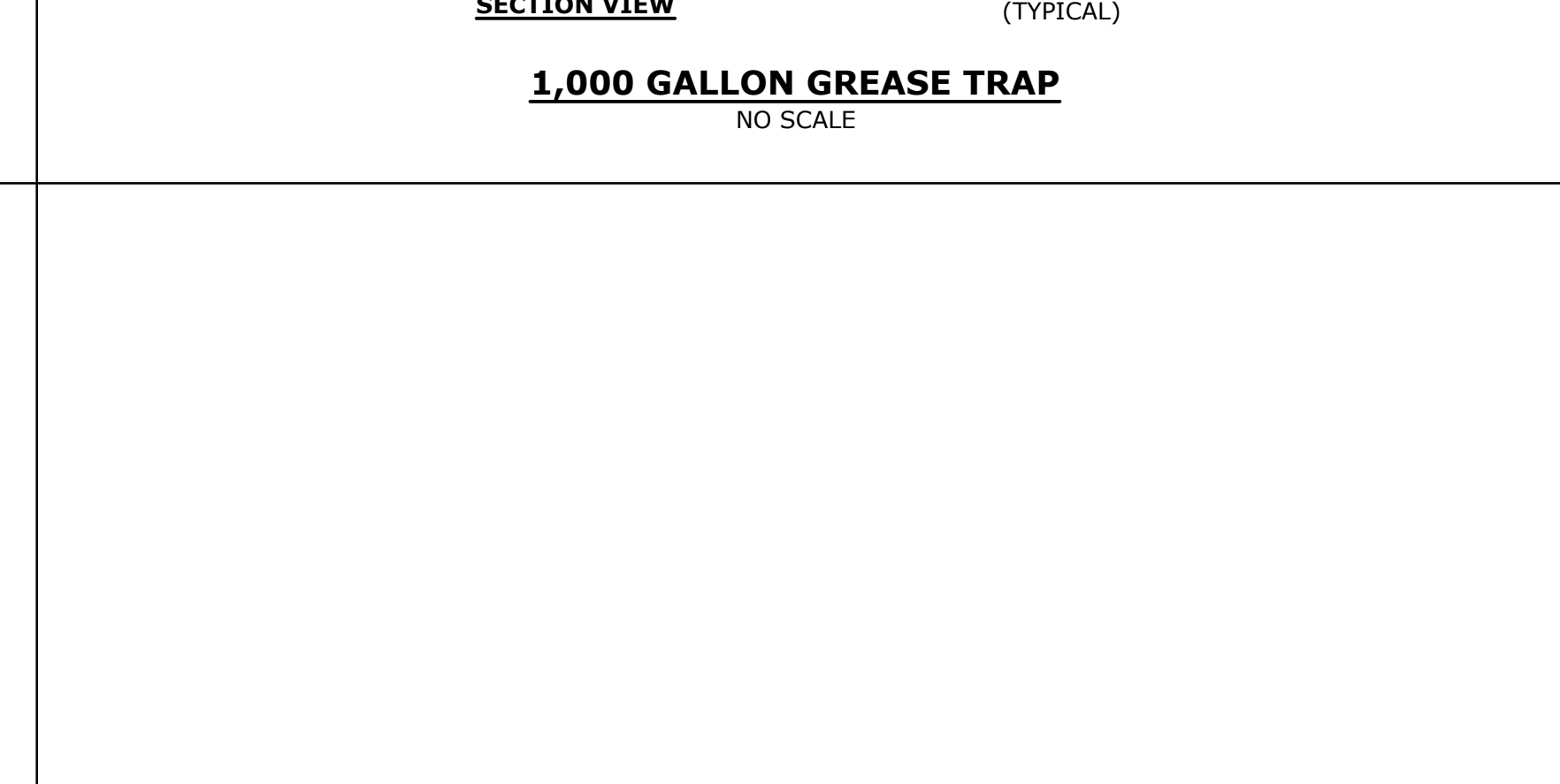
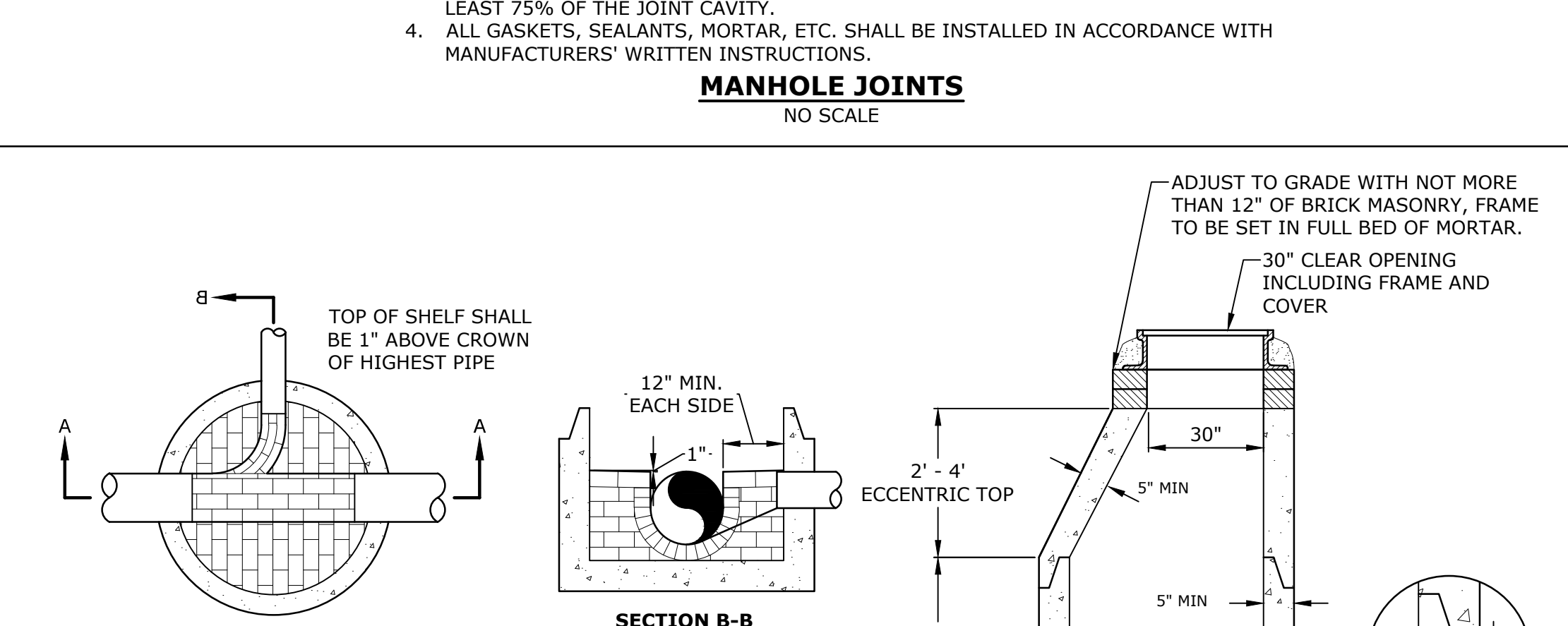
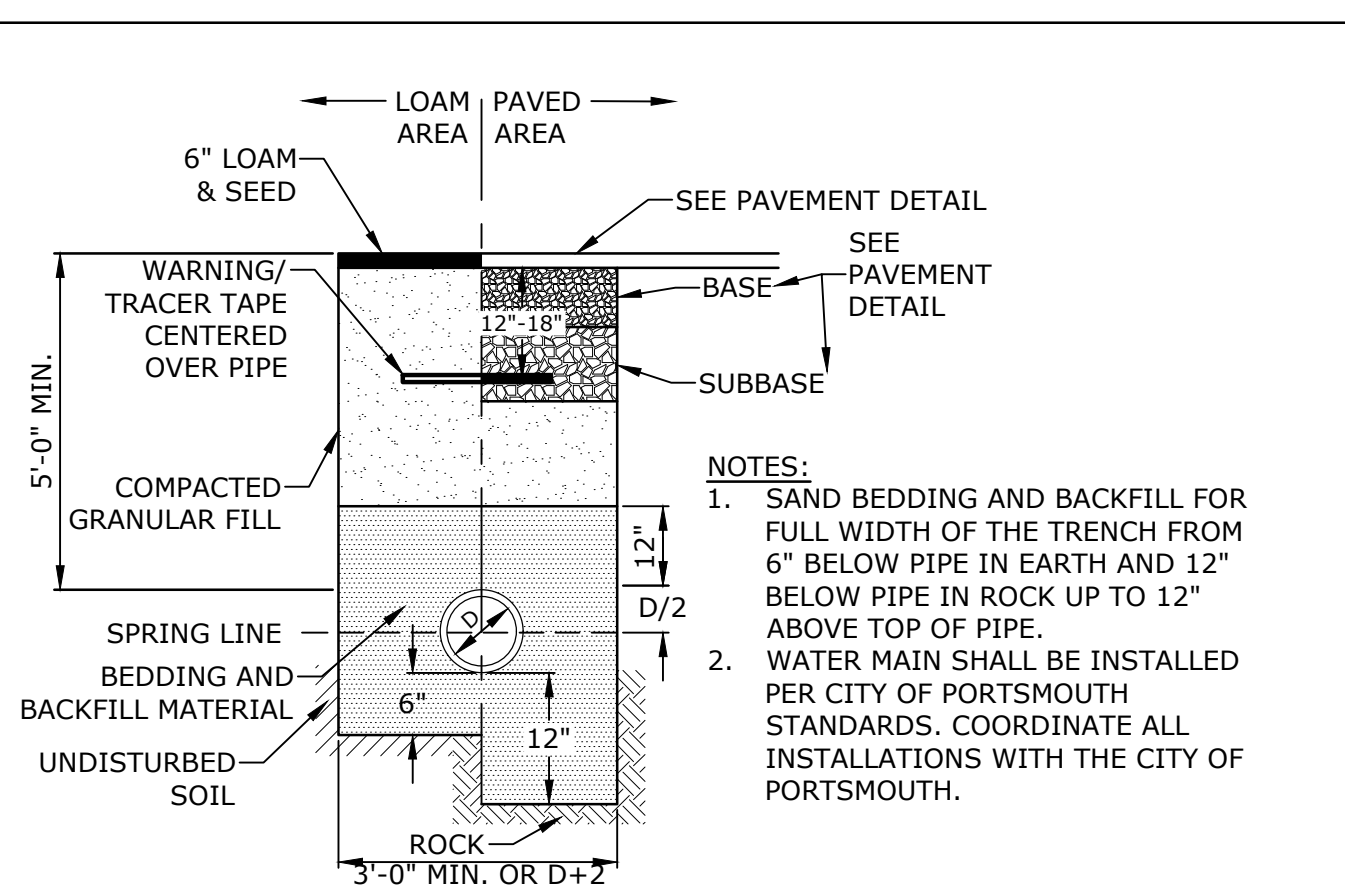
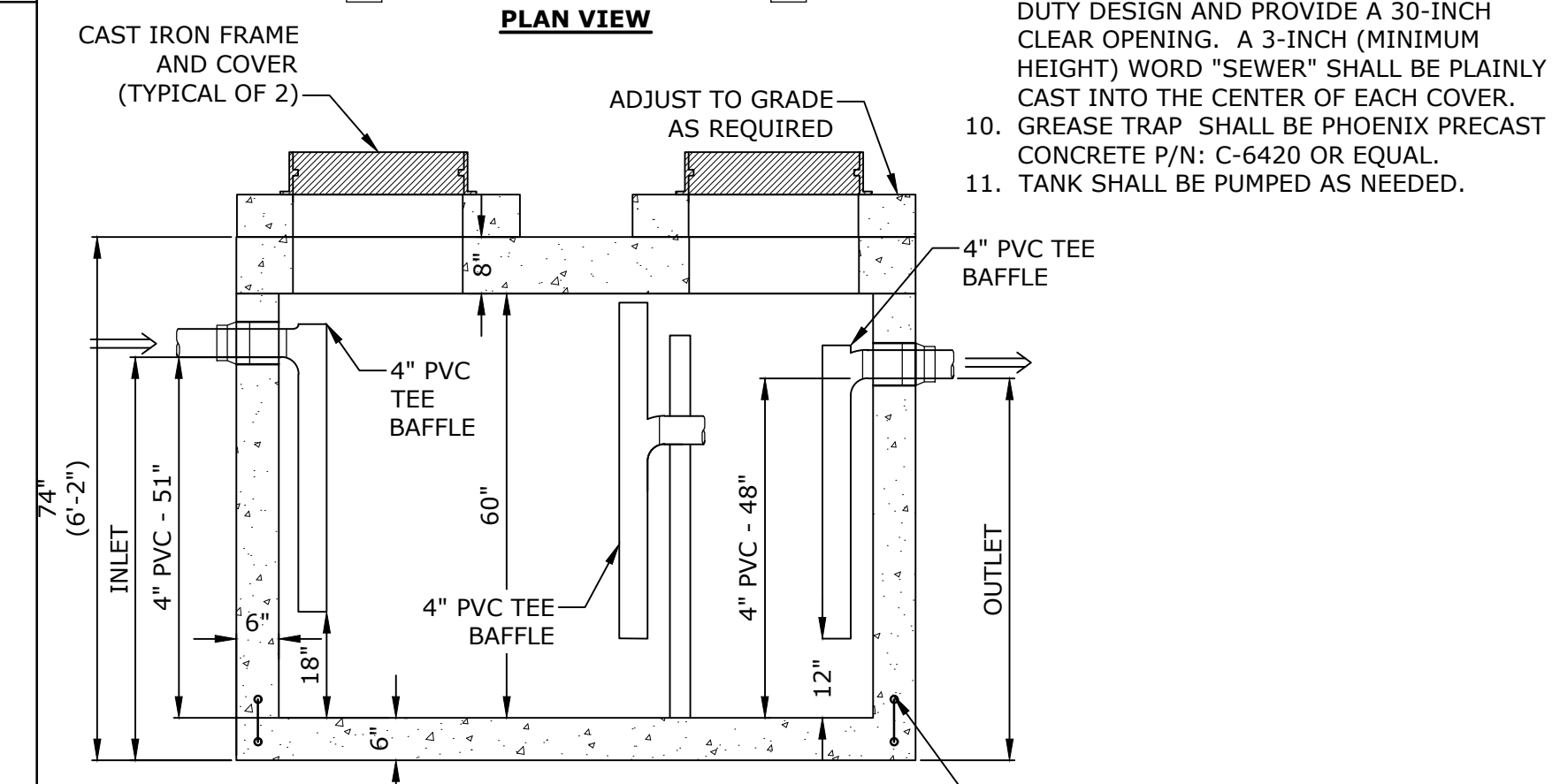
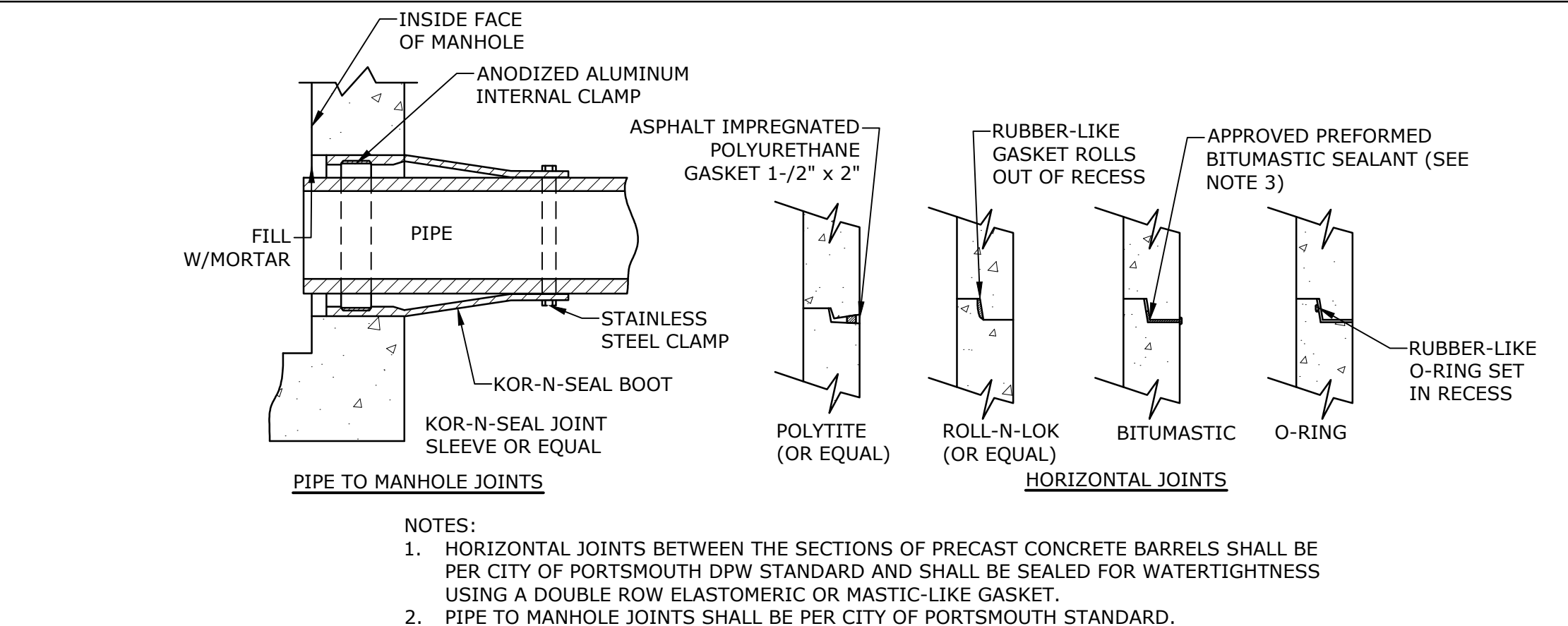
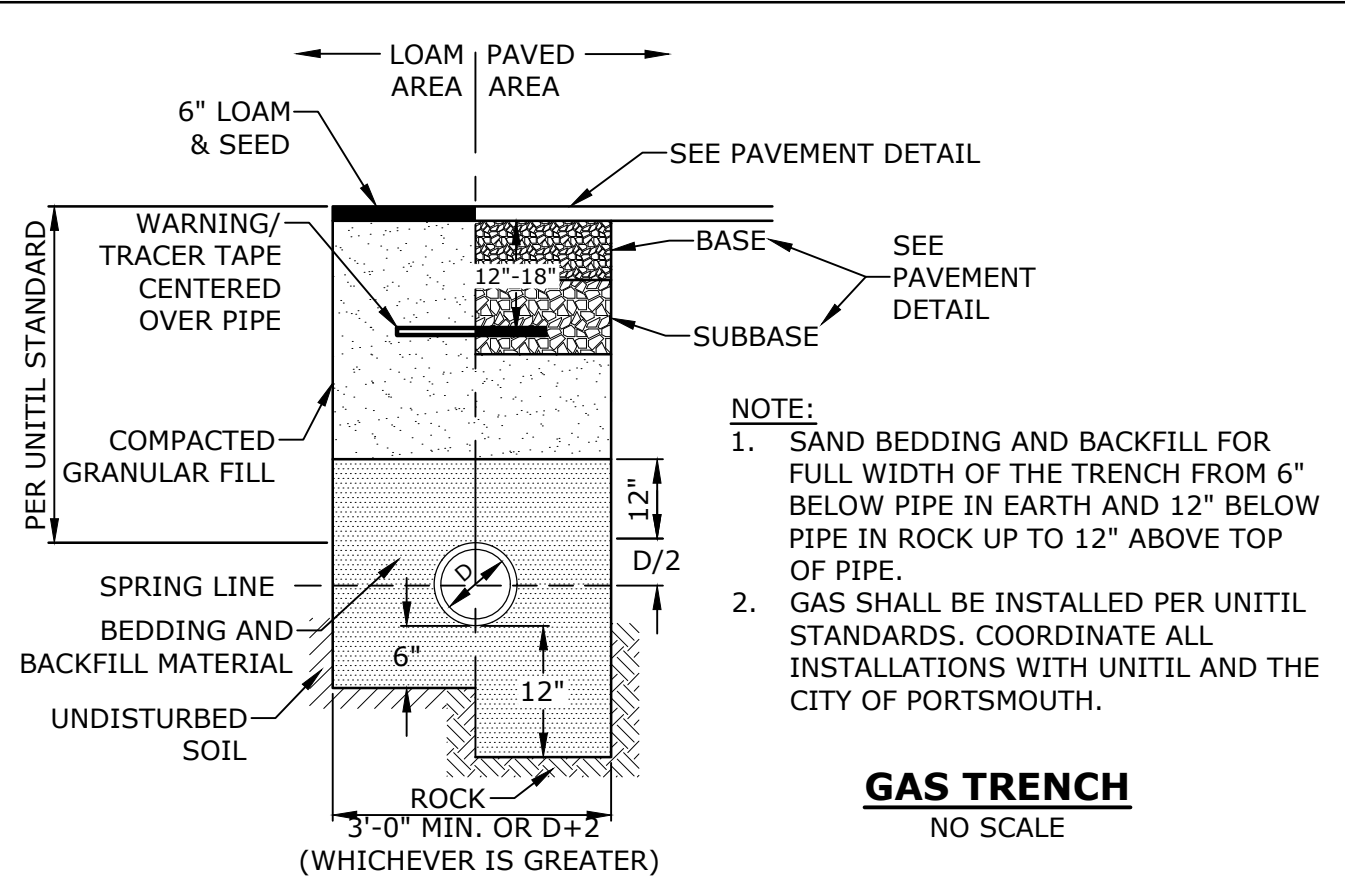
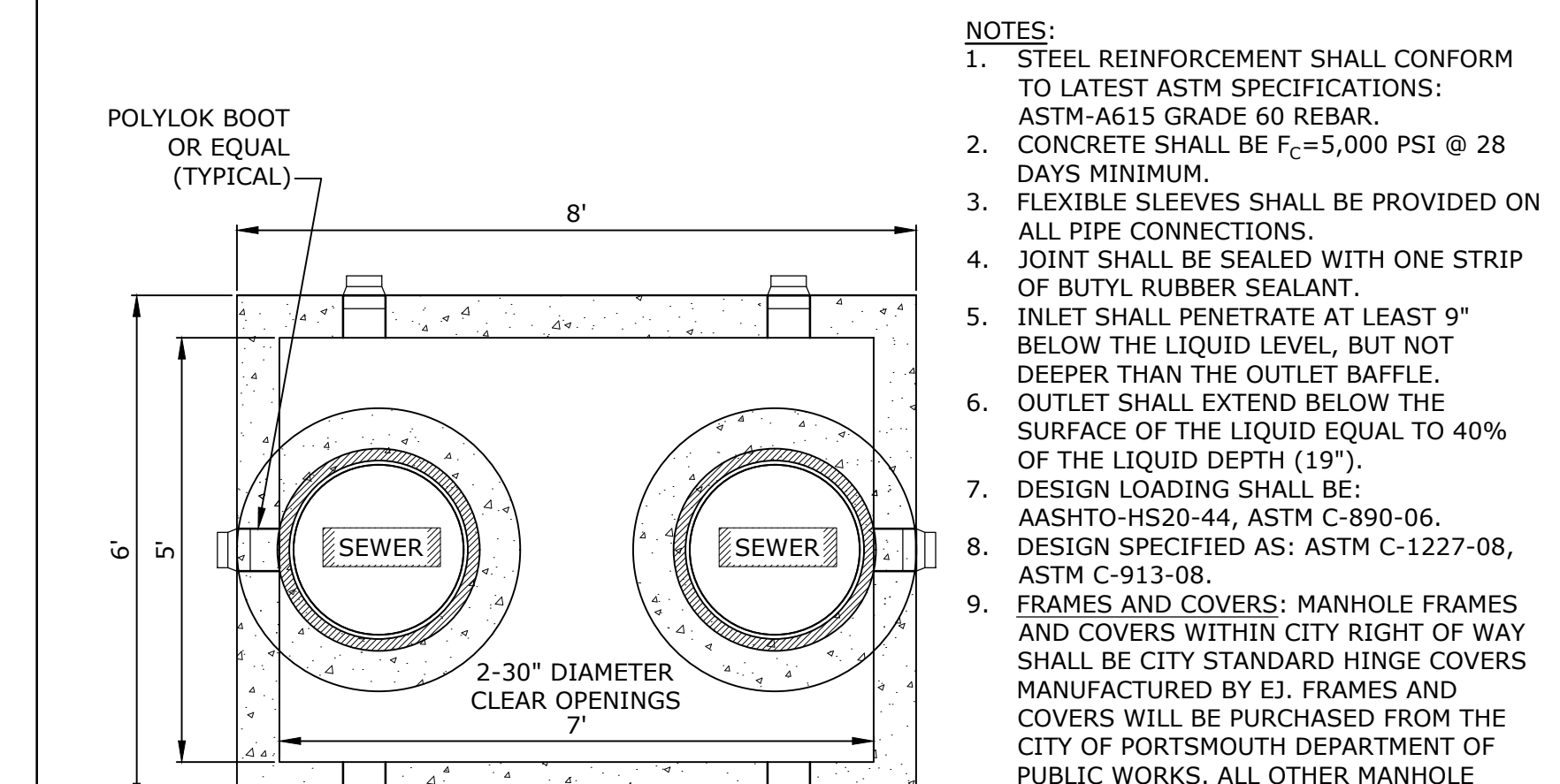
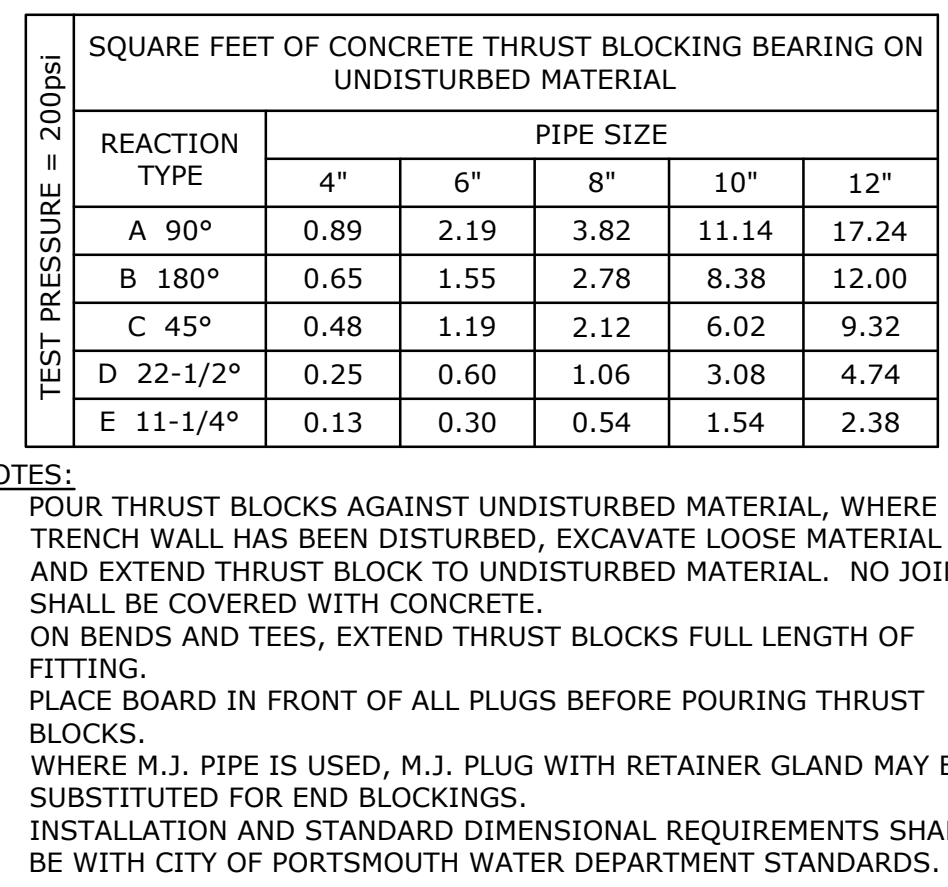
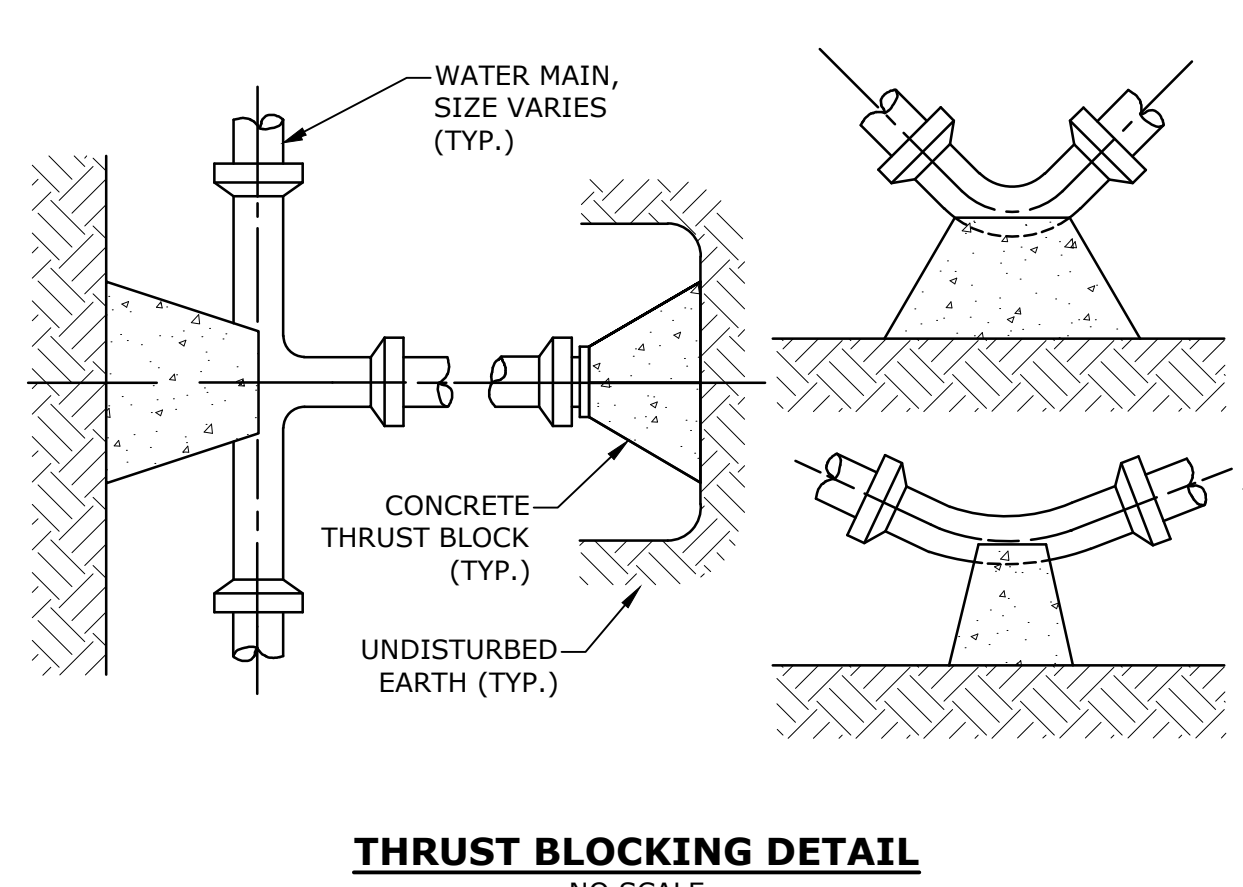
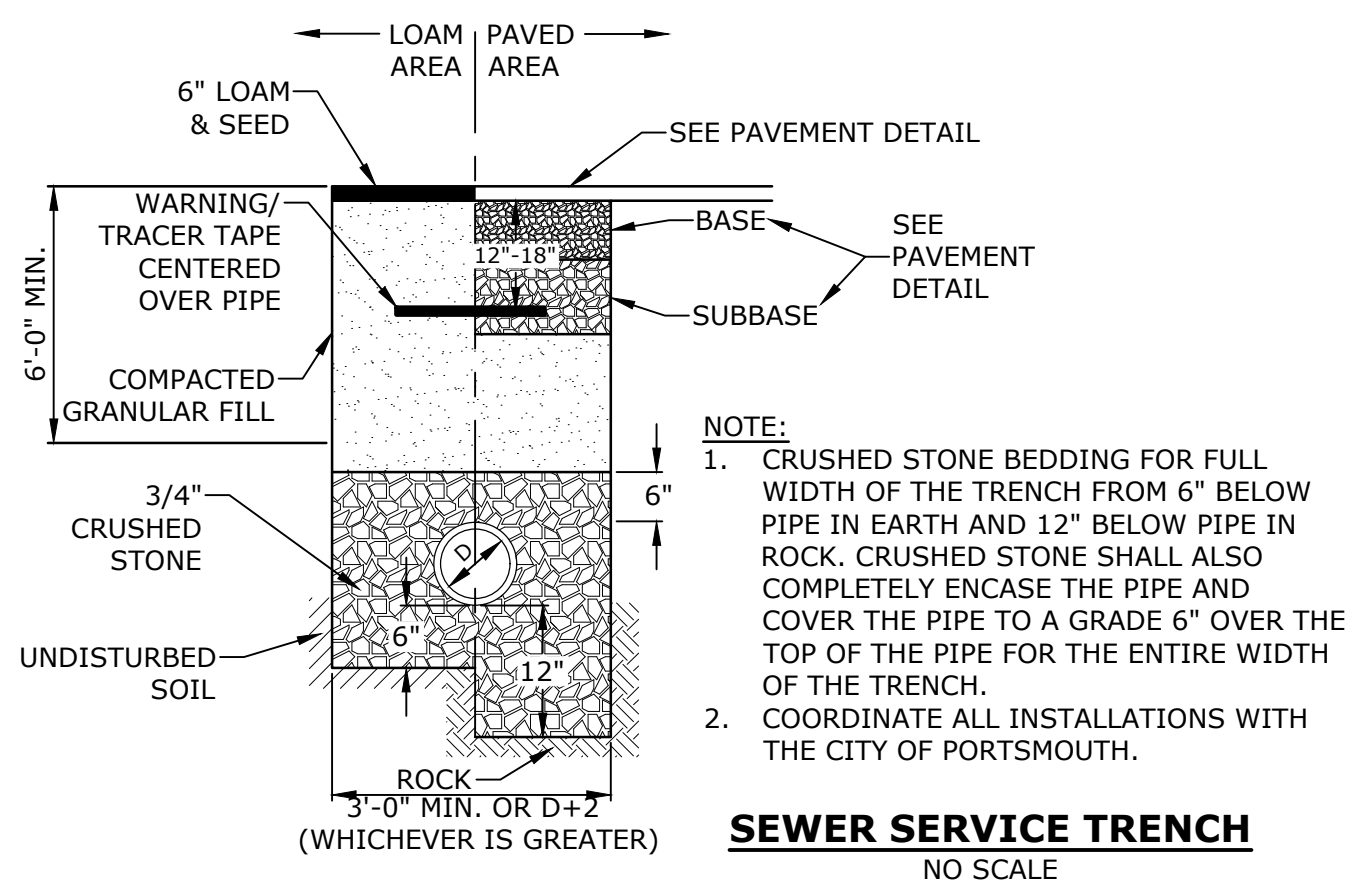
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CHECKED BY:	NAH/PMC
APPROVED BY:	BLM

DETAILS SHEET

SCALE: AS SHOWN

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Proposed Mixed Use Development

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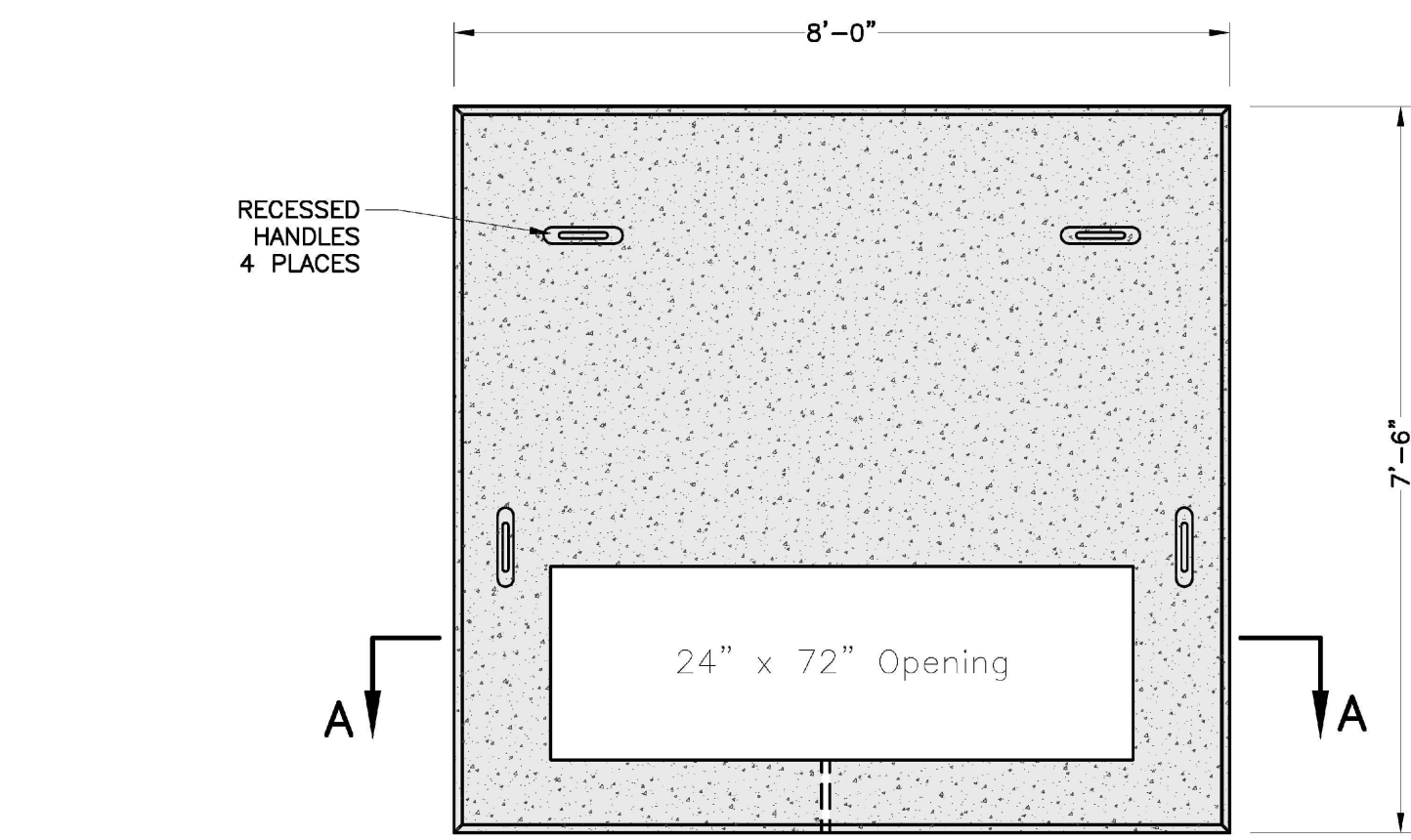
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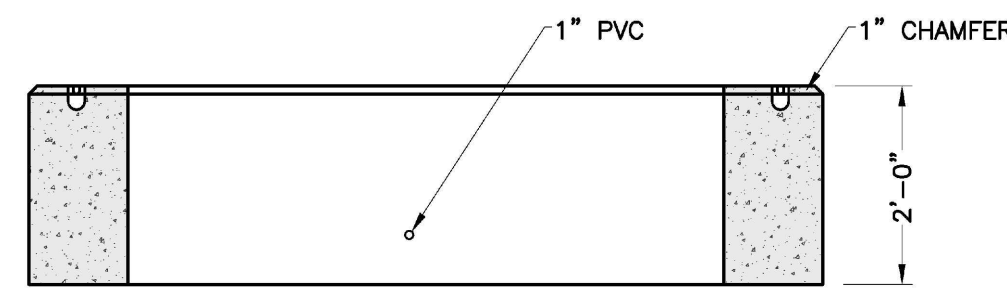
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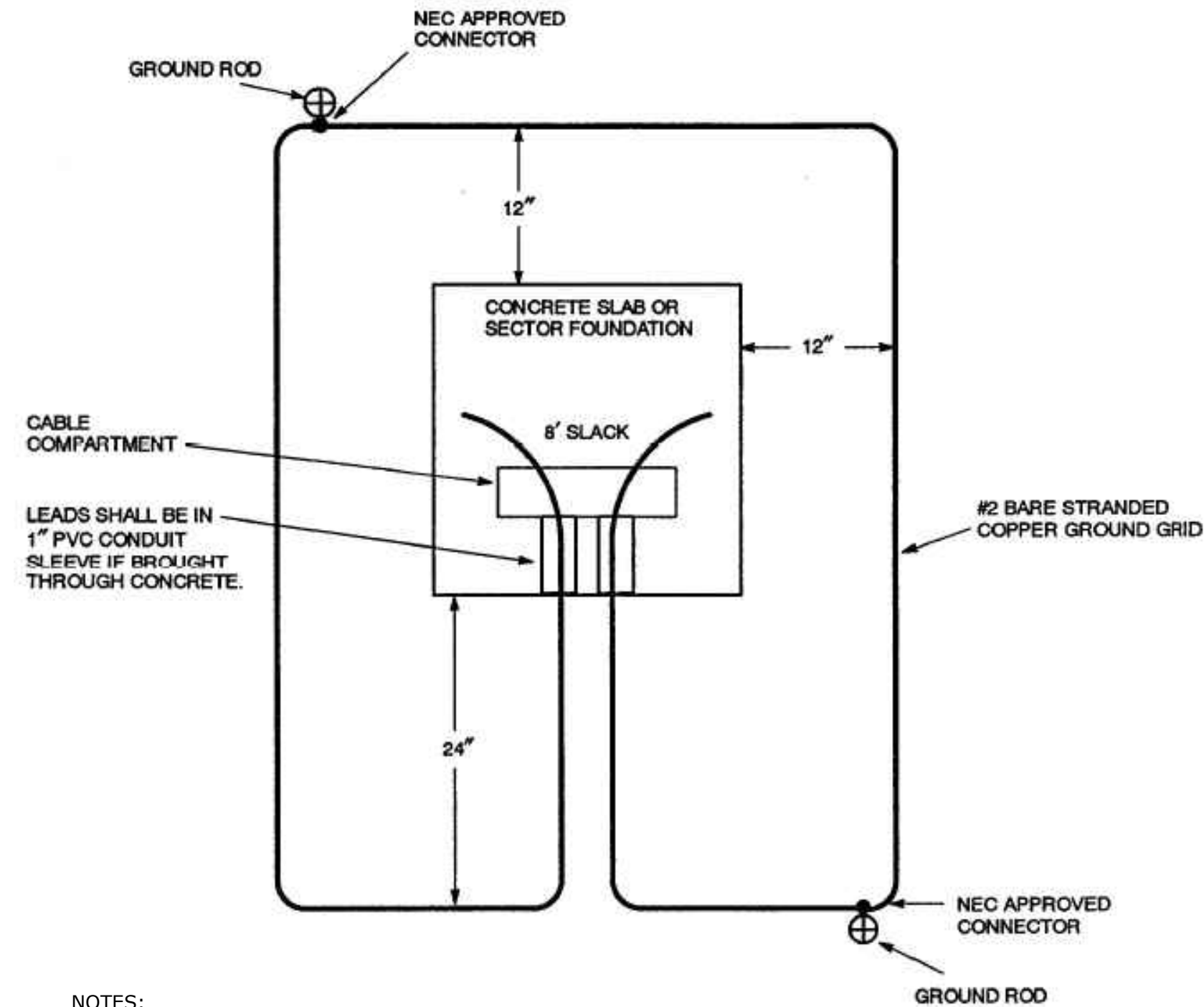
PLAN



SECTION A-A

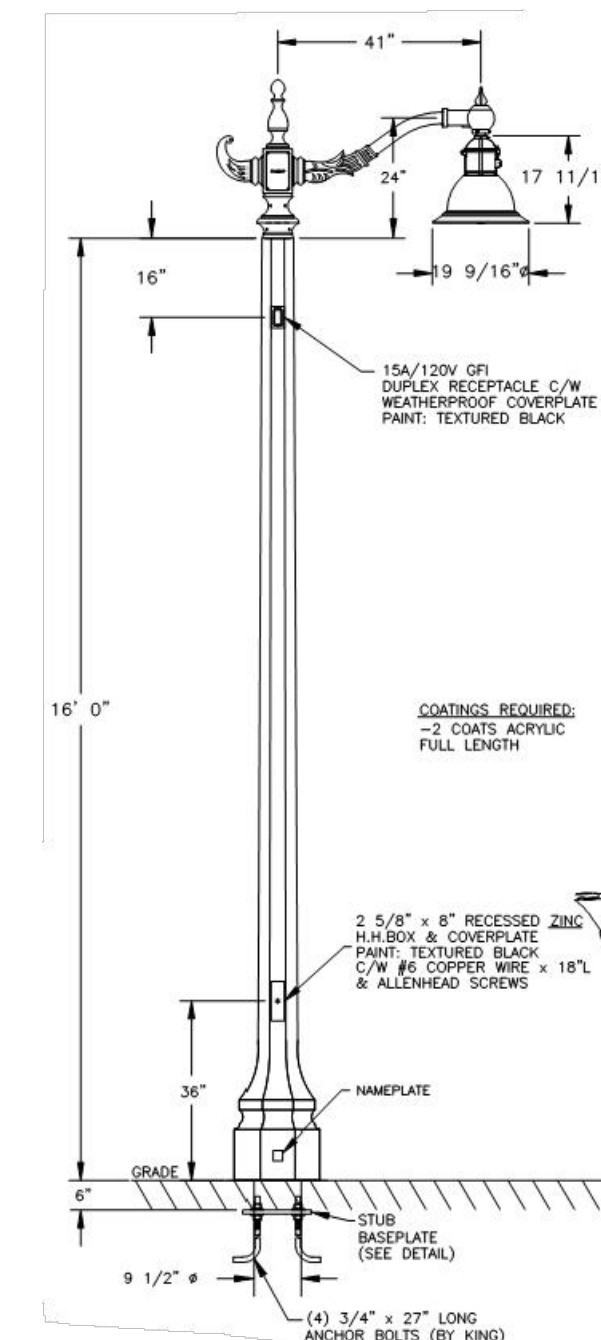
- NOTES:
1. DIMENSIONS SHOWN REPRESENT TYPICAL REQUIREMENTS. MANHOLE LOCATIONS AND REQUIREMENTS SHALL BE COORDINATED WITH EVERSOURCE PRIOR TO CONSTRUCTION
 2. CONCRETE MINIMUM STRENGTH - 4,000 PSI @ 28 DAYS
 3. STEEL REINFORCEMENT - ASTM A615, GRADE 60
 4. PAD MEETS OR EXCEEDS EVERSOURCE SPECIFICATIONS

3-PHASE TRANSFORMER PAD
NO SCALE



- NOTES:
- THE GROUND GRID SHALL BE SUPPLIED AND INSTALLED BY THE CONTRACTOR AND IS TO BE BURIED AT LEAST 12 INCHES BELOW GRADE. EIGHT FEET OF EXTRA WIRE FOR EACH GROUND GRID LEG SHALL BE LEFT EXPOSED IN THE CABLE COMPARTMENT TO ALLOW FOR THE CONNECTION TO THE TRANSFORMER. THE TWO 8-FOOT GROUND RODS MAY BE EITHER GALVANIZED STEEL OR COPPERWELD AND THEY SHALL BE CONNECTED TO THE GRID WITH NEC APPROVED CONNECTORS.

PAD-MOUNTED EQUIPMENT GROUNDING GRID DETAIL
NO SCALE

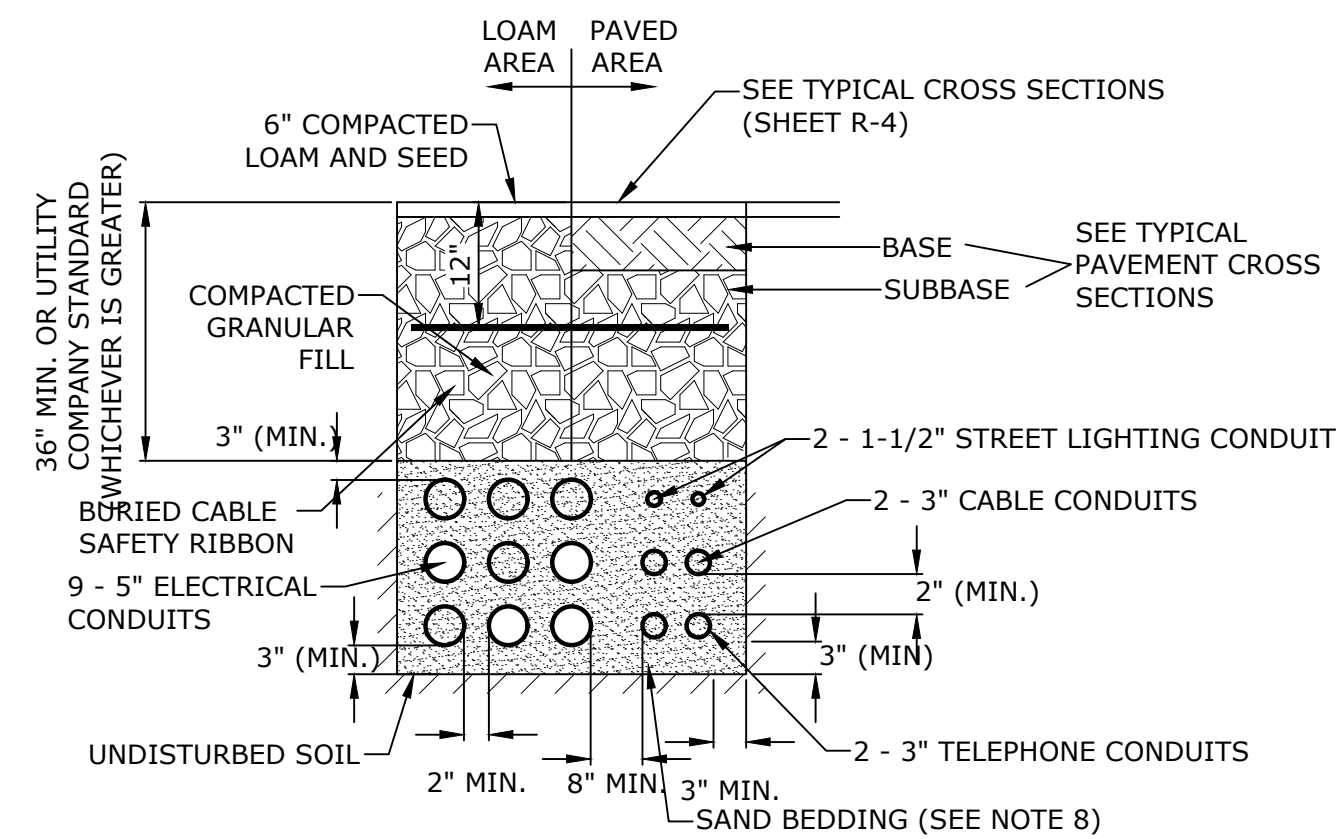


- LUMINAIRE SPECIFICATIONS:**
 CATALOGUE NO.: K729-P4FL-II-60(SSL)
 -7030-120-277-3K S/F KPL20
 GLOBE MAT'L: FLAT ARRAY, CLEAR FLAT LENS
 IES CLASSIFIC.: TYPE II
 WATTAGE: 60W (7030 SERIES)
 LIGHT SOURCE: SOLID STATE LIGHTING
 LINE VOLTAGE: 120:277V
 CCT: 3000K
 PAINT: TEXTURED BLACK
 OPTIONS: S/F KPL-20 LEVELING DEVICE

- ARM SPECIFICATIONS:**
 CATALOGUE NO.: (MOD.) KA72-T-1-3
 MATERIAL: ALUMINUM
 PAINT: TEXTURED BLACK
 OPTIONS: KPL20 LEVELING DEVICE

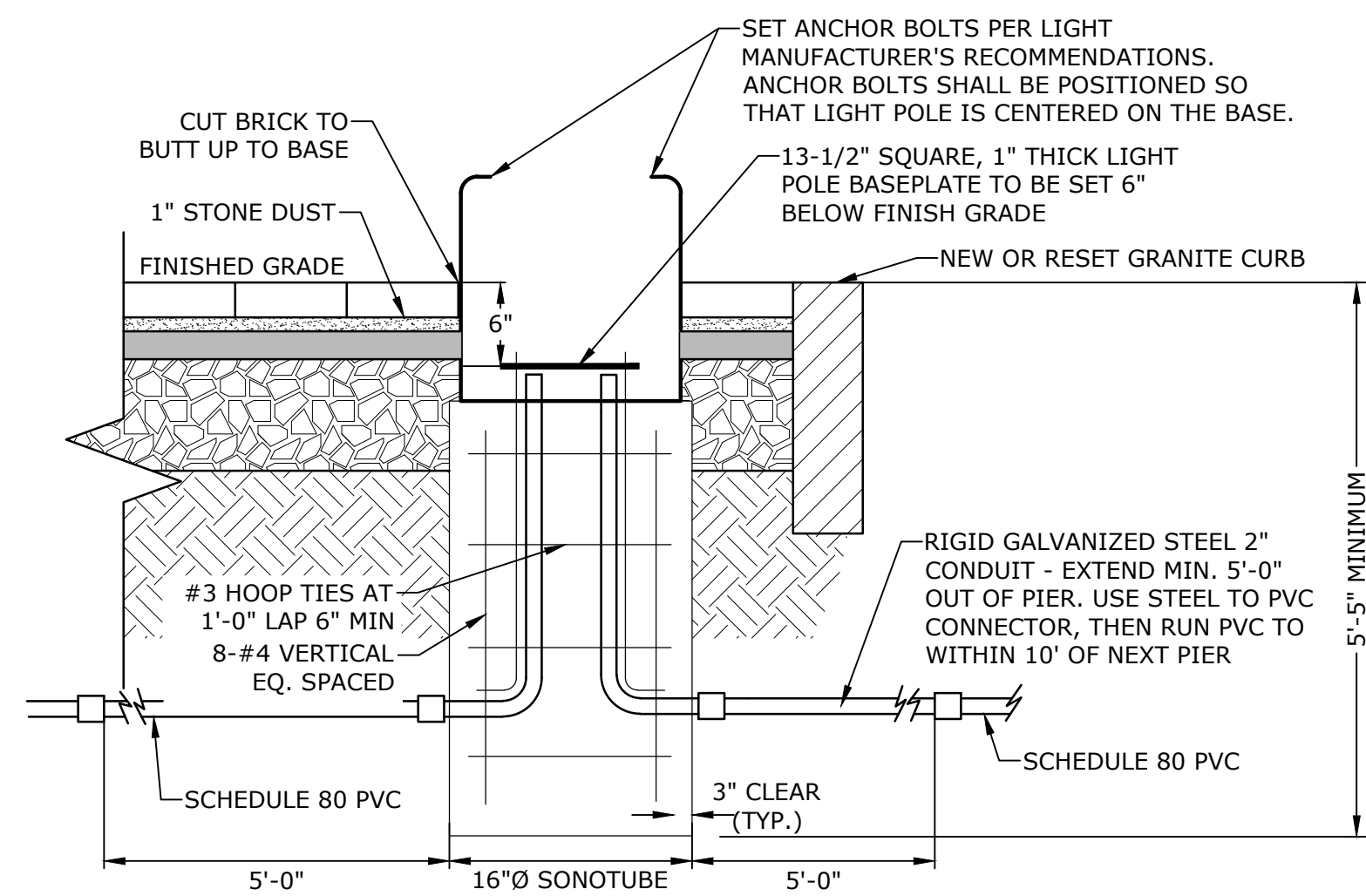
- POLE SPECIFICATIONS:**
 CATALOGUE NO.: KBH16-G-S11-SBP
 C/W 140-30/100 & DR
 OCTAGONAL
 ECLIPSE
 FINISH: POLISHED
 POLE TOP: 6 3/8" FL/FL
 POLE BUTT: 9 1/2" Ø
 POLE LENGTH: 16' 6"
 APPROX. WEIGHT: 1,190 LBS.
 MIN. RACEWAY: 1 1/8" Ø

NORTH END LIGHT POLE & FIXTURE
NO SCALE



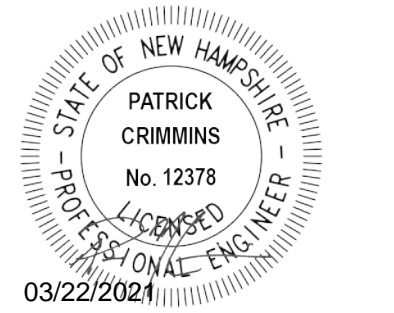
- NOTES:
1. NUMBER, MATERIAL, AND SIZE OF UTILITY CONDUITS TO BE DETERMINED BY LOCAL UTILITY OR AS SHOWN ON ELECTRICAL DRAWINGS. CONTRACTOR TO PROVIDE ONE SPARE CONDUIT FOR EACH UTILITY TO BUILDING.
 2. DIMENSIONS SHOWN REPRESENT OWNERS MINIMUM REQUIREMENTS. ACTUAL DIMENSIONS MAY BE GREATER BASED ON UTILITY COMPANY STANDARDS, BUT SHALL NOT BE LESS THAN THOSE SHOWN. NO CONDUIT RUN SHALL EXCEED 360 DEGREES IN TOTAL BENDS.
 3. A SUITABLE PULLING STRING, CAPABLE OF 200 POUNDS OF PULL, MUST BE INSTALLED IN THE CONDUIT BEFORE UTILITY COMPANY IS NOTIFIED TO INSTALL CABLE. THE STRING SHOULD BE BLOWN INTO THE CONDUIT AFTER THE RUN IS ASSEMBLED TO AVOID BONDING THE STRING TO THE CONDUIT.
 4. UTILITY COMPANY MUST BE GIVEN THE OPPORTUNITY TO INSPECT THE CONDUIT PRIOR TO BACKFILL. THE CONTRACTOR IS RESPONSIBLE FOR ALL REPAIRS SHOULD THE UTILITY COMPANY BE UNABLE TO INSTALL ITS CABLE IN A SUITABLE MANNER.
 5. ALL CONDUIT INSTALLATIONS MUST CONFORM TO THE CURRENT EDITION OF THE NATIONAL ELECTRIC SAFETY CODE, STATE AND LOCAL CODES AND ORDINANCES, AND, WHERE APPLICABLE, THE NATIONAL ELECTRIC CODE.
 6. ALL 90° SWEEPS WILL BE MADE USING RIGID GALVANIZED STEEL. SWEEPS WITH A 36 TO 48 INCH RADIUS.
 7. SAND BEDDING TO BE REPLACED WITH CONCRETE ENCASEMENT WHERE COVER IS LESS THAN 3 FEET, WHEN LOCATED BELOW PAVEMENT, OR WHERE SHOWN ON THE UTILITIES PLAN.

ELECTRICAL AND COMMUNICATION CONDUIT
NO SCALE



- NOTES:
1. REFER TO ELECTRICAL PLANS FOR WIRING DETAILS.
 2. CONCRETE: 4000 PSI, AIR ENTRAINED STEEL: 60 KSI
 3. LIGHT POLE FOUNDATIONS SHALL BE PLACED PRIOR TO INSTALLATION OF BRICK PAVERS.
 4. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR APPROVAL, TO INCLUDE PERFORMANCE SPECIFICATIONS, CALCULATIONS AND NH LICENSED STRUCTURAL ENGINEER'S STAMP FOR LIGHT POLE FOUNDATION.
 5. STANDARD BASE SHALL BE CONSTRUCTED UNLESS THERE IS CONFLICT WITH THE EXISTING DUCT BANK. SPREAD FOOTING BASE SHALL BE USED IN LIEU OF STANDARD BASE IN LOCATIONS WHERE TOP OF DUCT BANK ELEVATION WILL CONFLICT WITH STANDARD POLE BASE DEPTH. CONTRACTOR SHALL VERIFY LOCATIONS WHERE SPREAD FOOTINGS ARE REQUIRED PRIOR TO CONSTRUCTION. SEE NOTE#4 FOR SUBMITTAL REQUIREMENTS.

NORTH END LIGHT FIXTURE BASE
NO SCALE



Proposed Mixed Use Development

North Mill Pond Holdings, LLC

Portsmouth, New Hampshire

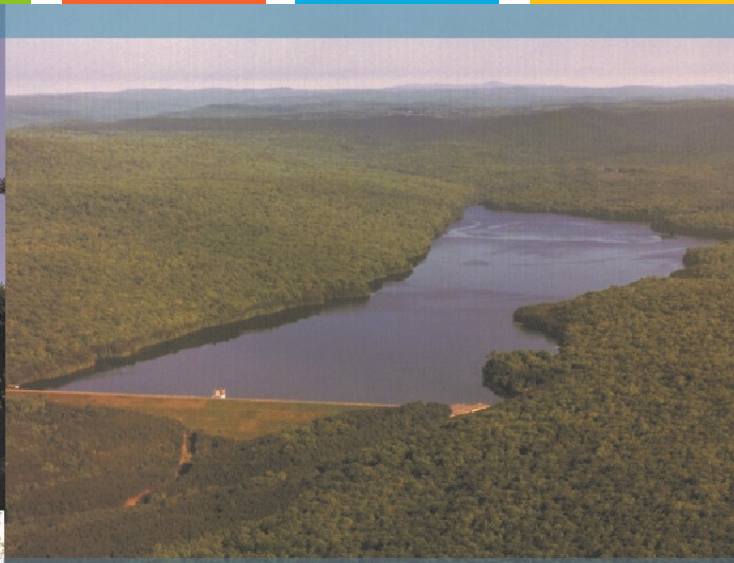
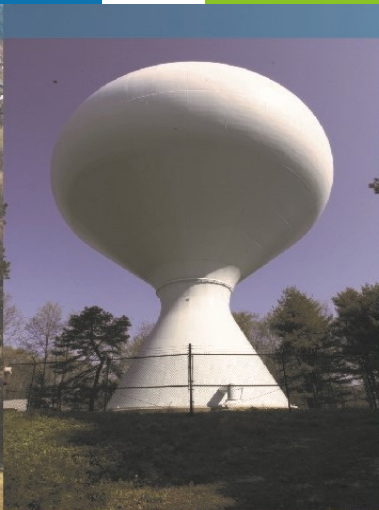
MARK	DATE	DESCRIPTION
C	3/22/2021	TAC Submission
B	3/10/2021	Design Review Resubmission
A	12/1/2020	TAC Work Session

PROJECT NO: P-0595-007
 DATE: December 22, 2020
 FILE: P-0595-007-DTLS.DWG
 DRAWN BY: CJK
 CHECKED BY: NAH/PMC
 APPROVED BY: BLM

DETAILS SHEET

SCALE: AS SHOWN

C-507

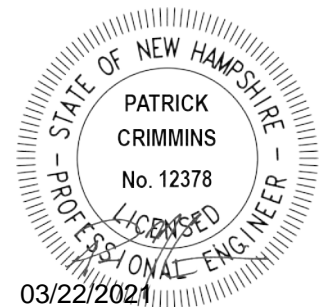


Proposed Mixed Use Development
 Raynes Avenue
 Portsmouth, NH

Drainage Analysis

North Mill Pond Holdings, LLC

March 22, 2021



Tighe&Bond

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A Site Specific Soils Report
B Extreme Precipitation Tables

Section 1

Project Description

The proposed project is located at 1 Raynes Avenue, 31 Raynes Avenue & 203 Maplewood Avenue and is comprised of four (4) parcels that are bounded by Raynes Avenue to the south, Maplewood Avenue to the west, North Mill Pond to the north, and municipal land to the east, which is the future site of the North Mill Pond community park. The existing parcels are listed below.

Tax Map/Lot No.	Area (ac)
123 / 10	0.170
123 / 12	0.140
123 / 13	1.323
123 / 14	0.906

The proposed project will include the construction of two (2) 5-story buildings. The first is a mixed-use residential building that has a first-floor residential lobby and two (2) commercial spaces, and 60 upper floor residential units. The second is a hotel building with 128 rooms at the corner of Raynes Ave and Vaughan Street. The project will include associated site improvements such as paving, utilities, lighting, landscaping and community space. The community space will be located on the land between North Mill Pond's mean high water (MHW) line to the 50ft buffer and will be deeded to the City of Portsmouth as community space designated for the City's North Mill Pond Trail project.

1.1 On-Site Soil Description

The site is a highly disturbed site along the North Mill Pond. The property shows evidence of what appears to be very old filling and grading associated with the existing development. The site consists of terrain that is generally flat and slopes from the south and west to the north to North Mill Pond. The existing property has an approximate high point of elevation of 14 near the corner of the property at the intersection of Raynes Ave and Maplewood Ave

A site specific soils survey was conducted by Leonard Lord, PhD, CSS, CWS of Tighe & Bond, Inc and can be found in Appendix A of this Report. Based on the soil survey, the runoff analyzed within these studies has been modeled using mostly Hydrologic Soil Group C soils and some portions of Hydrologic Soil Group A soils, as much of the site is comprised of Udorthents with two drainage classifications, moderately poorly drained soils and portions of well drained soils.

1.2 Pre- and Post-Development Comparison

The pre-development and post-development watershed areas have been analyzed at one point of analysis. While the point of analysis has remained unchanged, the contributing sub-catchment areas varied between pre-development and post-development conditions. These adjustments were made to reflect the differences in drainage patterns between the existing and proposed conditions. The overall area analyzed as part of this drainage analysis was held constant. PA-1 assesses flows that discharge directly to North Mill Pond via overland flow or various outlets.

Since North Mill Pond is a tidal water, NHDES does not require peak runoff control requirements to be met (Env-Wq 1507.06(d)). However, detention systems are included on the development site for the purpose of mitigating temperature differences between the stormwater runoff and the North Mill Pond, therefore peak runoff requirements have been met and can be found in section 4 of this report.

1.3 Calculation Methods

The design storms analyzed in this study are the 2-year, 10-year, 25-year and 50-year 24-hour duration storm events. The stormwater modeling system, HydroCAD 10.0 was utilized to predict the peak runoff rates from these storm events. The peak discharge rates were determined by analyzing Type III 24-hour storm events. The rainfall data for these storm events was obtained from the data published by the Northeast Regional Climate Center at Cornell University, with an additional 15% added factor of safety as required by Env-Wq 1503.08(I).

The time of concentration was computed using the TR-55 Method, which provides a means of determining the time for an entire watershed to contribute runoff to a specific location via sheet flows, shallow concentrated flow and channel flow. Runoff curve numbers were calculated by estimating the coverage areas and then summing the curve number for the coverage area as a percent of the entire watershed.

References:

1. HydroCAD Stormwater Modeling System, by HydroCAD Software Solutions LLC, Chocorua, New Hampshire.
2. New Hampshire Stormwater Management Manual, Volume 2, Post-Construction Best Management Practices Selection and Design, December 2008.
3. "Extreme Precipitation in New York & New England." Extreme Precipitation in New York & New England by Northeast Regional Climate Center (NRCC), 26 June 2012.

Section 2

Pre-Development Conditions

To analyze the pre-development condition, the site has been divided into one (1) distinct points of analysis (PA-1). This point of analysis and watershed is depicted on the plan entitled "Pre-Development Watershed Plan", Sheet C-801.

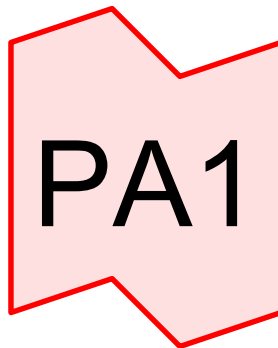
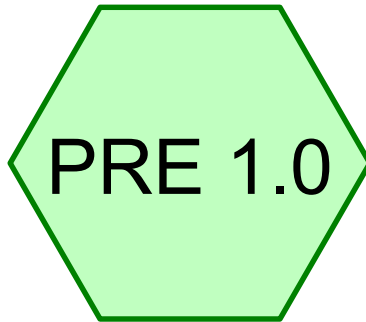
The point of analysis and its contributing watershed areas are described below:

Point of Analysis (PA-1)

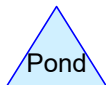
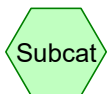
Pre-development Watershed 1.0 (PRE 1.0) is comprised of mostly impervious surfaces from paved parking and structures, as well as some disturbed forested areas to the northeast, and a run down pier. Banks along the shoreline of North Mill Pond consist of lawn, various species associated with disturbed sites, and rip rap slope. Runoff from this watershed area travels via overland flow or underground drainage system to discharge into North Mill Pond. The runoff is currently untreated before discharge.

2.1 Pre-Development Calculations

2.2 Pre-Development Watershed Plans



POINT OF ANALYSIS 1



P-0595-007 PRE

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Page 2

Area Listing (all nodes)

Area (acres)	CN	Description (subcatchment-numbers)
0.007	39	>75% Grass cover, Good, HSG A (PRE 1.0)
0.628	74	>75% Grass cover, Good, HSG C (PRE 1.0)
1.117	98	Paved parking, HSG C (PRE 1.0)
0.068	98	Rock embankment, HSG C (PRE 1.0)
0.456	98	Roofs, HSG C (PRE 1.0)
0.056	98	Unconnected pavement, HSG A (PRE 1.0)
0.204	70	Woods, Good, HSG C (PRE 1.0)
2.537	90	TOTAL AREA

P-0595-007 PRE

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Soil Listing (all nodes)

Area (acres)	Soil Group	Subcatchment Numbers
0.063	HSG A	PRE 1.0
0.000	HSG B	
2.474	HSG C	PRE 1.0
0.000	HSG D	
0.000	Other	
2.537		TOTAL AREA

P-0595-007 PRE

Type III 24-hr 2 Year Storm Rainfall=3.68"

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Page 4

Time span=0.00-48.00 hrs, dt=0.04 hrs, 1201 points
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentPRE 1.0:

Runoff Area=110,529 sf 66.92% Impervious Runoff Depth=2.62"
Flow Length=189' Tc=5.0 min CN=90 Runoff=7.82 cfs 0.553 af

Link PA1: POINT OF ANALYSIS1

Inflow=7.82 cfs 0.553 af
Primary=7.82 cfs 0.553 af

Total Runoff Area = 2.537 ac Runoff Volume = 0.553 af Average Runoff Depth = 2.62"
33.08% Pervious = 0.839 ac 66.92% Impervious = 1.698 ac

P-0595-007 PRE

Type III 24-hr 10 Year Storm Rainfall=5.59"

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Time span=0.00-48.00 hrs, dt=0.04 hrs, 1201 points
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentPRE 1.0:

Runoff Area=110,529 sf 66.92% Impervious Runoff Depth=4.45"
Flow Length=189' Tc=5.0 min CN=90 Runoff=12.94 cfs 0.940 af

Link PA1: POINT OF ANALYSIS1

Inflow=12.94 cfs 0.940 af
Primary=12.94 cfs 0.940 af

Total Runoff Area = 2.537 ac Runoff Volume = 0.940 af Average Runoff Depth = 4.45"
33.08% Pervious = 0.839 ac 66.92% Impervious = 1.698 ac

Summary for Subcatchment PRE 1.0:

Runoff = 12.94 cfs @ 12.07 hrs, Volume= 0.940 af, Depth= 4.45"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
Type III 24-hr 10 Year Storm Rainfall=5.59"

Area (sf)	CN	Description
2,435	98	Unconnected pavement, HSG A
317	39	>75% Grass cover, Good, HSG A
19,880	98	Roofs, HSG C
27,362	74	>75% Grass cover, Good, HSG C
8,883	70	Woods, Good, HSG C
* 2,980	98	Rock embankment, HSG C
48,672	98	Paved parking, HSG C
110,529	90	Weighted Average
36,562		33.08% Pervious Area
73,967		66.92% Impervious Area
2,435		3.29% Unconnected

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
0.4	33	0.0280	1.35		Sheet Flow, Smooth surfaces n= 0.011 P2= 3.68"
0.9	121	0.0250	2.37		Shallow Concentrated Flow, Grassed Waterway Kv= 15.0 fps
0.1	35	0.1400	5.61		Shallow Concentrated Flow, Grassed Waterway Kv= 15.0 fps
1.4	189	Total, Increased to minimum Tc = 5.0 min			

Summary for Link PA1: POINT OF ANALYSIS 1

Inflow Area = 2.537 ac, 66.92% Impervious, Inflow Depth = 4.45" for 10 Year Storm event

Inflow = 12.94 cfs @ 12.07 hrs, Volume= 0.940 af

Primary = 12.94 cfs @ 12.07 hrs, Volume= 0.940 af, Atten= 0%, Lag= 0.0 min

Primary outflow = Inflow, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs

P-0595-007 PRE

Type III 24-hr 25 Year Storm Rainfall=7.08"

Prepared by Tighe & Bond

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Time span=0.00-48.00 hrs, dt=0.04 hrs, 1201 points
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentPRE 1.0:

Runoff Area=110,529 sf 66.92% Impervious Runoff Depth=5.90"
Flow Length=189' Tc=5.0 min CN=90 Runoff=16.90 cfs 1.248 af

Link PA1: POINT OF ANALYSIS1

Inflow=16.90 cfs 1.248 af
Primary=16.90 cfs 1.248 af

Total Runoff Area = 2.537 ac Runoff Volume = 1.248 af Average Runoff Depth = 5.90"
33.08% Pervious = 0.839 ac 66.92% Impervious = 1.698 ac

P-0595-007 PRE

Type III 24-hr 50 Year Storm Rainfall=8.48"

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Page 8

Time span=0.00-48.00 hrs, dt=0.04 hrs, 1201 points
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

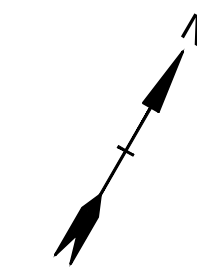
SubcatchmentPRE 1.0:

Runoff Area=110,529 sf 66.92% Impervious Runoff Depth=7.28"
Flow Length=189' Tc=5.0 min CN=90 Runoff=20.59 cfs 1.539 af




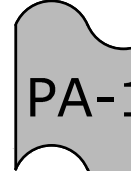
Link PA1: POINT OF ANALYSIS1

Inflow=20.59 cfs 1.539 af
Primary=20.59 cfs 1.539 af

Total Runoff Area = 2.537 ac Runoff Volume = 1.539 af Average Runoff Depth = 7.28"
33.08% Pervious = 0.839 ac 66.92% Impervious = 1.698 ac



LEGEND

-  PRE-DEVELOPMENT WATERSHED BOUNDARY
-  LONGEST FLOW PATH
-  PRE DEVELOPMENT WATERSHED AREA DESIGNATION
-  POINT OF ANALYSIS

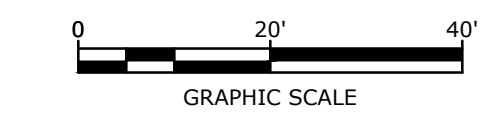
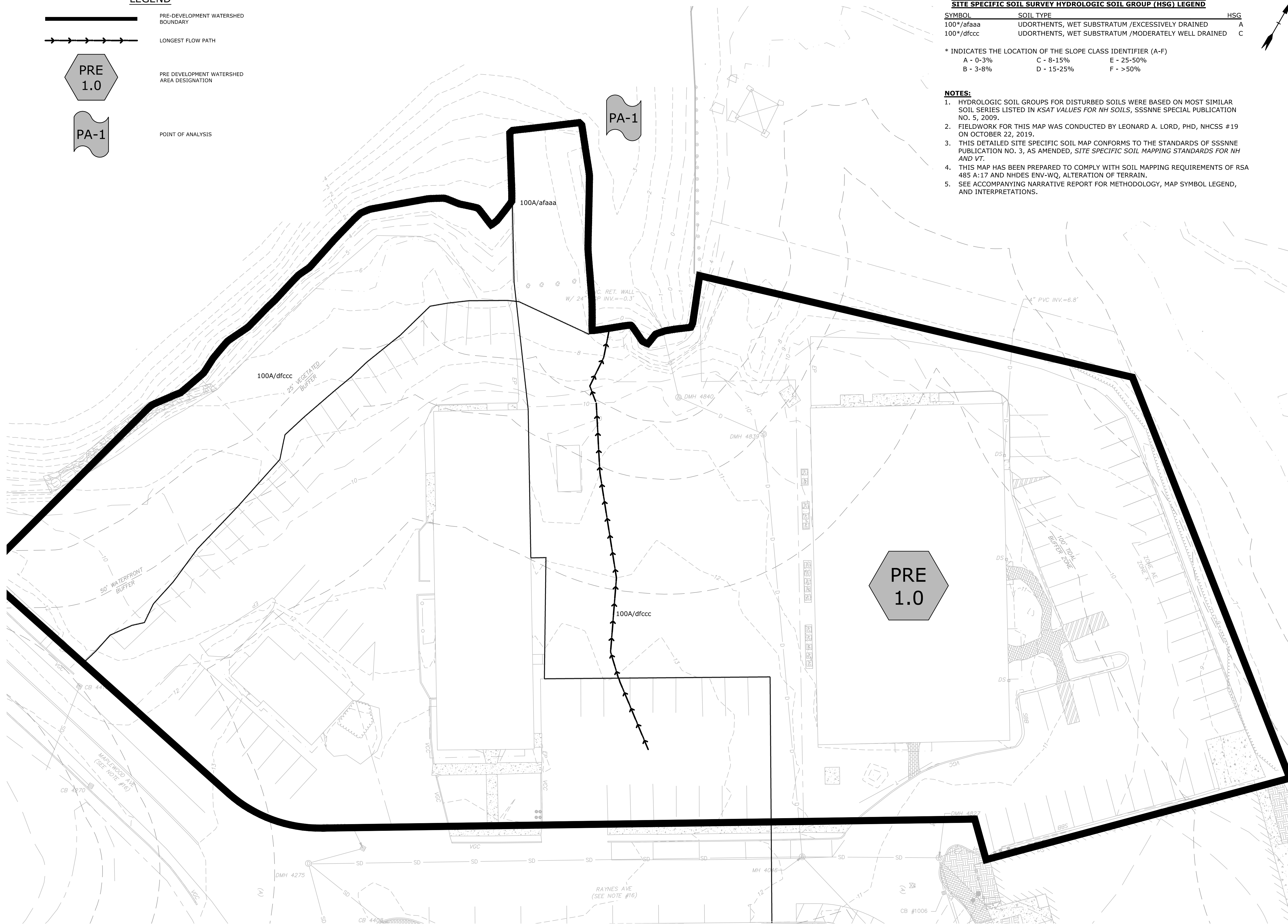
SITE SPECIFIC SOIL SURVEY HYDROLOGIC SOIL GROUP (HSG) LEGEND

SYMBOL	SOIL TYPE	HSG
100*/afaaa	UDORTHERTS, WET SUBSTRATUM /EXCESSIVELY DRAINED	A
100*/dfccc	UDORTHERTS, WET SUBSTRATUM /MODERATELY WELL DRAINED	C

* INDICATES THE LOCATION OF THE SLOPE CLASS IDENTIFIER (A-F)
 A - 0-3% C - 8-15% E - 25-50%
 B - 3-8% D - 15-25% F - >50%

NOTES:

1. HYDROLOGIC SOIL GROUPS FOR DISTURBED SOILS WERE BASED ON MOST SIMILAR SOIL SERIES LISTED IN *KSAT VALUES FOR NH SOILS*, SSSNNE SPECIAL PUBLICATION NO. 5, 2009.
2. FIELDWORK FOR THIS MAP WAS CONDUCTED BY LEONARD A. LORD, PHD, NHCSS #19 ON OCTOBER 22, 2019.
3. THIS DETAILED SITE SPECIFIC SOIL MAP CONFORMS TO THE STANDARDS OF SSSNNE PUBLICATION NO. 3, AS AMENDED, *SITE SPECIFIC SOIL MAPPING STANDARDS FOR NH AND VT*.
4. THIS MAP HAS BEEN PREPARED TO COMPLY WITH SOIL MAPPING REQUIREMENTS OF RSA 485 A:17 AND NHDES ENV-WQ, ALTERATION OF TERRAIN.
5. SEE ACCOMPANYING NARRATIVE REPORT FOR METHODOLOGY, MAP SYMBOL LEGEND, AND INTERPRETATIONS.



Proposed Mixed Use Development

North Mill Pond Holdings, LLC

Portsmouth, New Hampshire

MARK	DATE	DESCRIPTION
C	3/22/2021	TAC Submission
B	3/10/2021	Design Review Resubmission
A	12/1/2020	TAC Work Session

PROJECT NO:	P-0595-007
DATE:	December 22, 2020
FILE:	P-0595-007-HYDRO.DWG
DRAWN BY:	CJK
CHECKED BY:	NAH/PMC
APPROVED BY:	BLM

PRE-DEVELOPMENT WATERSHED PLAN

SCALE: AS SHOWN

Last Saved: 3/19/2021 9:47am By: CJK
 Project: 0595-007 Hydros (P) 0595-007 Hydros (P) 0595-007 Hydros (P) 0595-007 Hydros (P)
 Title: 0595-007 Hydros (P) 0595-007 Hydros (P) 0595-007 Hydros (P) 0595-007 Hydros (P)

Section 3

Post-Development Conditions

The post-development condition was analyzed by dividing the watersheds into seven (7) watershed areas. Stormwater runoff from these sub-catchment areas flow via subsurface drainage systems prior to discharging to North Mill Pond. Like the pre-development condition, flows from these sub-catchment areas are modeled at one point of analysis at North Mill Pond (PA-1). As per Env-Wq 1507.06(d), since North Mill Pond is tidal water the peak runoff control requirements do not apply. However, the peak runoff requirements have been met due to the onsite underground detention basin and these comparisons can be found in Section 4 of this report.

Two underground detention system are included on the development site for the purpose of mitigating temperature differences between the stormwater runoff and the North Mill Pond. The detention systems and outlet structures have been sized to detain the WQV with a drain down time of 24 hours, prior to discharging to the treatment units. This detention basin is used to mitigate increased temperature of the initial surface runoff. Flows greater than the 2-year storm event are designed to bypass the treatment unit.

The point of analysis (PA-1) and its sub-catchment areas are depicted on the plan entitled "Post-Development Watershed Plan," Sheet C-802. The point of analysis and it's contributing watershed areas are described below:

Point of Analysis (PA-1)

Post-development Watershed 1.1 (POST 1.1) is comprised of mostly the southern building and associated impervious areas on the south end of the site. Runoff from this watershed area travels via overland flow or roof leader to deep sump catch basins and an underground detention system. The detention system and outlet structure have been sized to detain the WQV with a drain down time greater than 24 hours, prior to discharging to the treatment unit, a Contech Jellyfish Stormwater Filter (JFF-1). Flows exiting the Jellyfish Filter discharge to North Mill Pond (PA-1). The pipe network is protected by a backflow preventer within the outlet invert of a manhole structure at the most downstream location.

Post-development Watershed 1.2 (POST 1.2) like POST 1.1, is comprised of mostly the northern building and associated impervious areas on the north side of the site. Runoff from this watershed area travels via overland flow or roof leader to deep sump catch basins and an underground detention system. The detention system and outlet structure have been sized to detain the WQV with a drain down time greater than 24 hours, prior to discharging to the treatment unit, a Contech Jellyfish Stormwater Filter (JFF-2). Flows exiting the Jellyfish Filter discharge to North Mill Pond (PA-1). As previously stated, the pipe network is protected by a backflow preventer within the outlet invert of a manhole structure at the most downstream location.

Post-development Watershed 1.3 (POST 1.3) is comprised mostly of porous pavement multi use path located between the proposed development and the North Mill Pond as well as some grassed landscape areas. Runoff from the watershed infiltrates through the filter media section under the porous pavement and discharges to an underdrain. Due to the poor onsite soils and high groundwater elevation the porous pavement section has been lined with an impermeable liner and an underdrain has been provided. The underdrain connects to the closed drainage system on site, ultimately discharging to the North Mill Pond.

Post-development Watershed 1.4 (POST 1.4) is nearly identical to POST 1.3 and is comprised mostly of porous pavement multi use path located between the proposed development and the North Mill Pond as well as some grassed landscape areas. Runoff from the watershed infiltrates through the filter media section under the porous pavement and discharges to an underdrain. Due to the poor onsite soils and high groundwater elevation the porous pavement section has been lined with an impermeable liner and an underdrain has been provided. The underdrain also connects to the closed drainage system on site, ultimately discharging to the North Mill Pond.

Post-development Watershed 1.5 (POST 1.5) is comprised mostly of grassy areas and a proposed boat/kayak launch and reconstructed timber pier. Runoff from this watershed simply sheets toward and discharges into North Mill Pond, as in the existing condition. There are no proposed impervious surfaces that are within this watershed area that would require treatment.

Post-development Watershed 1.6 (POST 1.6) is also comprised mostly of grassy area along the northern side of the property. Runoff from this watershed simply sheets north and discharges into North Mill Pond, as in the existing condition. There are no proposed impervious surfaces that are within this watershed area that would require treatment.

Post-development Watershed 1.7 (POST 1.7) is comprised of a small strip of sidewalk and landscaping in between the proposed buildings and the city right of way along Raynes Ave. The runoff from this Subcatchment sheets directly onto the street to the existing closed drainage system, ultimately discharging to North Mill Pond via the previously mentioned drainage system.

3.1 Post-Development Calculations

3.2 Post-Development Watershed Plans

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Area Listing (all nodes)

Area (acres)	CN	Description (subcatchment-numbers)
0.007	39	>75% Grass cover, Good, HSG A (POST 1.3, POST 1.5)
0.649	74	>75% Grass cover, Good, HSG C (POST 1.1, POST 1.2, POST 1.3, POST 1.4, POST 1.5, POST 1.6, POST 1.7)
0.056	98	Paved parking, HSG A (POST 1.3, POST 1.5)
1.022	98	Paved parking, HSG C (POST 1.1, POST 1.2, POST 1.3, POST 1.4, POST 1.5, POST 1.7)
0.068	98	Rock embankment, HSG C (POST 1.5)
0.735	98	Roofs, HSG C (POST 1.1, POST 1.2)
2.537	92	TOTAL AREA

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Soil Listing (all nodes)

Area (acres)	Soil Group	Subcatchment Numbers
0.063	HSG A	POST 1.3, POST 1.5
0.000	HSG B	
2.474	HSG C	POST 1.1, POST 1.2, POST 1.3, POST 1.4, POST 1.5, POST 1.6, POST 1.7
0.000	HSG D	
0.000	Other	
2.537		TOTAL AREA

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Type III 24-hr 2 Year Storm Rainfall=3.68"

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Time span=0.00-48.00 hrs, dt=0.04 hrs, 1201 points
 Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
 Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentPOST 1.1:	Runoff Area=29,978 sf 99.19% Impervious Runoff Depth=3.45" Flow Length=114' Tc=5.0 min CN=98 Runoff=2.52 cfs 0.198 af
SubcatchmentPOST 1.2:	Runoff Area=38,901 sf 97.58% Impervious Runoff Depth=3.33" Flow Length=85' Tc=5.0 min CN=97 Runoff=3.23 cfs 0.248 af
SubcatchmentPOST 1.3:	Runoff Area=13,558 sf 33.08% Impervious Runoff Depth=1.93" Flow Length=59' Slope=0.0430 '/' Tc=5.0 min CN=82 Runoff=0.72 cfs 0.050 af
SubcatchmentPOST 1.4:	Runoff Area=3,521 sf 22.47% Impervious Runoff Depth=1.71" Flow Length=33' Slope=0.0270 '/' Tc=5.0 min CN=79 Runoff=0.16 cfs 0.011 af
SubcatchmentPOST 1.5:	Runoff Area=16,946 sf 35.03% Impervious Runoff Depth=1.93" Flow Length=60' Slope=0.0520 '/' Tc=5.0 min CN=82 Runoff=0.90 cfs 0.063 af
SubcatchmentPOST 1.6:	Runoff Area=3,725 sf 0.00% Impervious Runoff Depth=1.37" Flow Length=37' Slope=0.0610 '/' Tc=5.0 min CN=74 Runoff=0.14 cfs 0.010 af
SubcatchmentPOST 1.7:	Runoff Area=3,900 sf 77.85% Impervious Runoff Depth=2.91" Flow Length=92' Slope=0.0350 '/' Tc=5.0 min CN=93 Runoff=0.30 cfs 0.022 af
Pond JFF 1: CONTECH JELLY FISH FILTER	Peak Elev=3.23' Inflow=2.18 cfs 0.198 af 18.0" Round Culvert n=0.013 L=7.0' S=0.0214 '/' Outflow=2.18 cfs 0.198 af
Pond JFF 2: CONTECH JELLY FISH FILTER	Peak Elev=3.32' Inflow=2.91 cfs 0.248 af 18.0" Round Culvert n=0.013 L=5.0' S=0.0200 '/' Outflow=2.91 cfs 0.248 af
Pond PDMH 12:	Peak Elev=2.60' Inflow=5.34 cfs 0.508 af 24.0" Round Culvert n=0.013 L=27.0' S=0.0704 '/' Outflow=5.34 cfs 0.508 af
Pond PDMH 2:	Peak Elev=3.05' Inflow=5.34 cfs 0.467 af 24.0" Round Culvert n=0.013 L=34.0' S=0.0029 '/' Outflow=5.34 cfs 0.467 af
Pond POND 1.1:	Peak Elev=5.90' Storage=2,862 cf Inflow=2.52 cfs 0.198 af Outflow=2.18 cfs 0.198 af
Pond POND 1.2:	Peak Elev=6.46' Storage=3,633 cf Inflow=3.23 cfs 0.248 af Outflow=2.91 cfs 0.248 af
Pond PP 1: POROUS PAVEMENT	Peak Elev=5.08' Storage=1,087 cf Inflow=0.72 cfs 0.050 af Outflow=0.12 cfs 0.032 af
Pond PP 2: POROUS PAVEMENT	Peak Elev=4.62' Storage=195 cf Inflow=0.16 cfs 0.011 af Outflow=0.06 cfs 0.008 af
Link PA1:	Inflow=6.28 cfs 0.580 af Primary=6.28 cfs 0.580 af

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Total Runoff Area = 2.537 ac Runoff Volume = 0.601 af Average Runoff Depth = 2.84"
25.86% Pervious = 0.656 ac 74.14% Impervious = 1.881 ac

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Type III 24-hr 10 Year Storm Rainfall=5.59"

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Time span=0.00-48.00 hrs, dt=0.04 hrs, 1201 points
 Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
 Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentPOST 1.1:	Runoff Area=29,978 sf 99.19% Impervious Runoff Depth=5.35" Flow Length=114' Tc=5.0 min CN=98 Runoff=3.85 cfs 0.307 af
SubcatchmentPOST 1.2:	Runoff Area=38,901 sf 97.58% Impervious Runoff Depth=5.24" Flow Length=85' Tc=5.0 min CN=97 Runoff=4.96 cfs 0.390 af
SubcatchmentPOST 1.3:	Runoff Area=13,558 sf 33.08% Impervious Runoff Depth=3.61" Flow Length=59' Slope=0.0430 '/' Tc=5.0 min CN=82 Runoff=1.34 cfs 0.094 af
SubcatchmentPOST 1.4:	Runoff Area=3,521 sf 22.47% Impervious Runoff Depth=3.32" Flow Length=33' Slope=0.0270 '/' Tc=5.0 min CN=79 Runoff=0.32 cfs 0.022 af
SubcatchmentPOST 1.5:	Runoff Area=16,946 sf 35.03% Impervious Runoff Depth=3.61" Flow Length=60' Slope=0.0520 '/' Tc=5.0 min CN=82 Runoff=1.68 cfs 0.117 af
SubcatchmentPOST 1.6:	Runoff Area=3,725 sf 0.00% Impervious Runoff Depth=2.84" Flow Length=37' Slope=0.0610 '/' Tc=5.0 min CN=74 Runoff=0.29 cfs 0.020 af
SubcatchmentPOST 1.7:	Runoff Area=3,900 sf 77.85% Impervious Runoff Depth=4.78" Flow Length=92' Slope=0.0350 '/' Tc=5.0 min CN=93 Runoff=0.48 cfs 0.036 af
Pond JFF 1: CONTECH JELLY FISH FILTER	Peak Elev=3.66' Inflow=3.57 cfs 0.307 af 18.0" Round Culvert n=0.013 L=7.0' S=0.0214 '/' Outflow=3.57 cfs 0.307 af
Pond JFF 2: CONTECH JELLY FISH FILTER	Peak Elev=3.77' Inflow=4.72 cfs 0.390 af 18.0" Round Culvert n=0.013 L=5.0' S=0.0200 '/' Outflow=4.72 cfs 0.390 af
Pond PDMH 12:	Peak Elev=2.99' Inflow=9.30 cfs 0.827 af 24.0" Round Culvert n=0.013 L=27.0' S=0.0704 '/' Outflow=9.30 cfs 0.827 af
Pond PDMH 2:	Peak Elev=3.47' Inflow=8.76 cfs 0.732 af 24.0" Round Culvert n=0.013 L=34.0' S=0.0029 '/' Outflow=8.76 cfs 0.732 af
Pond POND 1.1:	Peak Elev=6.02' Storage=3,031 cf Inflow=3.85 cfs 0.307 af Outflow=3.57 cfs 0.307 af
Pond POND 1.2:	Peak Elev=6.60' Storage=3,810 cf Inflow=4.96 cfs 0.390 af Outflow=4.72 cfs 0.390 af
Pond PP 1: POROUS PAVEMENT	Peak Elev=5.43' Storage=1,618 cf Inflow=1.34 cfs 0.094 af Outflow=0.52 cfs 0.076 af
Pond PP 2: POROUS PAVEMENT	Peak Elev=4.89' Storage=275 cf Inflow=0.32 cfs 0.022 af Outflow=0.17 cfs 0.019 af
Link PA1:	Inflow=11.20 cfs 0.964 af Primary=11.20 cfs 0.964 af

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Total Runoff Area = 2.537 ac Runoff Volume = 0.986 af Average Runoff Depth = 4.66"
25.86% Pervious = 0.656 ac 74.14% Impervious = 1.881 ac

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Type III 24-hr 10 Year Storm Rainfall=5.59"

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Summary for Subcatchment POST 1.1:

Runoff = 3.85 cfs @ 12.07 hrs, Volume= 0.307 af, Depth= 5.35"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
Type III 24-hr 10 Year Storm Rainfall=5.59"

Area (sf)	CN	Description
17,365	98	Roofs, HSG C
242	74	>75% Grass cover, Good, HSG C
12,371	98	Paved parking, HSG C
29,978	98	Weighted Average
242		0.81% Pervious Area
29,736		99.19% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
0.5	50	0.0400	1.69		Sheet Flow, Smooth surfaces n= 0.011 P2= 3.68"
0.3	64	0.0360	3.85		Shallow Concentrated Flow, Paved Kv= 20.3 fps
0.8	114	Total, Increased to minimum Tc = 5.0 min			

Summary for Subcatchment POST 1.2:

Runoff = 4.96 cfs @ 12.07 hrs, Volume= 0.390 af, Depth= 5.24"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
Type III 24-hr 10 Year Storm Rainfall=5.59"

Area (sf)	CN	Description
14,635	98	Roofs, HSG C
942	74	>75% Grass cover, Good, HSG C
23,324	98	Paved parking, HSG C
38,901	97	Weighted Average
942		2.42% Pervious Area
37,959		97.58% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
0.6	50	0.0300	1.51		Sheet Flow, Smooth surfaces n= 0.011 P2= 3.68"
0.2	35	0.0270	3.34		Shallow Concentrated Flow, Paved Kv= 20.3 fps
0.8	85	Total, Increased to minimum Tc = 5.0 min			

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Summary for Subcatchment POST 1.3:

Runoff = 1.34 cfs @ 12.08 hrs, Volume= 0.094 af, Depth= 3.61"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
Type III 24-hr 10 Year Storm Rainfall=5.59"

Area (sf)	CN	Description
200	98	Paved parking, HSG A
6	39	>75% Grass cover, Good, HSG A
9,067	74	>75% Grass cover, Good, HSG C
4,285	98	Paved parking, HSG C
13,558	82	Weighted Average
9,073		66.92% Pervious Area
4,485		33.08% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
4.4	59	0.0430	0.22		Sheet Flow, Grass: Short n= 0.150 P2= 3.68"
4.4	59	Total, Increased to minimum Tc = 5.0 min			

Summary for Subcatchment POST 1.4:

Runoff = 0.32 cfs @ 12.08 hrs, Volume= 0.022 af, Depth= 3.32"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
Type III 24-hr 10 Year Storm Rainfall=5.59"

Area (sf)	CN	Description
2,730	74	>75% Grass cover, Good, HSG C
791	98	Paved parking, HSG C
3,521	79	Weighted Average
2,730		77.53% Pervious Area
791		22.47% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
3.3	33	0.0270	0.16		Sheet Flow, Grass: Short n= 0.150 P2= 3.68"
3.3	33	Total, Increased to minimum Tc = 5.0 min			

Summary for Subcatchment POST 1.5:

Runoff = 1.68 cfs @ 12.08 hrs, Volume= 0.117 af, Depth= 3.61"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
Type III 24-hr 10 Year Storm Rainfall=5.59"

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Type III 24-hr 10 Year Storm Rainfall=5.59"

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Area (sf)	CN	Description
2,235	98	Paved parking, HSG A
311	39	>75% Grass cover, Good, HSG A
* 2,980	98	Rock embankment, HSG C
10,699	74	>75% Grass cover, Good, HSG C
721	98	Paved parking, HSG C
16,946	82	Weighted Average
11,010		64.97% Pervious Area
5,936		35.03% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
4.1	60	0.0520	0.24		Sheet Flow, Grass: Short n= 0.150 P2= 3.68"
4.1	60	Total, Increased to minimum Tc = 5.0 min			

Summary for Subcatchment POST 1.6:

Runoff = 0.29 cfs @ 12.08 hrs, Volume= 0.020 af, Depth= 2.84"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
Type III 24-hr 10 Year Storm Rainfall=5.59"

Area (sf)	CN	Description
3,725	74	>75% Grass cover, Good, HSG C
0	98	Paved parking, HSG C
3,725	74	Weighted Average
3,725		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
2.6	37	0.0610	0.23		Sheet Flow, Grass: Short n= 0.150 P2= 3.68"
2.6	37	Total, Increased to minimum Tc = 5.0 min			

Summary for Subcatchment POST 1.7:

Runoff = 0.48 cfs @ 12.07 hrs, Volume= 0.036 af, Depth= 4.78"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
Type III 24-hr 10 Year Storm Rainfall=5.59"

Area (sf)	CN	Description
864	74	>75% Grass cover, Good, HSG C
3,036	98	Paved parking, HSG C
3,900	93	Weighted Average
864		22.15% Pervious Area
3,036		77.85% Impervious Area

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Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
0.5	50	0.0350	1.61		Sheet Flow, Smooth surfaces n= 0.011 P2= 3.68"
0.2	42	0.0350	3.80		Shallow Concentrated Flow, Paved Kv= 20.3 fps
0.7	92	Total, Increased to minimum Tc = 5.0 min			

Summary for Pond JFF 1: CONTECH JELLY FISH FILTER

Inflow Area = 0.688 ac, 99.19% Impervious, Inflow Depth = 5.35" for 10 Year Storm event
 Inflow = 3.57 cfs @ 12.10 hrs, Volume= 0.307 af
 Outflow = 3.57 cfs @ 12.10 hrs, Volume= 0.307 af, Atten= 0%, Lag= 0.0 min
 Primary = 3.57 cfs @ 12.10 hrs, Volume= 0.307 af

Routing by Dyn-Stor-Ind method, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs

Peak Elev= 3.66' @ 12.13 hrs

Flood Elev= 11.50'

Device	Routing	Invert	Outlet Devices
#1	Primary	2.35'	18.0" Round Culvert L= 7.0' Ke= 0.500 Inlet / Outlet Invert= 2.35' / 2.20' S= 0.0214 '/' Cc= 0.900 n= 0.013, Flow Area= 1.77 sf

Primary OutFlow Max=2.79 cfs @ 12.10 hrs HW=3.60' TW=3.46' (Dynamic Tailwater)↑**1=Culvert** (Outlet Controls 2.79 cfs @ 2.40 fps)**Summary for Pond JFF 2: CONTECH JELLY FISH FILTER**

Inflow Area = 0.893 ac, 97.58% Impervious, Inflow Depth = 5.24" for 10 Year Storm event
 Inflow = 4.72 cfs @ 12.10 hrs, Volume= 0.390 af
 Outflow = 4.72 cfs @ 12.10 hrs, Volume= 0.390 af, Atten= 0%, Lag= 0.0 min
 Primary = 4.72 cfs @ 12.10 hrs, Volume= 0.390 af

Routing by Dyn-Stor-Ind method, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs

Peak Elev= 3.77' @ 12.12 hrs

Flood Elev= 11.25'

Device	Routing	Invert	Outlet Devices
#1	Primary	2.40'	18.0" Round Culvert L= 5.0' Ke= 0.500 Inlet / Outlet Invert= 2.40' / 2.30' S= 0.0200 '/' Cc= 0.900 n= 0.013, Flow Area= 1.77 sf

Primary OutFlow Max=3.86 cfs @ 12.10 hrs HW=3.70' TW=3.45' (Dynamic Tailwater)↑**1=Culvert** (Outlet Controls 3.86 cfs @ 3.17 fps)

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Summary for Pond PDMH 12:

Inflow Area = 2.063 ac, 84.59% Impervious, Inflow Depth = 4.81" for 10 Year Storm event
Inflow = 9.30 cfs @ 12.10 hrs, Volume= 0.827 af
Outflow = 9.30 cfs @ 12.10 hrs, Volume= 0.827 af, Atten= 0%, Lag= 0.0 min
Primary = 9.30 cfs @ 12.10 hrs, Volume= 0.827 af

Routing by Dyn-Stor-Ind method, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
Peak Elev= 2.99' @ 12.10 hrs
Flood Elev= 9.25'

Device	Routing	Invert	Outlet Devices
#1	Primary	1.60'	24.0" Round Culvert L= 27.0' Ke= 0.500 Inlet / Outlet Invert= 1.60' / -0.30' S= 0.0704 '/' Cc= 0.900 n= 0.013, Flow Area= 3.14 sf

Primary OutFlow Max=9.08 cfs @ 12.10 hrs HW=2.96' TW=0.00' (Dynamic Tailwater)
↑1=Culvert (Inlet Controls 9.08 cfs @ 3.98 fps)

Summary for Pond PDMH 2:

Inflow Area = 1.671 ac, 97.19% Impervious, Inflow Depth = 5.26" for 10 Year Storm event
Inflow = 8.76 cfs @ 12.10 hrs, Volume= 0.732 af
Outflow = 8.76 cfs @ 12.10 hrs, Volume= 0.732 af, Atten= 0%, Lag= 0.0 min
Primary = 8.76 cfs @ 12.10 hrs, Volume= 0.732 af

Routing by Dyn-Stor-Ind method, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
Peak Elev= 3.47' @ 12.10 hrs
Flood Elev= 10.30'

Device	Routing	Invert	Outlet Devices
#1	Primary	1.80'	24.0" Round Culvert L= 34.0' Ke= 0.500 Inlet / Outlet Invert= 1.80' / 1.70' S= 0.0029 '/' Cc= 0.900 n= 0.013, Flow Area= 3.14 sf

Primary OutFlow Max=8.43 cfs @ 12.10 hrs HW=3.45' TW=2.96' (Dynamic Tailwater)
↑1=Culvert (Outlet Controls 8.43 cfs @ 4.12 fps)

Summary for Pond POND 1.1:

Inflow Area = 0.688 ac, 99.19% Impervious, Inflow Depth = 5.35" for 10 Year Storm event
Inflow = 3.85 cfs @ 12.07 hrs, Volume= 0.307 af
Outflow = 3.57 cfs @ 12.10 hrs, Volume= 0.307 af, Atten= 7%, Lag= 1.8 min
Primary = 3.57 cfs @ 12.10 hrs, Volume= 0.307 af

Routing by Dyn-Stor-Ind method, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
Peak Elev= 6.02' @ 12.10 hrs Surf.Area= 2,496 sf Storage= 3,031 cf
Flood Elev= 7.25' Surf.Area= 2,496 sf Storage= 4,456 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)
Center-of-Mass det. time= 238.1 min (983.4 - 745.3)

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Volume	Invert	Avail.Storage	Storage Description
#1A	2.75'	0 cf	27.13'W x 92.00'L x 5.50'H Field A 13,726 cf Overall - 5,470 cf Embedded = 8,255 cf x 0.0% Voids
#2A	3.25'	4,566 cf	ADS N-12 48" x 16 Inside #1 Inside= 47.7"W x 47.7"H => 12.40 sf x 20.00'L = 248.0 cf Outside= 54.0"W x 54.0"H => 14.86 sf x 20.00'L = 297.1 cf 4 Rows of 4 Chambers 24.13' Header x 12.40 sf x 2 = 598.3 cf Inside
		4,566 cf	Total Available Storage

Storage Group A created with Chamber Wizard

Device	Routing	Invert	Outlet Devices
#1	Primary	3.00'	18.0" Round Culvert L= 7.0' Ke= 0.500 Inlet / Outlet Invert= 3.00' / 2.85' S= 0.0214 '/ Cc= 0.900 n= 0.013, Flow Area= 1.77 sf
#2	Device 1	3.00'	1.1" Vert. Orifice/Grate C= 0.600
#3	Device 1	5.60'	Custom Weir/Orifice, Cv= 2.62 (C= 3.28) Head (feet) 0.00 1.00 Width (feet) 4.00 4.00

Primary OutFlow Max=3.50 cfs @ 12.10 hrs HW=6.01' TW=3.60' (Dynamic Tailwater)

- 1=Culvert (Passes 3.50 cfs of 12.79 cfs potential flow)
- 2=Orifice/Grate (Orifice Controls 0.05 cfs @ 7.48 fps)
- 3=Custom Weir/Orifice (Weir Controls 3.45 cfs @ 2.10 fps)

Summary for Pond POND 1.2:

Inflow Area = 0.893 ac, 97.58% Impervious, Inflow Depth = 5.24" for 10 Year Storm event
 Inflow = 4.96 cfs @ 12.07 hrs, Volume= 0.390 af
 Outflow = 4.72 cfs @ 12.10 hrs, Volume= 0.390 af, Atten= 5%, Lag= 1.4 min
 Primary = 4.72 cfs @ 12.10 hrs, Volume= 0.390 af

Routing by Dyn-Stor-Ind method, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
 Peak Elev= 6.60' @ 12.10 hrs Surf.Area= 2,496 sf Storage= 3,810 cf
 Flood Elev= 7.25' Surf.Area= 2,496 sf Storage= 4,456 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)
 Center-of-Mass det. time= 267.0 min (1,019.6 - 752.6)

Volume	Invert	Avail.Storage	Storage Description
#1A	2.75'	0 cf	27.13'W x 92.00'L x 5.50'H Field A 13,726 cf Overall - 5,470 cf Embedded = 8,255 cf x 0.0% Voids
#2A	3.25'	4,566 cf	ADS N-12 48" x 16 Inside #1 Inside= 47.7"W x 47.7"H => 12.40 sf x 20.00'L = 248.0 cf Outside= 54.0"W x 54.0"H => 14.86 sf x 20.00'L = 297.1 cf 4 Rows of 4 Chambers 24.13' Header x 12.40 sf x 2 = 598.3 cf Inside
		4,566 cf	Total Available Storage

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Type III 24-hr 10 Year Storm Rainfall=5.59"

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Storage Group A created with Chamber Wizard

Device	Routing	Invert	Outlet Devices
#1	Primary	3.05'	18.0" Round Culvert L= 6.0' Ke= 0.500 Inlet / Outlet Invert= 3.05' / 2.90' S= 0.0250 '/ Cc= 0.900 n= 0.013, Flow Area= 1.77 sf
#2	Device 1	3.05'	1.1" Vert. Orifice/Grate C= 0.600
#3	Device 1	6.10'	Custom Weir/Orifice, Cv= 2.62 (C= 3.28) Head (feet) 0.00 1.00 Width (feet) 4.00 4.00

Primary OutFlow Max=4.61 cfs @ 12.10 hrs HW=6.59' TW=3.70' (Dynamic Tailwater)

- ↑ 1=Culvert (Passes 4.61 cfs of 14.22 cfs potential flow)
- ↑ 2=Orifice/Grate (Orifice Controls 0.05 cfs @ 8.19 fps)
- ↑ 3=Custom Weir/Orifice (Weir Controls 4.55 cfs @ 2.30 fps)

Summary for Pond PP 1: POROUS PAVEMENT

Inflow Area = 0.311 ac, 33.08% Impervious, Inflow Depth = 3.61" for 10 Year Storm event
 Inflow = 1.34 cfs @ 12.08 hrs, Volume= 0.094 af
 Outflow = 0.52 cfs @ 12.31 hrs, Volume= 0.076 af, Atten= 61%, Lag= 14.1 min
 Primary = 0.52 cfs @ 12.31 hrs, Volume= 0.076 af

Routing by Dyn-Stor-Ind method, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
 Peak Elev= 5.43' @ 12.31 hrs Surf.Area= 3,857 sf Storage= 1,618 cf
 Flood Elev= 8.80' Surf.Area= 3,857 sf Storage= 3,386 cf

Plug-Flow detention time= 164.6 min calculated for 0.076 af (81% of inflow)
 Center-of-Mass det. time= 90.1 min (900.9 - 810.8)

Volume	Invert	Avail.Storage	Storage Description	
#1	4.38'	3,386 cf	Custom Stage Data (Prismatic) Listed below (Recalc)	
Elevation (feet)	Surf.Area (sq-ft)	Voids (%)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)
4.38	3,857	0.0	0	0
5.95	3,857	40.0	2,422	2,422
6.95	3,857	10.0	386	2,808
7.45	3,857	30.0	579	3,386
7.80	3,857	0.0	0	3,386

Device	Routing	Invert	Outlet Devices
#1	Primary	4.88'	6.0" Vert. Underdrain C= 0.600
#2	Device 1	4.38'	10.000 in/hr Filter Media Infiltration over Surface area

Primary OutFlow Max=0.52 cfs @ 12.31 hrs HW=5.43' TW=2.49' (Dynamic Tailwater)

- ↑ 1=Underdrain (Orifice Controls 0.52 cfs @ 2.63 fps)
- ↑ 2=Filter Media Infiltration (Passes 0.52 cfs of 0.89 cfs potential flow)

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Type III 24-hr 10 Year Storm Rainfall=5.59"

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Summary for Pond PP 2: POROUS PAVEMENT

Inflow Area = 0.081 ac, 22.47% Impervious, Inflow Depth = 3.32" for 10 Year Storm event
 Inflow = 0.32 cfs @ 12.08 hrs, Volume= 0.022 af
 Outflow = 0.17 cfs @ 12.08 hrs, Volume= 0.019 af, Atten= 46%, Lag= 0.2 min
 Primary = 0.17 cfs @ 12.08 hrs, Volume= 0.019 af

Routing by Dyn-Stor-Ind method, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs
 Peak Elev= 4.89' @ 12.20 hrs Surf.Area= 755 sf Storage= 275 cf
 Flood Elev= 8.40' Surf.Area= 755 sf Storage= 640 cf

Plug-Flow detention time= 115.6 min calculated for 0.019 af (84% of inflow)
 Center-of-Mass det. time= 49.3 min (868.0 - 818.7)

Volume	Invert	Avail.Storage	Storage Description	
#1	3.98'	640 cf	Custom Stage Data (Prismatic) Listed below (Recalc)	
Elevation (feet)	Surf.Area (sq-ft)	Voids (%)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)
3.98	755	0.0	0	0
5.45	755	40.0	444	444
6.55	755	10.0	83	527
7.05	755	30.0	113	640
7.40	755	0.0	0	640

Device	Routing	Invert	Outlet Devices
#1	Primary	4.48'	6.0" Vert. Underdrain C= 0.600
#2	Device 1	3.98'	10.000 in/hr Filter Media Infiltration over Surface area

Primary OutFlow Max=0.17 cfs @ 12.08 hrs HW=4.79' TW=2.97' (Dynamic Tailwater)

- ↑1=Underdrain (Passes 0.17 cfs of 0.24 cfs potential flow)
- ↑2=Filter Media Infiltration (Exfiltration Controls 0.17 cfs)

Summary for Link PA1:

Inflow Area = 2.537 ac, 74.14% Impervious, Inflow Depth = 4.56" for 10 Year Storm event
 Inflow = 11.20 cfs @ 12.09 hrs, Volume= 0.964 af
 Primary = 11.20 cfs @ 12.09 hrs, Volume= 0.964 af, Atten= 0%, Lag= 0.0 min

Primary outflow = Inflow, Time Span= 0.00-48.00 hrs, dt= 0.04 hrs

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Type III 24-hr 25 Year Storm Rainfall=7.08"

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Time span=0.00-48.00 hrs, dt=0.04 hrs, 1201 points
 Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
 Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentPOST 1.1:	Runoff Area=29,978 sf 99.19% Impervious Runoff Depth=6.84" Flow Length=114' Tc=5.0 min CN=98 Runoff=4.88 cfs 0.392 af
SubcatchmentPOST 1.2:	Runoff Area=38,901 sf 97.58% Impervious Runoff Depth=6.72" Flow Length=85' Tc=5.0 min CN=97 Runoff=6.31 cfs 0.500 af
SubcatchmentPOST 1.3:	Runoff Area=13,558 sf 33.08% Impervious Runoff Depth=4.99" Flow Length=59' Slope=0.0430 '/' Tc=5.0 min CN=82 Runoff=1.83 cfs 0.129 af
SubcatchmentPOST 1.4:	Runoff Area=3,521 sf 22.47% Impervious Runoff Depth=4.66" Flow Length=33' Slope=0.0270 '/' Tc=5.0 min CN=79 Runoff=0.45 cfs 0.031 af
SubcatchmentPOST 1.5:	Runoff Area=16,946 sf 35.03% Impervious Runoff Depth=4.99" Flow Length=60' Slope=0.0520 '/' Tc=5.0 min CN=82 Runoff=2.29 cfs 0.162 af
SubcatchmentPOST 1.6:	Runoff Area=3,725 sf 0.00% Impervious Runoff Depth=4.11" Flow Length=37' Slope=0.0610 '/' Tc=5.0 min CN=74 Runoff=0.42 cfs 0.029 af
SubcatchmentPOST 1.7:	Runoff Area=3,900 sf 77.85% Impervious Runoff Depth=6.25" Flow Length=92' Slope=0.0350 '/' Tc=5.0 min CN=93 Runoff=0.62 cfs 0.047 af
Pond JFF 1: CONTECH JELLY FISH FILTER	Peak Elev=4.03' Inflow=4.59 cfs 0.392 af 18.0" Round Culvert n=0.013 L=7.0' S=0.0214 '/' Outflow=4.59 cfs 0.392 af
Pond JFF 2: CONTECH JELLY FISH FILTER	Peak Elev=4.20' Inflow=6.06 cfs 0.500 af 18.0" Round Culvert n=0.013 L=5.0' S=0.0200 '/' Outflow=6.06 cfs 0.500 af
Pond PDMH 12:	Peak Elev=3.24' Inflow=12.03 cfs 1.079 af 24.0" Round Culvert n=0.013 L=27.0' S=0.0704 '/' Outflow=12.03 cfs 1.079 af
Pond PDMH 2:	Peak Elev=3.77' Inflow=11.24 cfs 0.939 af 24.0" Round Culvert n=0.013 L=34.0' S=0.0029 '/' Outflow=11.24 cfs 0.939 af
Pond POND 1.1:	Peak Elev=6.09' Storage=3,139 cf Inflow=4.88 cfs 0.392 af Outflow=4.59 cfs 0.392 af
Pond POND 1.2:	Peak Elev=6.70' Storage=3,920 cf Inflow=6.31 cfs 0.500 af Outflow=6.06 cfs 0.500 af
Pond PP 1: POROUS PAVEMENT	Peak Elev=5.73' Storage=2,084 cf Inflow=1.83 cfs 0.129 af Outflow=0.73 cfs 0.112 af
Pond PP 2: POROUS PAVEMENT	Peak Elev=5.23' Storage=379 cf Inflow=0.45 cfs 0.031 af Outflow=0.17 cfs 0.028 af
Link PA1:	Inflow=14.68 cfs 1.270 af Primary=14.68 cfs 1.270 af

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Type III 24-hr 25 Year Storm Rainfall=7.08"

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Total Runoff Area = 2.537 ac Runoff Volume = 1.291 af Average Runoff Depth = 6.11"
25.86% Pervious = 0.656 ac 74.14% Impervious = 1.881 ac

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Type III 24-hr 50 Year Storm Rainfall=8.48"

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Time span=0.00-48.00 hrs, dt=0.04 hrs, 1201 points
 Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
 Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentPOST 1.1:	Runoff Area=29,978 sf 99.19% Impervious Runoff Depth=8.24" Flow Length=114' Tc=5.0 min CN=98 Runoff=5.85 cfs 0.473 af
SubcatchmentPOST 1.2:	Runoff Area=38,901 sf 97.58% Impervious Runoff Depth=8.12" Flow Length=85' Tc=5.0 min CN=97 Runoff=7.57 cfs 0.604 af
SubcatchmentPOST 1.3:	Runoff Area=13,558 sf 33.08% Impervious Runoff Depth=6.32" Flow Length=59' Slope=0.0430 '/' Tc=5.0 min CN=82 Runoff=2.29 cfs 0.164 af
SubcatchmentPOST 1.4:	Runoff Area=3,521 sf 22.47% Impervious Runoff Depth=5.96" Flow Length=33' Slope=0.0270 '/' Tc=5.0 min CN=79 Runoff=0.57 cfs 0.040 af
SubcatchmentPOST 1.5:	Runoff Area=16,946 sf 35.03% Impervious Runoff Depth=6.32" Flow Length=60' Slope=0.0520 '/' Tc=5.0 min CN=82 Runoff=2.87 cfs 0.205 af
SubcatchmentPOST 1.6:	Runoff Area=3,725 sf 0.00% Impervious Runoff Depth=5.36" Flow Length=37' Slope=0.0610 '/' Tc=5.0 min CN=74 Runoff=0.55 cfs 0.038 af
SubcatchmentPOST 1.7:	Runoff Area=3,900 sf 77.85% Impervious Runoff Depth=7.64" Flow Length=92' Slope=0.0350 '/' Tc=5.0 min CN=93 Runoff=0.74 cfs 0.057 af
Pond JFF 1: CONTECH JELLY FISH FILTER	Peak Elev=4.52' Inflow=5.54 cfs 0.473 af 18.0" Round Culvert n=0.013 L=7.0' S=0.0214 '/' Outflow=5.54 cfs 0.473 af
Pond JFF 2: CONTECH JELLY FISH FILTER	Peak Elev=4.70' Inflow=7.33 cfs 0.604 af 18.0" Round Culvert n=0.013 L=5.0' S=0.0200 '/' Outflow=7.33 cfs 0.604 af
Pond PDMH 12:	Peak Elev=3.50' Inflow=14.51 cfs 1.316 af 24.0" Round Culvert n=0.013 L=27.0' S=0.0704 '/' Outflow=14.51 cfs 1.316 af
Pond PDMH 2:	Peak Elev=4.22' Inflow=13.58 cfs 1.134 af 24.0" Round Culvert n=0.013 L=34.0' S=0.0029 '/' Outflow=13.58 cfs 1.134 af
Pond POND 1.1:	Peak Elev=6.16' Storage=3,231 cf Inflow=5.85 cfs 0.473 af Outflow=5.54 cfs 0.473 af
Pond POND 1.2:	Peak Elev=6.78' Storage=4,012 cf Inflow=7.57 cfs 0.604 af Outflow=7.33 cfs 0.604 af
Pond PP 1: POROUS PAVEMENT	Peak Elev=6.14' Storage=2,496 cf Inflow=2.29 cfs 0.164 af Outflow=0.89 cfs 0.146 af
Pond PP 2: POROUS PAVEMENT	Peak Elev=6.18' Storage=499 cf Inflow=0.57 cfs 0.040 af Outflow=0.17 cfs 0.037 af
Link PA1:	Inflow=17.85 cfs 1.559 af Primary=17.85 cfs 1.559 af

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Type III 24-hr 50 Year Storm Rainfall=8.48"

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Total Runoff Area = 2.537 ac Runoff Volume = 1.581 af Average Runoff Depth = 7.48"
25.86% Pervious = 0.656 ac 74.14% Impervious = 1.881 ac

Section 4

Peak Rate Comparison

The following table summarizes and compares the pre- and post-development peak runoff rates from the 2-year, 10-year, 25-year and 50-year storm events at the point of analysis.

Table 4.1
Comparison of Pre- and Post-Development Flows (CFS)

	2-Year Storm	10-Year Storm	25-Year Storm	50-Year Storm
Pre-Development Watershed				
PA-1	7.82	12.94	16.90	20.59
Post-Development Watershed				
PA-1	6.28	11.20	14.68	17.85

Section 5 Mitigation Description

The stormwater management system has been designed to provide stormwater treatment as required by the City of Portsmouth Site Review Regulations and NHDES AoT Regulations (Env-Wq 1500).

5.1 Pre-Treatment Methods for Protecting Water Quality

Pre-treatment for the stormwater filtration systems consist of deep sump catch basins.

5.2 Treatment Methods for Protecting Water Quality.

The runoff from proposed impervious areas will be treated by various Contech Jellyfish stormwater filtration systems. These Jellyfish systems are sized to treat the Water Quality Flows of their respective subcatchment areas. Each system is outfitted with an internal bypass that diverts peak flows away from treatment. The BMP worksheet for these treatment practices have been included in Section 5 of this report.

The multiuse path along the North Mill Pond will be constructed as lined porous pavement with and underdrain. The underdrain will discharge to the closed drainage system prior to discharging to the Pond.

BMP	Total Suspended Solids	Total Nitrogen	Total Phosphorus
Jellyfish Filter w/Pretreatment ¹	91%	53%	61%
Porous Pavement w/Underdrain ²	90%	10%	45%

1. Pollutant removal calculations for Jellyfish Filter with deep sump catchbasin pretreatment shown in Table 4.2.
2. Pollutant removal efficiencies from NH Stormwater Manual Volume 2, Appendix B.

Table 5.2 – Pollutant Removal Calculations				
Contech Jellyfish Filter				
BMP	TSS Removal Rate	Starting TSS Load	TSS Removed	Remaining TSS Load
Deep Sump Catchbasin w/Hood ¹	0.15	1.00	0.15	0.85
Jellyfish Filter ²	0.89	0.85	0.76	0.09
Total Suspended Solids Removed:				91%
	TN Removal Rate	Starting TN Load	TN Removed	Remaining TN Load
Deep Sump Catchbasin w/Hood ¹	0.05	1.00	0.05	0.95
Jellyfish Filter ²	0.51	0.95	0.48	0.47
Total Nitrogen Removed:				53%
	TP Removal Rate	Starting TP Load	TP Removed	Remaining TP Load
Deep Sump Catchbasin w/Hood ¹	0.05	1.00	0.05	0.95
Jellyfish Filter ²	0.59	0.95	0.56	0.39
Total Phosphorus Removed:				61%

1. Pollutant removal efficiencies from NH Stormwater Manual Volume 2, Appendix E.
2. Pollutant removal efficiencies from Contech Engineered Solutions, Jellyfish Filter Stormwater Treatment performance testing results.

Section 6

BMP Worksheets and Sizing Memos



CONTECH Stormwater Solutions Inc. Engineer:
Date Prepared:

DRA
3/16/2021

Site Information

Project Name	Proposed Mixed Use Dev - JFF1
Project State	NH
Project City	Portsmouth
Total Drainage Area, Ad	0.87 ac
Post Development Impervious Area, Ai	0.68 ac
Pervious Area, Ap	0.19 ac
% Impervious	78%
Runoff Coefficient, Rc	0.75

Mass Loading Calculations

Mean Annual Rainfall, P	50 in
Agency Required % Removal	80%
Percent Runoff Capture	90%
Mean Annual Runoff, Vt	107076 ft ³
Event Mean Concentration of Pollutant, EMC	75 mg/l
Annual Mass Load, M total	501.04 lbs

Filter System

Filtration Brand	Jelly Fish
Cartridge Length	54 in

Jelly Fish Sizing

Mass to be Captured by System	400.83 lbs
Water Quality Flow	0.66 cfs

Method to Use

FLOW BASED

Summary

Flow	Treatment Flow Rate	0.80 cfs
	Required Size	JFPD0806-4-1



CONTECH Stormwater Solutions Inc. Engineer:
Date Prepared:

DRA
3/16/2021

Site Information

Project Name	Proposed Mixed Use Dev- JFF2
Project State	NH
Project City	Portsmouth
Total Drainage Area, Ad	0.86 ac
Post Development Impervious Area, Ai	0.83 ac
Pervious Area, Ap	0.03 ac
% Impervious	97%
Runoff Coefficient, Rc	0.92

Mass Loading Calculations

Mean Annual Rainfall, P	50 in
Agency Required % Removal	80%
Percent Runoff Capture	90%
Mean Annual Runoff, Vt	129047 ft ³
Event Mean Concentration of Pollutant, EMC	75 mg/l
Annual Mass Load, M total	603.85 lbs

Filter System

Filtration Brand	Jelly Fish
Cartridge Length	54 in

Jelly Fish Sizing

Mass to be Captured by System	483.08 lbs
Water Quality Flow	0.79 cfs

Method to Use

FLOW BASED

Summary

Flow	Treatment Flow Rate	0.80 cfs
	Required Size	JFPD0806-4-1



FILTRATION PRACTICE DESIGN CRITERIA (Env-Wq 1508.07)

Type/Node Name: _____

PP-1

Enter the type of filtration practice (e.g., bioretention system) and the node name in the drainage analysis, if applicable.

		Check if you reviewed the restrictions on unlined systems outlined in Env-Wq 1508.07(a).	
0.31	ac	A = Area draining to the practice	
0.10	ac	A _I = Impervious area draining to the practice	
0.33	decimal	I = Percent impervious area draining to the practice, in decimal form	
0.35	unitless	R _v = Runoff coefficient = 0.05 + (0.9 x I)	
0.11	ac-in	WQV = 1" x R _v x A	
390	cf	WQV conversion (ac-in x 43,560 sf/ac x 1ft/12")	
97	cf	25% x WQV (check calc for sediment forebay volume)	
292	cf	75% x WQV (check calc for surface sand filter volume)	
		Method of Pretreatment? (not required for clean or roof runoff)	
	cf	V _{SED} = Sediment forebay volume, if used for pretreatment	≥ 25%WQV
Calculate time to drain if system IS NOT underdrained:			
3,857	sf	A _{SA} = Surface area of the practice	
	iph	K _{sat} _{DESIGN} = Design infiltration rate ¹	
		If K _{sat} (prior to factor of safety) is < 0.50 iph, has an underdrain been provided? (Use the calculations below)	
YES	Yes/No		
-	hours	T _{DRAIN} = Drain time = V / (A _{SA} * I _{DESIGN})	≤ 72-hrs
Calculate time to drain if system IS underdrained:			
5.05	ft	E _{WQV} = Elevation of WQV (attach stage-storage table)	
0.08	cfs	Q _{WQV} = Discharge at the E _{WQV} (attach stage-discharge table)	
2.71	hours	T _{DRAIN} = Drain time = 2WQV/Q _{WQV}	≤ 72-hrs
5.45	feet	E _{FC} = Elevation of the bottom of the filter course material ²	
4.48	feet	E _{UD} = Invert elevation of the underdrain (UD), if applicable	
	feet	E _{SHWT} = Elevation of SHWT (if none found, enter the lowest elevation of the test pit)	
	feet	E _{ROCK} = Elevation of bedrock (if none found, enter the lowest elevation of the test pit)	
0.97	feet	D _{FC to UD} = Depth to UD from the bottom of the filter course	≥ 1'
5.45	feet	D _{FC to ROCK} = Depth to bedrock from the bottom of the filter course	≥ 1'
5.45	feet	D _{FC to SHWT} = Depth to SHWT from the bottom of the filter course	≥ 1'
6.14	ft	Peak elevation of the 50-year storm event (infiltration can be used in analysis)	
7.80	ft	Elevation of the top of the practice	
YES		50 peak elevation ≤ Elevation of the top of the practice	← yes
If a surface sand filter or underground sand filter is proposed:			
YES	ac	Drainage Area check.	< 10 ac
	cf	V = Volume of storage ³ (attach a stage-storage table)	≥ 75%WQV
	inches	D _{FC} = Filter course thickness	18", or 24" if within GPA
Sheet		Note what sheet in the plan set contains the filter course specification.	
Yes/No		Access grate provided?	← yes

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Type III 24-hr 50 Year Storm Rainfall=8.48"

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Stage-Area-Storage for Pond PP 1: POROUS PAVEMENT

Elevation (feet)	Surface (sq-ft)	Storage (cubic-feet)	Elevation (feet)	Surface (sq-ft)	Storage (cubic-feet)
4.38	3,857	0	6.98	3,857	2,843
4.43	3,857	77	7.03	3,857	2,900
4.48	3,857	154	7.08	3,857	2,958
4.53	3,857	231	7.13	3,857	3,016
4.58	3,857	309	7.18	3,857	3,074
4.63	3,857	386	7.23	3,857	3,132
4.68	3,857	463	7.28	3,857	3,190
4.73	3,857	540	7.33	3,857	3,248
4.78	3,857	617	7.38	3,857	3,305
4.83	3,857	694	7.43	3,857	3,363
4.88	3,857	771	7.48	3,857	3,386
4.93	3,857	849	7.53	3,857	3,386
4.98	3,857	926	7.58	3,857	3,386
5.03	3,857	1,003	7.63	3,857	3,386
5.08	3,857	1,080	7.68	3,857	3,386
5.13	3,857	1,157	7.73	3,857	3,386
5.18	3,857	1,234	7.78	3,857	3,386
5.23	3,857	1,311	7.83	3,857	3,386
5.28	3,857	1,389	7.88	3,857	3,386
5.33	3,857	1,466	7.93	3,857	3,386
5.38	3,857	1,543	7.98	3,857	3,386
5.43	3,857	1,620	8.03	3,857	3,386
5.48	3,857	1,697	8.08	3,857	3,386
5.53	3,857	1,774	8.13	3,857	3,386
5.58	3,857	1,851	8.18	3,857	3,386
5.63	3,857	1,929	8.23	3,857	3,386
5.68	3,857	2,006	8.28	3,857	3,386
5.73	3,857	2,083	8.33	3,857	3,386
5.78	3,857	2,160	8.38	3,857	3,386
5.83	3,857	2,237	8.43	3,857	3,386
5.88	3,857	2,314	8.48	3,857	3,386
5.93	3,857	2,391	8.53	3,857	3,386
5.98	3,857	2,434	8.58	3,857	3,386
6.03	3,857	2,453	8.63	3,857	3,386
6.08	3,857	2,472	8.68	3,857	3,386
6.13	3,857	2,492	8.73	3,857	3,386
6.18	3,857	2,511	8.78	3,857	3,386
6.23	3,857	2,530			
6.28	3,857	2,549			
6.33	3,857	2,569			
6.38	3,857	2,588			
6.43	3,857	2,607			
6.48	3,857	2,627			
6.53	3,857	2,646			
6.58	3,857	2,665			
6.63	3,857	2,684			
6.68	3,857	2,704			
6.73	3,857	2,723			
6.78	3,857	2,742			
6.83	3,857	2,762			
6.88	3,857	2,781			
6.93	3,857	2,800			



FILTRATION PRACTICE DESIGN CRITERIA (Env-Wq 1508.07)

Type/Node Name: _____

Enter the type of filtration practice (e.g., bioretention system) and the node name in the drainage analysis, if applicable.

		Check if you reviewed the restrictions on unlined systems outlined in Env-Wq 1508.07(a).	
0.08	ac	A = Area draining to the practice	
0.02	ac	A _I = Impervious area draining to the practice	
0.23	decimal	I = Percent impervious area draining to the practice, in decimal form	
0.25	unitless	Rv = Runoff coefficient = 0.05 + (0.9 x I)	
0.02	ac-in	WQV = 1" x Rv x A	
73	cf	WQV conversion (ac-in x 43,560 sf/ac x 1ft/12")	
18	cf	25% x WQV (check calc for sediment forebay volume)	
55	cf	75% x WQV (check calc for surface sand filter volume)	
		Method of Pretreatment? (not required for clean or roof runoff)	
	cf	V _{SED} = Sediment forebay volume, if used for pretreatment	≥ 25%WQV
Calculate time to drain if system IS NOT underdrained:			
755	sf	A _{SA} = Surface area of the practice	
	iph	K _{sat} _{DESIGN} = Design infiltration rate ¹	
		If K _{sat} (prior to factor of safety) is < 0.50 iph, has an underdrain been provided? (Use the calculations below)	
YES	Yes/No		
-	hours	T _{DRAIN} = Drain time = V / (A _{SA} * I _{DESIGN})	≤ 72-hrs
Calculate time to drain if system IS underdrained:			
4.75	ft	E _{WQV} = Elevation of WQV (attach stage-storage table)	
0.17	cfs	Q _{WQV} = Discharge at the E _{WQV} (attach stage-discharge table)	
0.24	hours	T _{DRAIN} = Drain time = 2WQV/Q _{WQV}	≤ 72-hrs
5.95	feet	E _{FC} = Elevation of the bottom of the filter course material ²	
4.88	feet	E _{UD} = Invert elevation of the underdrain (UD), if applicable	
	feet	E _{SHWT} = Elevation of SHWT (if none found, enter the lowest elevation of the test pit)	
	feet	E _{ROCK} = Elevation of bedrock (if none found, enter the lowest elevation of the test pit)	
1.07	feet	D _{FC to UD} = Depth to UD from the bottom of the filter course	≥ 1'
5.95	feet	D _{FC to ROCK} = Depth to bedrock from the bottom of the filter course	≥ 1'
5.95	feet	D _{FC to SHWT} = Depth to SHWT from the bottom of the filter course	≥ 1'
6.18	ft	Peak elevation of the 50-year storm event (infiltration can be used in analysis)	
7.40	ft	Elevation of the top of the practice	
YES		50 peak elevation ≤ Elevation of the top of the practice	← yes
If a surface sand filter or underground sand filter is proposed:			
YES	ac	Drainage Area check.	< 10 ac
	cf	V = Volume of storage ³ (attach a stage-storage table)	≥ 75%WQV
	inches	D _{FC} = Filter course thickness	18", or 24" if within GPA
Sheet		Note what sheet in the plan set contains the filter course specification.	
Yes/No		Access grate provided?	← yes

P-0595-007 POST

Prepared by Tighe & Bond

HydroCAD® 10.00-20 s/n 03436 © 2017 HydroCAD Software Solutions LLC

Type III 24-hr 50 Year Storm Rainfall=8.48"

Printed 3/19/2021

Stage-Area-Storage for Pond PP 2: POROUS PAVEMENT

Elevation (feet)	Surface (sq-ft)	Storage (cubic-feet)	Elevation (feet)	Surface (sq-ft)	Storage (cubic-feet)
3.98	755	0	6.58	755	534
4.03	755	15	6.63	755	545
4.08	755	30	6.68	755	556
4.13	755	45	6.73	755	568
4.18	755	60	6.78	755	579
4.23	755	76	6.83	755	590
4.28	755	91	6.88	755	602
4.33	755	106	6.93	755	613
4.38	755	121	6.98	755	624
4.43	755	136	7.03	755	636
4.48	755	151	7.08	755	640
4.53	755	166	7.13	755	640
4.58	755	181	7.18	755	640
4.63	755	196	7.23	755	640
4.68	755	211	7.28	755	640
4.73	755	227	7.33	755	640
4.78	755	242	7.38	755	640
4.83	755	257	7.43	755	640
4.88	755	272	7.48	755	640
4.93	755	287	7.53	755	640
4.98	755	302	7.58	755	640
5.03	755	317	7.63	755	640
5.08	755	332	7.68	755	640
5.13	755	347	7.73	755	640
5.18	755	362	7.78	755	640
5.23	755	378	7.83	755	640
5.28	755	393	7.88	755	640
5.33	755	408	7.93	755	640
5.38	755	423	7.98	755	640
5.43	755	438	8.03	755	640
5.48	755	446	8.08	755	640
5.53	755	450	8.13	755	640
5.58	755	454	8.18	755	640
5.63	755	458	8.23	755	640
5.68	755	461	8.28	755	640
5.73	755	465	8.33	755	640
5.78	755	469	8.38	755	640
5.83	755	473			
5.88	755	476			
5.93	755	480			
5.98	755	484			
6.03	755	488			
6.08	755	492			
6.13	755	495			
6.18	755	499			
6.23	755	503			
6.28	755	507			
6.33	755	510			
6.38	755	514			
6.43	755	518			
6.48	755	522			
6.53	755	525			

Section 7 Long-Term Operation & Maintenance Plan

It is the intent of this Operation and Maintenance Plan to identify the areas of this site that need special attention and consideration, as well as implementing a plan to assure routine maintenance. By identifying the areas of concern as well as implementing a frequent and routine maintenance schedule the site will maintain a high-quality stormwater runoff.

7.1 Contact/Responsible Party

Maintenance Area	Contact/Responsible Party
Development Site	North Mill Pond Holdings, LLC 1359 Hooksett Road Hooksett NH, 03106
North Mill Pond Trail (City Easement)	City of Portsmouth DPW 680 Peverly Hill Road Portsmouth, NH 03801

(Note: The contact information for the Contact/Responsible Party shall be kept current. If ownership changes, the Operation and Maintenance Plan must be transferred to the new party.)

7.2 Maintenance Items

Maintenance of the following items shall be recorded:

- Litter/Debris Removal
- Landscaping
- Catchbasin Cleaning
- Pavement Sweeping
- Contech Jellyfish Filtration System
- Porous Pavement

The following maintenance items and schedule represent the minimum action required. Periodic site inspections shall be conducted, and all measures must be maintained in effective operating condition. The following items shall be observed during site inspection and maintenance:

- Inspect vegetated areas, particularly slopes and embankments for areas of erosion. Replant and restore as necessary
- Inspect catch basins for sediment buildup
- Inspect site for trash and debris

7.3 Overall Site Operation & Maintenance Schedule

Maintenance Item	Frequency of Maintenance	Responsible Party
Litter/Debris Removal	Weekly	North Mill Pond Holdings, LLC
Pavement Sweeping - Sweep impervious areas to remove sand and litter.	Annually	North Mill Pond Holdings, LLC
Landscaping - Landscaped islands to be maintained and mulched.	Maintained as required and mulched each Spring	North Mill Pond Holdings, LLC
Catch Basin (CB) Cleaning - CB to be cleaned of solids and oils.	Annually	North Mill Pond Holdings, LLC
Jelly Fish Units	In accordance with Manufacturer's Recommendations	North Mill Pond Holdings, LLC
Underground Detention Basin - Visual observation of sediment levels within system	Annually	North Mill Pond Holdings, LLC
Porous Pavement - Clean using a vacuum sweeper	Bi-Annually	City of Portsmouth DPW

Contech Jellyfish Filter System Inspection/Maintenance Requirements		
Inspection/ Maintenance	Frequency	Action
Inspect vault for sediment build up, static water, plugged media and bypass condition	One (1) time annually and after any rainfall event exceeding 2.5" in a 24-hr period	Maintenance required for any of the following: <ul style="list-style-type: none"> - >4" of sediment on the vault floor - >1/4" of sediment on top of the cartridge - .4" of static water above the cartridge bottom more than 24 hours after a rain event - If pore space between media is absent. - If vault is in bypass condition during an average rainfall event.
Replace Cartridges	As required by inspection, 1-5 years.	<ul style="list-style-type: none"> - Remove filter cartridges per manufacturer methods. - Vacuum sediment from vault. - Install new cartridges per manufacturer methods

Porous Asphalt Inspection/Maintenance Requirements		
Inspection/ Maintenance	Frequency	Action
Monitor for sediment build up, particularly in the winter.	Two (2) – Four (4) Times Annually.	- Clean with vacuum sweeper, bi-annually - Loose debris such as leaves or can be removed using a power/leaf blower or gutter broom. Fall and spring cleanup should be accompanied by pavement vacuuming.
Inspect Adjacent Vegetation	Two (2) – Four (4) Times Annually.	- Repair or replace any eroded areas.
Inspect for standing water -Within 30 minutes following a rain event.	One (1) – Two (2) Times Annually	- Use of a power washer or compressed air blower at an angle of 30 degrees or less can be effective, vacuum or vacuum sweeper if necessary.
Damage to pavement	As needed	- Repairs should be made as identified.

Additional Porous Asphalt Operation and Maintenance Requirements:

- ***No winter sanding or salting of porous pavements is permitted***
- Watering plants as necessary during the first growing season.
- Never reseal or repave with impermeable materials.
- Inspect annually for pavement deterioration or spalling.
- Monitor periodically to ensure the pavement surface drains effectively after storms.

7.3.1 Disposal Requirements

Disposal of debris, trash, sediment and other waste material should be done at suitable disposal/recycling sites and in compliance with all applicable local, state and federal waste regulations.

7.3.2 Snow & Ice Management for Standard Asphalt and Walkways

Snow storage areas shall be located such that no direct untreated discharges are possible to receiving waters from the storage site (snow storage areas have been shown on the Site Plan). The property manager will be responsible for timely snow removal from all private sidewalks, driveways, and parking areas. Snow removal will be hauled off-site and legally disposed of when snowbanks exceed 6 feet in height. Salt storage areas shall be covered or located such that no direct untreated discharges are possible to receiving waters from the storage site. Salt and sand shall be used to the minimum extent practical (refer to the attached for de-icing application rate guideline from the New Hampshire Stormwater Management Manual, Volume 2,).

7.4 Chloride Management Plan

Winter Operational Guidelines

The following Chloride Management Plan is for the Raynes Avenue, Mixed Use Development in Portsmouth, New Hampshire. The Plan includes operational guidelines including winter operator certification requirements, weather monitoring, equipment calibration requirements, mechanical removal, and salt usage evaluation and monitoring. Due to the evolving nature of chloride management efforts, the Chlorides Management Plan will be reviewed annually, in advance of the winter season, to reflect the current management standards.

7.4.1 Background Information

The Raynes Avenue, Mixed Use Development is located along the North Mill Pond in Portsmouth, New Hampshire.

7.4.2 Operational Guidelines – Chloride Management

All private contractors engaged at the development site for the purposes of winter operational snow removal and surface maintenance, are responsible for assisting in meeting compliance for the following protocols. Private contractors are expected to minimize the effects of the use of de-icing, anti-icing and pretreatment materials by adhering to the strict guidelines outlined below.

The winter operational de-icing, anti-icing and pretreatment materials will adhere to the following protocols:

7.4.2.1 Winter Operator Certification Requirements

All private contractors engaged at the premises for the purpose of winter operational snow removal and surface maintenance must be current UNHT2 Green SnowPro Certified operators or equivalent and will use only pre-approved

methods for spreading abrasives on private roadways and parking lots. All private contractors engaged at the premises for the purpose of winter operational snow removal and surface maintenance shall provide to the property management two copies of the annual UNHT2 Green SnowPro certificate or equivalent for each operator utilized on the premises. The annual UNHT2 Green SnowPro certificate or equivalent for each operator will be available on file in the Facilities Management office and be present in the vehicle/carrier at all times.

7.4.2.2 Improved Weather Monitoring

The property manager will coordinate weather information for use by winter maintenance contractors. This information in conjunction with site specific air/ground surface temperature monitoring will ensure that private contractors engaged at the premises for the purpose of winter operational snow removal and surface maintenance will make more informed decisions as to when and to what extent de-icing, anti-icing and pretreatment materials are applied to private roadways, sidewalks, and parking lots.

7.4.2.3 Equipment Calibration Requirements

All equipment utilized on the premises for the purpose of winter operational snow removal and surface maintenance will conform to the following calibration requirements.

7.4.2.3.1 Annual Calibration Requirements

All private contractors engaged at the premises for the purpose of winter operational snow removal and surface maintenance shall provide two copies of the annual calibration report for each piece of equipment utilized on the premises. Each calibration report shall include the vehicle/carrier VIN number and the serial numbers for each component including, but not limited to, spreader control units, salt aggregate spreader equipment, brining/pre-wetting equipment, ground speed orientation unit, and air/ground surface temperature monitor. Annual calibration reports will be available on file in the Facilities Management office and be present in the vehicle/carrier at all times.

Prior to each use, each vehicle/carrier operator will perform a systems check to verify that unit settings remain within the guidelines established by the Management Team in order to accurately dispense material. All private contractors engaged at the premises for the purpose of winter operational snow removal and surface maintenance will be subject to spot inspections by members of the Property Management Team to ensure that each vehicle/carrier is operating in a manner consistent with the guidelines set herein or State and Municipal regulations. All units will be recalibrated, and the updated calibration reports will be provided each time repairs or maintenance procedures affect the hydraulic system of the vehicle/carrier.

7.4.2.4 Increased Mechanical Removal Capabilities

All private contractors engaged at the premises will endeavor to use mechanical removal means on a more frequent basis for roadways, parking lots and sidewalks. Dedicating more manpower and equipment to increase snow removal frequencies prevents the buildup of snow and the corresponding need for de-icing, anti-icing and pretreatment materials. Shortened maintenance routes, with shorter service intervals, will be used to stay ahead of snowfall. Minimized snow and ice packing will reduce the need for abrasives, salt aggregates, and/or brining solution to restore surfaces back to bare surface states after winter precipitation events.

After storm events the management team will be responsible for having the streets swept to recapture un-melted de-icing materials, when practical.

7.4.3 Salt Usage Evaluation and Monitoring

All private contractors engaged at the premises for the purpose of winter operational snow removal and surface maintenance shall provide two copies of a storm report, which includes detailed information regarding treatment areas and the use of de-icing, anti-icing and pretreatment materials applied for the removal of snow and surface maintenance on the premises. The property manager will maintain copies of Summary Documents, including copies of the Storm Reports, operator certifications, equipment used for roadway and sidewalk winter maintenance, calibration reports and amount of de-icing materials used.

7.4.4 Summary

The above-described methodologies are incorporated into the Operational Manual and are to be used to qualify and retain all private contractors engaged at the Raynes Avenue premises for the purpose of winter operational snow removal and surface maintenance. This section of the Manual is intended to be an adaptive management document that is modified as required based on experience gained from past practices and technological advancements that reflect chloride BMP standards. All employees directly involved with winter operational activities are required to review this document and the current standard Best Management Practices published by the UNH Technology Transfer (T2) program annually. All employees directly involved with winter operational activities, and all private contractors engaged at the premises for the purposes of winter operational snow removal and surface maintenance, must be current UNHT2 Green SnowPro Certified operators or equivalent and undergo the necessary requirements to maintain this certification annually.

Deicing Application Rate Guidelines

24' of pavement (typical two-lane road)

These rates are not fixed values, but rather the middle of a range to be selected and adjusted by an agency according to its local conditions and experience.

Pavement Temp. (°F) and Trend (↑↓)	Weather Condition	Maintenance Actions	Pounds per two-lane mile			
			Salt Prewetted / Pretreated with Salt Brine	Salt Prewetted / Pretreated with Other Blends	Dry Salt*	Winter Sand (abrasives)
> 30° ↑	Snow	Plow, treat intersections only	80	70	100*	Not recommended
	Freezing Rain	Apply Chemical	80 - 160	70 - 140	100 - 200*	Not recommended
30° ↓	Snow	Plow and apply chemical	80 - 160	70 - 140	100 - 200*	Not recommended
	Freezing Rain	Apply Chemical	150 - 200	130 - 180	180 - 240*	Not recommended
25° - 30° ↑	Snow	Plow and apply chemical	120 - 160	100 - 140	150 - 200*	Not recommended
	Freezing Rain	Apply Chemical	150 - 200	130 - 180	180 - 240*	Not recommended
25° - 30° ↓	Snow	Plow and apply chemical	120 - 160	100 - 140	150 - 200*	Not recommended
	Freezing Rain	Apply Chemical	160 - 240	140 - 210	200 - 300*	400
20° - 25° ↑	Snow or Freezing Rain	Plow and apply chemical	160 - 240	140 - 210	200 - 300*	400
20° - 25° ↓	Snow	Plow and apply chemical	200 - 280	175 - 250	250 - 350*	Not recommended
	Freezing Rain	Apply Chemical	240 - 320	210 - 280	300 - 400*	400
15° - 20° ↑	Snow	Plow and apply chemical	200 - 280	175 - 250	250 - 350*	Not recommended
	Freezing Rain	Apply Chemical	240 - 320	210 - 280	300 - 400*	400
15° - 20° ↓	Snow or Freezing Rain	Plow and apply chemical	240 - 320	210 - 280	300 - 400*	500 for freezing rain
0° - 15° ↑↓	Snow	Plow, treat with blends, sand hazardous areas	Not recommended	300 - 400	Not recommended	500 - 750 spot treatment as needed
< 0°	Snow	Plow, treat with blends, sand hazardous areas	Not recommended	400 - 600**	Not recommended	500 - 750 spot treatment as needed

* Dry salt is not recommended. It is likely to blow off the road before it melts ice.

** A blend of 6 - 8 gal/ton MgCl₂ or CaCl₂ added to NaCl can melt ice as low as -10°.

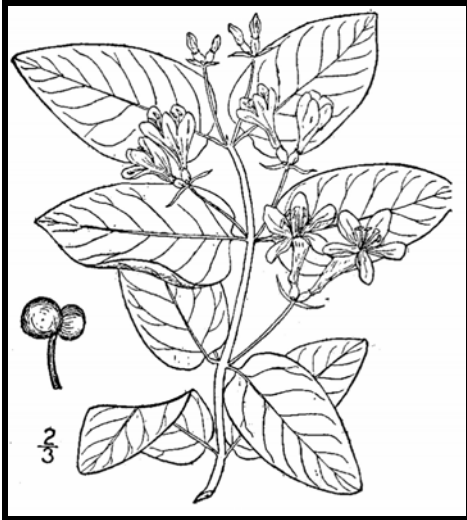
Anti-icing Route Data Form				
Truck Station:				
Date:				
Air Temperature	Pavement Temperature	Relative Humidity	Dew Point	Sky
Reason for applying:				
Route:				
Chemical:				
Application Time:				
Application Amount:				
Observation (first day):				
Observation (after event):				
Observation (before next application):				
Name:				

7.5 Invasive Species

With respect to a particular ecosystem, any species, including its seeds, eggs, spores, or other biological material capable of propagating that species, that is not native to that ecosystem is classified as an invasive species. Refer to the following fact sheet prepared by the University of New Hampshire Cooperative Extension entitled Methods for Disposing Non-Native Invasive Plants for recommended methods to dispose of invasive plant species.



Prepared by the Invasives Species Outreach Group, volunteers interested in helping people control invasive plants. Assistance provided by the Piscataquog Land Conservancy and the NH Invasives Species Committee. Edited by Karen Bennett, Extension Forestry Professor and Specialist.



Tatarian honeysuckle

Lonicera tatarica

USDA-NRCS PLANTS Database / Britton, N.L., and A. Brown. 1913. *An illustrated flora of the northern United States, Canada and the British Possessions*. Vol. 3: 282.

Non-native invasive plants crowd out natives in natural and managed landscapes. They cost taxpayers billions of dollars each year from lost agricultural and forest crops, decreased biodiversity, impacts to natural resources and the environment, and the cost to control and eradicate them.

Invasive plants grow well even in less than desirable conditions such as sandy soils along roadsides, shaded wooded areas, and in wetlands. In ideal conditions, they grow and spread even faster. There are many ways to remove these non-native invasives, but once removed, care is needed to dispose the removed plant material so the plants don't grow where disposed.

Knowing how a particular plant reproduces indicates its method of spread and helps determine

the appropriate disposal method. Most are spread by seed and are dispersed by wind, water, animals, or people. Some reproduce by vegetative means from pieces of stems or roots forming new plants. Others spread through both seed and vegetative means.

Because movement and disposal of viable plant parts is restricted (see NH Regulations), viable invasive parts can't be brought to most transfer stations in the state. Check with your transfer station to see if there is an approved, designated area for invasives disposal. This fact sheet gives recommendations for rendering plant parts non-viable.

Control of invasives is beyond the scope of this fact sheet. For information about control visit www.nhinvasives.org or contact your UNH Cooperative Extension office.

New Hampshire Regulations

Prohibited invasive species shall only be disposed of in a manner that renders them nonliving and nonviable. (Agr. 3802.04)

No person shall collect, transport, import, export, move, buy, sell, distribute, propagate or transplant any living and viable portion of any plant species, which includes all of their cultivars and varieties, listed in Table 3800.1 of the New Hampshire prohibited invasive species list. (Agr 3802.01)

How and When to Dispose of Invasives?

To prevent seed from spreading remove invasive plants before seeds are set (produced). Some plants continue to grow, flower and set seed even after pulling or cutting. Seeds can remain viable in the ground for many years. If the plant has flowers or seeds, place the flowers and seeds in a heavy plastic bag “head first” at the weeding site and transport to the disposal site. The following are general descriptions of disposal methods. See the chart for recommendations by species.

Burning: Large woody branches and trunks can be used as firewood or burned in piles. For outside burning, a written fire permit from the local forest fire warden is required unless the ground is covered in snow. Brush larger than 5 inches in diameter can't be burned. Invasive plants with easily airborne seeds like black swallow-wort with mature seed pods (indicated by their brown color) shouldn't be burned as the seeds may disperse by the hot air created by the fire.

Bagging (solarization): Use this technique with softer-tissue plants. Use heavy black or clear plastic bags (contractor grade), making sure that no parts of the plants poke through. Allow the bags to sit in the sun for several weeks and on dark pavement for the best effect.

Tarping and Drying: Pile material on a sheet of plastic and cover with a tarp, fastening the tarp to the ground and monitoring it for escapes. Let the material dry for several weeks, or until it is clearly nonviable.

Chipping: Use this method for woody plants that don't reproduce vegetatively.

Burying: This is risky, but can be done with watchful diligence. Lay thick plastic in a deep pit before placing the cut up plant material in the hole. Place the material away from the edge of the plastic before covering it with more heavy plastic. Eliminate as much air as possible and toss in soil to weight down the material in the pit. Note that the top of the buried material should be at least three feet underground. Japanese knotweed should be at least 5 feet underground!

Drowning: Fill a large barrel with water and place soft-tissue plants in the water. Check after a few weeks and look for rotted plant material (roots, stems, leaves, flowers). Well-rotted plant material may be composted. A word of caution- seeds may still be viable after using this method. Do this before seeds are set. This method isn't used often. Be prepared for an awful stink!

Composting: Invasive plants can take root in compost. Don't compost any invasives unless you know there is no viable (living) plant material left. Use one of the above techniques (bagging, tarping, drying, chipping, or drowning) to render the plants nonviable before composting. Closely examine the plant before composting and avoid composting seeds.






Japanese knotweed
Polygonum cuspidatum
USDA-NRCS PLANTS Database /
Britton, N.L., and A. Brown. 1913. *An illustrated flora of the northern United States, Canada and the British Possessions*. Vol. 1: 676.

Be diligent looking for seedlings for years in areas where removal and disposal took place.

Suggested Disposal Methods for Non-Native Invasive Plants

This table provides information concerning the disposal of removed invasive plant material. If the infestation is treated with herbicide and left in place, these guidelines don't apply. Don't bring invasives to a local transfer station, unless there is a designated area for their disposal, or they have been rendered non-viable. This listing includes wetland and upland plants from the New Hampshire Prohibited Invasive Species List. The disposal of aquatic plants isn't addressed.

Woody Plants	Method of Reproducing	Methods of Disposal
Norway maple <i>(Acer platanoides)</i> European barberry <i>(Berberis vulgaris)</i> Japanese barberry <i>(Berberis thunbergii)</i> autumn olive <i>(Elaeagnus umbellata)</i> burning bush <i>(Euonymus alatus)</i> Morrow's honeysuckle <i>(Lonicera morrowii)</i> Tatarian honeysuckle <i>(Lonicera tatarica)</i> showy bush honeysuckle <i>(Lonicera x bella)</i> common buckthorn <i>(Rhamnus cathartica)</i> glossy buckthorn <i>(Frangula alnus)</i>		<p>Prior to fruit/seed ripening</p> <p>Seedlings and small plants</p> <ul style="list-style-type: none"> ▪ Pull or cut and leave on site with roots exposed. No special care needed. <p>Larger plants</p> <ul style="list-style-type: none"> ▪ Use as firewood. ▪ Make a brush pile. ▪ Chip. ▪ Burn.
		<p>After fruit/seed is ripe</p> <p>Don't remove from site.</p> <ul style="list-style-type: none"> ▪ Burn. ▪ Make a covered brush pile. ▪ Chip once all fruit has dropped from branches. ▪ Leave resulting chips on site and monitor.
oriental bittersweet <i>(Celastrus orbiculatus)</i> multiflora rose <i>(Rosa multiflora)</i>		<p>Prior to fruit/seed ripening</p> <p>Seedlings and small plants</p> <ul style="list-style-type: none"> ▪ Pull or cut and leave on site with roots exposed. No special care needed. <p>Larger plants</p> <ul style="list-style-type: none"> ▪ Make a brush pile. ▪ Burn.
		<p>After fruit/seed is ripe</p> <p>Don't remove from site.</p> <ul style="list-style-type: none"> ▪ Burn. ▪ Make a covered brush pile. ▪ Chip – only after material has fully dried (1 year) and all fruit has dropped from branches. Leave resulting chips on site and monitor.

Non-Woody Plants	Method of Reproducing	Methods of Disposal
<p>garlic mustard (<i>Alliaria petiolata</i>)</p> <p>spotted knapweed (<i>Centaurea maculosa</i>)</p> <ul style="list-style-type: none"> ▪ Sap of related knapweed can cause skin irritation and tumors. Wear gloves when handling. <p>black swallow-wort (<i>Cynanchum nigrum</i>)</p> <ul style="list-style-type: none"> ▪ May cause skin rash. Wear gloves and long sleeves when handling. <p>pale swallow-wort (<i>Cynanchum rossicum</i>)</p> <p>giant hogweed (<i>Heracleum mantegazzianum</i>)</p> <ul style="list-style-type: none"> ▪ Can cause major skin rash. Wear gloves and long sleeves when handling. <p>dame's rocket (<i>Hesperis matronalis</i>)</p> <p>perennial pepperweed (<i>Lepidium latifolium</i>)</p> <p>purple loosestrife (<i>Lythrum salicaria</i>)</p> <p>Japanese stilt grass (<i>Microstegium vimineum</i>)</p> <p>mile-a-minute weed (<i>Polygonum perfoliatum</i>)</p>	<p>Fruits and Seeds</p> 	<p>Prior to flowering</p> <p>Depends on scale of infestation</p> <p>Small infestation</p> <ul style="list-style-type: none"> ▪ Pull or cut plant and leave on site with roots exposed. <p>Large infestation</p> <ul style="list-style-type: none"> ▪ Pull or cut plant and pile. (You can pile onto or cover with plastic sheeting). ▪ Monitor. Remove any re-sprouting material. <hr/> <p>During and following flowering</p> <p>Do nothing until the following year or remove flowering heads and bag and let rot.</p> <p>Small infestation</p> <ul style="list-style-type: none"> ▪ Pull or cut plant and leave on site with roots exposed. <p>Large infestation</p> <ul style="list-style-type: none"> ▪ Pull or cut plant and pile remaining material. (You can pile onto plastic or cover with plastic sheeting). ▪ Monitor. Remove any re-sprouting material.
<p>common reed (<i>Phragmites australis</i>)</p> <p>Japanese knotweed (<i>Polygonum cuspidatum</i>)</p> <p>Bohemian knotweed (<i>Polygonum x bohemicum</i>)</p>	<p>Fruits, Seeds, Plant Fragments</p> <p>Primary means of spread in these species is by plant parts. Although all care should be given to preventing the dispersal of seed during control activities, the presence of seed doesn't materially influence disposal activities.</p>	<p>Small infestation</p> <ul style="list-style-type: none"> ▪ Bag all plant material and let rot. ▪ Never pile and use resulting material as compost. ▪ Burn. <p>Large infestation</p> <ul style="list-style-type: none"> ▪ Remove material to unsuitable habitat (dry, hot and sunny or dry and shaded location) and scatter or pile. ▪ Monitor and remove any sprouting material. ▪ Pile, let dry, and burn.

January 2010

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Managing Invasive Plants

Methods of Control

by Christopher Mattrick

They're out there. The problem of invasive plants is as close as your own backyard.

Maybe a favorite dogwood tree is struggling in the clutches of an Oriental bittersweet vine. Clawlike canes of multiflora rose are scratching at the side of your house. That handsome burning bush you planted few years ago has become a whole clump in practically no time ... but what happened to the azalea that used to grow right next to it?

If you think controlling or managing invasive plants on your property is a daunting task, you're not alone. Though this topic is getting lots of attention from federal, state, and local government agencies, as well as the media, the basic question for most homeowners is simply, "How do I get rid of the invasive plants in my own landscape?" Fortunately, the best place to begin to tackle this complex issue is in our own backyards and on local conservation lands. We hope the information provided here will help you take back your yard. We won't kid you—there's some work involved, but the payoff in beauty, wildlife habitat, and peace of mind makes it all worthwhile.

PLAN OF ATTACK

Three broad categories cover most invasive plant control: mechanical, chemical, and biological. Mechanical control means physically removing plants from the environment



Spraying chemicals to control invasive plants.

through cutting or pulling. Chemical control uses herbicides to kill plants and inhibit regrowth. Techniques and chemicals used will vary depending on the species. Biological controls use plant diseases or insect predators, typically from the targeted species' home range. Several techniques may be effective in controlling a single species, but there is usually one preferred method—the one that is most resource efficient with minimal impact on non-target species and the environment.

MECHANICAL CONTROL METHODS

Mechanical treatments are usually the first ones to look at when evaluating an invasive plant removal project. These procedures do not require special licensing or introduce chemicals into the environment. They do require permits in some situations, such as wetland zones. [See sidebar on page 23.] Mechanical removal is highly labor intensive and creates a significant amount of site disturbance, which can lead to rapid reinvasion if not handled properly.

Pulling and digging

Many herbaceous plants and some woody species (up to about one inch in diameter), if present in limited quantities, can be pulled out or dug up. It's important to remove as much of the root system as possible; even a small portion can restart the infestation. Pull plants by hand or use a digging fork, as shovels can shear off portions of the root system, allowing for regrowth. To remove larger woody stems (up to about three inches in diameter), use a Weed Wrench™, Root Jack, or Root Talon. These tools, available from several manufacturers, are designed to remove the aboveground portion of the plant as well as the entire root system. It's easiest to undertake this type of control in the spring or early summer when soils are moist and plants come out more easily.



Using tools to remove woody stems.



Volunteers hand pulling invasive plants.

Suffocation

Try suffocating small seedlings and herbaceous plants. Place double or triple layers of thick UV-stabilized plastic sheeting, either clear or black (personally I like clear), over the infestation and secure the plastic with stakes or weights. Make sure the plastic extends at least five feet past the edge of infestation on all sides. Leave the plastic in place for at least two years. This technique will kill everything beneath the plastic—invasive and non-invasive plants alike. Once the plastic is removed, sow a cover crop such as annual rye to prevent new invasions.

Cutting or mowing

This technique is best suited for locations you can visit and treat often. To be effective, you will need to mow or cut infested areas three or four times a year for up to five years. The goal is to interrupt the plant's ability to photosynthesize by removing as much leafy material as possible. Cut the plants at ground level and remove all resulting debris from the site. With this treatment, the infestation may actually appear to get worse at first, so you will need to be as persistent as the invasive plants themselves. Each time you cut the plants back, the root system gets slightly larger, but must also rely on its energy reserves to push up new growth. Eventually, you will exhaust these reserves and the plants will die. This may take many years, so you have to remain committed to this process once you start; otherwise the treatment can backfire, making the problem worse.

CHEMICAL CONTROL METHODS

Herbicides are among the most effective and resource-efficient tools to treat invasive species. Most of the commonly known invasive plants can be treated using only two herbicides—glyphosate (the active ingredient in Roundup™ and Rodeo™) and triclopyr (the active ingredient in Brush-B-Gone™ and Garlon™). Glyphosate is non-selective, meaning it kills everything it contacts. Triclopyr is selective and does not injure monocots (grasses, orchids, lilies, etc.). Please read labels and follow directions precisely for both environmental and personal safety. These are relatively benign herbicides, but improperly used they can still cause both short- and long-term health and environmental problems. Special aquatic formulations are required when working in wetland zones. You are required to have a state-issued pesticide applicator license when applying these chemicals on land you do not own. To learn more about the pesticide regulations in your state, visit or call your state's pesticide control division, usually part of the state's Department of Agriculture. In wetland areas, additional permits are usually required by the Wetlands Protection Act. [See sidebar on page 23.]

Foliar applications

When problems are on a small scale, this type of treatment is usually applied with a backpack sprayer or even a small handheld spray bottle. It is an excellent way to treat large monocultures of herbaceous plants, or to spot-treat individual plants that are difficult to remove mechanically, such as goutweed, swallowwort, or purple loosestrife. It is also an effective treatment for some woody species, such as Japanese barberry, multiflora rose, Japanese honeysuckle, and Oriental bittersweet that grow in dense masses or large numbers over many acres. The herbicide mixture should contain no more than five percent of the active ingredient, but it is important to follow the instructions on the product label. This treatment is most effective when the plants are actively growing, ideally when they are flowering or beginning to form fruit. It has been shown that plants are often more susceptible to this type of treatment if the existing stems are cut off and the regrowth is treated. This is especially true for Japanese knotweed. The target plants should be thoroughly wetted with the herbicide on a day when there is no rain in the forecast for the next 24 to 48 hours.

Cut stem treatments

There are several different types of cut stem treatments, but here we will review only the one most commonly used. All treatments of this type require a higher concentration of the active ingredient than is used in foliar applications. A 25 to 35 percent solution of the active ingredient should be used for cut stem treatments, but read and follow all label instructions. In most cases, the appropriate herbicide is glyphosate, except for Oriental bittersweet, on which triclopyr should be used. This treatment can be used on all woody stems, as well as phragmites and Japanese knotweed.

For woody stems, treatments are most effective when applied in the late summer and autumn—between late August and November. Stems should be cut close to the ground, but not so close that you will lose track of them. Apply herbicide directly to the cut surface as soon as possible after cutting. Delaying the application will reduce the effectiveness of the treatment. The herbicide can be applied with a sponge, paintbrush, or spray bottle.



Cut stem treatment tools.

For phragmites and Japanese knotweed, treatment is the same, but the timing and equipment are different. Plants should be treated anytime from mid-July through September, but the hottest, most humid days of the summer are best

for this method. Cut the stems halfway between two leaf nodes at a comfortable height. Inject (or squirt) herbicide into the exposed hollow stem. All stems in an infestation should be treated. A wash bottle is the most effective application tool, but you can also use an eyedropper, spray bottle, or one of the recently developed high-tech injection systems.

It is helpful to mix a dye in with the herbicide solution. The dye will stain the treated surface and mark the areas that have been treated, preventing unnecessary reapplication. You can buy a specially formulated herbicide dye, or use food coloring or laundry dye.

There is not enough space in this article to describe all the possible ways to control invasive plants. You can find other treatments, along with more details on the above-described methods, and species-specific recommendations on The Nature Conservancy Web site (tncweeds.ucdavis.edu). An upcoming posting on the Invasive Plant Atlas of New England (www.ipane.org) and the New England Wild Flower Society (www.newfs.org) Web sites will also provide further details.



Hollow stem injection tools.

Biological controls—still on the horizon

Biological controls are moving into the forefront of control methodology, but currently the only widely available and applied biocontrol relates to purple loosestrife. More information on purple loosestrife and other biological control projects can be found at www.invasiveplants.net.

DISPOSAL OF INVASIVE PLANTS

Proper disposal of removed invasive plant material is critical to the control process. Leftover plant material can cause new infestations or reinfest the existing project area. There are many appropriate ways to dispose of invasive plant debris. I've listed them here in order of preference.

- 1. Burn it**—Make a brush pile and burn the material following local safety regulations and restrictions, or haul it to your town's landfill and place it in their burn pile.
- 2. Pile it**—Make a pile of the woody debris. This technique will provide shelter for wildlife as well.
- 3. Compost it**—Place all your herbaceous invasive plant debris in a pile and process as compost. Watch the pile closely for resprouts and remove as necessary. Do not use the resulting compost in your garden. The pile is for invasive plants only.



Injecting herbicide into the hollow stem of phragmites.

4. Dry it/cook it—Place woody debris out on your driveway or any asphalt surface and let it dry out for a month. Place herbaceous material in a doubled-up black trash bag and let it cook in the sun for one month. At the end of the month, the material should be non-viable and you can dump it or dispose of it with the trash. The method assumes there is no viable seed mixed in with the removed material.

Care should be taken in the disposal of all invasive plants, but several species need extra attention. These are the ones that have the ability to sprout vigorously from plant fragments and should ideally be burned or dried prior to disposal: Oriental bittersweet, multiflora rose, Japanese honeysuckle, phragmites, and Japanese knotweed.

Christopher Mattrick is the former Senior Conservation Programs Manager for New England Wild Flower Society, where he managed conservation volunteer and invasive and rare plant management programs. Today, Chris and his family work and play in the White Mountains of New Hampshire, where he is the Forest Botanist and Invasive Species Coordinator for the White Mountain National Forest.



Controlling Invasive Plants in Wetlands

Special concerns; special precautions

Control of invasive plants in or around wetlands or bodies of water requires a unique set of considerations. Removal projects in wetland zones can be legal and effective if handled appropriately. In many cases, herbicides may be the least disruptive tools with which to remove invasive plants. You will need a state-issued pesticide license to apply herbicide on someone else's property, but all projects in wetland or aquatic systems fall under the jurisdiction of the Wetlands Protection Act and therefore require a permit. *Yes, even hand-pulling that colony of glossy buckthorn plants from your own swampland requires a permit.* Getting a permit for legal removal is fairly painless if you plan your project carefully.

1. Investigate and understand the required permits and learn how to obtain them. The entity charged with the enforcement of the Wetlands Protection Act varies from state to state. For more information in your state, contact:

ME: Department of Environmental Protection
www.state.me.us/dep/blwq/docstand/nrapage.htm

NH: Department of Environmental Services
www.des.state.nh.us/wetlands/

VT: Department of Environmental Conservation
www.anr.state.vt.us/dec/waterq/permits/htm/pm_cud.htm

MA: Consult your local town conservation commission

RI: Department of Environmental Management
www.dem.ri.gov/programs/benviron/water/permits/fresh/index.htm

CT: Consult your local town Inland Wetland and Conservation Commission

2. Consult an individual or organization with experience in this area. Firsthand experience in conducting projects in wetland zones and navigating the permitting process is priceless. Most states have wetland scientist societies whose members are experienced in working in wetlands and navigating the regulations affecting them. A simple Web search will reveal the contact point for these societies. Additionally, most environmental consulting firms and some nonprofit organizations have skills in this area.

3. Develop a well-written and thorough project plan. You are more likely to be successful in obtaining a permit for your project if you submit a project plan along with your permit application. The plan should include the reasons for the project, your objectives in completing the project, how you plan to reach those objectives, and how you will monitor the outcome.

4. Ensure that the herbicides you plan to use are approved for aquatic use. Experts consider most herbicides harmful to water quality or aquatic organisms, but rate some formulations as safe for aquatic use. Do the research and select an approved herbicide, and then closely follow the instructions on the label.

5. If you are unsure—research, study, and most of all, ask for help. Follow the rules. The damage caused to aquatic systems by the use of an inappropriate herbicide or the misapplication of an appropriate herbicide not only damages the environment, but also may reduce public support for safe, well-planned projects.

7.6 Annual Updates and Log Requirements

The Owner and/or Contact/Responsible Party shall review this Operation and Maintenance Plan once per year for its effectiveness and adjust the plan and deed as necessary.

A log of all preventative and corrective measures for the stormwater system shall be kept on-site and be made available upon request by any public entity with administrative, health environmental or safety authority over the site including NHDES.

Copies of the Stormwater Maintenance report shall be submitted to the City of Portsmouth on an annual basis.

Stormwater Management Report						
Mixed Use Development		Raynes Avenue – Map 123 Lots 10, 12, 13 & 14				
BMP Description	Date of Inspection	Inspector	BMP Installed and Operating Properly?	Cleaning / Corrective Action Needed	Date of Cleaning / Repair	Performed By
Deep Sump CB's			<input type="checkbox"/> Yes <input type="checkbox"/> No			
Underground Detention			<input type="checkbox"/> Yes <input type="checkbox"/> No			
Jellyfish Filter 1			<input type="checkbox"/> Yes <input type="checkbox"/> No			
Jellyfish Filter 2			<input type="checkbox"/> Yes <input type="checkbox"/> No			
Porous Pavement			<input type="checkbox"/> Yes <input type="checkbox"/> No			

Stormwater Management Report						
City of Portsmouth		North Mill Pond Trail				
BMP Description	Date of Inspection	Inspector	BMP Installed and Operating Properly?	Cleaning / Corrective Action Needed	Date of Cleaning / Repair	Performed By
Porous Pavement			<input type="checkbox"/> Yes <input type="checkbox"/> No			

J:\P\0595 Pro Con General Proposals\0595-007 Raynes Ave Hotel\Report_Evaluation\Applications\City of Portsmouth\20210322 TAC Submission\Drainage\0595-007_Drainage Report.docx

APPENDIX A



ProCon, LLC
31 Raynes Avenue Project
Portsmouth, NH

SITE SPECIFIC SOIL MAP

June 2020

Tighe&Bond
Engineers | Environmental Specialists

1.0 Introduction

This report is provided in conjunction with a 1.35 +/- acre Site Specific Soil Map (SSSM) prepared by Tighe & Bond for a parcel at 31 Raynes Avenue in Portsmouth, NH. The purpose of the mapping was to assist in the evaluation of drainage and other soil-related uses associated with site improvements, and may be used as part of an Alteration of Terrain (AoT) permit application.

2.0 Methods

Fieldwork for the soil mapping was completed October 22, 2019 based on *Site-Specific Soil Mapping Standards for New Hampshire and Vermont, Version 5.0*, (Society of Soil Scientists of Northern New England [SSSNNE] Special Publication No. 3, December 2017). The poorly and very poorly drained soil types under this system are based on the most recent version of *Field Indicators for Identifying Hydric Soils in New England, Version 4* (New England Interstate Water Pollution Control Commission, 2018).

The soil legend for this map is based on the soil series currently mapped in the State of New Hampshire as published in the *New Hampshire State-Wide Numerical Soils Legend* (USDA Natural Resources Conservation Service, Issue #10, 2011). Since this soil map includes disturbed soils and may be used for an AoT application, the map symbols are composed of two major parts separated by a forward slash (/). The first part of the soil symbol includes a numerical identifier from the state-wide soil legend, followed by a letter indicating the slope class (e.g., 299A). Slope class identifiers are as follows:

A	0-3%	D	15-25%
B	3-8%	E	25-50%
C	8-15%	F	>50%

The second part of the symbol is based on the SSSNNE Disturbed Soil Supplemental Symbols, which are included within the Site Specific Soil Map (SSSM) standards. This portion of the symbol translates as follows:

Character 1: Drainage Class

- a-Excessively Drained
- b-Somewhat Excessively Drained
- c-Well Drained
- d-Moderately Well Drained
- e-Somewhat Poorly Drained
- f-Poorly Drained
- g-Very Poorly Drained
- h-Not Determined

Character 2: Parent Material (of naturally formed soil only, if present)

- a-No natural soil within 60 inches
- b-Glaciofluvial deposits (outwash/terraces of sand or sand and gravel)
- c-Glacial till material (active ice)
- d-Glaciolacustrine very fine sand and silt deposits (glacial lakes)
- e-Loamy/sandy over silt/clay deposits
- f-Marine silt and clay deposits (ocean waters)
- g-Alluvial deposits (floodplains)
- h-Organic materials-fresh water wetlands
- i-Organic materials-tidal wetlands

Character 3: Restrictive Properties

- a-None
- b-Bouldery surface with more than 15% of the surface covered with boulders
- c-Mineral restrictive layer(s) are present in the soil profile less than 40 inches below the soil surface such as hard pan, platy structure or clayey texture with consistence of at least firm (i.e. more than 20 newtons).
- d-Bedrock in the soil profile; 0-20 inches
- e-Bedrock in the soil profile; 20-60 inches
- f-Areas where depth to bedrock is so variable that a single soil type cannot be applied, will be mapped as a complex of soil types
- g-Subject to flooding
- h-Manufactured impervious surface including pavement, concrete, or built-up surfaces (e.g. buildings) with no morphological restrictive layer within control section

Character 4: Estimated Ksat (most limiting layer excluding symbol 3h above)

- a-High
- b-Moderate
- c-Low
- d-Not determined *See "Guidelines for Ksat Class Placement" in Chapter 3 of the Soil Survey Manual, USDA

Character 5: Hydrologic Soil Group

- a-Group A
- b-Group B
- c-Group C
- d-Group D
- e-Not determined

SSSM report standards require estimates of the maximum size of *limiting* inclusions for the entire soil map and an estimate of the percentage of *dissimilar* inclusions within each map unit. *Limiting* inclusions are soils "...that differ appreciably in one or more soil properties from the named soil in a map unit. The difference in soil properties is more restrictive and may affect use and management." *Dissimilar* inclusions are "...soils that either do not share limits of some important diagnostic properties of the named taxon, or, in the professional judgment of the soil scientist, have different use or management requirements." The maximum size of any limiting inclusions in this soil map is estimated to be less than 2,000 square feet. Any dissimilar inclusions noted during the mapping are listed below within the map unit descriptions.

3.0 Site Features

The parcel is a highly disturbed site along the North Mill Pond. The property shows evidence of what appears to be very old filling and grading associated with the existing development.

4.0 Soil Map Unit Descriptions

Below are descriptions for each of the map units found on the accompanying SSSM. The "*" after the numerical map unit symbol represents a placeholder for the slope class indicators described above.

100*/afaaa—Udorthents, wet substratum

Landscape Setting: Soils that have been filled and leveled over what was originally hydric soils. On this site this map unit represents fill that was used to construct a pier.

Drainage Class: Excessively drained

Parent Material of Natural Soil: Fill over marine silts and clays at <60 inches (presumed).

Typical Textures: Very gravelly sand (mixed sand and crushed stone)

Hydrologic Soil Group: A

Dissimilar Inclusions: None noted

Limiting Inclusions: None noted

Additional Notes: Soils in these areas have properties that are similar to the Hinckley soil series for Hydrologic Soil Group determination

100*/dfccc—Udorthents, wet substratum

Landscape Setting: Soils that have been filled and leveled over what was originally hydric soils

Drainage Class: Moderately well drained.

Parent Material: Fill over marine silts and clays at <60 inches (presumed).

Typical Textures: Very gravelly sandy loam and gravelly silty clay loam fill

Hydrologic Soil Group: C

Dissimilar Inclusions: None noted

Limiting Inclusions: Slopes along the shore are steeper than the mapped unit and are affected by tidal inundation. These areas comprise less than 10% of the unit

Additional Notes: Soils in these areas have properties that are similar to the Elmridge soil series for Hydrologic Soil Group determination

Site Specific Soil Map Legend

31 Raynes Avenue, Portsmouth, NH

Slope Class Identifiers

A	0-3%	D	15-25%
B	3-8%	E	25-50%
C	8-15%	F	>50%

Map Unit Symbols

<u>Map Number* /Disturbed Soil Numerator**</u>	<u>Soil Map Unit Name</u>	<u>Hydrologic Soil Group</u>
100*/afaaa	Udorthents, wet substratum / excessively drained, over marine silts and clays, no restrictive layer within 40 inches, high Ksat, Hydrologic Soil Group A	A
100*/dfccc	Udorthents, wet substratum / moderately well drained, over marine silts and clays, with a restrictive layer within 40 inches, low Ksat, Hydrologic Soil Group C	C

*Indicates the location of the slope class identifier (A-F)

**Supplemental symbols are used to further characterize disturbed soils for Alteration of Terrain permits

Soil Mapping Notes:

1. Hydrologic soil groups for disturbed soils were based on most similar soil series listed in *Ksat Values for NH Soils*, SSSNNE Special Publication No. 5, 2009.
2. Fieldwork for this map was conducted by Leonard A. Lord, PhD, NHCSS #19 on October 22, 2019.
3. This detailed Site Specific Soil Map conforms to the standards of SSSNNE Publication No. 3, as amended, *Site Specific Soil Mapping Standards for NH and VT*.
4. This map has been prepared to comply with soil mapping requirements of RSA 485 A:17 and NHDES Env-Wq, Alteration of Terrain.
5. See accompanying narrative report for methodology, map symbol legend, and interpretations.



APPENDIX B

Extreme Precipitation Tables

Northeast Regional Climate Center

Data represents point estimates calculated from partial duration series. All precipitation amounts are displayed in inches.

Smoothing	Yes
State	New Hampshire
Location	
Longitude	70.764 degrees West
Latitude	43.080 degrees North
Elevation	0 feet
Date/Time	Fri, 24 Jul 2020 12:23:19 -0400

Extreme Precipitation Estimates

	5min	10min	15min	30min	60min	120min		1hr	2hr	3hr	6hr	12hr	24hr	48hr		1day	2day	4day	7day	10day	
1yr	0.26	0.40	0.50	0.65	0.81	1.04	1yr	0.70	0.98	1.21	1.56	2.03	2.65	2.92	1yr	2.35	2.81	3.22	3.94	4.54	1yr
2yr	0.32	0.50	0.62	0.81	1.02	1.30	2yr	0.88	1.18	1.52	1.94	2.48	3.20	3.57	2yr	2.84	3.43	3.93	4.67	5.32	2yr
5yr	0.37	0.58	0.73	0.97	1.25	1.61	5yr	1.08	1.47	1.89	2.43	3.14	4.06	4.57	5yr	3.59	4.40	5.03	5.93	6.69	5yr
10yr	0.41	0.65	0.82	1.11	1.45	1.89	10yr	1.25	1.72	2.23	2.89	3.74	4.86	5.52	10yr	4.30	5.31	6.07	7.09	7.96	10yr
25yr	0.48	0.76	0.97	1.33	1.77	2.33	25yr	1.53	2.14	2.77	3.62	4.73	6.16	7.09	25yr	5.45	6.81	7.78	9.00	10.03	25yr
50yr	0.53	0.86	1.10	1.53	2.07	2.75	50yr	1.78	2.52	3.28	4.31	5.65	7.37	8.57	50yr	6.53	8.24	9.40	10.79	11.95	50yr
100yr	0.59	0.96	1.24	1.76	2.41	3.25	100yr	2.08	2.97	3.90	5.15	6.75	8.83	10.36	100yr	7.82	9.96	11.35	12.93	14.24	100yr
200yr	0.67	1.10	1.42	2.04	2.82	3.82	200yr	2.43	3.51	4.60	6.11	8.06	10.58	12.52	200yr	9.37	12.04	13.71	15.50	16.98	200yr
500yr	0.80	1.31	1.71	2.48	3.47	4.75	500yr	2.99	4.37	5.75	7.68	10.19	13.45	16.11	500yr	11.90	15.49	17.61	19.72	21.44	500yr

Lower Confidence Limits

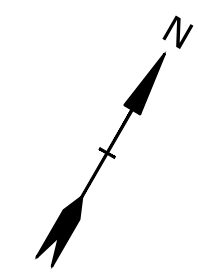
	5min	10min	15min	30min	60min	120min		1hr	2hr	3hr	6hr	12hr	24hr	48hr		1day	2day	4day	7day	10day	
1yr	0.23	0.36	0.44	0.59	0.73	0.88	1yr	0.63	0.86	0.92	1.33	1.68	2.23	2.48	1yr	1.97	2.39	2.86	3.18	3.88	1yr
2yr	0.31	0.49	0.60	0.81	1.00	1.19	2yr	0.86	1.16	1.37	1.82	2.34	3.05	3.45	2yr	2.70	3.31	3.82	4.54	5.07	2yr
5yr	0.35	0.54	0.67	0.92	1.17	1.40	5yr	1.01	1.37	1.61	2.12	2.73	3.78	4.18	5yr	3.34	4.02	4.71	5.52	6.23	5yr
10yr	0.38	0.59	0.73	1.02	1.32	1.60	10yr	1.14	1.56	1.80	2.39	3.06	4.36	4.85	10yr	3.86	4.66	5.42	6.39	7.17	10yr
25yr	0.44	0.67	0.83	1.18	1.56	1.90	25yr	1.34	1.86	2.10	2.76	3.54	4.70	5.87	25yr	4.16	5.64	6.62	7.76	8.65	25yr
50yr	0.48	0.73	0.91	1.31	1.76	2.17	50yr	1.52	2.12	2.34	3.07	3.93	5.31	6.77	50yr	4.70	6.51	7.68	9.00	9.98	50yr
100yr	0.53	0.81	1.01	1.46	2.00	2.47	100yr	1.73	2.41	2.62	3.42	4.35	5.96	7.81	100yr	5.28	7.51	8.92	10.45	11.52	100yr
200yr	0.59	0.89	1.12	1.63	2.27	2.81	200yr	1.96	2.75	2.93	3.79	4.79	6.68	9.01	200yr	5.91	8.66	10.34	12.15	13.31	200yr
500yr	0.68	1.02	1.31	1.90	2.70	3.36	500yr	2.33	3.28	3.41	4.32	5.46	7.76	10.87	500yr	6.87	10.45	12.58	14.86	16.11	500yr

Upper Confidence Limits

	5min	10min	15min	30min	60min	120min		1hr	2hr	3hr	6hr	12hr	24hr	48hr		1day	2day	4day	7day	10day	
1yr	0.28	0.44	0.54	0.72	0.89	1.08	1yr	0.77	1.06	1.26	1.74	2.21	2.98	3.16	1yr	2.64	3.04	3.58	4.37	5.04	1yr
2yr	0.34	0.52	0.64	0.86	1.07	1.27	2yr	0.92	1.24	1.48	1.96	2.52	3.42	3.70	2yr	3.03	3.56	4.08	4.83	5.62	2yr
5yr	0.40	0.62	0.76	1.05	1.34	1.62	5yr	1.15	1.58	1.88	2.53	3.25	4.33	4.96	5yr	3.84	4.77	5.37	6.37	7.15	5yr
10yr	0.47	0.72	0.89	1.24	1.61	1.97	10yr	1.39	1.93	2.28	3.11	3.95	5.33	6.20	10yr	4.72	5.96	6.82	7.83	8.74	10yr
25yr	0.57	0.87	1.09	1.55	2.04	2.57	25yr	1.76	2.51	2.95	4.07	5.15	7.77	8.34	25yr	6.88	8.02	9.15	10.33	11.40	25yr
50yr	0.67	1.02	1.27	1.82	2.46	3.12	50yr	2.12	3.05	3.59	5.00	6.32	9.73	10.46	50yr	8.62	10.06	11.45	12.71	13.95	50yr
100yr	0.79	1.19	1.49	2.15	2.95	3.80	100yr	2.55	3.72	4.37	6.15	7.76	12.18	13.11	100yr	10.78	12.61	14.32	15.68	17.08	100yr
200yr	0.92	1.39	1.76	2.54	3.55	4.64	200yr	3.06	4.54	5.33	7.58	9.53	15.29	16.45	200yr	13.53	15.82	17.94	19.34	20.91	200yr
500yr	1.14	1.70	2.19	3.18	4.52	6.02	500yr	3.90	5.89	6.92	10.01	12.54	20.67	22.22	500yr	18.29	21.37	24.18	25.50	27.33	500yr

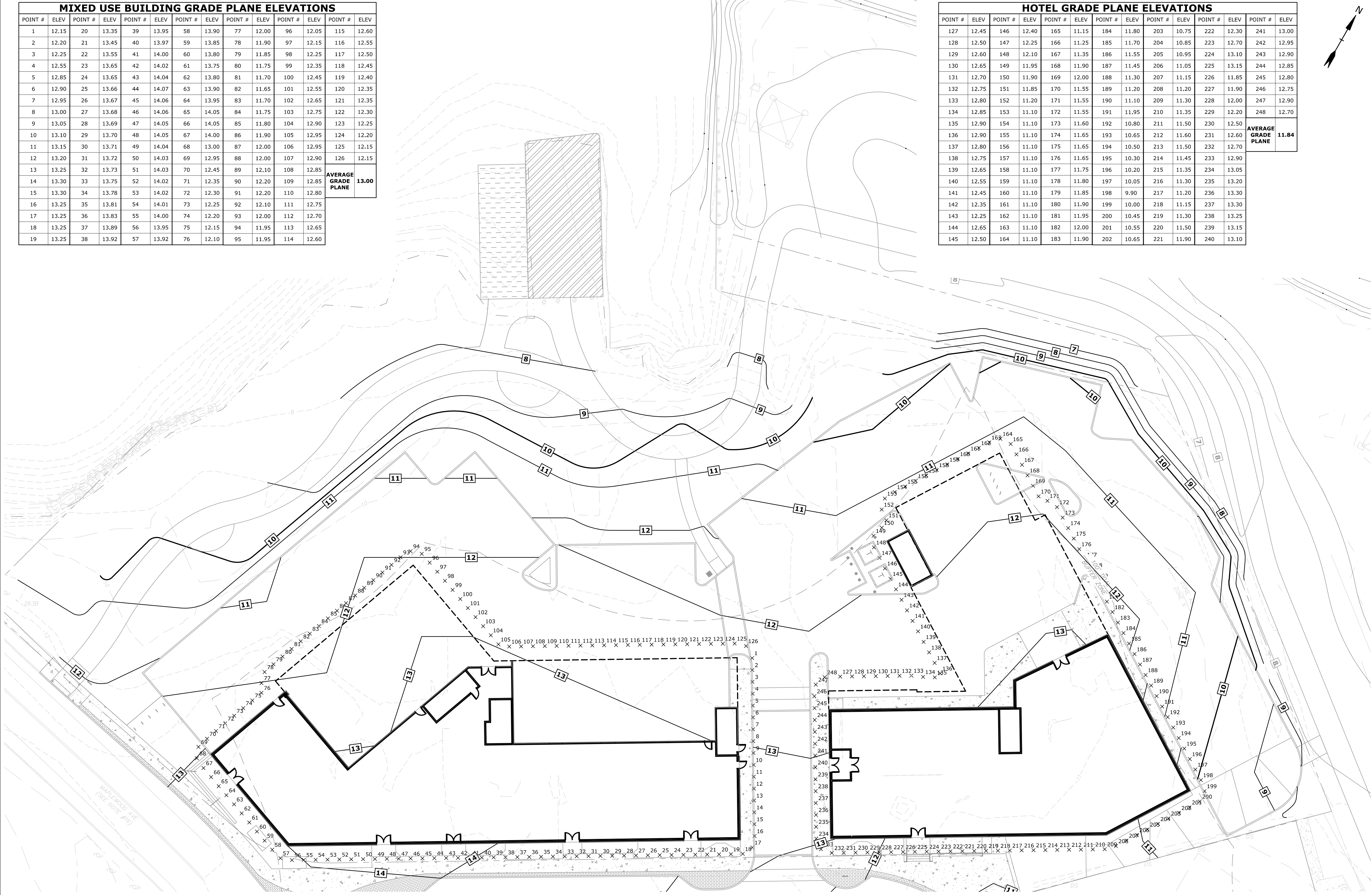


Coastal and Great Bay Region Precipitation Increase		
	24-hr Storm Event (in.)	24-hr Storm Event + 15% (in.)
1 Year	2.65	3.05
2 Year	3.20	3.68
10 Year	4.86	5.59
25 Year	6.16	7.08
50 Year	7.37	8.48
100 Year	8.83	10.15



MIXED USE BUILDING GRADE PLANE ELEVATIONS													
POINT #	ELEV	POINT #	ELEV	POINT #	ELEV	POINT #	ELEV	POINT #	ELEV	POINT #	ELEV	POINT #	ELEV
1	12.15	20	13.35	39	13.95	58	13.90	77	12.00	96	12.05	115	12.60
2	12.20	21	13.45	40	13.97	59	13.85	78	11.90	97	12.15	116	12.55
3	12.25	22	13.55	41	14.00	60	13.80	79	11.85	98	12.25	117	12.50
4	12.55	23	13.65	42	14.02	61	13.75	80	11.75	99	12.35	118	12.45
5	12.85	24	13.65	43	14.04	62	13.80	81	11.70	100	12.45	119	12.40
6	12.90	25	13.66	44	14.07	63	13.90	82	11.65	101	12.55	120	12.35
7	12.95	26	13.67	45	14.06	64	13.95	83	11.70	102	12.65	121	12.35
8	13.00	27	13.68	46	14.06	65	14.05	84	11.75	103	12.75	122	12.30
9	13.05	28	13.69	47	14.05	66	14.05	85	11.80	104	12.90	123	12.25
10	13.10	29	13.70	48	14.05	67	14.00	86	11.90	105	12.95	124	12.20
11	13.15	30	13.71	49	14.04	68	13.00	87	12.00	106	12.95	125	12.15
12	13.20	31	13.72	50	14.03	69	12.95	88	12.00	107	12.90	126	12.15
13	13.25	32	13.73	51	14.03	70	12.45	89	12.10	108	12.85		
14	13.30	33	13.75	52	14.02	71	12.35	90	12.20	109	12.85		
15	13.30	34	13.78	53	14.02	72	12.30	91	12.20	110	12.80		
16	13.25	35	13.81	54	14.01	73	12.25	92	12.10	111	12.75		
17	13.25	36	13.83	55	14.00	74	12.20	93	12.00	112	12.70		
18	13.25	37	13.89	56	13.95	75	12.15	94	11.95	113	12.65		
19	13.25	38	13.92	57	13.92	76	12.10	95	11.95	114	12.60		
												AVERAGE GRADE PLANE	13.00

HOTEL GRADE PLANE ELEVATIONS													
POINT #	ELEV	POINT #	ELEV	POINT #	ELEV	POINT #	ELEV	POINT #	ELEV	POINT #	ELEV	POINT #	ELEV
127	12.45	146	12.40	165	11.15	184	11.80	203	10.75	222	12.30	241	13.00
128	12.50	147	12.25	166	11.25	185	11.70	204	10.85	223	12.70	242	12.95
129	12.60	148	12.10	167	11.35	186	11.55	205	10.95	224	13.10	243	12.90
130	12.65	149	11.95	168	11.90	187	11.45	206	11.05	225	13.15	244	12.85
131	12.70	150	11.90	169	12.00	188	11.30	207	11.15	226	11.85	245	12.80
132	12.75	151	11.85	170	11.55	189	11.20	208	11.20	227	11.90	246	12.75
133	12.80	152	11.20	171	11.55	190	11.10	209	11.30	228	12.00	247	12.90
134	12.85	153	11.10	172	11.55	191	11.95	210	11.35	229	12.20	248	12.70
135	12.90	154	11.10	173	11.60	192	10.80	211	11.50	230	12.50		
136	12.90	155	11.10	174	11.65	193	10.65	212	11.60	231	12.60		
137	12.80	156	11.10	175	11.65	194	10.50	213	11.50	232	12.70		
138	12.75	157	11.10	176	11.65	195	10.30	214	11.45	233	12.90		
139	12.65	158	11.10	177	11.75	196	10.20	215	11.35	234	13.05		
140	12.55	159	11.10	178	11.80	197	10.05	216	11.30	235	13.20		
141	12.45	160	11.10	179	11.85	198	9.90	217	11.20	236	13.30		
142	12.35	161	11.10	180	11.90	199	10.00	218	11.15	237	13.30		
143	12.25	162	11.10	181	11.95	200	10.45	219	11.30	238	13.25		
144	12.65	163	11.10	182	12.00	201	10.55	220	11.50	239	13.15		
145	12.50	164	11.10	183	11.90	202	10.65	221	11.90	240	13.10		
												AVERAGE GRADE PLANE	11.84



BUILDING ELEVATIONS AND HEIGHTS					
	GRADE PLANE ELEVATION	BUILDING ELEVATION		BUILDING HEIGHT	
		ALLOWED	PROPOSED	ALLOWED	PROPOSED
MIXED USE	13.00'	73.00'	<73.00'	60.00'	<60.00'
HOTEL	11.84'	71.84'	<71.84'	60.00'	<60.00'

Last Saved: 3/19/2021 10:05:15 AM By: CJKrcnk
 Plotted On: 12/22/2020 10:05:15 AM By: CJKrcnk
 Tighe & Bond 210 P0595-007-Exhibits.dwg
 Figures/AutoCAD/Sheet/P-0595-007-EXHIBITS.dwg

Proposed Mixed Use Development

North Mill Pond Holdings, LLC

Portsmouth, New Hampshire

MARK	DATE	DESCRIPTION
C	3/22/2021	TAC Submission
B	3/10/2021	Design Review Resubmission
A	12/1/2020	TAC Work Session

PROJECT NO:	P-0595-007
DATE:	December 22, 2020
FILE:	P-0595-007-EXHIBITS.DWG
DRAWN BY:	CJK
CHECKED BY:	NAH/PMC
APPROVED BY:	BLM

GRADE PLANE EXHIBIT

SCALE: AS SHOWN



RAYNES AVE - PORTSMOUTH, NH

3/18/2021

SITE LANDSCAPE PLAN



31 Raynes Avenue, Portsmouth, NH: Wetland & Buffer Report

TO: Patrick Crimmins, PE
FROM: Leonard A. Lord, PhD, CSS, CWS
DATE: January 6, 2020
PROJECT: P-0595-007

On October 29, 2019, Tighe & Bond delineated and assessed tidal wetlands and their 100-foot buffers at 31 Raynes Avenue in Portsmouth, NH. This 1.35-acre parcel lies along the northwestern end of North Mill Pond.

Methods

The wetland delineation was based on criteria specified in the *Corps of Engineers Wetlands Delineation Manual, Technical Report Y-87-1* (January 1987) and the *Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Northcentral and Northeast Region* (January 2012). The Highest Observable Tide Line (HOTL) was delineated based on the definition found in the NH Department of Environmental Services (NHDES) Wetland Rules Env-Wt 101.49/Env-Wt 602.23. Wetlands were classified based on the *Classification of Wetlands and Deepwater Habitats of the United States* (Cowardin et al., 1979). The only wetlands located on the parcel are tidal wetlands (HOTL), which were delineated with sequentially-numbered flagging labelled 1B-1 to 1B-27.

Important wetland functions and values were also assessed and summarized in the vicinity of the parcel. The assessment was based on the *Maine Citizens Guide to Evaluating, Restoring, and Managing Tidal Marshes* (Bryan et al., 1997) and *The Highway Methodology Workbook Supplement—Wetland Functions and Values: A Descriptive Approach*, NAEPP-360-1-30a, US Army Corps of Engineers, New England Division, (September 1999).

Wetlands

Wetlands on this site were generally classified as estuarine intertidal rocky shore, rubble, regularly flooded (E2RS2N), though some areas exhibited more of a cobble-gravel substrate. The wetland edge slopes sharply along the southern portion of the site and is armored with rip rap. The northern portion of the wetland edge includes an old boat ramp, an old pier filled with sand and crushed stone, and a culvert outlet and headwall. Sparse halophytic vegetation along the upper portion of the tidal wetland edge includes sea lavender (*Limonium carolinianum*), salt meadow grass (*Spartina patens*), and seaside goldenrod (*Solidago sempervirens*). Important wetland functions in this portion of North Mill Pond include recreation potential and aesthetic quality, though both functions are impacted by the density and character of the surrounding urban development.

Tidal Buffer

The 100-foot tidal buffer on this parcel consists primarily of maintained lawn, a commercial building, and a parking lot. There is also an old wood-framed pier that is filled with sand and

crushed stone. There are small patches of shrubby vegetation and small trees at the tops of the slopes between the lawn and tidal wetlands, particularly at both ends of the wetland delineation. Species in these patches include autumn olive (*Elaeagnus umbellata*), staghorn sumac (*Rhus typhina*), Japanese knotweed (*Polygonum cuspidatum*), Norway maple (*Acer platanoides*), and Asiatic bittersweet (*Celastrus orbiculatus*). The highly developed tidal buffer provides some vegetated permeable surfaces to help reduce and filter runoff, but otherwise does little to enhance and protect the downgradient tidal wetland.

J:\P\0595 Pro Con General Proposals\0595-007 Raynes Ave Hotel\Environmental\Raynes+Green Wetlands+Soils\Raynes Ave Wetland-Buffer Rept 2020-1-9.docx

Photographic Log

Client: ProCon

Job Number: P-0595-007

Site: 31 Raynes Avenue, Portsmouth, NH

Photograph No.: 1	Date: 10/29/2019	Direction Taken: Northeast
Description: Steep wetland bank armored with riprap along the southern wetland edge.		
		

Photograph No.: 2	Date: 10/29/2019	Direction Taken: Southwest
Description: Culvert outlet, steep bank, and filled pier along northern wetland edge.		
		

Photographic Log

Client: ProCon

Job Number: P-0595-007

Site: 31 Raynes Avenue, Portsmouth, NH

Photograph No.: 3	Date: 10/29/2019	Direction Taken: North
Description: Grassed portion of the tidal buffer. Tidal wetland boundary marked with pink flags extends over the top of the slope into the lawn in the background.		

Photograph No.: 4	Date: 10/29/2019	Direction Taken: Southeast
Description: Commercial buildings and parking lot in the tidal buffer viewed from near the wetland edge.		

Photographic Log

Client: ProCon

Job Number: P-0595-007

Site: 31 Raynes Avenue, Portsmouth, NH

Photograph No.: 5	Date: 10/29/2019	Direction Taken: North
--------------------------	-------------------------	-------------------------------

Description: View of an old boat launch to the left and an old pier framed with wood and filled with sand and crushed stone to the right.



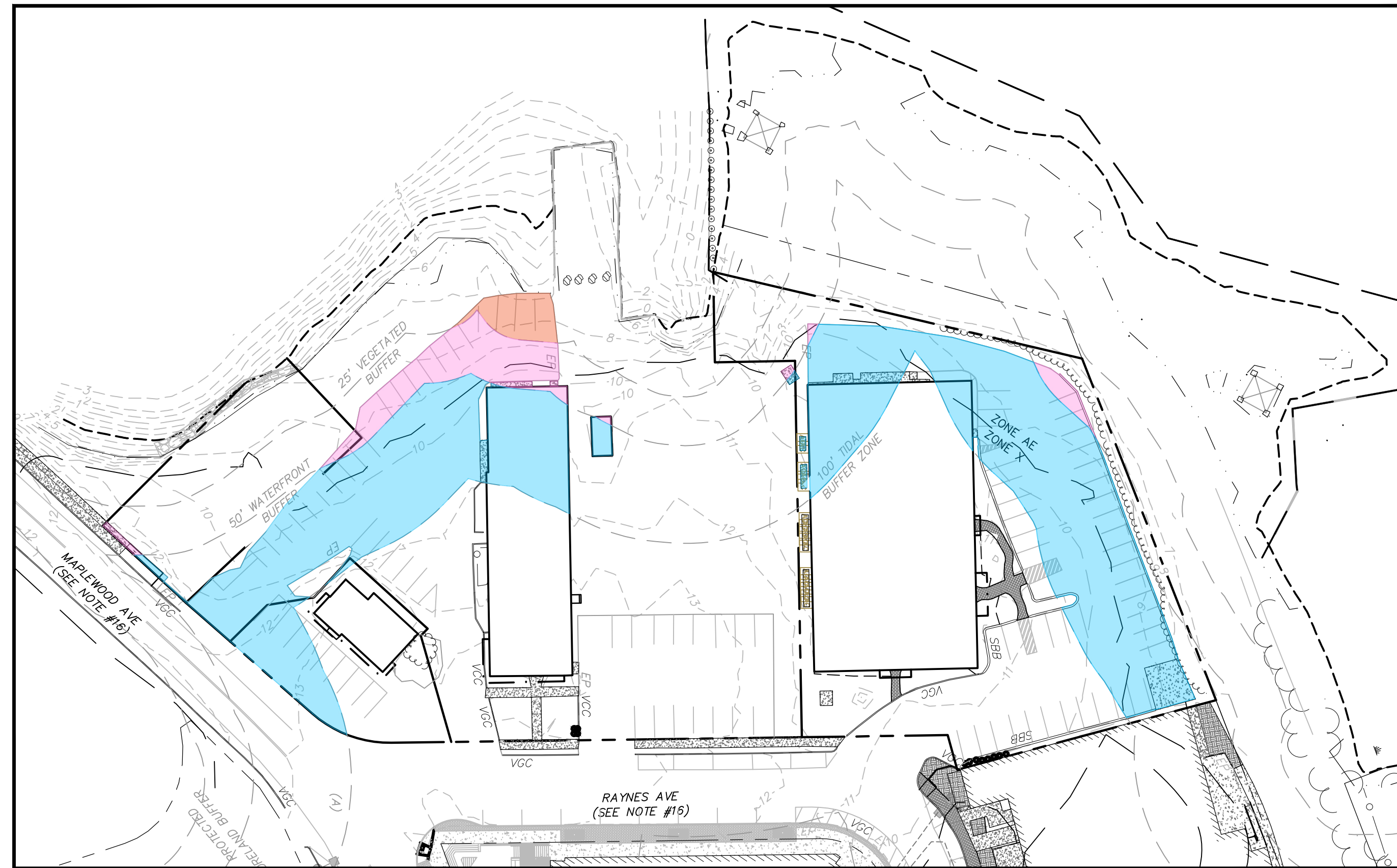
Photograph No.: 6	Date: 10/29/2019	Direction Taken: Northwest
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Description: Shrubby vegetation in the tidal buffer at the northern end of the site.



PROPOSED MIXED USE DEVELOPMENT PORTSMOUTH, NEW HAMPSHIRE

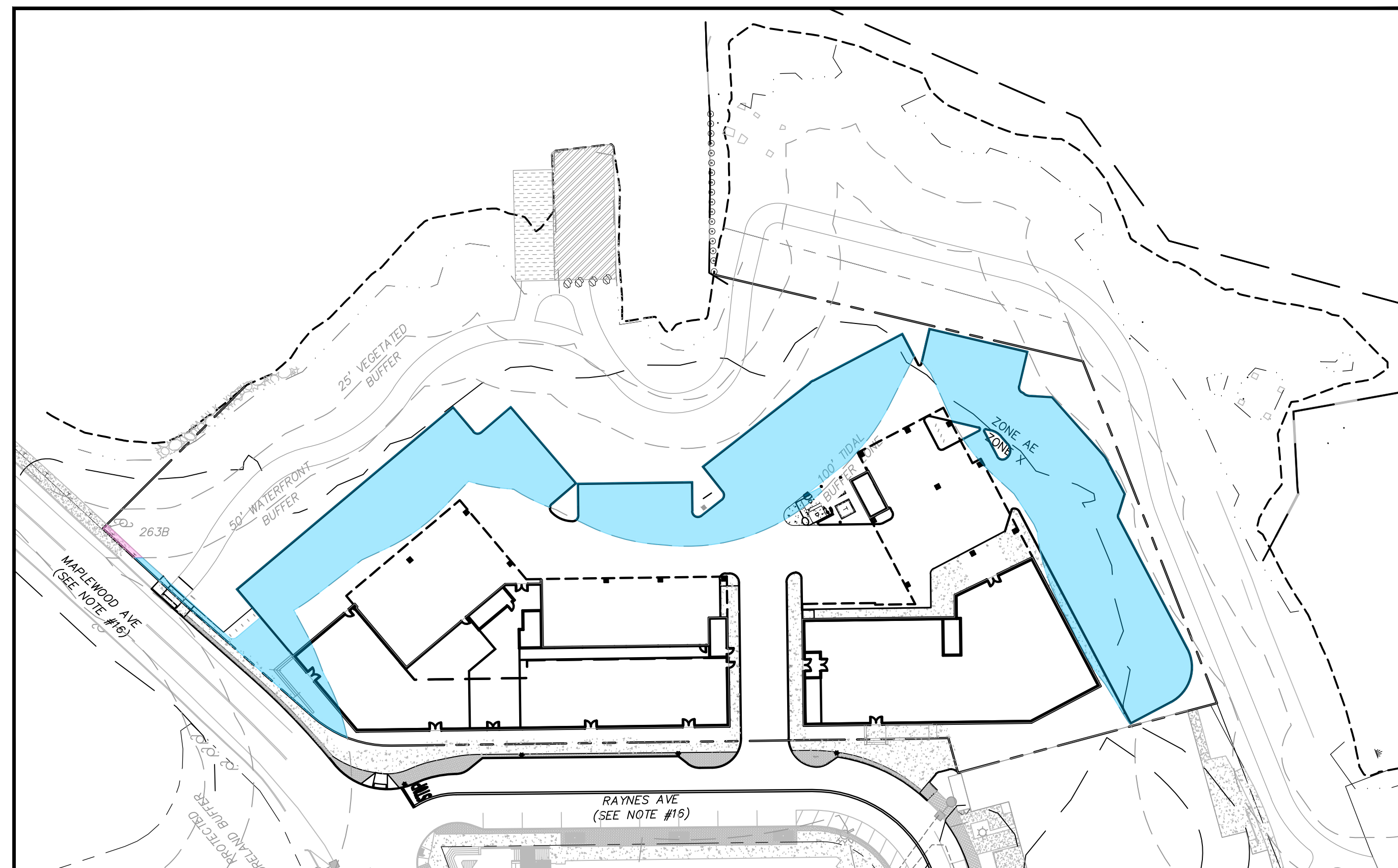
WETLAND BUFFER IMPERVIOUS SURFACE EXHIBIT



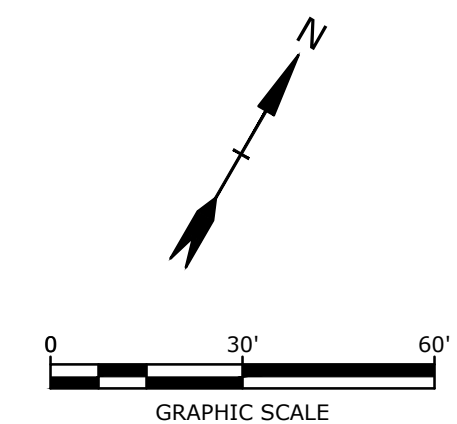
EXISTING CONDITIONS IMPERVIOUS SURFACE

Impervious Surface Within Buffer Area			
Wetland Buffer Setback	Existing Impervious Surface	Previous CC Work Session Site Plan	Current Proposed Site Plan
0 - 25 FT	848 SF	220 SF	0 SF
25 - 50 FT	3,006 SF	3,762 SF	67 SF (1)
50 - 100 FT	24,473 SF	28,411 SF	24,528 SF
Total Impervious Surface	28,327 SF	32,393 SF	24,595 SF

(1) Existing City Sidewalk Area



PROPOSED DEVELOPMENT IMPERVIOUS SURFACE



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Engineers | Environmental Specialists

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COMMUNITY OPEN SPACE:



GREENWAY
COMMUNITY SPACE

REQUIRED

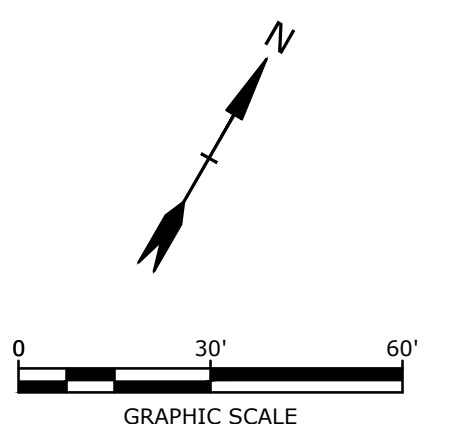
PROVIDED

TOTAL LOT AREA: 110,415 SF
COMMUNITY OPEN SPACE (20% OF TOTAL)

21,274 SF
20%

22,342 SF
20.2%

PROPOSED MIXED USE
DEVELOPMENT
PORTSMOUTH, NEW HAMPSHIRE
COMMUNITY SPACE EXHIBIT



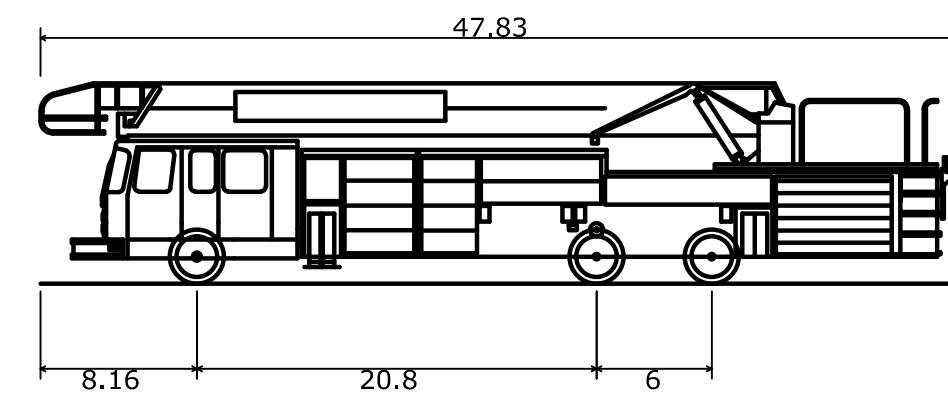
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March 22, 2021
P-0595-007-C-DSGN.dwg

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PROPOSED MIXED USE DEVELOPMENT PORTSMOUTH, NEW HAMPSHIRE

FIRE TRUCK TURNING EXHIBIT

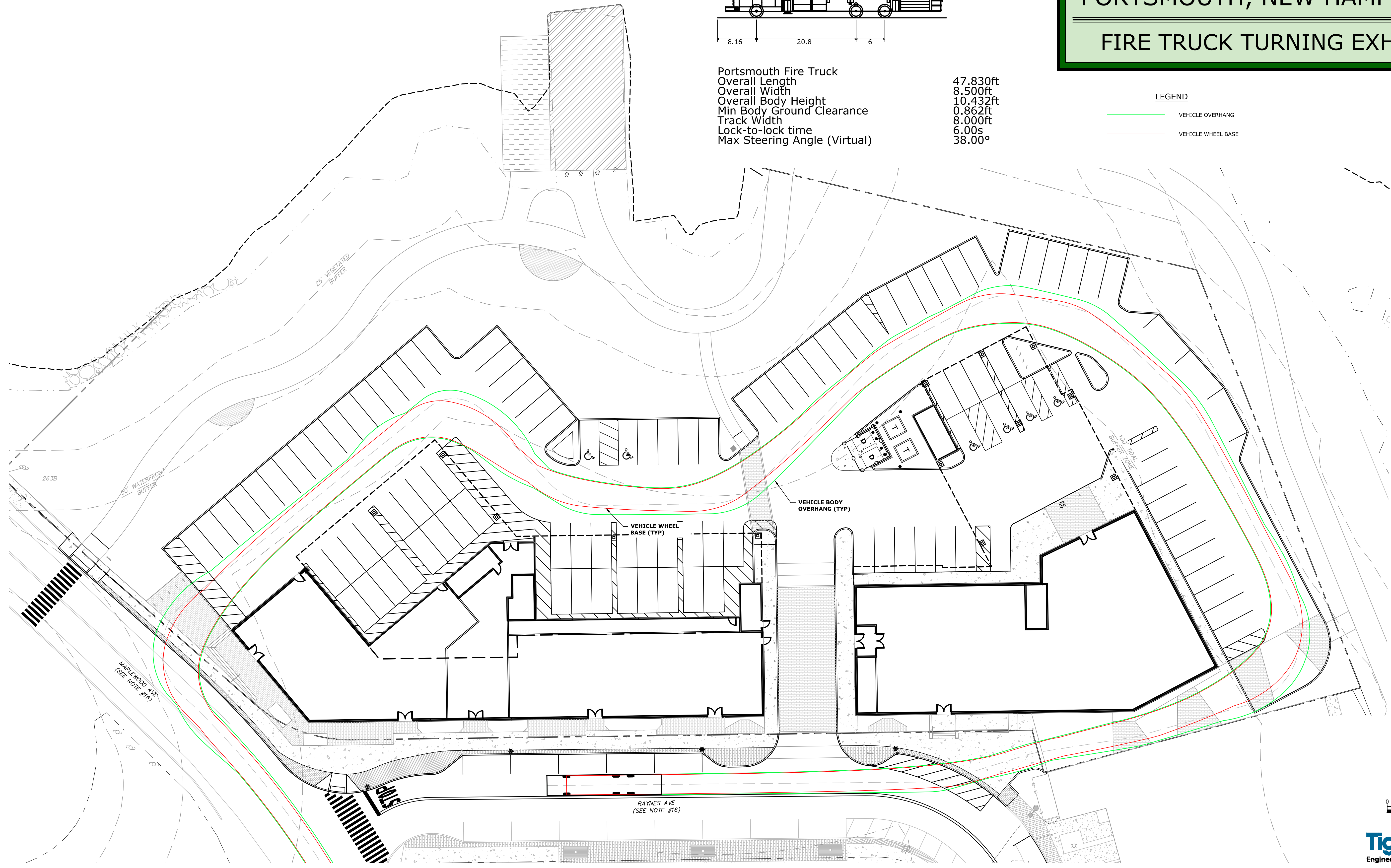


Portsmouth Fire Truck	
Overall Length	47.830ft
Overall Width	8.500ft
Overall Body Height	10.432ft
Min Body Ground Clearance	0.862ft
Track Width	8.000ft
Lock-to-lock time	6.00s
Max Steering Angle (Virtual)	38.00°

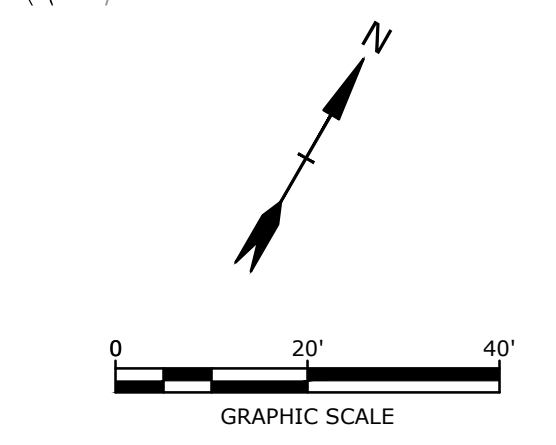
LEGEND

— VEHICLE OVERHANG

— VEHICLE WHEEL BASE



Last Save Date: March 19, 2021 10:58 AM By: CRZCUIK
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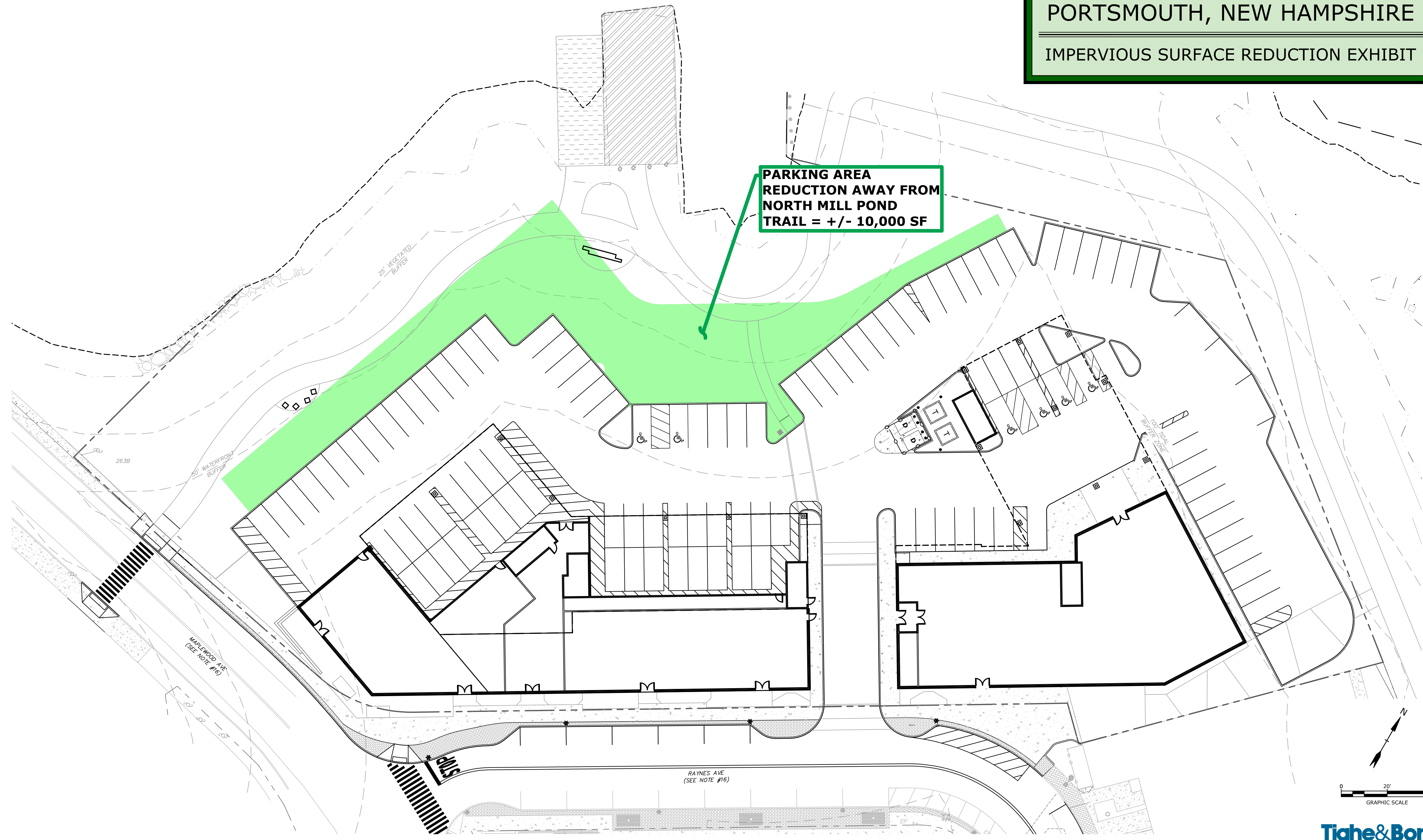


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Engineers | Environmental Specialists

March 22, 2021
P-0595-007-C-DSGN.dwg

PROPOSED MIXED USE
DEVELOPMENT
PORTSMOUTH, NEW HAMPSHIRE
IMPERVIOUS SURFACE REDUCTION EXHIBIT

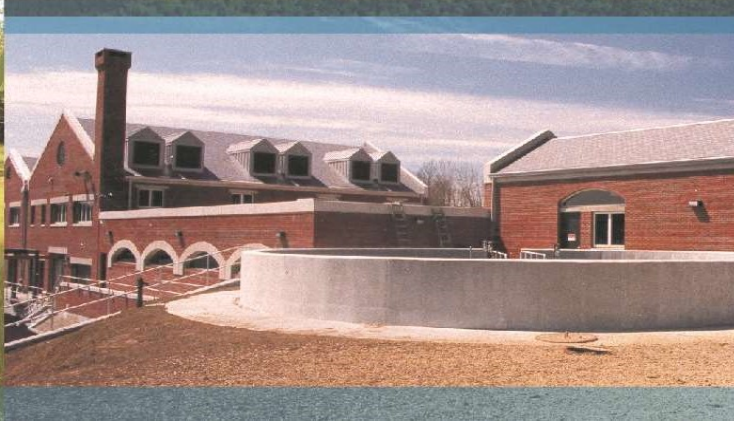
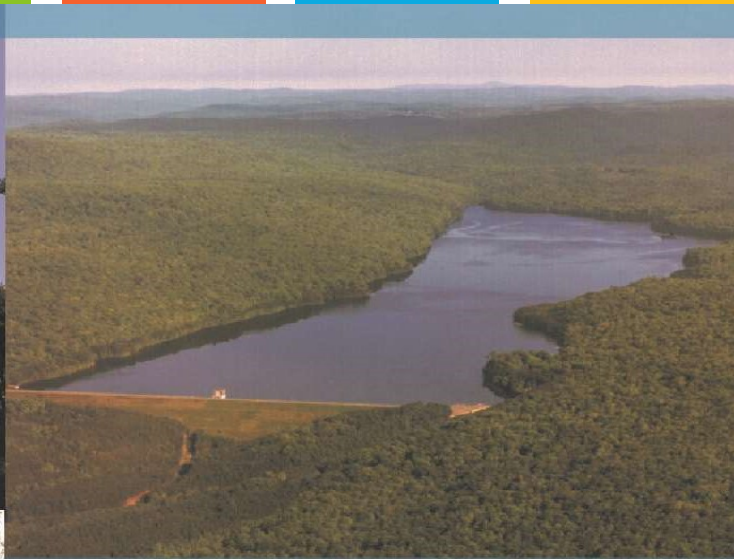
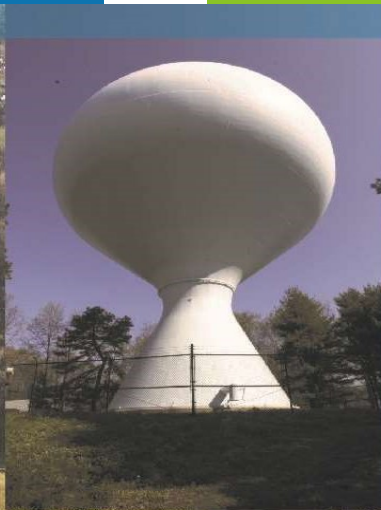
PARKING AREA
REDUCTION AWAY FROM
NORTH MILL POND
TRAIL = +/- 10,000 SF



Last Save Date: March 22, 2021 12:57 PM By: CRZCUIK
Plot Date: Monday, March 22, 2021 Plotted By: Colter Krzcuik
TSS File Location: J:\P\0595 Proj Con General Proposals\0595-007-Raynes Ave Hotel\Drawings_Figures\AutoCAD\Sheets\0595-007-EXHIBITS.dwg Layout Tab: IMPERVIOUS

Tighe & Bond

March 22, 2021
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Raynes Avenue Development
Portsmouth, NH

Traffic Impact Study

NORTH MILL POND HOLDINGS, LLC

March 22, 2021

Tighe&Bond

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- A. Tables
- B. Figures
- C. Site Plan
- D. Capacity Analysis Methodology
- E. Capacity Analyses Worksheets
- F. Trip Generation
- G. 111 Maplewood Avenue Traffic Evaluation
- H. 111 Maplewood Avenue Traffic Evaluation Response to Comments

Section 1

Introduction

Tighe & Bond has prepared this *Traffic Evaluation* to summarize the potential changes in the traffic operations resulting from the construction of a 128-room hotel, 60-unit residential building, 5,200 square feet (sf) of retail space, and 4,400 sf of restaurant space (the Project) located on the north side of Raynes Avenue in Portsmouth, New Hampshire (the Site).

The Site is bounded by Raynes Avenue to the south, Maplewood Avenue to the west, and Mill Pond to the north and east. The Site is currently developed with three buildings consisting of the Vanguard Key Club (1 Raynes Avenue), a vacant office building (31 Raynes Avenue), and a vacant laundromat (203 Maplewood Avenue). Paved/ unpaved parking areas and lawn/landscaping are provided for the existing uses. A portion of the parking area is currently used for paid parking.

Vehicular access to the Site will be provided via a new driveway located just west of the existing Vanguard Key Club curb cut, along the north side of Raynes Avenue. As part of the Project, approximately 111 parking spaces will be provided for use by the Project. The Project will close the two existing curb cuts on the north side of Raynes Avenue, as well as the nondelineated curb cut on Maplewood Avenue. A copy of the site plan is included in Appendix C.

The trip generation analysis indicates that the Project can be expected to generate approximately 101 trips during the weekday afternoon peak hour (58 entering trips, 43 exiting trips), the time at which traffic volumes on the adjacent roadway network are at their peak.

A weekday afternoon peak hour traffic operations analysis was prepared for the study intersections. The analysis was conducted for four different scenarios:

- 2020 No-Build scenario – 111 Maplewood Avenue Traffic Evaluation 2020 Build Condition traffic volumes
- 2020 Build scenario – adds the Project-generated traffic volumes to the 2020 No-Build scenario
- 2030 No-Build scenario – 111 Maplewood Avenue Traffic Evaluation 2030 Build Condition traffic volumes
- 2030 Build scenario – adds the Project-generated traffic volumes to the 2030 No-Build scenario

The Study builds off the 111 Maplewood Avenue Traffic Evaluation, conducted for a proposed office and retail development on the same block in 2019. Due to the expected reduction in traffic volumes as a result of the COVID-19 pandemic, traffic counts were not collected to support this traffic evaluation. Instead, the 2020 and 2030 Build traffic volumes from the previous study were used as the No-Build Condition traffic volumes for this evaluation.

The remainder of the report summarizes the evaluation which includes a description of the study area, traffic volumes during the weekday evening commuter peak period, trip generation estimates for the Project, estimated trip distribution patterns for the new Project generated trips, traffic volume projections for the analysis scenarios, traffic operations analysis for the study area intersections, and a summary of the study findings.

Section 2

Existing Conditions

This section includes a description of existing study area roadway geometry, intersection geometry, intersection traffic control, and data collection efforts within the study area. Figure 1 shows the location of the Site in context with the surrounding roadway network and study area.

2.1 Roadway Descriptions

Raynes Avenue is a two-lane roadway (one lane in each direction) that runs east-west between Maplewood Avenue and Vaughan Street. On-street parallel parking and sidewalks are provided on both sides of Raynes Avenue in the vicinity of the Project. The roadway has a posted speed limit of 25 miles per hour (mph) near the site.

The other study area roadways (Maplewood Avenue, Vaughan Street, Deer Street, Russell Street, and Market Street) within the study area have similar urban characteristics: two-lane roadways, on-street parallel parking, sidewalks, and low speed limits (25 mph or less). Land uses near the Site are a mix of commercial businesses, restaurants, hotels and residential.

2.2 Study Area Intersection Descriptions

Seven intersections were included for analysis in the study area. The study area is consistent with the study area used in the recently approved 111 Maplewood Avenue Traffic Evaluation.

Maplewood Avenue at Raynes Avenue

Raynes Avenue intersects Maplewood Avenue from the east to form a three-way unsignalized intersection. All approaches at this intersection provide a single general-purpose lane. Sidewalks are provided on both sides of Maplewood Avenue. On-street parallel parking is provided on both sides of Maplewood Avenue and Raynes Avenue. Maplewood Avenue operates with the right of way while the minor street approach of Raynes Avenue operates under stop control. A bike lane is striped along both sides of Maplewood Avenue.

Maplewood Avenue at Vaughan Street

Vaughan Street and a private driveway intersect Maplewood Avenue from the east and the west, respectively, to form a four-way unsignalized intersection. All approaches at this intersection provide a single general-purpose lane. Sidewalks are provided on both sides of Maplewood Avenue, but no crosswalks are provided at the intersection. On-street parallel parking is provided on both sides of Maplewood Avenue west of Vaughan Street and on both sides of Vaughan Street. A bike lane is striped along both sides of Maplewood Avenue north of the intersection and along Maplewood Avenue northbound approach south of the intersection. Maplewood Avenue operates with the right of way while the minor street approaches of Vaughan Street and the private driveway operate as the stop-controlled approaches.

Maplewood Avenue at Deer Street

Deer Street intersects Maplewood Avenue from the east and west to form a four-way signalized intersection. Maplewood Avenue southbound approach consists of left turn only lane and a right/through shared lane. Maplewood Avenue northbound approach consists of an exclusive left turn lane, exclusive through lane and an exclusive right turn lane. Deer Street eastbound approach consists of a single general-purpose lane. Deer Street westbound approach consists of an exclusive left turn lane and a right and through shared lane. The intersection is equipped with an exclusive actuated pedestrian phase. Each leg of the intersection has painted crosswalks.

Vaughan Street at Green Street

Green Street intersects Vaughan Street from the east, forming a three-way unsignalized intersection. Both roadways provide a single lane of travel in each direction. Vehicles exiting from Green Street operate under stop control. The width of Green Street ranges between 17 and 24 feet of pavement with no delineation of travel lanes or shoulders. A brick paver sidewalk exists on the east side of Green Street, south of the railroad tracks. On-street parking is allowed on the south side of Vaughan Street at the intersection.

Deer Street at Russell Street

Russell Street intersects Deer Street from the north to form a three-way unsignalized intersection. The southbound approach on Russell Street provides a single general-purpose lane that operates under a stop control. The westbound and eastbound approaches on Deer Street both provide a single general-purpose lane. The intersection provides sidewalks on all sides of the intersection approaches. A crosswalk is available for pedestrians crossing Deer Street east of Russell Street. On Street parking is available on all approaches.

Russell Street at Green Street

Green Street intersects Russell Street from the west to form a three-way unsignalized intersection. The eastbound approach of Green Street provides a single general-purpose lane that operates under stop control. The northbound and southbound approaches on Russell Street also both provide a single multi-use lane. Sidewalk is provided on both sides of Russell Street, but no crosswalks are provided at the intersection. On-street metered parking is provided on Russell Street south of Green Street.

Market Street at Russell Street

Russell Street intersects Market Street from the south, forming a three-way unsignalized intersection. Market Street eastbound consists of a through lane and a channelized right turn lane that operates as free flow movements. The westbound approach consists of a single through lane. The intersection geometry is designed to prohibit westbound left turns from Market Street to Russell Street. The Russell Street approach is a single lane that is wide enough for right turning vehicles to bypass waiting left turning vehicles. The Russell Street approach operates under stop control. Pedestrian crosswalks are provided along Russell Street and the westbound Market Street approach with sidewalks provided on all approaches. It is noted that the intersection is fully signalized with mast arms, vehicular and pedestrian signal heads, etc. However, the signal indications are in flashing mode, with yellow indications facing Market Street and red indication facing Russell Street.

2.3 Existing Traffic Data

Evaluation of the traffic impacts related to the Project requires the quantification of existing roadway and traffic conditions throughout the study area. As previously stated, turning movement counts were not conducted at this time due to expected lower than normal traffic volumes in the study area due to the effects of the COVID-19 pandemic. Therefore, the 2020 No-Build Condition traffic volumes used for this evaluation are the 2020 Build Condition traffic volumes developed for the 111 Maplewood Avenue Traffic Evaluation.

Manual turning movement and vehicle classification counts (TMCs) at the study area intersections for the previous study were collected in January 2019 during the weekday afternoon peak period (4:00 PM to 6:00 PM). A review of the data indicated that the weekday evening peak hour occurs between 5:00 PM and 6:00 PM. The traffic count data is provided in the 111 Maplewood Traffic Evaluation, located in Appendix F.

2.3.1 Seasonal Variation

The counts from the previous study were seasonally adjusted to peak month conditions based on nearby traffic volume count stations located in proximity to Portsmouth. Specifically, based on data available from the Urban Highway (Group 4) continuous count stations for years 2014 to 2016, a seasonal adjustment factor of 19 percent was used in the analysis. Detailed calculations are provided in the 111 Maplewood Avenue Traffic Evaluation report, located in Appendix F.

Section 3

No-Build Conditions

The following section describes the estimation of traffic volumes in the study area for the No-Build scenarios. The 2020 and 2030 No-Build scenarios will serve as the baseline for comparison purposes to measure the impacts of the Project.

3.1 Planned Roadway and Intersection Projects

Information obtained from the City traffic department staff was used to identify planned roadway improvement and new development projects in the area that could affect future traffic conditions. The following improvements, described in record studies prepared for other projects in the area, were considered when developing the No-Build conditions analysis.

- *Market Street/Russell Street reconstruction:* The City is in the early planning stages for the construction of a roundabout at the intersection of Market Street/Russell Street. At this time, no detailed plans have been developed. Therefore, this improvement is not included in the future conditions presented in this study. It is anticipated that a roundabout configuration would have a beneficial effect on the traffic operations and safety at the intersection.
- *North End Portsmouth Development (also referred to as the "Harbor Corp Project") Off-Site Improvements:* The timetable for this project is currently unknown. However, since the development related traffic volumes are included in the No-Build analysis, traffic improvements proposed for this development were also take into consideration, where applicable.
- *Maplewood Avenue Corridor Project:* The Maplewood Avenue corridor improvement project includes full depth pavement construction/reclamation, sidewalk construction, drainage/water/sewer improvements, traffic calming measures, pavement striping, and improvements to bicycle accommodations. The Project extends between Woodbury Avenue to the west and Dennett Street to the east. Construction will be completed in late 2020/ early 2021.
- *Maplewood Avenue Road Diet:* The City has conducted preliminary planning for a possible Maplewood Avenue Road Diet Project. The concept of the road diet would consider one through travel lane along Maplewood Avenue with auxiliary turn lanes provided, where necessary, at the intersections with Deer Street, Hanover Street, and Islington Street. This would present an opportunity for landscaped islands and/or improved bicycle accommodations. These improvements were not included in the future-year conditions as the construction timetable undetermined.
- *Maplewood Avenue Railroad Crossing:* NHDOT has been designing improvements for several rail crossings in the State. As part of the project, the DOT is seeking to reconstruct the at-grade crossing along Maplewood Avenue immediately north of Deer Street, as well as the railroad crossing on Green Street immediately west of Russell Street. The improvements are set to include new signage, railroad gates and signals where appropriate. However, this project has been delayed and implementation dates are currently unknown.

- As part of the Deer Street Garage and Mixed-Use Development, the eastbound shared left-through-right lane is proposed to be restriped as a dedicated left-turn lane and shared through-right lane. This is reflected in the 2030 No Build and 2030 Build conditions.

3.2 Traffic Growth

The 2030 No-Build Condition traffic volumes were developed using the 2030 Build Condition traffic volumes from the previous study. In the previous study, two components of traffic growth were considered. The first component was to estimate an annual average traffic growth rate. Based on a review of recent studies in the vicinity of the Project, a one percent per year background traffic growth rate was assumed in the analysis.

The second component to determining traffic growth was identifying any proposed development projects that are near or within the study area. Based on discussions with the City of Portsmouth staff during the previous study, it was determined that the following projects are either planned, under construction, or partially occupied. Traffic volumes related to these projects were obtained from record studies and distributed through the study area to develop the 2030 No Build Condition traffic volumes during the previous study.

- *Deer Street Garage and Mixed-Use Development:* This project will be located in the northwest corner of the Maplewood Avenue/Deer Street intersection. The traffic study for the project indicates that the full build-out of the project consists of a 600-stall municipal public parking garage with 4,700 sf of integral retail; and four mixed-use buildings. The four mixed-use buildings include a combination of 80 residential apartments, 108 hotel rooms, 41,300 sf of office, 20,000 sf of retail, 9,900 sf of restaurants, a 4,700 sf bar, and a 2,700 sf bank.
- *299 Vaughan Street:* This project is located at the corner of the intersection Vaughan Street and Green Street. It involves the demolition of an auto parts store and construction of a 143-room hotel with approximately 2,900 square feet of leasable commercial/retail space. This project has been constructed and is occupied.
- *40 Bridge Street:* This project consists of constructing a 4,025 sf restaurant and six residential condos. The project has been constructed.
- *75 Congress Street:* This project consists of constructing 10 residential condos. Due to the low traffic-generating nature of this land use and the limited number of units, traffic generated by the development was assumed to be included as part of the 1.0 percent annual background growth rate.
- *Harbor Corp Redevelopment:* This project consists of constructing a 98-room hotel and conference center, 14 condominium units, a 40,000 sf grocery store, and a 540-space parking garage.
- *172 Hanover Street:* The project consists of renovating a 7,000 sf restaurant that has been vacant for several years.
- *30 Maplewood Avenue:* The vacancy assumptions for this development that were included in the Deer Street garage traffic study were used in the current study as well.

- *46-64 Maplewood Avenue:* This project consists of constructing 22 residential apartments and 13,475 sf of retail space. The project has been constructed.
- *173-175 Market Street:* This project consists of constructing 3,331 sf of commercial space, 1,759 sf of office space, and six residential condos. The project has been constructed.
- *111 Maplewood Avenue:* This project includes 74,000 sf of office space with ancillary commercial space. The project is currently under construction and is expected to be completed in early 2021.

It is assumed that other smaller developments or small vacancies in existing developments are captured by the background traffic growth rate assumptions used in the previous study analysis.

3.3 Traffic Operations Analysis

Capacity and queue analyses were conducted for the 2020 and 2030 No Build Conditions during the afternoon peak hour using Trafficware Synchro Studio 10 – Traffic Analysis Software. The software conducts the analysis based up on the methodology provided in the Highway Capacity Manual, 6th Edition, 2016. The analysis results are categorized in terms of Level of Service (LOS), which describes the qualitative intersection operation conditions based on the calculated average delay per vehicle. A summary of the HCM capacity analysis methodology and a detailed definition of LOS is provided in Appendix D. The queue analysis results are summarized in terms of the 50th percentile queue length, and the 95th percentile queue length. The 50th percentile queue length represents the approximate average queue length, and the 95th percentile queue length represents the design queue length under peak traffic conditions. Tables 1 and 2 summarize the capacity and queue analyses results, respectively, Capacity analyses worksheets with full inputs, settings, and results are provided in Appendix E.

The analysis for the Maplewood Avenue/Deer Street signalized intersection indicates that failing operations are experienced on the side street approaches of the intersection under 2020 No Build conditions. These failing operations are exacerbated under 2030 No Build conditions when additional traffic volumes from future potential developments are included in the analysis. Vehicular queues exceed available storage on the westbound left-turn lanes during both 2020 and 2030 No Build conditions.

The unsignalized intersections generally experience acceptable intersection operations under 2020 No Build conditions but see a degradation in operations under 2030 No Build conditions. Under 2020 No Build conditions, the westbound approach at the intersection of Maplewood Avenue at Vaughan Street experiences LOS E, while the eastbound left movement at the intersection of Market Street at Russell Street experiences LOS F. When the future potential development volumes are added and ambient growth rate is applied to develop 2030 No Build traffic volumes, side street approaches at each unsignalized intersection except the Raynes Avenue at Site Driveway and Vaughan Street at Green Street intersections experience failing operations on side street approaches. The failing operations experienced on several side street approaches are consistent with the operations shown on previous traffic studies in the area. Significant queuing is experienced on the eastbound left approach at the intersection of Market Street at Russell Street under both 2020 and 2030 No Build conditions.

Section 4

Build Conditions

The Project includes a new 128-room hotel, 60-unit residential building, 5,200 square feet (sf) of retail space, and 4,400 sf of restaurant space plus approximately 111 parking spaces will be provided on Site. Additional parking demand for the Site may be handled by off-site parking areas, including the Foundry Place parking garage accessed via Deer Street and Bridge Street. The following sections describe the methodology to estimate the total number of Project-generated trips and their distribution within the study area roadway network.

4.1 Site Access

Site access will be provided via one unsignalized full access driveway on Raynes Avenue, just west of the existing Vanguard Key Club driveway. The existing driveways at the Vanguard Key Club, vacant laundry business, and vacant office building will be closed as part of the project.

Adequate sight distance will be provided in each direction at the proposed site driveway.

4.2 Trip Generation

Site generated traffic volumes for the Project were estimated using rates published in the Institute of Transportation Engineers (ITE) Trip Generation, 10th edition, 2017. ITE provides data to estimate the total number of vehicular trips associated with a site based on the specific land uses. To estimate the trip generation for the Project, ITE Land Use Code (LUC) 310 – Hotel, LUC 220 – Multifamily Housing (Mid-Rise), LUC 820 – Retail/Shopping Center, and LUC 931 – Quality Restaurant were used. The proposed trip generation for the weekday afternoon peak hour is presented in Table 3.

Mixed-use developments typically generate shared trips, also known as internal capture. The internal capture rate for the proposed development was determined using the National Cooperative Highway Research Program (NCHRP) Report 684: Enhancing Internal Trip Capture Estimation for Mixed-Use Developments by the Transportation Research Board, 2011. The total internal capture rate is estimated to be 22 percent for entering vehicles, and 29 percent for exiting vehicles. The detailed calculation spreadsheet is included in Appendix F.

Pass-by trips, or vehicles who are already on the roadway adjacent to the site (Raynes Avenue) were deducted from the trip generation estimate according to rates suggested by ITE. A pass-by trip rate of 34% was used for the retail component and a pass-by rate of 43% be used for the restaurant portion of the project. Similar pass-by rates were utilized for previously approved developments in the area.

Because there were no existing counts available for the three existing driveways on the site frontage on Raynes Avenue, existing site traffic volumes were not deducted from the trip generation estimate. This results in a conservative existing traffic volume estimate as it was observed that a portion of the parking areas are currently used as paid parking areas. Additional parking on the east side of the site is currently utilized by the Vanguard Key Club.

4.3 Arrival and Departure Distribution

The trip distribution identifies the various travel paths for vehicles arriving and leaving the Project site. Trip distribution patterns for the Project were based on a review of previous traffic studies conducted for nearby projects and observed travel patterns.

The following arrival/ departure distributions are anticipated for the residential, hotel, and restaurant trips:

- 30% to/from the west via Maplewood Avenue
- 30% to/from the east via Maplewood Avenue
- 40% to/from the northwest via Market Street

The following arrival/ departure distributions are anticipated for the retail trips:

- 30% to/from the west via Maplewood Avenue
- 55% to/from the east via Maplewood Avenue
- 15% to/from the northwest via Market Street

The trip distribution pattern for the hotel, residential, and restaurant uses as well as the retail use are shown in Figure 4. The vehicular trips associated with the Project were assigned to the study area and are shown in Figure 5 for the weekday afternoon peak hour.

4.4 Traffic Operations Analysis

The traffic operations analyses are based on the 2020 and 2030 Build traffic volumes. These volumes were developed by adding the Project-generated trips to the 2020 and 2030 No-Build traffic volume networks. The Build Condition traffic volume networks are shown in Figure 6 and Figure 7, respectively.

The signalized intersection of Maplewood Avenue at Deer Street experiences improved operations under the 2020 Build condition over the 2020 No-Build Condition as a result of minor timing and phasing modifications to the signal. Under 2030 Build conditions, the intersection continues to experience failing operations. It is important to note that the additional traffic volume generated by the development does not have a detrimental effect on the intersection operations. The failing operations shown are consistent with other previously approved developments in the area.

At the unsignalized locations within the study area, failing operations are experienced on the side street approaches of Maplewood Avenue at Raynes Avenue, Maplewood Avenue at Vaughan Street, and Market Street at Russell Street under 2020 Build conditions. Similarly, as shown in the 2030 No Build conditions, 2030 Build conditions experiences failing operations on all side street approaches except the intersection of Raynes Avenue at the Site Driveway and Vaughan Street at Green Street. These failing operations on side streets are typical in congested urban areas. Long queues for the eastbound left movement at the intersection of Market Street at Russell Street are increased further during 2020 and 2030 Build conditions.

The proposed development is not expected to significantly degrade intersection operations beyond what is already expected to be experienced under 2020 and 2030 No Build conditions.

Section 5

Conclusions and Recommendations

The Raynes Avenue redevelopment proposes to demolish the three existing buildings on the Site and construct a mixed-use development which will include a 128-room hotel, 60 residential units, 5,200 square feet (sf) of retail space, and 4,400 sf of restaurant space. The Site will provide approximately 111 parking spaces. Site access will be provided via a single full-access driveway on the north side of Raynes Avenue. The proposed redevelopment is expected to be complete in 2022.

The proposed redevelopment is expected to generate 101 vehicle trips (58 entering, 43 exiting) during the afternoon peak hour. The site trip generation includes an internal capture credit of 22% and 29% for entering and exiting vehicles, respectively. A pass-by trip rate of 34% was used for the retail use, while a 43% pass-by rate was used for the restaurant use. This methodology is in line with the industry standard and consistent with methodology utilized in similar traffic studies conducted by others in the area.

Capacity analysis indicates that when potential future projects in the area are all constructed, substantial traffic volumes will be added to the study area network which in turn could add delay and increase congestion at a number of intersections within the study area, especially for the side street movements. Site generated traffic represents a relatively small percentage of the cumulative traffic volume expected to be generated by the potential future projects.

As the planned projects get implemented, and the traffic improvements associated with the projects are designed, additional consideration should be given to accommodate side street movements. System-wide traffic improvement measures, such as promotion of reduced automobile usage, enhanced transit services to the area and promotion of remote/underutilized parking areas can also be considered by the City to reduce the volume of vehicular traffic generated within the downtown street network during peak times.

Based on the results of the foregoing analysis, it is the professional opinion of Tighe & Bond that the additional traffic expected to be generated from the Raynes Avenue development is not expected to significantly impact traffic operations within the study area.

APPENDIX A
Tables

TABLE 1

Intersection Operation Summary - Vehicular Levels of Service / Average Delay (sec/veh)

		Weekday Afternoon Peak Hour			
	Lane Use	2020 No Build	2020 Build	2030 No Build	2030 Build
Traffic Signal - Maplewood Avenue at Deer Street					
Overall		F / 105.5	F / 104.3	F / 113.4	F / 114.5
Deer Street	EB	F / 319.7	F / 319.7	-- / --	-- / --
	EBL	-- / --	-- / --	F / 200.1	F / 200.1
	EBTR	-- / --	-- / --	D / 46.8	D / 46.8
Maplewood Avenue	WBL	F / 240.2	F / 240.2	F / 400.7	F / 400.7
	WBTR	C / 34.9	C / 34.9	D / 37.6	D / 37.6
	NBL	B / 13.5	B / 13.5	B / 19.7	B / 19.7
	NBT	B / 18.7	B / 19.1	D / 39.4	D / 42.1
	NBR	A / 2.6	A / 2.9	A / 3.9	A / 3.9
	SBTR	A / 7.9	A / 7.9	C / 21.0	C / 22.8
		B / 11.7	B / 11.9	E / 65.1	E / 70.9
Unsignalized TWSC - Raynes Avenue at Site Driveway					
Overall		-- / --	-- / --	-- / --	-- / --
Raynes Avenue	EB	-- / --	A / 7.6	-- / --	A / 7.6
Site Driveway	SB	-- / --	A / 9.8	-- / --	A / 9.9
Unsignalized TWSC - Maplewood Avenue at Raynes Avenue					
Overall		-- / --	-- / --	-- / --	-- / --
Raynes Avenue	WB	D / 32.2	F / 52.7	F / 106.7	F / 247.4
Maplewood Avenue	SB	A / 9.7	A / 9.9	B / 10.8	B / 11.1
Unsignalized TWSC - Maplewood Avenue at Vaughan Street					
Overall		-- / --	-- / --	-- / --	-- / --
Vaughan Street	WB	E / 46.8	F / 51.8	F / 171.9	F / 188.3
Maplewood Avenue	SB	A / 9.6	A / 9.6	B / 10.5	B / 10.7
Unsignalized TWSC - Vaughan Street at Green Street					
Overall		-- / --	-- / --	-- / --	-- / --
Green Street	WB	A / 9.1	A / 9.2	A / 9.2	A / 9.3
Vaughan Street	SB	A / 7.5	A / 7.5	A / 7.5	A / 7.5
Unsignalized TWSC - Deer Street at Russell Street					
Overall		-- / --	-- / --	-- / --	-- / --
Deer Street	EB	A / 8.0	A / 8.0	A / 8.6	A / 8.6
Russell Street	SB	B / 13.1	B / 13.1	F / 57.8	F / 57.8
Unsignalized TWSC - Russell Street at Green Street					
Overall		-- / --	-- / --	-- / --	-- / --
Russell Street	NB	A / 8.6	A / 8.7	A / 9.1	A / 9.2
Green Street	EB	C / 21.8	D / 25.1	E / 40.2	F / 54.8
Unsignalized TWSC - Market Street at Russell Street					
Overall		-- / --	-- / --	-- / --	-- / --
Russell Street	EBL	F / 359.0	F / 392.1	F / 770.6	F / 808.5
	EBR	B / 10.6	B / 10.6	B / 10.8	B / 10.8

TABLE 2

Intersection Operation Summary - Vehicular 50th / 95th Percentile Queue (In Feet)

		Weekday Afternoon Peak Hour				
Lane Use	Available Storage	2020 No Build	2020 Build	2030 No Build	2030 Build	
Traffic Signal - Maplewood Avenue at Deer Street						
Deer Street	EB	590	465 / 430	465 / 430	-- / --	-- / --
	EBL	590	-- / --	-- / --	247 / 236	247 / 236
	EBTR	590	-- / --	-- / --	199 / 200	199 / 200
	WBL	100	306 / 381	306 / 381	356 / 426	356 / 426
	WBTR	350	113 / 149	113 / 149	160 / 202	160 / 202
	NBL	100	7 / 19	7 / 19	19 / 39	19 / 39
Maplewood Avenue	NBT	350	220 / 309	233 / 326	327 / 496	344 / 524
	NBR	350	2 / 35	4 / 38	0 / 45	0 / 45
	SBL	150	15 / 28	15 / 28	29 / 51	29 / 51
	SBTR	>500	183 / 226	191 / 234	451 / 573	472 / 594
Unsignalized TWSC - Raynes Avenue at Site Driveway						
Raynes Avenue	EB	175	-- / --	3	-- / --	3
Site Driveway	SB	90	-- / --	5	-- / --	5
Unsignalized TWSC - Maplewood Avenue at Raynes Avenue						
Raynes Avenue	WB	175	73	127	175	298
Maplewood Avenue	SB	>500	8	10	10	13
Unsignalized TWSC - Maplewood Avenue at Vaughan Street						
Vaughan Street	WB	325	73	80	163	170
Maplewood Avenue	SB	>500	3	3	3	3
Unsignalized TWSC - Vaughan Street at Green Street						
Green Street	WB	420	5	8	5	8
Vaughan Street	SB	400	3	5	3	5
Unsignalized TWSC - Deer Street at Russell Street						
Deer Street	EB	390	20	20	33	33
Russell Street	SB	650	78	78	415	415
Unsignalized TWSC - Russell Street at Green Street						
Russell Street	NB	410	0	0	0	0
Green Street	EB	420	33	50	68	105
Unsignalized TWSC - Market Street at Russell Street						
Russell Street	EBL	580	698	753	1280	1340
	EBR	150	0	0	0	0

TABLE 3

Site Generated Traffic Summary

Proposed - 128 Room Hotel			
Peak Hour Period	Enter	Exit	Total
Weekday Afternoon	36	34	70
Proposed - 60 Residential Units			
Peak Hour Period	Enter	Exit	Total
Weekday Afternoon	16	10	26
Proposed - 5,200 SF Retail Space			
Peak Hour Period	Enter	Exit	Total
Weekday Afternoon	10	10	20
Proposed - 4,400 SF Restaurant Space			
Peak Hour Period	Enter	Exit	Total
Weekday Afternoon	23	11	34
Total Trips			
Peak Hour Period	Enter	Exit	Total
Weekday Afternoon	85	65	150
Internal Capture¹			
Peak Hour Period	Enter	Exit	Total
Weekday Afternoon	19	19	38
Pass-By Trips (Retail)²			
Peak Hour Period	Enter	Exit	Total
Weekday Afternoon	1	1	2
Pass-By Trips (Restaurant)³			
Peak Hour Period	Enter	Exit	Total
Weekday Afternoon	7	2	9
Net Vehicular Trips (Total minus Pass-By Trips and Internal Capture)			
Peak Hour Period	Enter	Exit	Total
Weekday Afternoon	58	43	101

Source: Institute of Transportation Engineering, Trip Generation, 10th Edition, 2017.

Land Use - 220 Multifamily Housing (Mid-Rise)

310 Hotel

820 Retail

931 Quality Restaurant

¹NCHRP Report 684-Enhancing Internal Trip Capture Estimation for Mixed-Use Developments, Transportation Research Board, Washington, DC, 2011

²Average Pass-By Trip Percentage based on Table E.9 Pass-By and Non-Pass-By Trips Weekday, PM Peak Period, Land Use Code 820-Shopping Center, Trip Generation, 10th Edition, 2017

³Average Pass-By Trip Percentage based on Table E.30 Pass-By and Non-Pass-By Trips Weekday, PM Peak Period, Land Use Code 932-High-Turnover (Sit-Down) Restaurant, Trip Generation, 10th Edition, 2017

APPENDIX B
Figures



Legend



Study Area Intersection

Proposed Hotel
Raynes Avenue, Portsmouth, NH

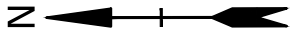
Study Area

DATE: 11/12/2020

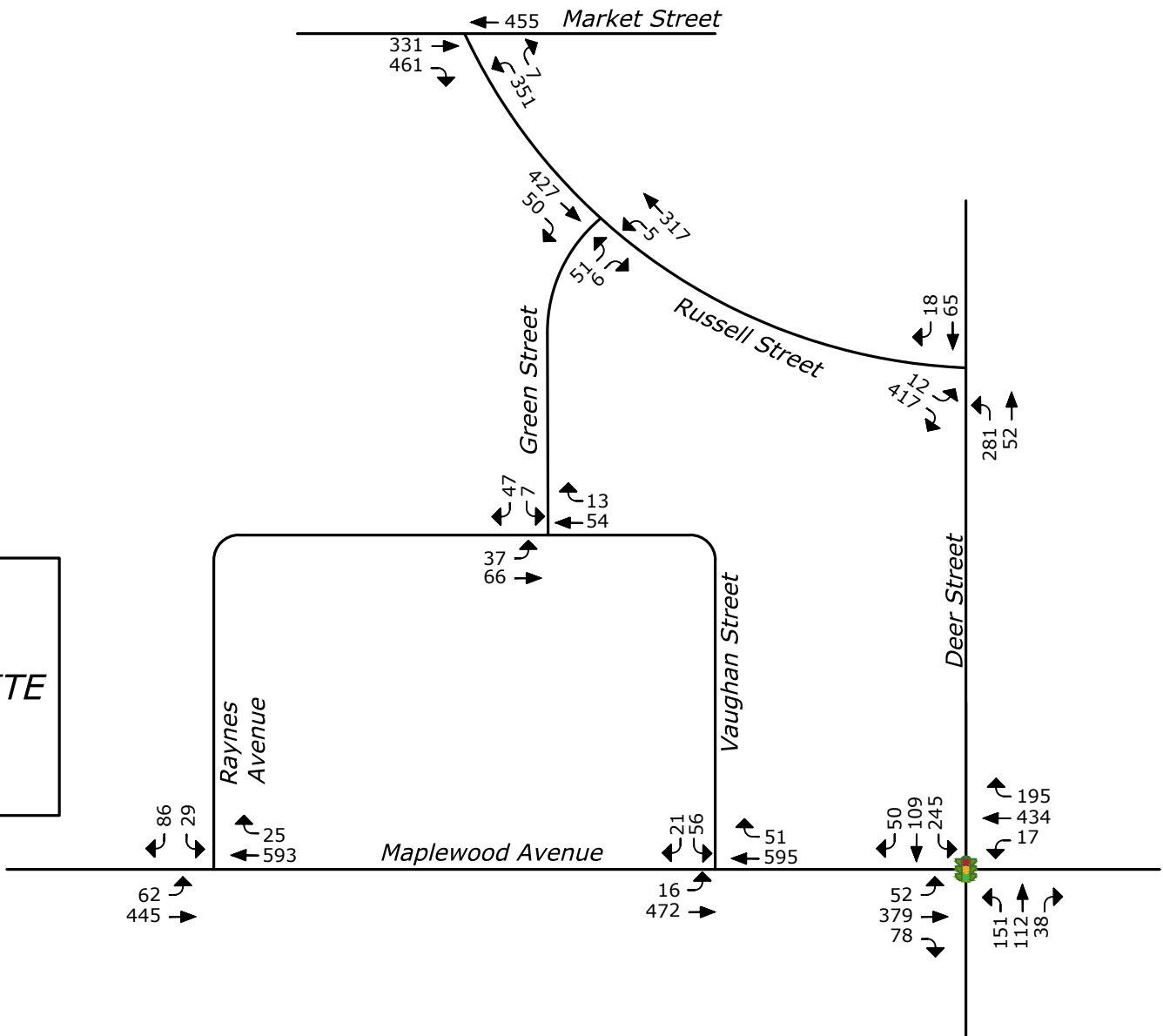
SCALE: 1" = 200'

FIGURE 1

Tighe & Bond
www.tighebond.com



SITE

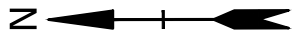


LEGEND

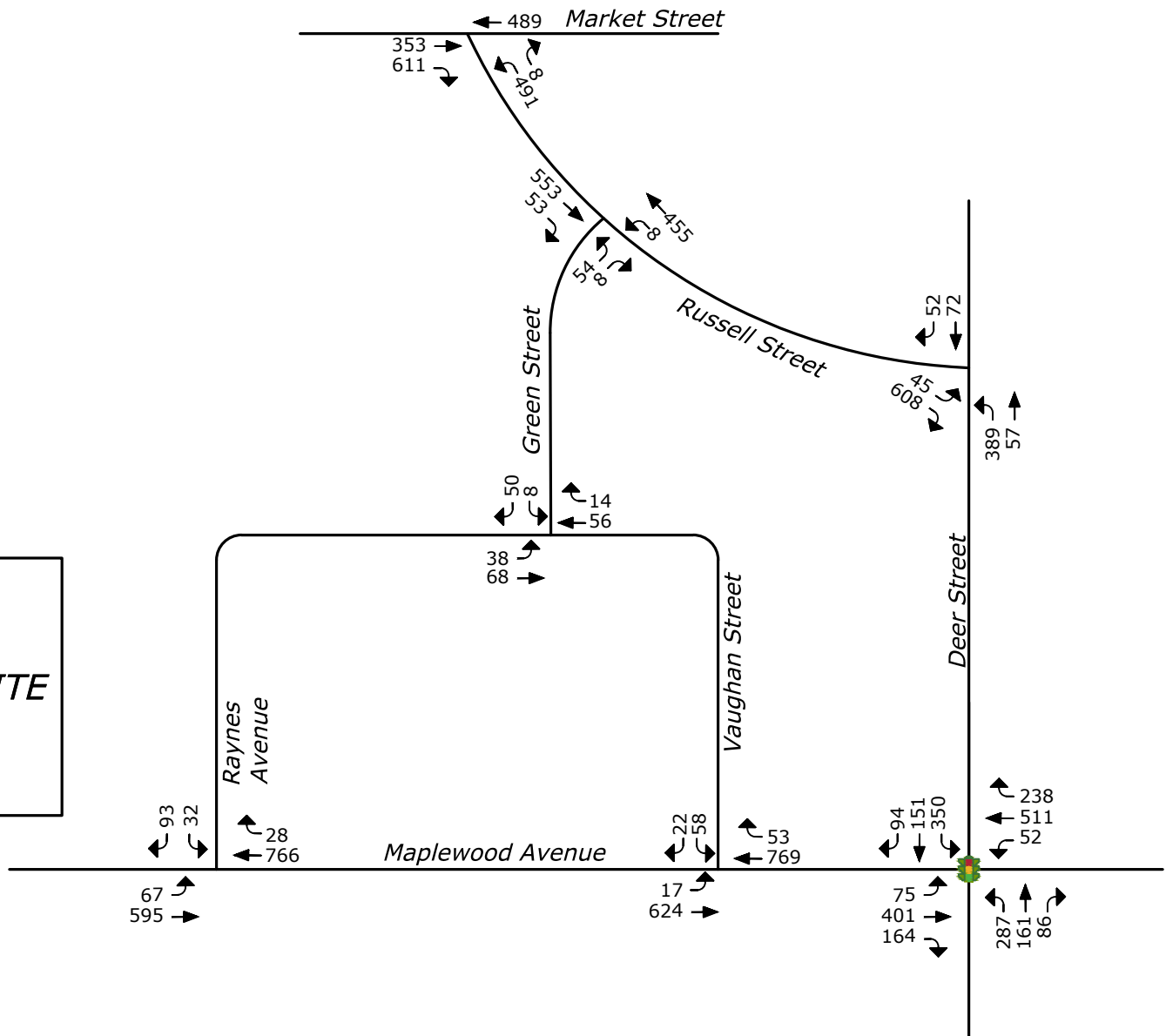


Traffic Signal

Proposed Hotel Raynes Avenue, Portsmouth, NH	
2020 No Build Peak Hour Traffic Volumes	
DATE: 11/12/2020	Tighe & Bond www.tighebond.com
SCALE: No Scale	
FIGURE 2	



SITE

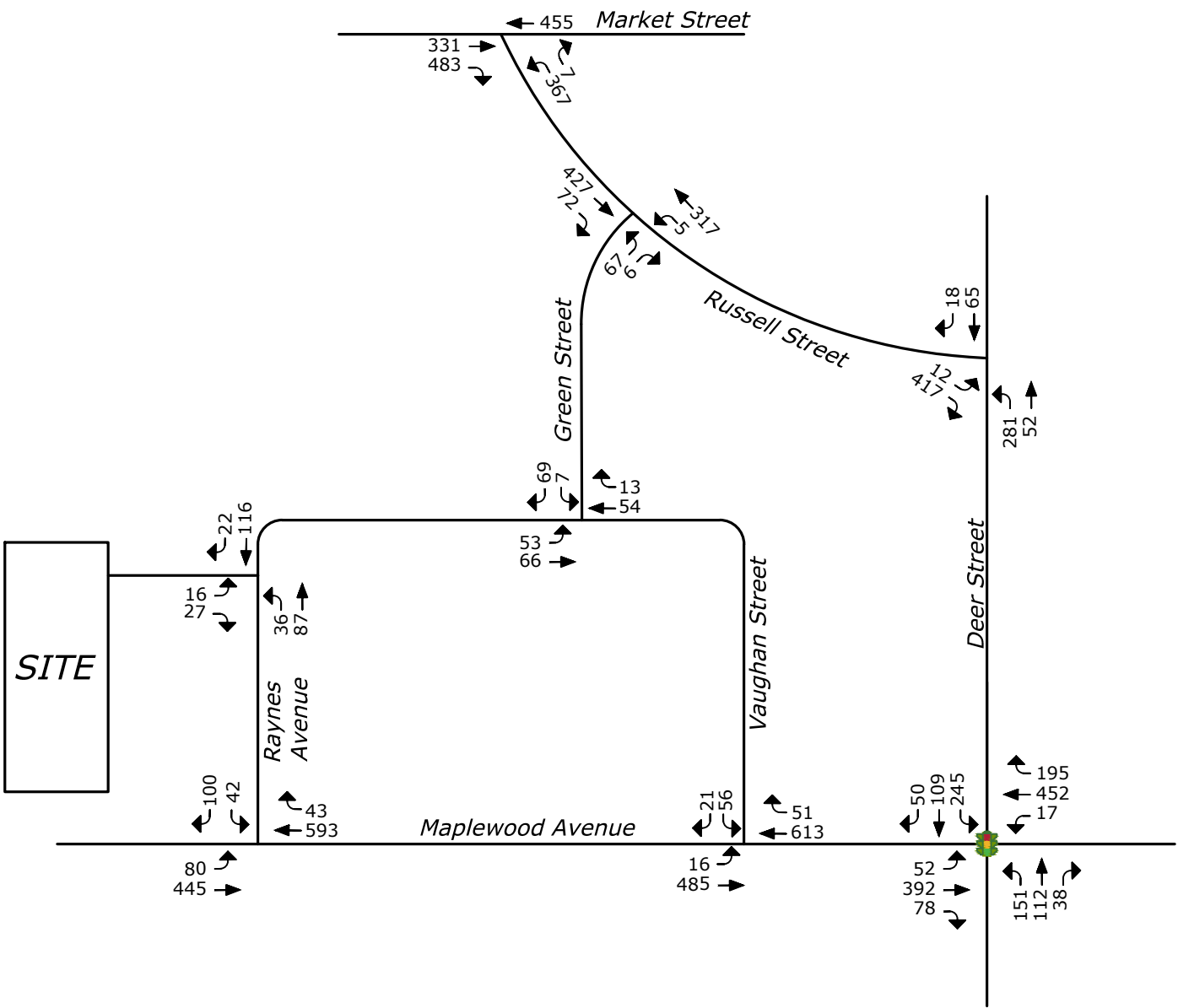
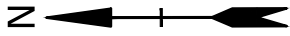


LEGEND



Traffic Signal

Proposed Hotel Raynes Avenue, Portsmouth, NH	
2030 No Build Peak Hour Traffic Volumes	
DATE: 11/12/2020	Tighe & Bond www.tighebond.com
SCALE: No Scale	
FIGURE 3	

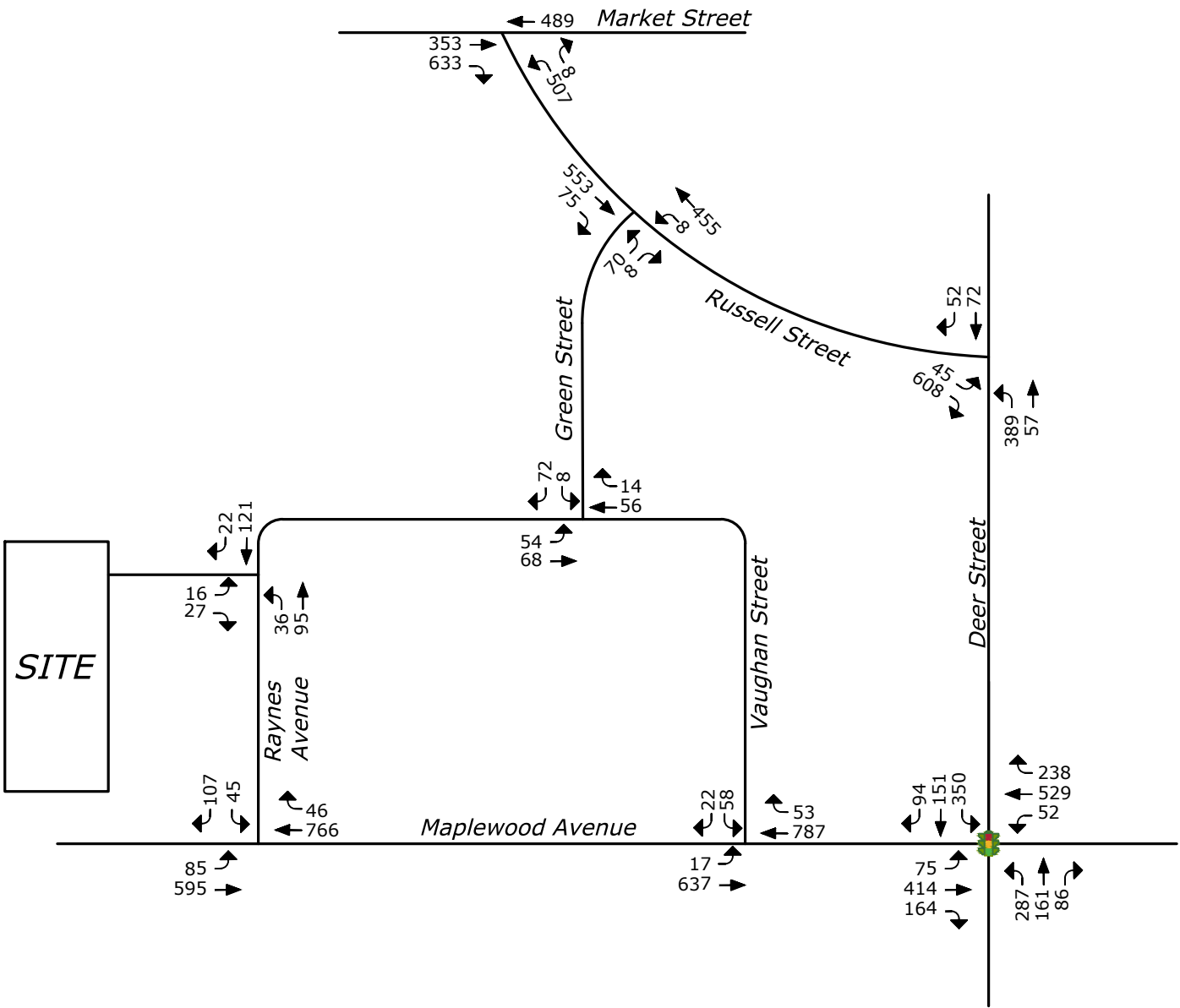
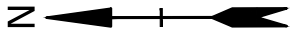


LEGEND




Traffic Signal

Proposed Hotel	
Raynes Avenue, Portsmouth, NH	
2020 Build Peak Hour Traffic Volumes	
DATE: 11/12/2020	Tighe & Bond www.tighebond.com
SCALE: No Scale	
FIGURE 6	

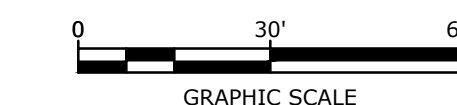


LEGEND

 Traffic Signal

Proposed Hotel Raynes Avenue, Portsmouth, NH	
2030 Build Peak Hour Traffic Volumes	
DATE: 11/12/2020	 www.tighebond.com
SCALE: No Scale	
FIGURE 7	

APPENDIX C
Site Development Plan



Proposed Hotel

PROCON, LLC

Portsmouth, New Hampshire

MARK	DATE	DESCRIPTION
PROJECT NO:	P-0595-007	
DATE:	11/17/2020	
FILE:	P-0595-007-C-DSGN.DWG	
DRAWN BY:	CKK	
CHECKED BY:	NAH/PMC	
APPROVED BY:	BLM	

SITE PLAN

SCALE: AS SHOWN

C-102

SITE DATA:

LOCATION: TAX MAP 123, LOT 10
TAX MAP 123, LOT 12
TAX MAP 123, LOT 13
TAX MAP 123, LOT 14
TAX MAP 123, LOT 12
RAYNES AVENUE

ZONING DISTRICT: CHARACTER DISTRICT 4 (CD4)
DOWNTOWN OVERLAY DISTRICT
NORTH END INCENTIVE OVERLAY DISTRICT
HISTORIC DISTRICT

PROPOSED USE: MULTI FAMILY DWELLING
HOTEL
RETAIL/RESTAURANT

PROPOSED LOT SIZE: ±2.53 ACRES (±110,415 SF)

DEVELOPMENT STANDARDS

BUILDING PLACEMENT (PRINCIPAL BUILDING):	REQUIRED	PROPOSED
MAXIMUM PRINCIPAL FRONT YARD:	15 FT	45 FT
MAXIMUM SECONDARY FRONT YARD:	12 FT	45 FT
SIDE YARD:	NR	NR
MINIMUM REAR YARD:	5 FT	N/A
MINIMUM FRONT LOT LINE BUILDOUT:	50%	±68.8%
BUILDING AND LOT OCCUPATION:		
MAXIMUM BUILDING BLOCK LENGTH:	200 FT	196 FT
MAXIMUM FACADE MODULATION LENGTH:	80 FT	<100 FT
MAXIMUM ENTRANCE SPACING:	50 FT	<50 FT
MAXIMUM BUILDING COVERAGE:	90%	±47.0%
MAXIMUM BUILDING FOOTPRINT:	*15,000 SF	17,565 SF 13,815 SF
MINIMUM LOT AREA:		
MINIMUM LOT AREA PER DWELLING UNIT:	NR	NR
MINIMUM OPEN SPACE:	10%	25.3%
MAXIMUM GROUND FLOOR GFA PER USE:	15,000 SF	6,514 SF

*ZONING ORDINANCE 10.5A46.20 ALLOWS 30,000SF BUILDING FOOTPRINT WITH 20% COMMUNITY SPACE.

BUILDING FORM (PRINCIPAL BUILDING):	REQUIRED	PROVIDED
BUILDING HEIGHT:	**LOT 12 -2ST, 3S' **LOT 13 -3ST, 40' **LOT 14 -4ST, 50'	
MAXIMUM FINISHED FLOOR SURFACE OF GROUND FLOOR ABOVE SIDEWALK GRADE:	36 IN	<36"
MINIMUM GROUND STORY HEIGHT:	12 FT	12 FT
MINIMUM SECOND STORY HEIGHT:	10 FT	10 FT
FACADE GLAZING:	70%	70%
ALLOWED ROOF TYPES:	FLAT, GABLE, HIP, GAMBREL, MANSARD	FLAT, GABLE, HIP, GAMBREL, MANSARD

**ZONING ORDINANCE 10.5A46.20 ALLOWS A 1-STORY, UP TO 10' HEIGHT INCREASE WITH 20% COMMUNITY SPACE.

COMMUNITY SPACE:	REQUIRED	PROVIDED
MAXIMUM BUILDING FOOTPRINT PLUS 1-STORY, MAX 10 FT	22,083 SF	26,296 SF
MINIMUM SIDEWALK WIDTH	20%	23.8%

INCENTIVES TO DEVELOPMENT STANDARDS

	REQUIRED	PROVIDED
MAXIMUM BUILDING FOOTPRINT	30,000 SF	20,155 SF
PLUS 1-STORY, MAX 10 FT	**60 FT	55 FT
MINIMUM SIDEWALK WIDTH	***14 FT	14 FT

***ZONING ORDINANCE 10.5A46.10, FOOTNOTE 4 REQUIRES THE SIDEWALK WIDTH TO BE 10 FT PLUS AN EXTRA 2 FT FOR EACH STORY OF BUILDING HEIGHT ABOVE 3 STORIES.

OFF-STREET PARKING REQUIREMENTS

PARKING SPACES REQUIRED:	REQUIRED	PROVIDED
DWELLING UNITS:		
0 SF TO 500 SF, 0.5 SPACES PER UNIT	17 UNITS	8.5 SPACES
500 SF TO 750 SF, 1.0 SPACES PER UNIT	33 UNITS	33 SPACES
OVER 750 SF, 1.3 SPACES PER UNIT	10 UNITS	13 SPACES
TOTAL MINIMUM RESIDENTIAL SPACES REQUIRED =		55 SPACES

VISITORS:	REQUIRED	PROVIDED
1 SPACES PER 5 DWELLING UNITS	60 UNITS	12 SPACES

HOTEL:	REQUIRED	PROVIDED
0.75 SPACES PER GUEST ROOM	128 ROOMS	96 SPACES

DOWNTOWN OVERLAY DISTRICT - 4 SPACES

TOTAL MINIMUM PARKING SPACES REQUIRED = 159 SPACES

TOTAL PARKING SPACES PROVIDED = 156 SPACES

SIX (6) ADA ACCESSIBLE SPACES REQUIRED

PARKING STALL SIZE:	REQUIRED	PROVIDED
DRIVE AISLE:	8.5' X 19'	8.5' X 19'
	**22'	22'

***ZONING ORDINANCE 10.1114.21 ALLOWS MINIMUM 22' AISLE WIDTH FOR 90 DEGREE PARKING IN A PARKING STRUCTURE

BIKE SPACES REQUIRED:	REQUIRED	PROVIDED
1 BIKE SPACE / 10 PARKING SPACES	159 SPACES	16 SPACES

PROPOSED HOTEL GROSS FLOOR AREA

FLOOR	LOBBY (SF)	UNITS	FLOOR AREA (SF)	TOTAL (SF)
FIRST	7,400	0	0	7,400
SECOND	0	32	14,000	14,000
THIRD	0	32	14,000	14,000
FOURTH	0	32	14,000	14,000
FOURTH	0	32	14,000	14,000
TOTAL	7,400	128	56,000	63,400

PROPOSED MIXED USE GROSS FLOOR AREA

FLOOR	TENANT (SF)	UNITS	FLOOR AREA (SF)	TOTAL (SF)
FIRST	8,100	0	2,000	10,100
SECOND	0	17	15,200	15,200
THIRD	0	17	15,200	15,200
FOURTH	0	17	15,200	15,200
FOURTH	0	9	9,950	9,950
TOTAL	8,100	60	57,550	65,650

SITE NOTES:

1. STRIPE PARKING AREAS AS SHOWN, INCLUDING PARKING SPACES, STOP BARS, ADA SYMBOLS, PAINTED ISLANDS, CROSS WALKS, ARROWS, LEGENDS AND CENTERLINES SHALL BE THERMOPLASTIC MATERIAL. THERMOPLASTIC MATERIAL SHALL MEET THE REQUIREMENTS OF AASHTO M249. (ALL MARKINGS EXCEPT CENTERLINE AND MEDIAN ISLANDS TO BE CONSTRUCTED USING WHITE TRAFFIC PAINT; CENTERLINE AND MEDIAN ISLANDS TO BE CONSTRUCTED USING YELLOW TRAFFIC PAINT. ALL TRAFFIC PAINT SHALL MEET THE REQUIREMENTS OF AASHTO M248 TYPE "F").
2. ALL PAVEMENT MARKINGS AND SIGNS TO CONFORM TO "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKINGS", AND THE AMERICANS WITH DISABILITIES ACT REQUIREMENTS, LATEST EDITIONS.
3. SEE DETAILS FOR PARKING STALL MARKINGS, ADA SYMBOLS, SIGNS AND SIGN POSTS.
4. CENTERLINES SHALL BE FOUR (4) INCH WIDE YELLOW LINES. STOP BARS SHALL BE EIGHTEEN (18) INCHES WIDE.
5. PAINTED ISLANDS SHALL BE FOUR (4) INCH WIDE DIAGONAL LINES AT 3'-0" O.C. BORDERED BY FOUR (4) INCH WIDE LINES.
6. THE CONTRACTOR SHALL EMPLOY A NEW HAMPSHIRE LICENSED LAND SURVEYOR TO DETERMINE ALL LINES AND GRADES.
7. CLEAN AND COAT VERTICAL FACE OF EXISTING PAVEMENT AT SAW CUT LINE WITH RS-1 EMULSION IMMEDIATELY PRIOR TO PLACING NEW BITUMINOUS CONCRETE.
8. ALL MATERIALS AND CONSTRUCTION SHALL CONFORM WITH APPLICABLE FEDERAL, STATE, AND LOCAL CODES & SPECIFICATIONS.
9. COORDINATE ALL WORK WITHIN PUBLIC RIGHT OF WAY WITH THE CITY OF PORTSMOUTH.
10. CONTRACTOR TO SUBMIT AS-BUILT PLANS IN DIGITAL FORMAT (.DWG AND .PDF FILES) ON DISK TO THE OWNER AND ENGINEER UPON COMPLETION OF THE PROJECT. AS-BUILTS SHALL BE PREPARED AND CERTIFIED BY A NEW HAMPSHIRE LICENSED LAND SURVEYOR.
11. SEE ARCHITECTURAL/BUILDING DRAWINGS FOR ALL CONCRETE PADS & SIDEWALKS ADJACENT TO BUILDING.
12. ALL WORK SHALL CONFORM TO THE CITY OF PORTSMOUTH DEPARTMENT OF PUBLIC WORKS, STANDARD SPECIFICATIONS AND WITH THE STATE OF NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION, "STANDARD SPECIFICATIONS OF ROAD AND BRIDGE CONSTRUCTION", CURRENT EDITION.
13. CONTRACTOR TO PROVIDE BACKFILL AND COMPACTION AT CURB LINE AFTER CONCRETE FORMS FOR SIDEWALKS AND PADS HAVE BEEN STRIPPED. COORDINATE WITH BUILDING CONTRACTOR.
14. ALL LIGHT POLE BASES NOT PROTECTED BY A RAISED CURB SHALL BE PAINTED YELLOW.
15. COORDINATE ALL WORK ADJACENT TO BUILDING WITH BUILDING CONTRACTOR.
16. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING RETAINING WALL DESIGN FROM STRUCTURAL ENGINEER AND/OR WALL MANUFACTURER. CONTRACTOR SHALL FURNISH ALL LABOR, MATERIALS AND EQUIPMENT REQUIRED TO CONSTRUCT WALL IN ACCORDANCE WITH DESIGN APPROVED BY THE ENGINEER. RETAINING WALL SHALL BE SEGMENTAL BLOCK WALL SYSTEM AS SHOWN IN THE DETAILS.
17. ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
18. ALL CONDITIONS ON THIS PLAN SHALL REMAIN IN EFFECT IN PERPETUITY PURSUANT TO THE REQUIREMENTS OF THE SITE PLAN REVIEW REGULATIONS.
19. THE APPLICANT SHALL HAVE A SITE SURVEY CONDUCTED BY A RADIO COMMUNICATIONS CARRIER APPROVED BY THE CITY'S COMMUNICATIONS DIVISION. THE RADIO COMMUNICATIONS CARRIER MUST BE FAMILIAR AND CONVERSANT WITH THE POLICE AND RADIO CONFIGURATION. IF THE SITE SURVEY INDICATES IT IS NECESSARY TO INSTALL A SIGNAL REPEATER EITHER ON OR NEAR THE PROPOSED PROJECT, THOSE COSTS SHALL BE THE RESPONSIBILITY OF THE PROPERTY OWNER. THE OWNER SHALL COORDINATE WITH THE SUPERVISOR OF RADIO COMMUNICATIONS FOR THE CITY.
20. THE PROPOSED LOADING ZONE ON RAYNES AVE SHALL BE REVIEWED BY THE PARKING & TRAFFIC SAFETY COMMITTEE ON AUGUST 1, 2019. ANY ADDITIONAL LOADING ZONES WILL REQUIRE THE APPROVAL OF THE PARKING & TRAFFIC SAFETY COMMITTEE.
21. THE VALUE OF A FAIR CONTRIBUTION TOWARDS OFF-SITE PUBLIC IMPROVEMENT PROJECTS SHALL BE AGREED UPON BETWEEN THE APPLICANT AND CITY PRIOR TO PLANNING BOARD APPROVALS.
22. RAYNES AVE LAYOUT DESIGNED AS PART OF THE CITY OF PORTSMOUTH'S COMPLETE STREETS IMPROVEMENT PROJECT THAT IS BEING DESIGNED BY THE CITY'S CONSULTANT.
23. ALL TREES PLANTED ARE TO BE INSTALLED UNDER THE SUPERVISION OF THE CITY OF PORTSMOUTH DPW USING STANDARD INSTALLATION METHODS.
24. THE APPLICANT SHALL PREPARE A CONSTRUCTION MANAGEMENT AND MITIGATION PLAN (CMMP) FOR REVIEW AND APPROVAL BY THE CITY'S LEGAL AND PLANNING DEPARTMENTS.
25. A TEMPORARY SUPPORT OF EXCAVATION (SOE) PLAN SHALL BE PREPARED BY THE APPLICANT'S CONTRACTOR TO CONFIRM ANY TEMPORARY ENCUMBRANCES OF THE CITY'S RIGHT-OF-WAY. IF LICENSES ARE REQUIRED FOR THE SOE, THE APPLICANT WILL BE REQUIRED TO OBTAIN THESE FROM THE CITY PRIOR TO CONSTRUCTION.
26. THE APPLICANT AGREES TO EXECUTE A PROSPECTIVE DEVELOPMENT INCENTIVE AGREEMENT FOR THE EXCESS COMMUNITY SPACE AREAS PRIOR TO CONSTRUCTION.

TAX MAP 123, LOT 15-1
299 VAUGHAN STREET LLC
C/O CATHARTES PRIVATE
INVESTMENTS
100 SUMNER STREET, SUITE 1600
BOSTON, MA 02110
R.C.R.D. BOOK 5434, PAGE 2905

TAX MAP 123, LOT 15
CITY OF PORTSMOUTH
1 JUNKINS AVE
PORTSMOUTH, NH,
03801
R.C.R.D. BOOK 5904
PAGE 2777

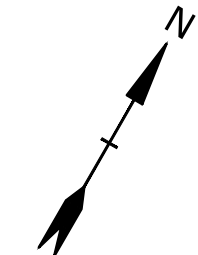
TAX MAP 124, LOT 9
319 VAUGHAN STREET CENTER LLC
104 GRAFTON DRIVE
PORTSMOUTH, NH 03801
R.C.R.D. BOOK 5506, PAGE 427

TAX MAP 124 LOT 8
111 MAPLEWOOD AVENUE LLC
210 COMMERCE WAY SUITE 300
PORTSMOUTH, NH 03801
R.C.R.D. BOOK 6026 PAGE 2219

TAX MAP 124 LOT 7-1
GIDEON WALKER HOUSE TRUST
JAMES H HOMES JR. TRUSTEE
154 MAPLEWOOD AVE
PORTSMOUTH, NH, 03801

LEGEND

- PROPERTY LINE
- PROPOSED PROPERTY LINE
- PROPOSED EDGE OF PAVEMENT
- PROPOSED CURB
- PROPOSED BUILDING
- PROPOSED PAVEMENT SECTION
- PROPOSED CONCRETE SIDEWALK
- PROPOSED BRICK SIDEWALK
- PAVING AND STRIPING AREA (SEE SITE NOTE #27)
- PROPOSED BOLLARD
- BUILDING
- TYPICAL
- COORDINATE
- 30'R
- VGC
- SGC



Last Saved: 11/16/2020 2:06pm By: CKK/CLK
Plotted On: Nov 15, 2020 10:55:05 AM
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APPENDIX D
Capacity Analysis Methodology

CAPACITY ANALYSIS METHODOLOGY

A primary result of capacity analysis is the assignment of levels of service to traffic facilities under various traffic flow conditions. The capacity analysis methodology is based on the concepts and procedures in the *Highway Capacity Manual* (HCM).¹ The concept of level of service (LOS) is defined as a qualitative measure describing operational conditions within a traffic stream and their perception by motorists and/or passengers. A level-of-service definition provides an index to quality of traffic flow in terms of such factors as speed, travel time, freedom to maneuver, traffic interruptions, comfort, convenience, and safety.

Six levels of service are defined for each type of facility. They are given letter designations from A to F, with LOS A representing the best operating conditions and LOS F the worst. Since the level of service of a traffic facility is a function of the traffic flows placed upon it, such a facility may operate at a wide range of levels of service, depending on the time of day, day of week, or period of year. A description of the operating condition under each level of service is provided below:

- *LOS A* describes conditions with little to no delay to motorists.
- *LOS B* represents a desirable level with relatively low delay to motorists.
- *LOS C* describes conditions with average delays to motorists.
- *LOS D* describes operations where the influence of congestion becomes more noticeable. Delays are still within an acceptable range.
- *LOS E* represents operating conditions with high delay values. This level is considered by many agencies to be the limit of acceptable delay.
- *LOS F* is considered to be unacceptable to most drivers with high delay values that often occur, when arrival flow rates exceed the capacity of the intersection.

Signalized Intersections

Levels of service for signalized intersections are also calculated using the operational analysis methodology of the HCM. The methodology for signalized intersections assesses the effects of signal type, timing, phasing, and progression; vehicle mix; and geometrics on average *control* delay. Control delay is used to establish the operating characteristics for an intersection or an approach to an intersection. Volume-to-capacity (v/c) ratios are also used to help signify the utilization of a lane group's capacity at an intersection. A v/c ratio of ≥ 1.00 represents conditions when the traffic signal cycle capacity is fully utilized and indicates a capacity failure. The level-of-service criteria for signalized intersections are shown in Table A-1.

¹*Highway Capacity Manual, 6TH Edition: A Guide for Multimodal Mobility Analysis*. Washington, D.C.: Transportation Research Board, 2016.

Unsignalized Intersections

Levels of service for unsignalized intersections are calculated using the operational analysis methodology of the HCM. The procedure accounts for lane configuration on both the minor and major street approaches, conflicting traffic stream volumes, and the type of intersection control (STOP, YIELD, or all-way STOP control). The definition of level of service for unsignalized intersections is a function of average *control* delay. Control delay at an unsignalized intersection is defined as the total elapsed time from when a vehicle stops at the end of the queue until the vehicle departs from the stop line. This time includes the time required for the vehicle to travel from the last-in-queue position to the first-in-queue position.

Volume-to-capacity (v/c) ratios are also used to help signify the utilization of a movement's capacity at an intersection. A v/c ratio of ≥ 1.00 represents conditions when the movement is fully utilized and indicates a capacity failure. The capacity of the movements is based on the distribution of gaps in the major street traffic stream, the selection of gaps to complete the desired movement, and the follow-up headways for each driver in the queue. When an unsignalized intersection is located within 0.25 miles of a signalized intersection, traffic flows may not be random and some platoon structure may exist, thereby affecting the minor street operations. The level-of-service criteria for unsignalized intersections are shown in Table A-1.

TABLE A-1
Level-of-Service Criteria for Intersections

Level of Service	Signalized Intersection Criteria	Unsignalized Intersection Criteria	V/C Ratio >1.00 ^a
	Average Control Delay (Seconds per Vehicle)	Average Control Delay (Seconds per Vehicle)	
A	≤ 10	≤ 10	F
B	>10 and ≤ 20	>10 and ≤ 15	F
C	>20 and ≤ 35	>15 and ≤ 25	F
D	>35 and ≤ 55	>25 and ≤ 35	F
E	>55 and ≤ 80	>35 and ≤ 50	F
F	>80	>50	F

Note: ^aFor approach-based and intersection-wide assessments, LOS is defined solely by control delay.





















Source: *Highway Capacity Manual, 6th Edition: A Guide for Multimodal Mobility Analysis*. Washington, D.C.: Transportation Research Board, 2016. Exhibit 19-8, Pg. 19-16.

For signalized intersections, this delay criterion may be applied in assigning level-of-service designations to individual lane groups, to individual intersection approaches, or to the entire intersection. For unsignalized intersections, this delay criterion may be applied in assigning level-of-service designations to individual lane groups on the minor street approaches or to the left turns from the major street approaches.

APPENDIX E

Capacity Analyses
2020 No Build Conditions

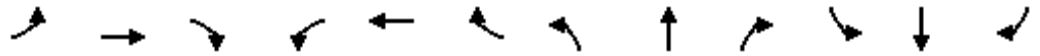
101: Maplewood Ave & Deer St
 2020 No Build Weekday PM Peak

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	151	112	38	245	109	50	17	434	195	52	379	78
Future Volume (vph)	151	112	38	245	109	50	17	434	195	52	379	78
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	10	13	13	12	14	14	11	11	13	11	11	11
Storage Length (ft)	0		0	0		100	0		0	0		0
Storage Lanes	0		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Fr _t		0.983			0.953				0.850		0.974	
Fl _t Protected		0.976		0.950			0.950			0.950		
Satd. Flow (prot)	0	1847	0	1770	1894	0	1711	1801	1636	1711	1754	0
Fl _t Permitted		0.603		0.501			0.456			0.337		
Satd. Flow (perm)	0	1141	0	933	1894	0	821	1801	1636	607	1754	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		6			20				219			11
Link Speed (mph)		25			25			25				30
Link Distance (ft)		363			453			585				231
Travel Time (s)		9.9			12.4			16.0				5.3
Peak Hour Factor	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Adj. Flow (vph)	229	170	58	322	143	66	20	499	224	63	462	95
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	457	0	322	209	0	20	499	224	63	557	0
Turn Type	Perm	NA		Perm	NA		Perm	NA	Prot	pm+pt	NA	
Protected Phases		4			8			2	2	1	6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		2	2	2	1	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		10.0	10.0	10.0	5.0	10.0	
Minimum Split (s)	11.0	11.0		11.0	11.0		16.0	16.0	16.0	11.0	16.0	
Total Split (s)	33.0	33.0		33.0	33.0		35.0	35.0	35.0	16.0	51.0	
Total Split (%)	30.0%	30.0%		30.0%	30.0%		31.8%	31.8%	31.8%	14.5%	46.4%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)		6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lead/Lag							Lag	Lag	Lag	Lead		
Lead-Lag Optimize?							Yes	Yes	Yes	Yes		
Recall Mode	Max	Max		None	None		C-Max	C-Max	C-Max	None	C-Max	
Act Effct Green (s)		27.0		27.0	27.0		60.4	60.4	60.4	71.0	71.0	
Actuated g/C Ratio		0.25		0.25	0.25		0.55	0.55	0.55	0.65	0.65	
v/c Ratio		1.61		1.41	0.44		0.04	0.51	0.22	0.14	0.49	
Control Delay		319.7		240.2	34.9		13.5	18.7	2.6	7.9	11.7	
Queue Delay		0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay		319.7		240.2	34.9		13.5	18.7	2.6	7.9	11.7	
LOS		F		F	C		B	B	A	A	B	
Approach Delay		319.7			159.4			13.7			11.3	
Approach LOS		F			F			B			B	
Queue Length 50th (ft)		~465		~306	113		7	220	2	15	183	

101: Maplewood Ave & Deer St
 2020 No Build Weekday PM Peak

Lane Group	Ø9
Lane Configurations	
Traffic Volume (vph)	
Future Volume (vph)	
Ideal Flow (vphpl)	
Lane Width (ft)	
Storage Length (ft)	
Storage Lanes	
Taper Length (ft)	
Lane Util. Factor	
Fr _t	
Fl _t Protected	
Satd. Flow (prot)	
Fl _t Permitted	
Satd. Flow (perm)	
Right Turn on Red	
Satd. Flow (RTOR)	
Link Speed (mph)	
Link Distance (ft)	
Travel Time (s)	
Peak Hour Factor	
Adj. Flow (vph)	
Shared Lane Traffic (%)	
Lane Group Flow (vph)	
Turn Type	
Protected Phases	9
Permitted Phases	
Detector Phase	
Switch Phase	
Minimum Initial (s)	1.0
Minimum Split (s)	26.0
Total Split (s)	26.0
Total Split (%)	24%
Yellow Time (s)	3.0
All-Red Time (s)	0.0
Lost Time Adjust (s)	
Total Lost Time (s)	
Lead/Lag	
Lead-Lag Optimize?	
Recall Mode	None
Act Effct Green (s)	
Actuated g/C Ratio	
v/c Ratio	
Control Delay	
Queue Delay	
Total Delay	
LOS	
Approach Delay	
Approach LOS	
Queue Length 50th (ft)	

101: Maplewood Ave & Deer St
 2020 No Build Weekday PM Peak

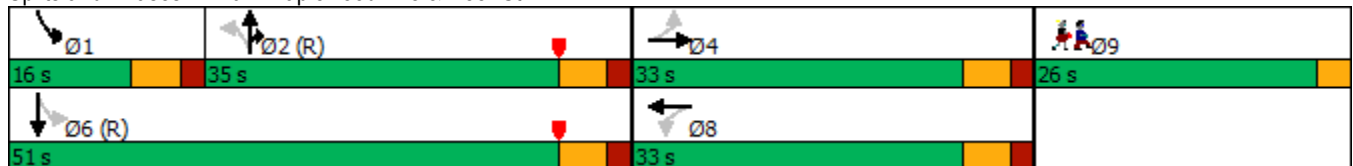


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 95th (ft)		#430		#381	149		19	309	35	28	226	
Internal Link Dist (ft)		283			373			505			151	
Turn Bay Length (ft)												
Base Capacity (vph)		284		229	479		450	988	996	492	1136	
Starvation Cap Reductn		0		0	0		0	0	0	0	0	
Spillback Cap Reductn		0		0	0		0	0	0	0	0	
Storage Cap Reductn		0		0	0		0	0	0	0	0	
Reduced v/c Ratio		1.61		1.41	0.44		0.04	0.51	0.22	0.13	0.49	

Intersection Summary

Area Type: Other
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 6 (5%), Referenced to phase 2:NBTL and 6:SBTL, Start of Yellow
 Natural Cycle: 130
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.61
 Intersection Signal Delay: 105.5 Intersection LOS: F
 Intersection Capacity Utilization 83.2% ICU Level of Service E
 Analysis Period (min) 15
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 101: Maplewood Ave & Deer St



101: Maplewood Ave & Deer St
2020 No Build Weekday PM Peak

Lane Group	Ø9
Queue Length 95th (ft)	
Internal Link Dist (ft)	
Turn Bay Length (ft)	
Base Capacity (vph)	
Starvation Cap Reductn	
Spillback Cap Reductn	
Storage Cap Reductn	
Reduced v/c Ratio	
Intersection Summary	

201: Maplewood Ave & Raynes Ave
 2020 No Build Weekday PM Peak

Intersection						
Int Delay, s/veh	3.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	29	86	593	25	62	445
Future Vol, veh/h	29	86	593	25	62	445
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	36	108	741	31	70	500

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	1397	757	0	0	772	0
Stage 1	757	-	-	-	-	-
Stage 2	640	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	155	408	-	-	843	-
Stage 1	463	-	-	-	-	-
Stage 2	525	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	137	408	-	-	843	-
Mov Cap-2 Maneuver	137	-	-	-	-	-
Stage 1	463	-	-	-	-	-
Stage 2	465	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	32.2	0	1.2
HCM LOS	D		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	272	843
HCM Lane V/C Ratio	-	-	0.528	0.083
HCM Control Delay (s)	-	-	32.2	9.7
HCM Lane LOS	-	-	D	A
HCM 95th %tile Q(veh)	-	-	2.9	0.3

202: Maplewood Ave & Vaughan St
 2020 No Build Weekday PM Peak

Intersection						
Int Delay, s/veh	3.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	Y		B			A
Traffic Vol, veh/h	56	21	595	51	16	472
Future Vol, veh/h	56	21	595	51	16	472
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	79	79	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	75	28	753	65	19	562

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1386	786	0	0	818
Stage 1	786	-	-	-	-
Stage 2	600	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	158	392	-	-	810
Stage 1	449	-	-	-	-
Stage 2	548	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	153	392	-	-	810
Mov Cap-2 Maneuver	153	-	-	-	-
Stage 1	449	-	-	-	-
Stage 2	529	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	46.8	0	0.3
HCM LOS	E		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	184	810
HCM Lane V/C Ratio	-	-	0.558	0.024
HCM Control Delay (s)	-	-	46.8	9.6
HCM Lane LOS	-	-	E	A
HCM 95th %tile Q(veh)	-	-	2.9	0.1

203: Vaughan St & Green St
 2020 No Build Weekday PM Peak

Intersection						
Int Delay, s/veh	3.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	TT		TT			TT
Traffic Vol, veh/h	7	47	54	13	37	66
Future Vol, veh/h	7	47	54	13	37	66
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	81	81	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	8	51	67	16	61	108

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	305	75	0	0	83	0
Stage 1	75	-	-	-	-	-
Stage 2	230	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	687	986	-	-	1514	-
Stage 1	948	-	-	-	-	-
Stage 2	808	-	-	-	-	-
Platoon blocked, %			-	-		
Mov Cap-1 Maneuver	657	986	-	-	1514	-
Mov Cap-2 Maneuver	657	-	-	-	-	-
Stage 1	948	-	-	-	-	-
Stage 2	773	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.1	0	2.7
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	926	1514
HCM Lane V/C Ratio	-	-	0.063	0.04
HCM Control Delay (s)	-	-	9.1	7.5
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0.1

204: Deer St & Russell St
2020 No Build Weekday PM Peak

Intersection						
Int Delay, s/veh	9.3					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	281	52	65	18	12	417
Future Vol, veh/h	281	52	65	18	12	417
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	88	88	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	312	58	74	20	13	463

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	94	0	-	0	766 84
Stage 1	-	-	-	-	84 -
Stage 2	-	-	-	-	682 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1500	-	-	-	371 975
Stage 1	-	-	-	-	939 -
Stage 2	-	-	-	-	502 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1500	-	-	-	291 975
Mov Cap-2 Maneuver	-	-	-	-	291 -
Stage 1	-	-	-	-	737 -
Stage 2	-	-	-	-	502 -

Approach	EB	WB	SB
HCM Control Delay, s	6.8	0	13.1
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1500	-	-	-	915
HCM Lane V/C Ratio	0.208	-	-	-	0.521
HCM Control Delay (s)	8	0	-	-	13.1
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0.8	-	-	-	3.1

205: Russell St & Green St
 2020 No Build Weekday PM Peak

Intersection						
Int Delay, s/veh	2.1					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	51	6	5	317	427	50
Future Vol, veh/h	51	6	5	317	427	50
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	58	58	85	85	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	88	10	6	373	508	60

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	923	538	568	0	-	0
Stage 1	538	-	-	-	-	-
Stage 2	385	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	299	543	1004	-	-	-
Stage 1	585	-	-	-	-	-
Stage 2	688	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	297	543	1004	-	-	-
Mov Cap-2 Maneuver	297	-	-	-	-	-
Stage 1	580	-	-	-	-	-
Stage 2	688	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	21.8	0.1	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1004	-	312	-	-
HCM Lane V/C Ratio	0.006	-	0.315	-	-
HCM Control Delay (s)	8.6	0	21.8	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0	-	1.3	-	-

206: Market St & Russell St
 2020 No Build Weekday PM Peak

Intersection						
Int Delay, s/veh	76.8					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗
Traffic Vol, veh/h	351	7	0	455	331	461
Future Vol, veh/h	351	7	0	455	331	461
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	71	71	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	433	9	0	641	394	549

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1035	394	-	0	-	0
Stage 1	394	-	-	-	-	-
Stage 2	641	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	~ 257	655	0	-	-	-
Stage 1	681	-	0	-	-	-
Stage 2	525	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	~ 257	655	-	-	-	-
Mov Cap-2 Maneuver	~ 257	-	-	-	-	-
Stage 1	681	-	-	-	-	-
Stage 2	525	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	352.2	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	-	257	655	-	-
HCM Lane V/C Ratio	-	1.686	0.013	-	-
HCM Control Delay (s)	-	\$ 359	10.6	-	-
HCM Lane LOS	-	F	B	-	-
HCM 95th %tile Q(veh)	-	27.9	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

APPENDIX E
Capacity Analyses
2020 Build Conditions

101: Maplewood Ave & Deer St
2020 Build Weekday PM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	151	112	38	245	109	50	17	452	195	52	392	78
Future Volume (vph)	151	112	38	245	109	50	17	452	195	52	392	78
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	10	13	13	12	14	14	11	11	13	11	11	11
Storage Length (ft)	0		0	0		100	0		0	0		0
Storage Lanes	0		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Fr _t		0.983			0.953				0.850		0.975	
Fl _t Protected		0.976		0.950			0.950			0.950		
Satd. Flow (prot)	0	1847	0	1770	1894	0	1711	1801	1636	1711	1756	0
Fl _t Permitted		0.603		0.501			0.446			0.322		
Satd. Flow (perm)	0	1141	0	933	1894	0	803	1801	1636	580	1756	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		6			20				211			11
Link Speed (mph)		25			25			25				30
Link Distance (ft)		363			453			585				231
Travel Time (s)		9.9			12.4			16.0				5.3
Peak Hour Factor	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Adj. Flow (vph)	229	170	58	322	143	66	20	520	224	63	478	95
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	457	0	322	209	0	20	520	224	63	573	0
Turn Type	Perm	NA		Perm	NA		Perm	NA	Prot	pm+pt	NA	
Protected Phases		4			8			2	2	1	6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		2	2	2	1	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		10.0	10.0	10.0	5.0	10.0	
Minimum Split (s)	11.0	11.0		11.0	11.0		16.0	16.0	16.0	11.0	16.0	
Total Split (s)	33.0	33.0		33.0	33.0		35.0	35.0	35.0	16.0	51.0	
Total Split (%)	30.0%	30.0%		30.0%	30.0%		31.8%	31.8%	31.8%	14.5%	46.4%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)		6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lead/Lag							Lag	Lag	Lag	Lead		
Lead-Lag Optimize?							Yes	Yes	Yes	Yes		
Recall Mode	Max	Max		None	None		C-Max	C-Max	C-Max	None	C-Max	
Act Effct Green (s)		27.0		27.0	27.0		60.4	60.4	60.4	71.0	71.0	
Actuated g/C Ratio		0.25		0.25	0.25		0.55	0.55	0.55	0.65	0.65	
v/c Ratio		1.61		1.41	0.44		0.05	0.53	0.23	0.14	0.50	
Control Delay		319.7		240.2	34.9		13.5	19.1	2.9	7.9	11.9	
Queue Delay		0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay		319.7		240.2	34.9		13.5	19.1	2.9	7.9	11.9	
LOS		F		F	C		B	B	A	A	B	
Approach Delay		319.7			159.4			14.2			11.5	
Approach LOS		F			F			B			B	
Queue Length 50th (ft)		~465		~306	113		7	233	4	15	191	

101: Maplewood Ave & Deer St
 2020 Build Weekday PM Peak

Lane Group	Ø9
Lane Configurations	
Traffic Volume (vph)	
Future Volume (vph)	
Ideal Flow (vphpl)	
Lane Width (ft)	
Storage Length (ft)	
Storage Lanes	
Taper Length (ft)	
Lane Util. Factor	
Fr _t	
Fl _t Protected	
Satd. Flow (prot)	
Fl _t Permitted	
Satd. Flow (perm)	
Right Turn on Red	
Satd. Flow (RTOR)	
Link Speed (mph)	
Link Distance (ft)	
Travel Time (s)	
Peak Hour Factor	
Adj. Flow (vph)	
Shared Lane Traffic (%)	
Lane Group Flow (vph)	
Turn Type	
Protected Phases	9
Permitted Phases	
Detector Phase	
Switch Phase	
Minimum Initial (s)	1.0
Minimum Split (s)	26.0
Total Split (s)	26.0
Total Split (%)	24%
Yellow Time (s)	3.0
All-Red Time (s)	0.0
Lost Time Adjust (s)	
Total Lost Time (s)	
Lead/Lag	
Lead-Lag Optimize?	
Recall Mode	None
Act Effct Green (s)	
Actuated g/C Ratio	
v/c Ratio	
Control Delay	
Queue Delay	
Total Delay	
LOS	
Approach Delay	
Approach LOS	
Queue Length 50th (ft)	

101: Maplewood Ave & Deer St
 2020 Build Weekday PM Peak

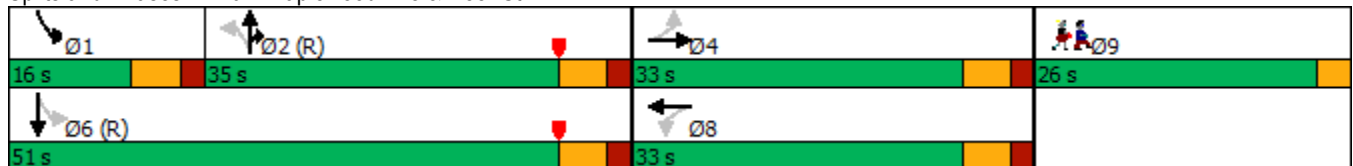


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 95th (ft)		#430		#381	149		19	326	38	28	234	
Internal Link Dist (ft)		283			373			505			151	
Turn Bay Length (ft)												
Base Capacity (vph)		284		229	479		440	988	992	477	1137	
Starvation Cap Reductn		0		0	0		0	0	0	0	0	
Spillback Cap Reductn		0		0	0		0	0	0	0	0	
Storage Cap Reductn		0		0	0		0	0	0	0	0	
Reduced v/c Ratio		1.61		1.41	0.44		0.05	0.53	0.23	0.13	0.50	

Intersection Summary

Area Type: Other
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 6 (5%), Referenced to phase 2:NBTL and 6:SBTL, Start of Yellow
 Natural Cycle: 130
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.61
 Intersection Signal Delay: 104.3
 Intersection LOS: F
 Intersection Capacity Utilization 83.8%
 ICU Level of Service E
 Analysis Period (min) 15
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 101: Maplewood Ave & Deer St



101: Maplewood Ave & Deer St
2020 Build Weekday PM Peak

Lane Group	Ø9
Queue Length 95th (ft)	
Internal Link Dist (ft)	
Turn Bay Length (ft)	
Base Capacity (vph)	
Starvation Cap Reductn	
Spillback Cap Reductn	
Storage Cap Reductn	
Reduced v/c Ratio	
Intersection Summary	

201: Maplewood Ave & Raynes Ave
2020 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	6.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W	W	T	T	S	S
Traffic Vol, veh/h	42	100	593	43	80	445
Future Vol, veh/h	42	100	593	43	80	445
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	53	125	741	54	90	500

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1448	768	0	0	795
Stage 1	768	-	-	-	-
Stage 2	680	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	145	402	-	-	826
Stage 1	458	-	-	-	-
Stage 2	503	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	123	402	-	-	826
Mov Cap-2 Maneuver	123	-	-	-	-
Stage 1	458	-	-	-	-
Stage 2	427	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	52.7	0	1.5
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	241	826
HCM Lane V/C Ratio	-	-	0.737	0.109
HCM Control Delay (s)	-	-	52.7	9.9
HCM Lane LOS	-	-	F	A
HCM 95th %tile Q(veh)	-	-	5.1	0.4

202: Maplewood Ave & Vaughan St
 2020 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	3.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	Y		B			A
Traffic Vol, veh/h	56	21	613	51	16	485
Future Vol, veh/h	56	21	613	51	16	485
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	79	79	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	75	28	776	65	19	577

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1424	809	0	0	841
Stage 1	809	-	-	-	-
Stage 2	615	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	150	380	-	-	794
Stage 1	438	-	-	-	-
Stage 2	539	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	145	380	-	-	794
Mov Cap-2 Maneuver	145	-	-	-	-
Stage 1	438	-	-	-	-
Stage 2	520	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	51.8	0	0.3
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	174	794
HCM Lane V/C Ratio	-	-	0.59	0.024
HCM Control Delay (s)	-	-	51.8	9.6
HCM Lane LOS	-	-	F	A
HCM 95th %tile Q(veh)	-	-	3.2	0.1

203: Vaughan St & Green St
2020 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	3.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	Y		P			4
Traffic Vol, veh/h	7	69	54	13	53	66
Future Vol, veh/h	7	69	54	13	53	66
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	81	81	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	8	74	67	16	87	108

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	357	75	0	0	83	0
Stage 1	75	-	-	-	-	-
Stage 2	282	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	641	986	-	-	1514	-
Stage 1	948	-	-	-	-	-
Stage 2	766	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	602	986	-	-	1514	-
Mov Cap-2 Maneuver	602	-	-	-	-	-
Stage 1	948	-	-	-	-	-
Stage 2	719	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.2	0	3.4
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	931	1514
HCM Lane V/C Ratio	-	-	0.088	0.057
HCM Control Delay (s)	-	-	9.2	7.5
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.3	0.2

204: Deer St & Russell St
2020 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	9.3					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	281	52	65	18	12	417
Future Vol, veh/h	281	52	65	18	12	417
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	88	88	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	312	58	74	20	13	463

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	94	0	-	0	766 84
Stage 1	-	-	-	-	84 -
Stage 2	-	-	-	-	682 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1500	-	-	-	371 975
Stage 1	-	-	-	-	939 -
Stage 2	-	-	-	-	502 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1500	-	-	-	291 975
Mov Cap-2 Maneuver	-	-	-	-	291 -
Stage 1	-	-	-	-	737 -
Stage 2	-	-	-	-	502 -

Approach	EB	WB	SB
HCM Control Delay, s	6.8	0	13.1
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1500	-	-	-	915
HCM Lane V/C Ratio	0.208	-	-	-	0.521
HCM Control Delay (s)	8	0	-	-	13.1
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0.8	-	-	-	3.1

205: Russell St & Green St
2020 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	2.9					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	67	6	5	317	427	72
Future Vol, veh/h	67	6	5	317	427	72
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	58	58	85	85	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	116	10	6	373	508	86

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	936	551	594	0	-	0
Stage 1	551	-	-	-	-	-
Stage 2	385	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	294	534	982	-	-	-
Stage 1	577	-	-	-	-	-
Stage 2	688	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	292	534	982	-	-	-
Mov Cap-2 Maneuver	292	-	-	-	-	-
Stage 1	572	-	-	-	-	-
Stage 2	688	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	25.1	0.1	0
HCM LOS	D		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	982	-	303	-	-
HCM Lane V/C Ratio	0.006	-	0.415	-	-
HCM Control Delay (s)	8.7	0	25.1	-	-
HCM Lane LOS	A	A	D	-	-
HCM 95th %tile Q(veh)	0	-	2	-	-

206: Market St & Russell St
2020 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	85.8					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗
Traffic Vol, veh/h	367	7	0	455	331	483
Future Vol, veh/h	367	7	0	455	331	483
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	71	71	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	453	9	0	641	394	575

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1035	394	-	0	-	0
Stage 1	394	-	-	-	-	-
Stage 2	641	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	~ 257	655	0	-	-	-
Stage 1	681	-	0	-	-	-
Stage 2	525	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	~ 257	655	-	-	-	-
Mov Cap-2 Maneuver	~ 257	-	-	-	-	-
Stage 1	681	-	-	-	-	-
Stage 2	525	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	\$ 385	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	-	257	655	-	-
HCM Lane V/C Ratio	-	1.763	0.013	-	-
HCM Control Delay (s)	-	\$ 392.1	10.6	-	-
HCM Lane LOS	-	F	B	-	-
HCM 95th %tile Q(veh)	-	30.1	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

301: Raynes Ave at Site Driveway
2020 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	2.3					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	36	87	113	22	16	27
Future Vol, veh/h	36	87	113	22	16	27
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	39	95	123	24	17	29

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	147	0	-	0	308
Stage 1	-	-	-	-	135
Stage 2	-	-	-	-	173
Critical Hdwy	4.12	-	-	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	2.218	-	-	-	3.518
Pot Cap-1 Maneuver	1435	-	-	-	684
Stage 1	-	-	-	-	891
Stage 2	-	-	-	-	857
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1435	-	-	-	664
Mov Cap-2 Maneuver	-	-	-	-	664
Stage 1	-	-	-	-	865
Stage 2	-	-	-	-	857

Approach	EB	WB	SB
HCM Control Delay, s	2.2	0	9.8
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1435	-	-	-	802
HCM Lane V/C Ratio	0.027	-	-	-	0.058
HCM Control Delay (s)	7.6	0	-	-	9.8
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0.1	-	-	-	0.2

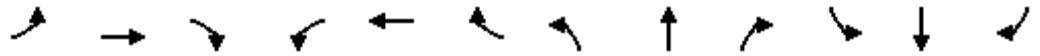
APPENDIX E

Capacity Analyses
2030 No Build Conditions

101: Maplewood Ave & Deer St
 2030 No Build Weekday PM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	287	161	86	350	151	94	52	511	238	75	401	164
Future Volume (vph)	287	161	86	350	151	94	52	511	238	75	401	164
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	10	13	13	12	14	14	11	11	13	11	11	11
Storage Length (ft)	0		0	0		100	0		0	0		0
Storage Lanes	1		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frt		0.948			0.942				0.850		0.956	
Flt Protected	0.950			0.950			0.950			0.950		
Satd. Flow (prot)	1652	1825	0	1770	1872	0	1711	1801	1636	1711	1721	0
Flt Permitted	0.326			0.237			0.111			0.166		
Satd. Flow (perm)	567	1825	0	441	1872	0	200	1801	1636	299	1721	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		27			31				274			25
Link Speed (mph)		25			25			25				30
Link Distance (ft)		363			453			585				231
Travel Time (s)		9.9			12.4			16.0				5.3
Peak Hour Factor	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Adj. Flow (vph)	435	244	130	461	199	124	60	587	274	91	489	200
Shared Lane Traffic (%)												
Lane Group Flow (vph)	435	374	0	461	323	0	60	587	274	91	689	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Prot	pm+pt	NA	
Protected Phases	7	4		3	8		5	2	2	1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2	2	1	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	10.0	10.0	5.0	10.0	
Minimum Split (s)	11.0	29.0		11.0	29.0		11.0	29.0	29.0	11.0	28.0	
Total Split (s)	14.0	29.0		14.0	29.0		11.0	41.0	41.0	11.0	41.0	
Total Split (%)	14.7%	30.5%		14.7%	30.5%		11.6%	43.2%	43.2%	11.6%	43.2%	
Yellow Time (s)	3.5	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	0.5	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)	4.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag	Lag	Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes	Yes	Yes	Yes	
Recall Mode	None	Max		None	None		None	C-Max	C-Max	None	C-Max	
Act Effct Green (s)	35.0	23.0		31.0	23.0		41.2	37.2	37.2	41.2	37.2	
Actuated g/C Ratio	0.37	0.24		0.33	0.24		0.43	0.39	0.39	0.43	0.39	
v/c Ratio	1.35	0.81		1.81	0.68		0.36	0.83	0.34	0.45	1.00	
Control Delay	200.1	46.8		400.7	37.6		19.7	39.4	3.9	21.0	65.1	
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay	200.1	46.8		400.7	37.6		19.7	39.4	3.9	21.0	65.1	
LOS	F	D		F	D		B	D	A	C	E	
Approach Delay		129.2			251.1			27.6			59.9	
Approach LOS		F			F			C			E	
Queue Length 50th (ft)	~247	199		~356	160		19	327	0	29	~451	

101: Maplewood Ave & Deer St
 2030 No Build Weekday PM Peak

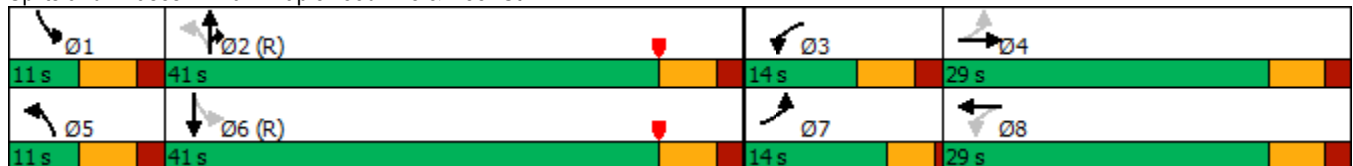


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 95th (ft)	#236	200		#426	202		39	#496	45	51	#573	
Internal Link Dist (ft)		283			373			505			151	
Turn Bay Length (ft)												
Base Capacity (vph)	323	462		255	476		166	705	807	204	688	
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	
Reduced v/c Ratio	1.35	0.81		1.81	0.68		0.36	0.83	0.34	0.45	1.00	

Intersection Summary

Area Type: Other
 Cycle Length: 95
 Actuated Cycle Length: 95
 Offset: 41 (43%), Referenced to phase 2:NBT and 6:SBT, Start of Yellow
 Natural Cycle: 140
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.81
 Intersection Signal Delay: 113.4 Intersection LOS: F
 Intersection Capacity Utilization 88.4% ICU Level of Service E
 Analysis Period (min) 15
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 101: Maplewood Ave & Deer St



201: Maplewood Ave & Raynes Ave
 2030 No Build Weekday PM Peak

Intersection						
Int Delay, s/veh	9.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	32	93	766	28	67	595
Future Vol, veh/h	32	93	766	28	67	595
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	40	116	958	35	75	669

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1795	976	0	0	993
Stage 1	976	-	-	-	-
Stage 2	819	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	88	305	-	-	696
Stage 1	365	-	-	-	-
Stage 2	433	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	73	305	-	-	696
Mov Cap-2 Maneuver	73	-	-	-	-
Stage 1	365	-	-	-	-
Stage 2	359	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	106.7	0	1.1
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	168	696
HCM Lane V/C Ratio	-	-	0.93	0.108
HCM Control Delay (s)	-	-	106.7	10.8
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	7	0.4

202: Maplewood Ave & Vaughan St
 2030 No Build Weekday PM Peak

Intersection						
Int Delay, s/veh	9.7					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	58	22	769	53	17	624
Future Vol, veh/h	58	22	769	53	17	624
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	79	79	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	77	29	973	67	20	743

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1790	1007	0	0	1040
Stage 1	1007	-	-	-	-
Stage 2	783	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	89	292	-	-	669
Stage 1	353	-	-	-	-
Stage 2	450	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	84	292	-	-	669
Mov Cap-2 Maneuver	84	-	-	-	-
Stage 1	353	-	-	-	-
Stage 2	427	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	171.9	0	0.3
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	104	669
HCM Lane V/C Ratio	-	-	1.026	0.03
HCM Control Delay (s)	-	-	171.9	10.5
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	6.5	0.1

203: Vaughan St & Green St
 2030 No Build Weekday PM Peak

Intersection						
Int Delay, s/veh	3.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	8	50	56	14	38	68
Future Vol, veh/h	8	50	56	14	38	68
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	81	81	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	54	69	17	62	111

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	313	78	0	0	86	0
Stage 1	78	-	-	-	-	-
Stage 2	235	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	680	983	-	-	1510	-
Stage 1	945	-	-	-	-	-
Stage 2	804	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	650	983	-	-	1510	-
Mov Cap-2 Maneuver	650	-	-	-	-	-
Stage 1	945	-	-	-	-	-
Stage 2	769	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.2	0	2.7
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	918	1510
HCM Lane V/C Ratio	-	-	0.068	0.041
HCM Control Delay (s)	-	-	9.2	7.5
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0.1

204: Deer St & Russell St
 2030 No Build Weekday PM Peak

Intersection						
Int Delay, s/veh	33.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	389	57	72	52	45	608
Future Vol, veh/h	389	57	72	52	45	608
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	88	88	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	432	63	82	59	50	676

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	141	0	-	0	1039 112
Stage 1	-	-	-	-	112 -
Stage 2	-	-	-	-	927 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1442	-	-	-	255 941
Stage 1	-	-	-	-	913 -
Stage 2	-	-	-	-	385 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1442	-	-	-	176 941
Mov Cap-2 Maneuver	-	-	-	-	176 -
Stage 1	-	-	-	-	629 -
Stage 2	-	-	-	-	385 -

Approach	EB	WB	SB
HCM Control Delay, s	7.5	0	57.8
HCM LOS			F

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1442	-	-	-	724
HCM Lane V/C Ratio	0.3	-	-	-	1.002
HCM Control Delay (s)	8.6	0	-	-	57.8
HCM Lane LOS	A	A	-	-	F
HCM 95th %tile Q(veh)	1.3	-	-	-	16.6

205: Russell St & Green St
 2030 No Build Weekday PM Peak

Intersection						
Int Delay, s/veh	3.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	54	8	8	455	553	53
Future Vol, veh/h	54	8	8	455	553	53
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	58	58	85	85	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	93	14	9	535	658	63

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1243	690	721	0	-	0
Stage 1	690	-	-	-	-	-
Stage 2	553	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	193	445	881	-	-	-
Stage 1	498	-	-	-	-	-
Stage 2	576	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	190	445	881	-	-	-
Mov Cap-2 Maneuver	190	-	-	-	-	-
Stage 1	491	-	-	-	-	-
Stage 2	576	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	40.2	0.2	0
HCM LOS	E		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	881	-	205	-	-
HCM Lane V/C Ratio	0.011	-	0.521	-	-
HCM Control Delay (s)	9.1	0	40.2	-	-
HCM Lane LOS	A	A	E	-	-
HCM 95th %tile Q(veh)	0	-	2.7	-	-

206: Market St & Russell St
 2030 No Build Weekday PM Peak

Intersection

Int Delay, s/veh 190.5

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↘	↗		↑	↑	↗
Traffic Vol, veh/h	491	8	0	489	353	611
Future Vol, veh/h	491	8	0	489	353	611
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	71	71	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	606	10	0	689	420	727

Major/Minor	Minor2	Major1	Major2
Conflicting Flow All	1109	420	0
Stage 1	420	-	-
Stage 2	689	-	-
Critical Hdwy	6.42	6.22	-
Critical Hdwy Stg 1	5.42	-	-
Critical Hdwy Stg 2	5.42	-	-
Follow-up Hdwy	3.518	3.318	-
Pot Cap-1 Maneuver	~ 232	633	0
Stage 1	663	-	0
Stage 2	~ 498	-	0
Platoon blocked, %			-
Mov Cap-1 Maneuver	~ 232	633	-
Mov Cap-2 Maneuver	~ 232	-	-
Stage 1	663	-	-
Stage 2	~ 498	-	-

Approach	EB	NB	SB
HCM Control Delay, s	758.4	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	-	232	633	-	-
HCM Lane V/C Ratio	-	2.613	0.016	-	-
HCM Control Delay (s)	-	770.6	10.8	-	-
HCM Lane LOS	-	F	B	-	-
HCM 95th %tile Q(veh)	-	51.2	0	-	-

Notes

~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

APPENDIX E
Capacity Analyses
2030 Build Conditions

101: Maplewood Ave & Deer St
2030 Build Weekday PM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	287	161	86	350	151	94	52	529	238	75	414	164
Future Volume (vph)	287	161	86	350	151	94	52	529	238	75	414	164
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	10	13	13	12	14	14	11	11	13	11	11	11
Storage Length (ft)	0		0	0		100	0		0	0		0
Storage Lanes	1		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frt		0.948			0.942				0.850		0.957	
Flt Protected	0.950			0.950			0.950			0.950		
Satd. Flow (prot)	1652	1825	0	1770	1872	0	1711	1801	1636	1711	1723	0
Flt Permitted	0.326			0.237			0.111			0.144		
Satd. Flow (perm)	567	1825	0	441	1872	0	200	1801	1636	259	1723	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		27			31				274			24
Link Speed (mph)		25			25			25				30
Link Distance (ft)		363			453			585				231
Travel Time (s)		9.9			12.4			16.0				5.3
Peak Hour Factor	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Adj. Flow (vph)	435	244	130	461	199	124	60	608	274	91	505	200
Shared Lane Traffic (%)												
Lane Group Flow (vph)	435	374	0	461	323	0	60	608	274	91	705	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Prot	pm+pt	NA	
Protected Phases	7	4		3	8		5	2	2	1	6	
Permitted Phases	4			8			2			6		
Detector Phase	7	4		3	8		5	2	2	1	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	10.0	10.0	5.0	10.0	
Minimum Split (s)	11.0	29.0		11.0	29.0		11.0	29.0	29.0	11.0	28.0	
Total Split (s)	14.0	29.0		14.0	29.0		11.0	41.0	41.0	11.0	41.0	
Total Split (%)	14.7%	30.5%		14.7%	30.5%		11.6%	43.2%	43.2%	11.6%	43.2%	
Yellow Time (s)	3.5	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	0.5	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)	4.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag	Lag	Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes	Yes	Yes	Yes	
Recall Mode	None	Max		None	None		None	C-Max	C-Max	None	C-Max	
Act Effct Green (s)	35.0	23.0		31.0	23.0		41.2	37.2	37.2	41.2	37.2	
Actuated g/C Ratio	0.37	0.24		0.33	0.24		0.43	0.39	0.39	0.43	0.39	
v/c Ratio	1.35	0.81		1.81	0.68		0.36	0.86	0.34	0.48	1.02	
Control Delay	200.1	46.8		400.7	37.6		19.7	42.1	3.9	22.8	70.9	
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay	200.1	46.8		400.7	37.6		19.7	42.1	3.9	22.8	70.9	
LOS	F	D		F	D		B	D	A	C	E	
Approach Delay		129.2			251.1			29.6			65.4	
Approach LOS		F			F			C			E	
Queue Length 50th (ft)	~247	199		~356	160		19	344	0	29	~472	

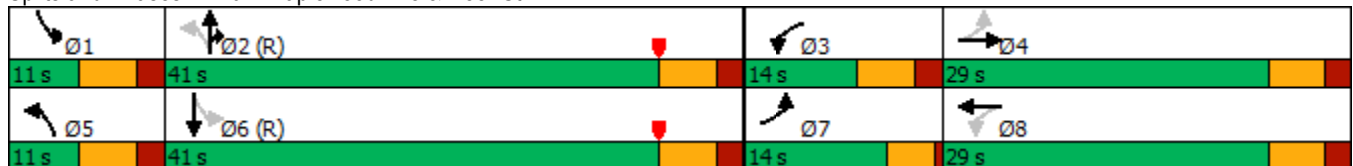
101: Maplewood Ave & Deer St
 2030 Build Weekday PM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 95th (ft)	#236	200		#426	202		39	#524	45	51	#594	
Internal Link Dist (ft)		283			373			505			151	
Turn Bay Length (ft)												
Base Capacity (vph)	323	462		255	476		166	705	807	188	688	
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	
Reduced v/c Ratio	1.35	0.81		1.81	0.68		0.36	0.86	0.34	0.48	1.02	

Intersection Summary

Area Type: Other
 Cycle Length: 95
 Actuated Cycle Length: 95
 Offset: 41 (43%), Referenced to phase 2:NBT and 6:SBT, Start of Yellow
 Natural Cycle: 140
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.81
 Intersection Signal Delay: 114.5 Intersection LOS: F
 Intersection Capacity Utilization 89.0% ICU Level of Service E
 Analysis Period (min) 15
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 101: Maplewood Ave & Deer St



201: Maplewood Ave & Raynes Ave
 2030 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	24.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W	W	T	T	T	T
Traffic Vol, veh/h	45	107	766	46	85	595
Future Vol, veh/h	45	107	766	46	85	595
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	56	134	958	58	96	669

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1848	987	0	0	1016
Stage 1	987	-	-	-	-
Stage 2	861	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	82	300	-	-	683
Stage 1	361	-	-	-	-
Stage 2	414	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	64	300	-	-	683
Mov Cap-2 Maneuver	64	-	-	-	-
Stage 1	361	-	-	-	-
Stage 2	321	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	247.4	0	1.4
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	143	683
HCM Lane V/C Ratio	-	-	1.329	0.14
HCM Control Delay (s)	-	-	247.4	11.1
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	11.9	0.5

202: Maplewood Ave & Vaughan St
 2030 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	10.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	58	22	787	53	17	637
Future Vol, veh/h	58	22	787	53	17	637
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	79	79	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	77	29	996	67	20	758

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1828	1030	0	0	1063
Stage 1	1030	-	-	-	-
Stage 2	798	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	84	283	-	-	655
Stage 1	344	-	-	-	-
Stage 2	443	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	80	283	-	-	655
Mov Cap-2 Maneuver	80	-	-	-	-
Stage 1	344	-	-	-	-
Stage 2	420	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	188.3	0	0.3
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	100	655
HCM Lane V/C Ratio	-	-	1.067	0.031
HCM Control Delay (s)	-	-	188.3	10.7
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	6.8	0.1

203: Vaughan St & Green St
 2030 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	3.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	8	72	56	14	54	68
Future Vol, veh/h	8	72	56	14	54	68
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	81	81	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	77	69	17	89	111

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	367	78	0	0	86
Stage 1	78	-	-	-	-
Stage 2	289	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	633	983	-	-	1510
Stage 1	945	-	-	-	-
Stage 2	760	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	593	983	-	-	1510
Mov Cap-2 Maneuver	593	-	-	-	-
Stage 1	945	-	-	-	-
Stage 2	712	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.3	0	3.3
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	922	1510
HCM Lane V/C Ratio	-	-	0.093	0.059
HCM Control Delay (s)	-	-	9.3	7.5
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.3	0.2

204: Deer St & Russell St
2030 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	33.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	389	57	72	52	45	608
Future Vol, veh/h	389	57	72	52	45	608
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	88	88	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	432	63	82	59	50	676

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	141	0	-	0	1039 112
Stage 1	-	-	-	-	112 -
Stage 2	-	-	-	-	927 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1442	-	-	-	255 941
Stage 1	-	-	-	-	913 -
Stage 2	-	-	-	-	385 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1442	-	-	-	176 941
Mov Cap-2 Maneuver	-	-	-	-	176 -
Stage 1	-	-	-	-	629 -
Stage 2	-	-	-	-	385 -

Approach	EB	WB	SB
HCM Control Delay, s	7.5	0	57.8
HCM LOS			F

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1442	-	-	-	724
HCM Lane V/C Ratio	0.3	-	-	-	1.002
HCM Control Delay (s)	8.6	0	-	-	57.8
HCM Lane LOS	A	A	-	-	F
HCM 95th %tile Q(veh)	1.3	-	-	-	16.6

205: Russell St & Green St
2030 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	5.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	70	8	8	455	553	75
Future Vol, veh/h	70	8	8	455	553	75
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	58	58	85	85	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	121	14	9	535	658	89

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1256	703	747	0	-	0
Stage 1	703	-	-	-	-	-
Stage 2	553	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	189	438	861	-	-	-
Stage 1	491	-	-	-	-	-
Stage 2	576	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	186	438	861	-	-	-
Mov Cap-2 Maneuver	186	-	-	-	-	-
Stage 1	484	-	-	-	-	-
Stage 2	576	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	54.8	0.2	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	861	-	198	-	-
HCM Lane V/C Ratio	0.011	-	0.679	-	-
HCM Control Delay (s)	9.2	0	54.8	-	-
HCM Lane LOS	A	A	F	-	-
HCM 95th %tile Q(veh)	0	-	4.2	-	-

206: Market St & Russell St
2030 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	202.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↘	↗		↑	↑	↗
Traffic Vol, veh/h	507	8	0	489	353	633
Future Vol, veh/h	507	8	0	489	353	633
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	71	71	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	626	10	0	689	420	754

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1109	420	-	0	-	0
Stage 1	420	-	-	-	-	-
Stage 2	689	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	~ 232	633	0	-	-	-
Stage 1	663	-	0	-	-	-
Stage 2	~ 498	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	~ 232	633	-	-	-	-
Mov Cap-2 Maneuver	~ 232	-	-	-	-	-
Stage 1	663	-	-	-	-	-
Stage 2	~ 498	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	796.1	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	-	232	633	-	-
HCM Lane V/C Ratio	-	2.698	0.016	-	-
HCM Control Delay (s)	-	\$ 808.5	10.8	-	-
HCM Lane LOS	-	F	B	-	-
HCM 95th %tile Q(veh)	-	53.6	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

301: Raynes Ave & Site Driveway
 2030 Build Weekday PM Peak

Intersection						
Int Delay, s/veh	2.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	36	95	121	22	16	27
Future Vol, veh/h	36	95	121	22	16	27
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	39	103	132	24	17	29

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	156	0	-	0	325 144
Stage 1	-	-	-	-	144 -
Stage 2	-	-	-	-	181 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1424	-	-	-	669 903
Stage 1	-	-	-	-	883 -
Stage 2	-	-	-	-	850 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1424	-	-	-	650 903
Mov Cap-2 Maneuver	-	-	-	-	650 -
Stage 1	-	-	-	-	857 -
Stage 2	-	-	-	-	850 -

Approach	EB	WB	SB
HCM Control Delay, s	2.1	0	9.9
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1424	-	-	-	789
HCM Lane V/C Ratio	0.027	-	-	-	0.059
HCM Control Delay (s)	7.6	0	-	-	9.9
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0.1	-	-	-	0.2

APPENDIX F
Trip Generation

NCHRP 8-51 Internal Trip Capture Estimation Tool					
Project Name:	Raynes Ave Hotel			Organization:	
Project Location:	Portsmouth, NH			Performed By:	
Scenario Description:				Date:	
Analysis Year:	2020/2030			Checked By:	
Analysis Period:	PM Street Peak Hour			Date:	

Table 1-P: Base Vehicle-Trip Generation Estimates (Single-Use Site Estimate)						
Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office				0		
Retail				20	10	10
Restaurant				34	23	11
Cinema/Entertainment				0		
Residential				26	16	10
Hotel				70	36	34
All Other Land Uses ²				0		
Total				150	85	65

Table 2-P: Mode Split and Vehicle Occupancy Estimates						
Land Use	Entering Trips			Exiting Trips		
	Veh. Occ.	% Transit	% Non-Motorized	Veh. Occ.	% Transit	% Non-Motorized
Office						
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						
All Other Land Uses ²						

Table 3-P: Average Land Use Interchange Distances (Feet Walking Distance)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office						
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						

Table 4-P: Internal Person-Trip Origin-Destination Matrix*						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	0	0	0	0
Retail	0		3	0	3	1
Restaurant	0	5		0	2	1
Cinema/Entertainment	0	0	0		0	0
Residential	0	1	2	0		0
Hotel	0	0	1	0	0	

Table 5-P: Computations Summary			
	Total	Entering	Exiting
All Person-Trips	150	85	65
Internal Capture Percentage	25%	22%	29%
External Vehicle-Trips ³	112	66	46
External Transit-Trips ⁴	0	0	0
External Non-Motorized Trips ⁴	0	0	0

Table 6-P: Internal Trip Capture Percentages by Land Use		
Land Use	Entering Trips	Exiting Trips
Office	N/A	N/A
Retail	60%	70%
Restaurant	26%	73%
Cinema/Entertainment	N/A	N/A
Residential	31%	30%
Hotel	6%	3%

¹Land Use Codes (LUCs) from *Trip Generation Informational Report*, published by the Institute of Transportation Engineers.

²Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

³Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P

⁴Person-Trips

*Indicates computation that has been rounded to the nearest whole number.

Estimation Tool Developed by the Texas Transportation Institute

Project Name:	Raynes Ave Hotel
Analysis Period:	PM Street Peak Hour

Table 7-P: Conversion of Vehicle-Trip Ends to Person-Trip Ends						
Land Use	Table 7-P (D): Entering Trips			Table 7-P (O): Exiting Trips		
	Veh. Occ.	Vehicle-Trips	Person-Trips*	Veh. Occ.	Vehicle-Trips	Person-Trips*
Office	1.00	0	0	1.00	0	0
Retail	1.00	10	10	1.00	10	10
Restaurant	1.00	23	23	1.00	11	11
Cinema/Entertainment	1.00	0	0	1.00	0	0
Residential	1.00	16	16	1.00	10	10
Hotel	1.00	36	36	1.00	34	34

Table 8-P (O): Internal Person-Trip Origin-Destination Matrix (Computed at Origin)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	0	0	0	0
Retail	0		3	0	3	1
Restaurant	0	5		1	2	1
Cinema/Entertainment	0	0	0		0	0
Residential	0	4	2	0		0
Hotel	0	5	23	0	1	

Table 8-P (D): Internal Person-Trip Origin-Destination Matrix (Computed at Destination)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		1	0	0	1	0
Retail	0		7	0	7	6
Restaurant	0	5		0	3	26
Cinema/Entertainment	0	0	1		1	0
Residential	0	1	3	0		4
Hotel	0	0	1	0	0	

Table 9-P (D): Internal and External Trips Summary (Entering Trips)						
Destination Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	0	0	0	0	0	0
Retail	6	4	10	4	0	0
Restaurant	6	17	23	17	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	5	11	16	11	0	0
Hotel	2	34	36	34	0	0
All Other Land Uses ³	0	0	0	0	0	0

Table 9-P (O): Internal and External Trips Summary (Exiting Trips)						
Origin Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	0	0	0	0	0	0
Retail	7	3	10	3	0	0
Restaurant	8	3	11	3	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	3	7	10	7	0	0
Hotel	1	33	34	33	0	0
All Other Land Uses ³	0	0	0	0	0	0

¹Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P

²Person-Trips

³Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

*Indicates computation that has been rounded to the nearest whole number.

APPENDIX G
111 Maplewood Avenue Traffic Evaluation

Proposed Office Building 111 Maplewood Avenue Portsmouth, NH

To: Eric Eby, PE
Parking and Transportation Engineer
Department of Public Works
City of Portsmouth, NH

FROM: Vinod Kalikiri, PE, PTOE

DATE: March 18, 2019

Tighe & Bond has prepared this *Traffic Evaluation* to summarize the projected changes in the traffic operations related to the construction of an approximately 74,000 square foot (sf) office building with ancillary commercial space (the Project) to be located at 111 Maplewood Avenue in Portsmouth, New Hampshire (the Site).

The 111 Maplewood Avenue property will be subdivided into two parcels. The proposed development will be located on the northern parcel, which is bounded by Maplewood Avenue on the south, Raynes Avenue on the north, and Vaughan Street on the east. The Site is currently developed with paved parking spaces that are used by the existing building located on the south parcel, and lawn/landscaping.

Vehicular access to the Site will be provided by a driveway located at the general location of the existing curb cut, along the south side of Vaughan Street. As part of the Project, 37 parking spaces will be provided on the north parcel for use by the proposed office and commercial uses. The site plan also shows an additional 13 spaces on the south parcel that can be accessed via the site driveway. The Project will also install curb extensions to better define the on-street parking along the perimeter of the Site. A copy of the site plan is included in the Appendix.

The trip generation analysis indicates that the Project can be expected to generate approximately 180 trips during the weekday evening peak hour (approximately 50 entering trips + 130 exiting trips). Approximately 60 percent of the Site traffic will be oriented to/from the north on Maplewood Avenue; 20 percent via Market Street and the remaining 20 percent to/from the south on Maplewood Avenue.

Detailed weekday evening peak hour traffic operations analysis was prepared for the study locations. The analysis was conducted for four different scenarios:

- 2020 No-Build scenario – includes an annual background traffic growth rate
- 2020 Build scenario – adds the Project-generated traffic volumes to the 2020 No-Build scenario
- 2030 No-Build scenario – includes an annual background traffic growth rate and traffic from nearby proposed development projects.
- 2030 Build scenario – adds the Project-generated traffic volumes to the 2030 No-Build scenario

The remainder of the report summarizes the evaluation which includes a description of the study area, traffic volume counts during the weekday evening commuter peak period, trip generation estimates for the Project, estimated trip distribution patterns for the new Project-related trips, traffic volume projections for each of the analysis scenarios, traffic operations analysis for the study area intersections, and a summary of the study findings.

Study Methodology

This traffic evaluation and its supporting analyses were conducted in accordance with New Hampshire Department of Transportation (NHDOT) and the City of Portsmouth guidelines and are described below. The study area and the peak analysis period included in the study were reviewed with City staff during a scoping meeting prior to initiating the traffic analysis.

An inventory of existing conditions was conducted and includes a description of the roadway and intersection geometries and the collection of existing traffic volumes. Existing vehicular traffic counts were collected at the study area intersections during the weekday evening commuter peak period. The traffic data collection effort forms the basis for the operations analysis conducted as part of this traffic evaluation.

The future conditions analyses evaluate traffic-related impacts associated with additional development and traffic growth, with and without the Project. An opening year evaluation was conducted for the year 2020 (with and without the Project) and a long-term evaluation was conducted for the year 2030 (with and without the Project).

Existing Conditions

This section includes a description of existing study area roadway geometry, intersection geometry, intersection traffic control, and data collection efforts within the study area. **Figure 1** shows the location of the Site in context with the surrounding roadway network and study area.

Roadway Descriptions

Maplewood Avenue is a two-lane roadway (one lane in each direction) that runs east-west between Woodbury Avenue and Congress Street. On-street parallel parking, bike lanes and sidewalks are provided on both sides of Maplewood Avenue in the vicinity of the Project. The roadway has a posted speed limit of 25 miles per hour (mph) near the site.

The other study area roadways (Raynes Avenue, Vaughan Street, Deer Street, Russell Street, and Market Street) within the study area have similar urban characteristics: two-lane roadway, on-street parallel parking, sidewalks, and low speed limits (25 mph or less). Land uses near the Site are a mix of commercial businesses, restaurants, hotels and residential.

Intersection Descriptions

Maplewood Avenue/Raynes Avenue

Raynes Avenue intersects Maplewood Avenue from the east to form a three-way unsignalized intersection. All approaches at this intersection provide a single general-purpose lane. Sidewalks are provided on both sides of Maplewood Avenue. On-street parallel parking is provided on both sides of Maplewood Avenue and Raynes Avenue. Maplewood Avenue

operates with the right of way while the minor street approach of Raynes Avenue operates under stop control. A bike lane is striped along both sides of Maplewood Avenue.

Maplewood Avenue/Vaughan Street

Vaughan Street and a private driveway intersect Maplewood Avenue from the east and the west, respectively, to form a four-way unsignalized intersection. All approaches at this intersection provide a single general-purpose lane. Sidewalks are provided on both sides of Maplewood Avenue, but no crosswalks are provided at the intersection. On-street parallel parking is provided on both sides of Maplewood Avenue west of Vaughan Street and on both sides of Vaughan Street. A bike lane is striped along both sides of Maplewood Avenue north of the intersection and along Maplewood Avenue northbound approach south of the intersection. Maplewood Avenue operates with the right of way while the minor street approaches of Vaughan Street and the private driveway operate as the stop-controlled approaches.

Maplewood Avenue/Deer Street

Deer Street intersects Maplewood Avenue from the east and west to form a four-way signalized intersection. Maplewood Avenue southbound approach consists of left turn only lane and a right/through shared lane. Maplewood Avenue northbound approach consists of an exclusive left turn lane, exclusive through lane and an exclusive right turn lane. Deer Street eastbound approach consists of a single general-purpose lane. Deer Street westbound approach consists of an exclusive left turn lane and a right and through shared lane. The intersection is equipped with an exclusive actuated pedestrian phase. Each leg of the intersection has painted crosswalks.

Vaughan Street/111 Maplewood Avenue North Driveway

111 Maplewood Ave driveway intersects Vaughan Street from the west to form a three-way unsignalized intersection. All approaches at this intersection provide a single general-purpose lane. Sidewalks and on-street parallel parking are provided on both sides of the Vaughan Street.

Vaughan Street/Green Street

Green Street intersects Vaughan Street from the east, forming a three-way unsignalized intersection. Both roadways provide a single lane of travel in each direction. Vehicles exiting from Green Street operate under stop control. The width of Green Street ranges between 17 and 24 feet of pavement with no delineation of travel lanes or shoulders. A brick paver sidewalk exists on the east side of Green Street, south of the railroad tracks. On-street parking is allowed on the south side of Vaughan Street at the intersection.

Deer Street/Russell Street

Russell Street intersects Deer Street from the north to form a three-way unsignalized intersection. The southbound approach on Russell Street provides a single general-purpose lane that operates under a stop control. The westbound and eastbound approaches on Deer Street both provide a single general-purpose lane. The intersection provides sidewalks on all sides of the intersection approaches. A crosswalk is available for pedestrians crossing Deer Street east of Russell Street. On Street parking is available on all approaches.

Russell Street/Green Street

Green Street intersects Russell Street from the west to form a three-way unsignalized intersection. The eastbound approach of Green Street provides a single general-purpose lane that operates under stop control. The northbound and southbound approaches on Russell Street also both provide a single multi-use lane. Sidewalk is provided on both sides of Russell

Street, but no crosswalks are provided at the intersection. On-street metered parking is provided on Russell Street south of Green Street.

Market Street/Russell Street

Russell Street intersects Market Street from the south, forming a three-way unsignalized intersection. Market Street eastbound consists of a through lane and a channelized right turn lane that operates as free flow movements. The westbound approach consists of a single through lane. The intersection geometry is designed to prohibit westbound left turns from Market Street to Russell Street. The Russell Street approach is a single lane that is wide enough for right turning vehicles to bypass waiting left turning vehicles. The Russell Street approach operates under stop control. Pedestrian crosswalks are provided along Russell Street and the westbound Market Street approach with sidewalks provided on all approaches. It is noted that the intersection is fully signalized with mast arms, vehicular and pedestrian signal heads, etc. However, the signal indications are in flashing mode, with yellow indications facing Market Street and red indication facing Russell Street.

Existing Traffic Data

Evaluation of the traffic impacts related to the Project requires the quantification of existing roadway and traffic conditions throughout the study area. Traffic conditions were determined by conducting manual turning movement and vehicle classification counts (TMCs) at the study area intersections during the weekday evening peak period (4:00 PM to 6:00 PM) in January 2019. A review of the data indicates that the weekday evening peak hour occurs between 5:00 PM and 6:00 PM. The traffic count data is provided in the Appendix.

Seasonal Variation

The counts were seasonally adjusted to peak month conditions based on nearby traffic volume count stations located in proximity to Portsmouth. Specifically, based on data available from the Urban Highway (Group 4) continuous count stations for years 2014 to 2016, a seasonal adjustment factor of 19 percent was used in the analysis. Detailed calculations are provided in the Appendix.

Future Conditions

The Project's impacts were evaluated for the years 2020 (opening year) and 2030 (10 years from opening year), in accordance with NHDOT traffic assessment guidelines. No-Build conditions (without Project-generated traffic) and Build conditions (with Project-generated traffic) were evaluated for both analysis years.

No-Build Conditions

The following section describes the estimation of traffic volumes in the study area for the No-Build scenarios. The No-Build scenarios will serve as the baseline for comparison purposes to measure the impacts of the Project.

Planned Roadway and Intersection Projects

Information obtain from the City traffic department staff was used to identify planned roadway development projects in the area that could affect future traffic conditions. The following improvements, described in record studies prepared for other projects in the area, were considered when developing the No-Build conditions analysis.

- *US Route 1 Bypass Bridge Project:* As a result of the US Route 1 By-pass bridge closure, vehicles accessing Downtown Portsmouth via Maplewood Avenue from the by-pass have migrated to alternate routes. To reflect the restored traffic volumes after the bridge construction is completed, estimated traffic volumes associated with the rerouting were obtained from record studies¹ and included in the analysis.
- *Market Street/Russell Street reconstruction:* The City is in the early planning stages for the construction of a roundabout at the intersection of Market Street/Russell Street. At this time, no detailed plans have been developed. Therefore, this improvement is not included in the future conditions presented in this study. It is anticipated that a roundabout configuration would have a beneficial effect on the traffic operations and safety at the intersection.
- *North End Portsmouth Development (also referred to as the "Harbor Corp Project") Off-Site Improvements:* The time table for this project is currently unknown. However, since the development related traffic volumes are included in the No-Build analysis, traffic improvements proposed for this development were also take into consideration, where applicable.
- *Maplewood Avenue Corridor Project:* The Maplewood Avenue corridor improvement project includes full depth pavement construction/reclamation, sidewalk construction, drainage/water/sewer improvements, traffic calming measures, pavement striping, and improvements to bicycle accommodations. The Project extends between Woodbury Avenue to the west and Dennett Street to the east. Construction will be completed in 2019.
- *Maplewood Avenue Road Diet:* The City has conducted preliminary planning for a possible Maplewood Avenue Road Diet Project. The concept of the road diet would consider one through travel lane along Maplewood Avenue with auxiliary turn lanes provided, where necessary, at the intersections with Deer Street, Hanover Street, and Islington Street. This would present an opportunity for landscaped islands and/or improved bicycle accommodations. These improvements were not included in the future-year conditions as the construction timetable undetermined.
- *Maplewood Avenue Railroad Crossing:* NHDOT has been designing improvements for several rail crossings in the State. As part of the project, the DOT is seeking to reconstruct the at-grade crossing along Maplewood Avenue immediately north of Deer Street, as well as the railroad crossing on Green Street immediately west of Russell Street. The improvements are set to include new signage, railroad gates and signals where appropriate. However, this project has been delayed and implementation dates are currently unknown.

Traffic Growth

To develop future base line traffic volume conditions, two components of traffic growth were considered. The first component to determining traffic growth is to estimate an annual average traffic growth rate. Based on a review of recent studies¹ in the vicinity of the Project, a one percent per year background traffic growth rate was assumed in the analysis.

¹ Traffic Impact Assessment for *Proposed Hotel at 299 Vaughan Street (March 2017)* and Traffic Impact and Access Study for *Deer Street Parking Garage & Deer Street Associates Development (December 2016)*

The second component to determining traffic growth is identifying any proposed development projects that are near or within the study area. Based on discussions with the City of Portsmouth staff, it was determined that the following projects are either planned, under construction, or partially occupied. Traffic volumes related to these projects were obtained from record studies¹ and distributed through the study area.

- *Deer Street Garage and Mixed-Use Development:* This project will be located in the northwest corner of the Maplewood Avenue/Deer Street intersection. The traffic study for the project indicates that the full build-out of the project consists of a 600-stall municipal public parking garage with 4,700 sf of integral retail; and four mixed-use buildings. The four mixed-use buildings include a combination of 80 residential apartments, 108 hotel rooms, 41,300 sf of office, 20,000 sf of retail, 9,900 sf of restaurants, a 4,700 sf bar, and a 2,700 sf bank.
- *299 Vaughan Street:* This project is located at the corner of the intersection Vaughan Street and Green Street. It involves the demolition of an auto parts store and construction of a 143-room hotel with approximately 2,900 square feet of leasable commercial/retail space. This project is not yet occupied.
- *40 Bridge Street:* This project consists of constructing a 4,025 sf restaurant and six residential condos. The project has been constructed.
- *75 Congress Street:* This project consists of constructing 10 residential condos. Due to the low traffic-generating nature of this land use and the limited number of units, traffic generated by the development was assumed to be included as part of the 1.0 percent annual background growth rate.
- *Harbor Corp Redevelopment:* This project consists of constructing a 98-room hotel and conference center, 14 condominium units, a 40,000 sf grocery store, and a 540-space parking garage.
- *172 Hanover Street:* The project consists of renovating a 7,000 sf restaurant that has been vacant for several years.
- *30 Maplewood Avenue:* The vacancy assumptions for this development that were included in the Deer Street garage traffic study were used in the current study as well.
- *46-64 Maplewood Avenue:* This project consists of constructing 22 residential apartments and 13,475 sf of retail space. The project is under construction.
- *173-175 Market Street:* This project consists of constructing 3,331 sf of commercial space, 1,759 sf of office space, and six residential condos. The project is currently under construction.

It is assumed that other smaller developments or small vacancies in existing developments are captured by the background traffic growth rate assumptions used in the analysis.

No-Build Traffic Volumes

The 2020 and 2030 No-Build weekday evening peak hour traffic volumes were developed by applying the one percent annual traffic growth rate to the seasonally adjusted 2019 traffic volumes. In addition, volumes from the background projects were added to the traffic networks. The resulting 2020 and 2030 No-Build weekday evening peak hour traffic volumes are shown in **Figure 2** and **Figure 3**, respectively.

Build Conditions

The Project will consist of a new 74,000 sf office building with ancillary commercial space. Limited parking will be available on the Site. Additional parking demand for the Site would be

handled by off-site parking areas, including potentially the Foundry Place parking garage accessed via Deer Street and Bridge Street. The following sections describe the methodology to estimate the total number of Project-generated trips and their distribution within the study area roadway network.

Trip Generation

To develop the trip generation characteristics of the new Project, data published in the Institute of Transportation Engineers (ITE) *Trip Generation Manual* were used. ITE provides data to estimate the total number of vehicular trips associated with a site based on the specific land uses. To estimate the trip generation for the Project, ITE Land Use Code (LUC) 710 – Office and LUC 820 – Retail/Shopping Center were used. The weekday daily and the morning and evening peak hour trip generation estimates for the Project are presented in Table 1.

Table 1: Weekday Evening Trip Generation

Time Period	Office ¹	Retail ²	Total
Weekday PM Peak Hour			
Enter	23	26	49
Exit	<u>106</u>	<u>26</u>	<u>132</u>
Total	129	52	181

1 Based on ITE LUC 710 – Office for 70,000 sf
 2 Based on ITE LUC 820 – Shopping Center for 4,000 sf

As shown in Table 1, weekday pm office and retail site generates 49 entering trips and 132 trips.

Trip Distribution

The trip distribution identifies the various travel paths for vehicles arriving and leaving the Project site. Trip distribution patterns for the Project were based on a review of traffic studies conducted for nearby projects² and Journey to Work data published by the United States Census.

For analysis purposes, it was assumed that approximately 25% of the proposed office traffic will travel to the Site. The remaining 75% of the office traffic was assumed to park in off-site parking areas in the area, most notably the Foundry Place garage. During the evening peak hour, since the office usage of the parking will be minimal, it was assumed that all commercial traffic would travel to the Site. In addition to Site generated traffic, traffic volume redistribution resulting from the elimination of the south parcel driveway on Maplewood Avenue was also taken into consideration. The trip distribution patterns are shown in **Figure 4**. The vehicular trips associated with the Project were assigned to the study area and are shown in **Figure 5** for the weekday evening peak hour.

² Traffic Impact and Access Study for *Deer Street Parking Garage & Deer Street Associates Development (December 2016)*

Build Traffic Volumes

The 2020 and 2030 Build traffic volume networks were developed by adding the Project-generated trips to the 2020 and 2030 No-Build traffic volume networks. The Build conditions traffic volume networks are shown in **Figure 6** and **Figure 7**, respectively.

Traffic Operations Analysis

Intersection capacity analyses were performed for the study area intersections based on the criteria published in the Highway Capacity Manual. Level of service (LOS) is the term that defines the conditions that may occur on a given roadway or at an intersection when accommodating various traffic volume loads. Levels of service range from A to F with LOS A representing the best operating conditions and LOS F representing congested conditions. The results are summarized in Table 2 and 3. Analysis worksheets are provided in the Appendix.

The analysis for the Maplewood Avenue/Deer Street signalized intersection indicates that when all planned development projects are constructed, fully occupied and are generating traffic at the levels projected in the individual studies, traffic operations at the intersection during the weekday evening peak hour, especially for the left turn movements from the Deer Street approaches and the southbound through movement on Maplewood Avenue can be expected to be congested. A review of the traffic volumes indicates that the proposed office development at 111 Maplewood Avenue would not substantially affect the operations of the intersection but would add to the future volumes at the intersection. When the geometric improvement at the intersection proposed by others are designed, additional refinements may be necessary to operate the intersection at optimal levels.

A review of the unsignalized intersections' analyses indicates that, as expected in busy urban corridors and shown in other studies prepared in the area, side street approaches at the Maplewood Avenue at Raynes Avenue and Maplewood Avenue at Vaughan Street intersections are projected to experience some delay. The intersection of Market Street at Russell Street also shows congested operations in the future without the implementation of major infrastructure improvements, like the proposed roundabout. All other unsignalized intersections in the study area generally show acceptable operations.

Conclusions

The Project is estimated to generate approximately 180 trips during the weekday evening peak hour (approximately 50 entering trips + 130 exiting trips). Approximately 60 percent of the Site traffic will be oriented to/from the north on Maplewood Avenue; 20 percent via Market Street and the remaining 20 percent to/from the south on Maplewood Avenue.

Capacity analysis indicates that when planned background projects in the area are all constructed, substantial traffic volumes will be added to the study area network which in turn could add delays and congestion at certain locations along Maplewood Avenue, especially for the side street movements. Site generated traffic represents a relatively small percentage of the cumulative traffic volume expected to be generated by the planned background projects.

As the planned projects get implemented, and the traffic improvements associated with the projects are design, additional consideration should be given to accommodate side street movements. System-wide traffic improvement measures, such as promotion of reduced automobile usage, enhanced transit services to the area and promotion of remote/under utilized parking areas can also be considered by the City to reduce the volume of vehicular traffic generated within the downtown street network during peak times.

TABLE 2: Signalized Intersection Operations Summary

Intersection / Lane Group	2020 No Build					2020 Build					2030 No Build					2030 Build				
	V/C	Del	LOS	50 th Q	95 th Q	V/C	Del	LOS	50 th Q	95 th Q	V/C	Del	LOS	50 th Q	95 th Q	V/C	Del	LOS	50 th Q	95 th Q
Maplewood Ave / Deer St																				
Deer St EBL	>1.2	>120	F	~181	#165	>1.2	>120	F	~273	#266	>1.2	>120	F	~205	#194	>1.2	>120	F	~261	#253
Deer St EBT/R	0.77	43	D	153	153	0.82	43	D	172	177	0.78	43	D	161	162	0.84	47	D	183	186
Deer St WBL	>1.2	>120	F	~212	#258	>1.2	>120	F	~213	#285	>1.2	>120	F	~247	#298	>1.2	>120	F	~260	#340
Deer St WBT/R	0.70	39	D	139	171	0.65	34	C	134	174	0.73	40	D	151	185	0.69	37	D	148	188
Maplewood Ave NBL	0.32	19	B	16	37	0.37	22	C	18	39	0.33	21	C	16	38	0.37	22	C	18	40
Maplewood Ave NBT	0.76	32	C	270	#429	0.83	37	D	272	#438	0.84	38	D	313	#492	0.88	42	D	318	#501
Maplewood Ave NBR	0.15	19	B	0	45	0.15	19	B	0	45	0.17	19	B	0	46	0.17	20	B	0	46
Maplewood Ave SBL	0.32	17	B	24	48	0.41	19	B	26	48	0.42	19	B	27	51	0.48	21	C	29	51
Maplewood Ave SBT/R	0.96	53	D	~394	#537	1.08	88	F	~430	#566	1.05	78	E	~470	#604	1.13	105	F	~512	#633
<i>Overall Intersection</i>	1.09	83	F			1.19	97	F			1.20	100	F			>1.2	>120	F		

LOS level-of-service
 Del Average intersection delay, measured in seconds
 v/c Volume to capacity ratio
 50th Q and 95th Q Percentile queues measured in feet
 # 95th percentile volume exceeds capacity, queue may be longer
 ~ Volume exceeds capacity. Queues are shown after two signal cycles

TABLE 3: Unsignalized Intersection Operations Summary

Intersection / Lane Group	2020 No Build				2020 Build				2030 No Build				2030 Build			
	V/C	Del	LOS	95 th Q	V/C	Del	LOS	95 th Q	V/C	Del	LOS	95 th Q	V/C	Del	LOS	95 th Q
Maplewood Ave / Raynes Ave:																
Maplewood Ave SBL/T	0.1	10	A	0.2	0.1	10	B	0.4	0.1	10	B	0.3	0.1	11	B	0.4
Raynes Ave WBL/R	0.6	45	E	3.1	0.9	90	F	6.8	0.7	71	F	4.7	1.1	>120	F	9.6
Maplewood Ave / Kennebunk Bank Driveway:																
Maplewood Ave SBL/T	0.0	10	A	0	NA	NA	NA	NA	0.0	10	A	0	NA	NA	NA	NA
Kennebunk Bank WBL/R	0.1	24	C	0.3	NA	NA	NA	NA	0.1	27	D	0.4	NA	NA	NA	NA
Maplewood Ave / Vaughan St:																
Maplewood Ave SBL/T	0.0	10	A	0.1	0.0	10	B	0.1	0.0	10	B	0.1	0.0	11	B	0.1
Vaughan St WBL/R	0.4	51	F	1.9	0.7	90	F	4.2	0.6	72	F	2.7	0.9	>120	F	5.5
Vaughan St / Kennebunk Bank Driveway:																
Vaughan St EBL/T	0.0	7	A	0	0.2	10	B	0.5	0.0	7	A	0	0.0	8	A	0
Kennebunk Bank SBL/R	0.0	9	A	0	0.0	8	A	0	0.0	9	A	0	0.1	10	A	0.3
Vaughan St / Green St:																
Vaughan St SBL/T	0.0	7	A	0.1	0.0	8	A	0.1	0.0	7	A	0.1	0.0	7	A	0
Green St WBL/R	0.1	9	A	0.2	0.1	9	A	0.2	0.1	9	A	0.2	0.1	9	A	0.2
Vaughan St / Site Driveway:																
Vaughan St NBL/T	0.0	7	A	0	0.0	8	A	0	0.0	8	A	0	0.0	8	A	0
Site Driveway EBL/R	0.0	10	A	0.1	0.2	10	B	0.5	0.0	10	A	0.1	0.2	11	B	0.6
Deer St / Russell St:																
Deer St EBL/T	0.3	8	A	1.1	0.3	8	A	1.1	0.3	9	A	1.2	0.3	9	A	1.3
Russell St SBL/R	0.8	29	D	9.6	0.9	32	D	10.2	0.9	43	E	13.1	1.0	47	E	14
Green St / Russell St:																
Russell St NBL/T	0.0	9	A	0	0.0	9	A	0	0.0	9	A	0	0.0	9	A	0
Green St EBL/R	0.3	27	D	1.4	0.4	32	D	2.1	0.4	32	D	1.9	0.5	39	E	2.7
Russell St / Market St:																
Russell St EBL	>1.2	>120	F	38.6	>1.2	>120	F	42.4	>1.2	>120	F	47.5	>1.2	>120	F	51.4
Russell St EBR	0.0	11	B	0	0.0	11	B	0	0.0	11	B	0	0.0	11	B	0

LOS level-of-service
 Del Average intersection delay, measured in seconds
 v/c Volume to capacity ratio
 95th Q Percentile queues measured in vehicles



Legend



Study Area Location

Proposed Office Building
111 Maplewood Avenue, Portsmouth NH

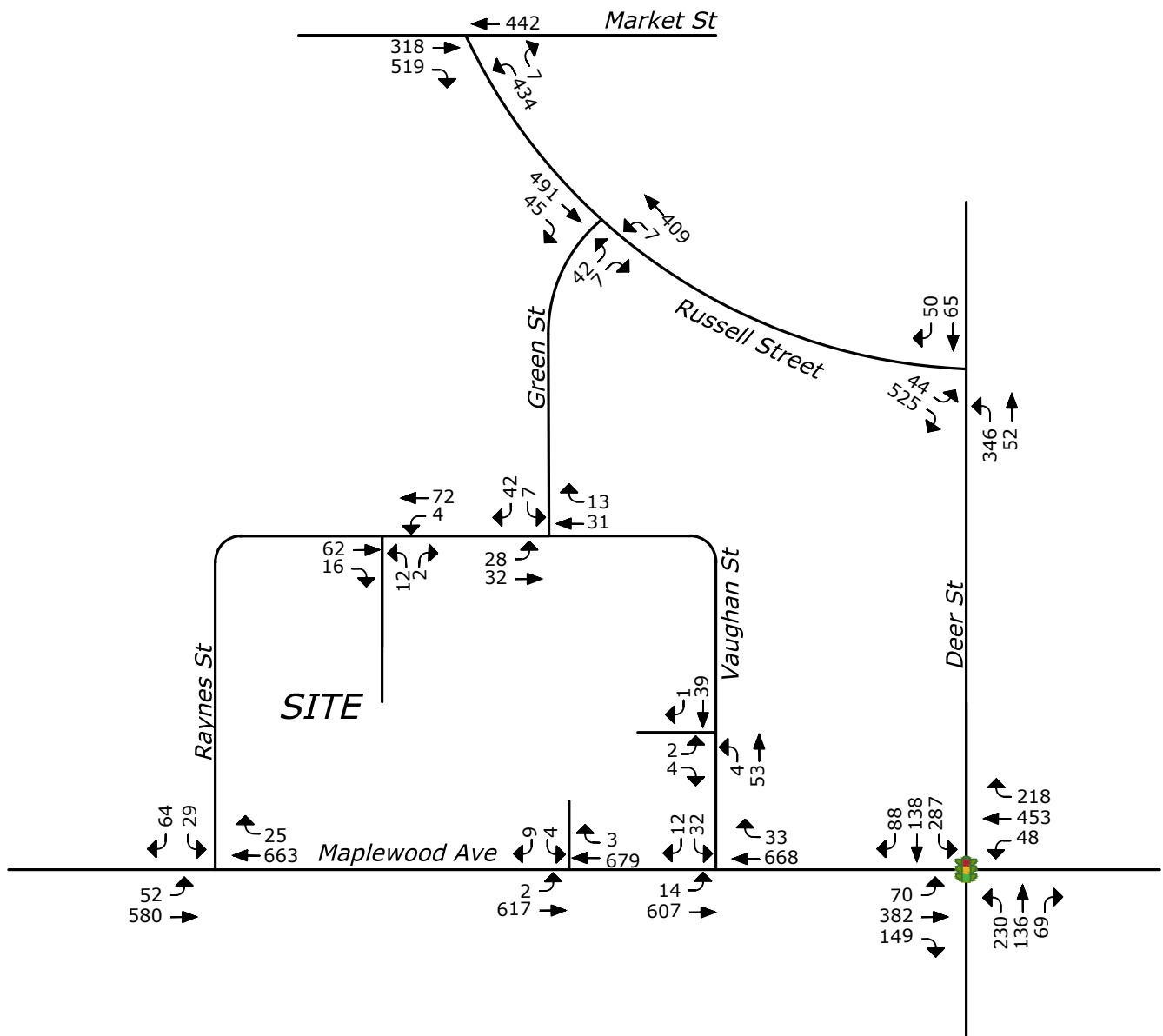
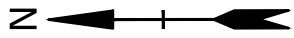
Study Area

DATE: 03/18/2019

SCALE: 1" = 200'

FIGURE 1

Tighe & Bond
www.tighebond.com

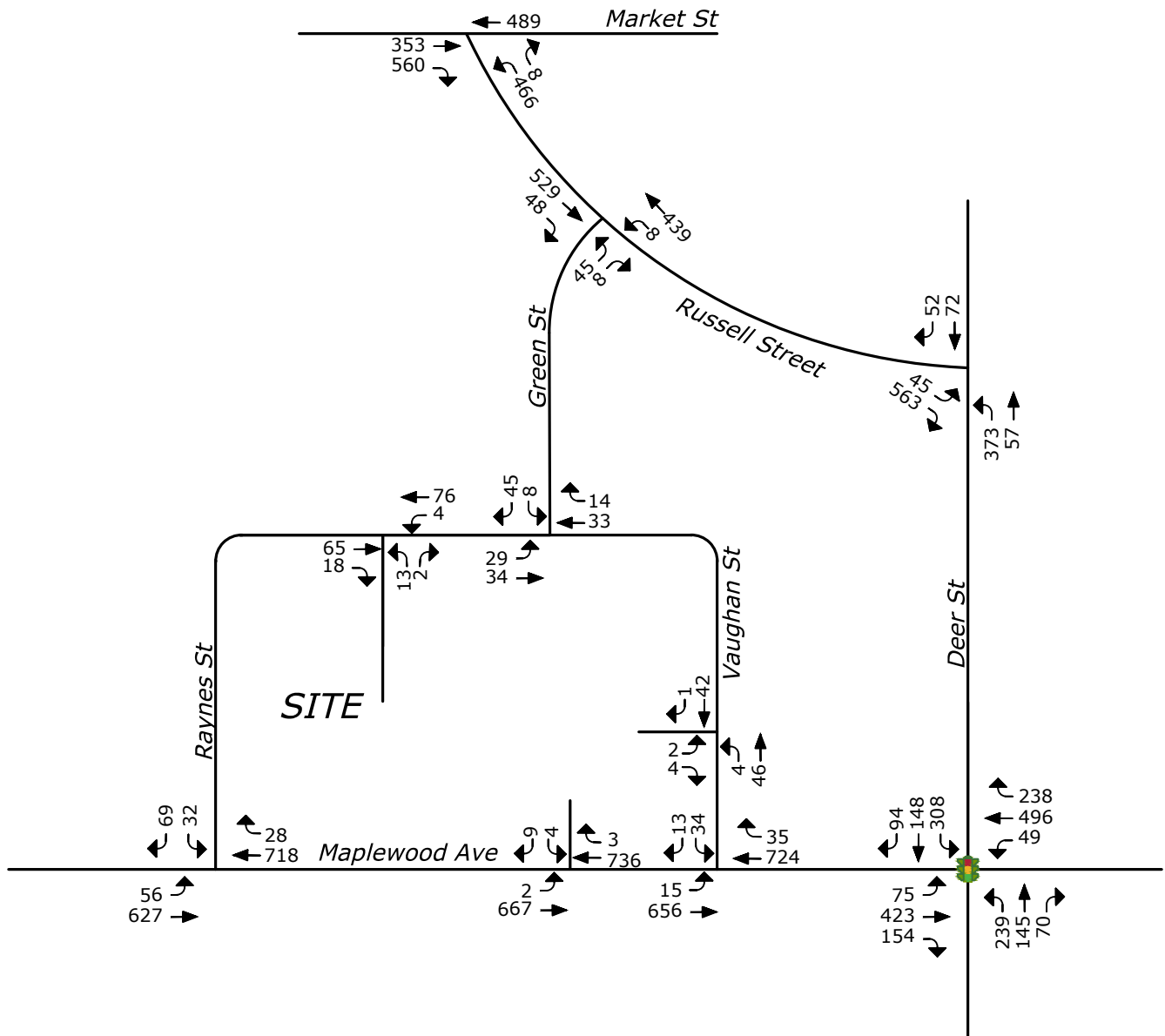
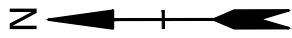


LEGEND



TRAFFIC SIGNAL

<p>Proposed Office Building 111 Maplewood Avenue, Portsmouth NH</p>	
<p>2020 No Build Peak Hour Traffic Volumes</p>	
DATE: 03/18/2019	 www.tighebond.com
SCALE: No Scale	
FIGURE 2	

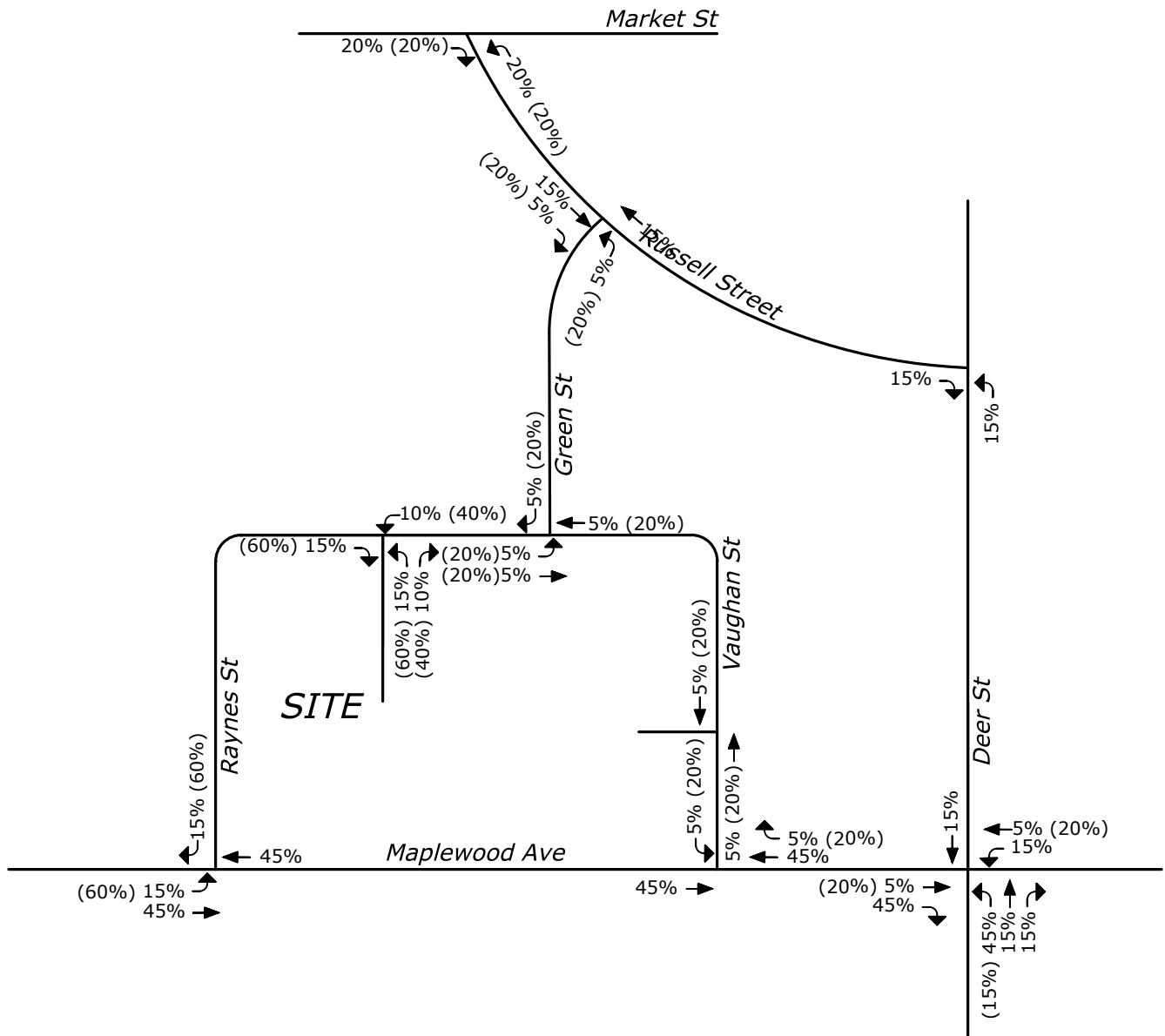
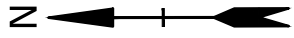


LEGEND



TRAFFIC SIGNAL

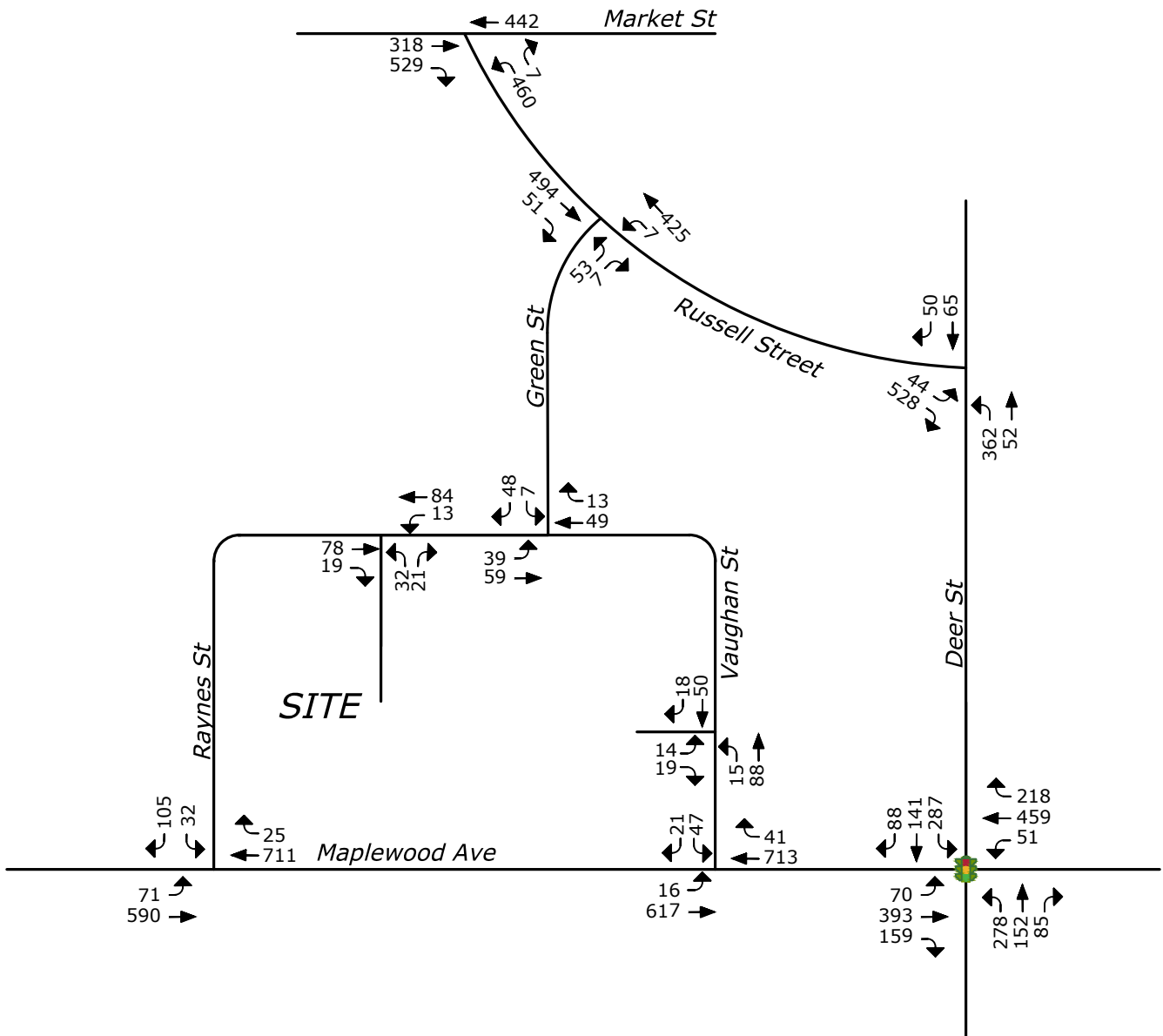
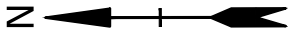
Proposed Office Building 111 Maplewood Avenue, Portsmouth NH	
2030 No Build Peak Hour Traffic Volumes	
DATE: 03/18/2019	 www.tighebond.com
SCALE: No Scale	
FIGURE 3	



LEGEND

- XX Office Trips
- (XX) Retail Trips

Proposed Office Building 111 Maplewood Avenue, Portsmouth NH	
Trip Distribution	
DATE: 03/18/2019	 www.tighebond.com
SCALE: No Scale	
FIGURE 4	

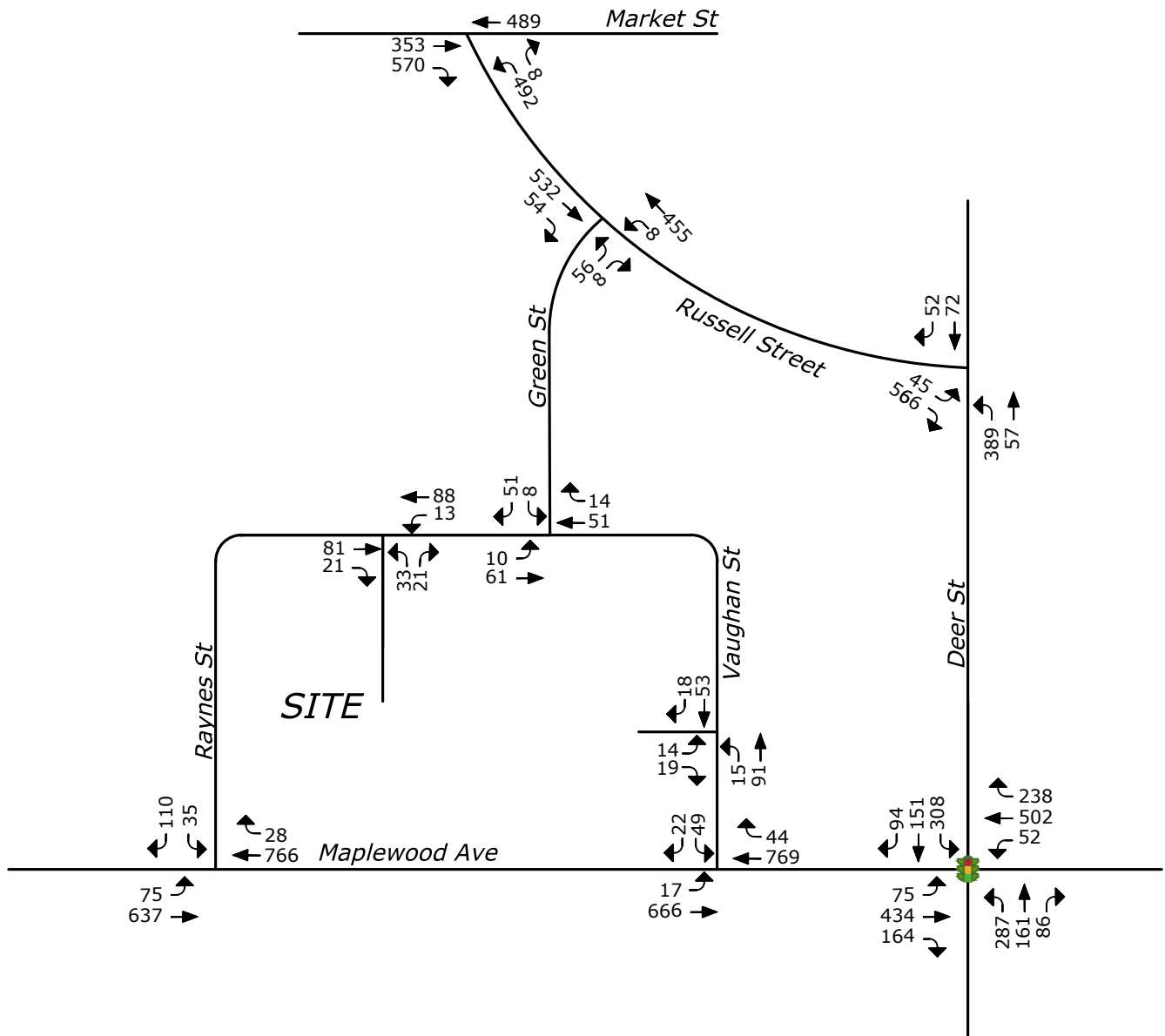
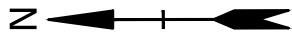


LEGEND



TRAFFIC SIGNAL

Proposed Office Building 111 Maplewood Avenue, Portsmouth NH	
2020 Build Peak Hour Traffic Volumes	
DATE: 03/18/2019	 www.tighebond.com
SCALE: No Scale	
FIGURE 6	



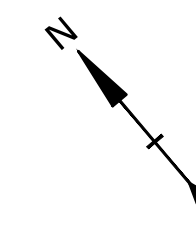
LEGEND



TRAFFIC SIGNAL

Proposed Office Building 111 Maplewood Avenue, Portsmouth NH	
2030 Build Peak Hour Traffic Volumes	
DATE: 03/18/2019	 www.tighebond.com
SCALE: No Scale	
FIGURE 7	

Site Plan



SITE DATA:
LOCATION: TAX MAP 124. LOT 8 OWNER: RJF-MAPLEWOOD, LLC
30 TEMPLE STREET, SUITE 400
NASHUA, NH 03060

ZONING DISTRICT: CHARACTER DISTRICT 5 (CD5)
DOWNTOWN OVERLAY DISTRICT
NORTH END INCENTIVE OVERLAY DISTRICT
HISTORIC DISTRICT

PROPOSED USE: OFFICE

PROPOSED LOT SIZE: ±0.98 ACRES (±42,794 SF)

PARKING REQUIREMENTS

PARKING SPACES REQUIRED:	OFFICE	DOWNTOWN OVERLAY DISTRICT	TOTAL MINIMUM PARKING SPACES REQUIRED =
	±74,000 SF	0 SPACES	0 SPACES

TOTAL PARKING SPACES PROVIDED:
TOTAL PARKING SPACES PROVIDED = 37 SPACES

TWO (2) ADA ACCESSIBLE SPACES REQUIRED

PARKING STALL SIZE:	REQUIRED	PROVIDED
DRIVE AISLE:	8.5' X 19'	8.5' X 19'
	***22'	22'

***ZONING ORDINANCE 10.1114.21 ALLOWS MINIMUM 22' AISLE WIDTH FOR 90 DEGREE PARKING IN A PARKING STRUCTURE

BIKE SPACES REQUIRED:	STOOP FACADE TYPE	ALLOWED ROOF TYPES
1 BIKE SPACE / 10 PARKING SPACES	20% - 50%	FLAT, GABLE, HIP, GAMBREL, MANSARD

DEVELOPMENT STANDARDS

BUILDING PLACEMENT (PRINCIPAL BUILDING):	REQUIRED	PROPOSED
MAXIMUM PRINCIPAL FRONT YARD:	5 FT	±12 FT
MAXIMUM SECONDARY FRONT YARD:	5 FT	±7 FT
SIDE YARD:	NR	N/A
MINIMUM REAR YARD:	5 FT	N/A
MINIMUM FRONT LOT LINE BUILDOUT:	80%	±90.7%

BUILDING AND LOT OCCUPATION:	REQUIRED	PROPOSED
MAXIMUM BUILDING BLOCK LENGTH:	225 FT	194 FT
MAXIMUM FACADE MODULATION LENGTH:	100 FT	<100 FT
MAXIMUM ENTRANCE SPACING:	50 FT	<50 FT
MAXIMUM BUILDING COVERAGE:	95%	±49.1%
MAXIMUM BUILDING FOOTPRINT:	*30,000 SF	21,000 SF
MINIMUM LOT AREA:	NR	NR
MINIMUM LOT AREA PER DWELLING UNIT:	NR	17.6%
MINIMUM OPEN SPACE:	5%	5%
MAXIMUM GROUND FLOOR GFA PER USE:	15,000 SF	11,301 SF

*ZONING ORDINANCE 10.5A46.20 ALLOWS 30,000SF BUILDING FOOTPRINT WITH 20% COMMUNITY SPACE.

BUILDING FORM (PRINCIPAL BUILDING):	REQUIRED	PROVIDED
BUILDING HEIGHT:	**60 FT	55 FT

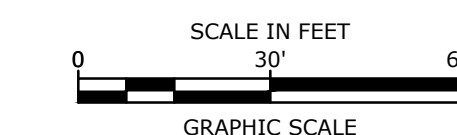
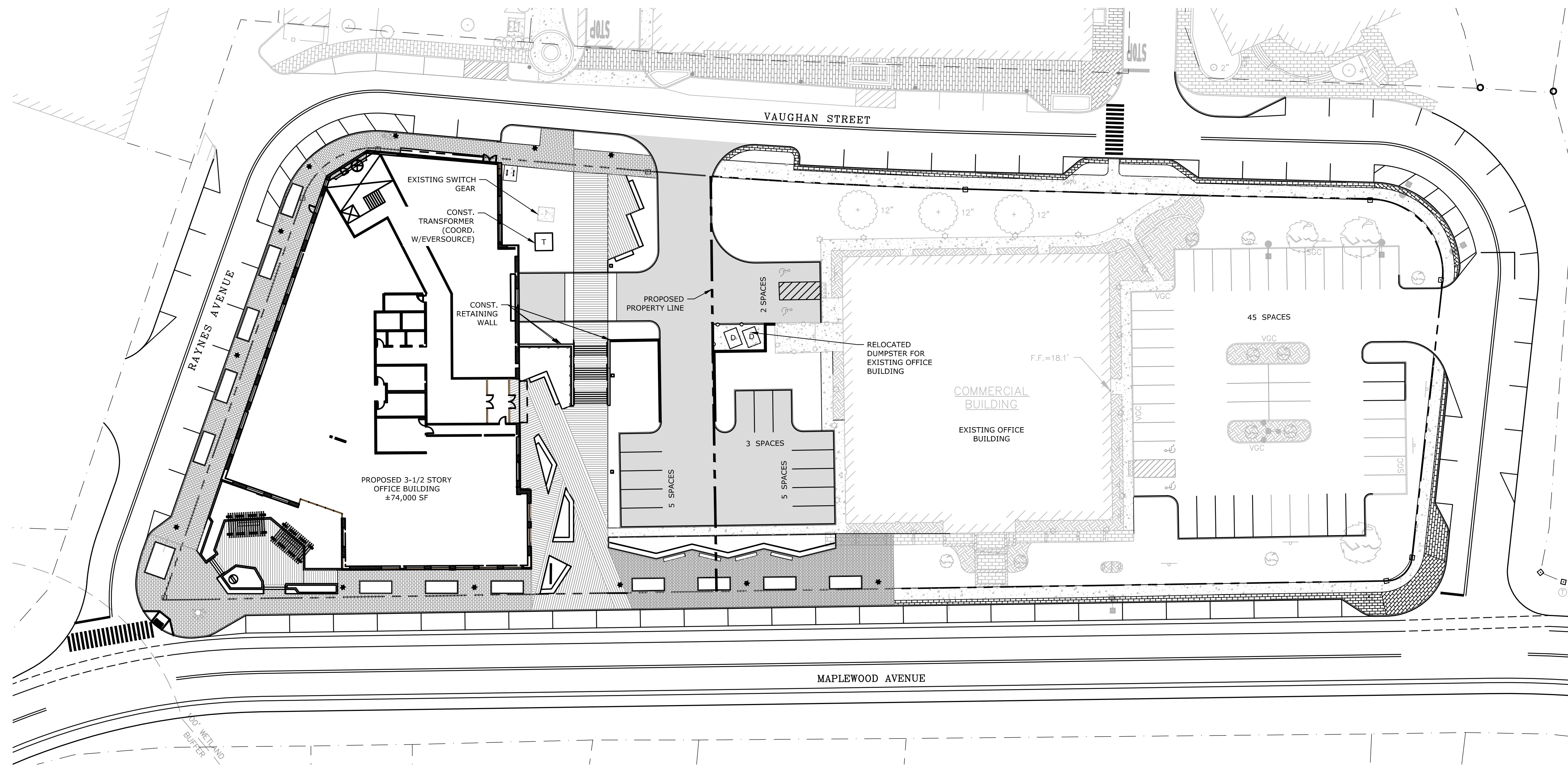
BUILDING FORM (PRINCIPAL BUILDING):	REQUIRED	PROVIDED
MAXIMUM FINISHED FLOOR SURFACE OF GROUND FLOOR ABOVE SIDEWALK GRADE:	36 IN	12 FT
MINIMUM GROUND STORY HEIGHT:	12 FT	10 FT
MINIMUM SECOND STORY HEIGHT:	10 FT	10 FT
FACADE GLAZING:	STOOP FACADE TYPE	20% - 50%
ALLOWED ROOF TYPES:	FLAT, GABLE, HIP, GAMBREL, MANSARD	FLAT

**ZONING ORDINANCE 10.5A46.20 ALLOWS A 1-STORY, UP TO 10' HEIGHT INCREASE WITH 20% COMMUNITY SPACE.

COMMUNITY SPACE:	REQUIRED	PROVIDED
	8,559 SF 20%	11,367 SF 26.6%

LEGEND

- PROPERTY LINE
- - - PROPOSED PROPERTY LINE
- ===== PROPOSED EDGE OF PAVEMENT
- ===== PROPOSED CURB
- [Hatched Box] PROPOSED BUILDING
- [Hatched Box] PROPOSED PAVEMENT SECTION
- [Dotted Box] PROPOSED CONCRETE SIDEWALK
- [Brick Box] PROPOSED BRICK SIDEWALK
- PROPOSED BOLLARD
- BLDG TYP BUILDING
- COORD COORDINATE
- 30'R PROPOSED CURB RADIUS
- VGC PROPOSED VERTICAL GRANITE CURB
- SGC PROPOSED SLOPED GRANITE CURB



Proposed Office Building

RW Norfolk Holdings, LLC

Portsmouth, New Hampshire

MARK	DATE	DESCRIPTION
A	3/18/2019	TAC Submission

PROJECT NO: K-0076-019
DATE: 03/18/2019
FILE: K-0076-019_C-SITE.dwg
DRAWN BY: NAH
CHECKED: PMC
APPROVED: BLM

OVERALL SITE PLAN

SCALE: AS SHOWN

Traffic Data

PDI File #: **196718 A**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Raynes Avenue W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars and Heavy Vehicles (Combined)

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	76	10	0	86	4	0	4	0	8	2	83	0	0	85	1	0	0	0	1	180
4:15 PM	0	66	7	0	73	6	0	4	0	10	4	105	0	0	109	0	0	0	0	0	192
4:30 PM	0	65	2	0	67	6	0	6	2	14	4	96	0	0	100	0	0	0	0	0	181
4:45 PM	0	90	8	0	98	9	0	1	0	10	2	101	0	0	103	0	0	0	0	0	211
Total	0	297	27	0	324	25	0	15	2	42	12	385	0	0	397	1	0	0	0	1	764
5:00 PM	0	80	10	0	90	15	0	6	0	21	7	137	0	0	144	0	0	0	0	0	255
5:15 PM	0	89	9	0	98	11	0	5	0	16	9	105	0	0	114	0	0	0	0	0	228
5:30 PM	0	107	6	0	113	11	0	10	0	21	4	104	0	0	108	0	0	0	0	0	242
5:45 PM	0	95	7	0	102	6	0	3	0	9	1	96	0	0	97	0	0	0	0	0	208
Total	0	371	32	0	403	43	0	24	0	67	21	442	0	0	463	0	0	0	0	0	933
Grand Total	0	668	59	0	727	68	0	39	2	109	33	827	0	0	860	1	0	0	0	1	1697
Approach %	0.0	91.9	8.1	0.0		62.4	0.0	35.8	1.8		3.8	96.2	0.0	0.0		100.0	0.0	0.0	0.0		
Total %	0.0	39.4	3.5	0.0	42.8	4.0	0.0	2.3	0.1	6.4	1.9	48.7	0.0	0.0	50.7	0.1	0.0	0.0	0.0	0.1	
Exiting Leg Total	895					94					708					0					1697
Cars	0	664	59	0	723	68	0	39	2	109	33	820	0	0	853	1	0	0	0	1	1686
% Cars	0.0	99.4	100.0	0.0	99.4	100.0	0.0	100.0	100.0	100.0	100.0	99.2	0.0	0.0	99.2	100.0	0.0	0.0	0.0	100.0	99.4
Exiting Leg Total	888					94					704					0					1686
Heavy Vehicles	0	4	0	0	4	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	11
% Heavy Vehicles	0.0	0.6	0.0	0.0	0.6	0.0	0.0	0.0	0.0	0.0	0.0	0.8	0.0	0.0	0.8	0.0	0.0	0.0	0.0	0.0	0.6
Exiting Leg Total	7					0					4					0					11

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:45 PM	0	90	8	0	98	9	0	1	0	10	2	101	0	0	103	0	0	0	0	0	211
5:00 PM	0	80	10	0	90	15	0	6	0	21	7	137	0	0	144	0	0	0	0	0	255
5:15 PM	0	89	9	0	98	11	0	5	0	16	9	105	0	0	114	0	0	0	0	0	228
5:30 PM	0	107	6	0	113	11	0	10	0	21	4	104	0	0	108	0	0	0	0	0	242
Total Volume	0	366	33	0	399	46	0	22	0	68	22	447	0	0	469	0	0	0	0	0	936
% Approach Total	0.0	91.7	8.3	0.0		67.6	0.0	32.4	0.0		4.7	95.3	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.855	0.825	0.000	0.883	0.767	0.000	0.550	0.000	0.810	0.611	0.816	0.000	0.000	0.814	0.000	0.000	0.000	0.000	0.000	0.918
Cars	0	366	33	0	399	46	0	22	0	68	22	444	0	0	466	0	0	0	0	0	933
Cars %	0.0	100.0	100.0	0.0	100.0	100.0	0.0	100.0	0.0	100.0	100.0	99.3	0.0	0.0	99.4	0.0	0.0	0.0	0.0	0.0	99.7
Heavy Vehicles	0	0	0	0	0	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	3
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.7	0.0	0.0	0.6	0.0	0.0	0.0	0.0	0.0	0.3
Cars Enter Leg	0	366	33	0	399	46	0	22	0	68	22	444	0	0	466	0	0	0	0	0	933
Heavy Enter Leg	0	0	0	0	0	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	3
Total Entering Leg	0	366	33	0	399	46	0	22	0	68	22	447	0	0	469	0	0	0	0	0	936
Cars Exiting Leg	490					55					388					0					933
Heavy Exiting Leg	3					0					0					0					3
Total Exiting Leg	493					55					388					0					936

PDI File #: **196718 A**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Raynes Avenue W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars-Combined (Motorcycles, Cars, Light Goods)

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	72	10	0	82	4	0	4	0	8	2	81	0	0	83	1	0	0	0	1	174
4:15 PM	0	66	7	0	73	6	0	4	0	10	4	104	0	0	108	0	0	0	0	0	191
4:30 PM	0	65	2	0	67	6	0	6	2	14	4	96	0	0	100	0	0	0	0	0	181
4:45 PM	0	90	8	0	98	9	0	1	0	10	2	99	0	0	101	0	0	0	0	0	209
Total	0	293	27	0	320	25	0	15	2	42	12	380	0	0	392	1	0	0	0	1	755
5:00 PM	0	80	10	0	90	15	0	6	0	21	7	136	0	0	143	0	0	0	0	0	254
5:15 PM	0	89	9	0	98	11	0	5	0	16	9	105	0	0	114	0	0	0	0	0	228
5:30 PM	0	107	6	0	113	11	0	10	0	21	4	104	0	0	108	0	0	0	0	0	242
5:45 PM	0	95	7	0	102	6	0	3	0	9	1	95	0	0	96	0	0	0	0	0	207
Total	0	371	32	0	403	43	0	24	0	67	21	440	0	0	461	0	0	0	0	0	931
Grand Total	0	664	59	0	723	68	0	39	2	109	33	820	0	0	853	1	0	0	0	1	1686
Approach %	0.0	91.8	8.2	0.0		62.4	0.0	35.8	1.8		3.9	96.1	0.0	0.0		100.0	0.0	0.0	0.0		
Total %	0.0	39.4	3.5	0.0	42.9	4.0	0.0	2.3	0.1	6.5	2.0	48.6	0.0	0.0	50.6	0.1	0.0	0.0	0.0	0.1	
Exiting Leg Total	888					94					704					0					1686

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:45 PM	0	90	8	0	98	9	0	1	0	10	2	99	0	0	101	0	0	0	0	0	209
5:00 PM	0	80	10	0	90	15	0	6	0	21	7	136	0	0	143	0	0	0	0	0	254
5:15 PM	0	89	9	0	98	11	0	5	0	16	9	105	0	0	114	0	0	0	0	0	228
5:30 PM	0	107	6	0	113	11	0	10	0	21	4	104	0	0	108	0	0	0	0	0	242
Total Volume	0	366	33	0	399	46	0	22	0	68	22	444	0	0	466	0	0	0	0	0	933
% Approach Total	0.0	91.7	8.3	0.0		67.6	0.0	32.4	0.0		4.7	95.3	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.855	0.825	0.000	0.883	0.767	0.000	0.550	0.000	0.810	0.611	0.816	0.000	0.000	0.815	0.000	0.000	0.000	0.000	0.000	0.918
Entering Leg	0	366	33	0	399	46	0	22	0	68	22	444	0	0	466	0	0	0	0	0	933
Exiting Leg	490					55					388					0					933
Total	889					123					854					0					1866

PDI File #: **196718 A**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Raynes Avenue W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	4	0	0	4	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	6
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Total	0	4	0	0	4	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	9
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
Total	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Grand Total	0	4	0	0	4	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	11
Approach %	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	36.4	0.0	0.0	36.4	0.0	0.0	0.0	0.0	0.0	0.0	63.6	0.0	0.0	63.6	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	7					0					4					0					11
Buses	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	4
% Buses	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	28.6	0.0	0.0	28.6	0.0	0.0	0.0	0.0	0.0	36.4
Exiting Leg Total	2					0					2					0					4
Single-Unit Trucks	0	2	0	0	2	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	7
% Single-Unit	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	71.4	0.0	0.0	71.4	0.0	0.0	0.0	0.0	0.0	63.6
Exiting Leg Total	5					0					2					0					7
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	4	0	0	4	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	6
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Total Volume	0	4	0	0	4	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	9
% Approach Total	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.625	0.000	0.000	0.625	0.000	0.000	0.000	0.000	0.000	0.375
Buses	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
Buses %	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	20.0	0.0	0.0	20.0	0.0	0.0	0.0	0.0	0.0	33.3
Single-Unit Trucks	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	6
Single-Unit %	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	80.0	0.0	0.0	80.0	0.0	0.0	0.0	0.0	0.0	66.7
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Buses	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
Single-Unit Trucks	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	6
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	0	4	0	0	4	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	9
Buses	1					0					2					0					3
Single-Unit Trucks	4					0					2					0					6
Articulated Trucks	0					0					0					0					0
Total Exiting Leg	5					0					4					0					9

PDI File #: **196718 A**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Raynes Avenue W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	61	7	0	68	2	0	4	0	6	2	69	0	0	71	1	0	0	0	1	146
4:15 PM	0	59	7	0	66	6	0	2	0	8	3	95	0	0	98	0	0	0	0	0	172
4:30 PM	0	51	0	0	51	5	0	6	2	13	4	78	0	0	82	0	0	0	0	0	146
4:45 PM	0	86	7	0	93	8	0	0	0	8	2	91	0	0	93	0	0	0	0	0	194
Total	0	257	21	0	278	21	0	12	2	35	11	333	0	0	344	1	0	0	0	1	658
5:00 PM	0	73	9	0	82	13	0	6	0	19	7	125	0	0	132	0	0	0	0	0	233
5:15 PM	0	83	7	0	90	11	0	5	0	16	8	98	0	0	106	0	0	0	0	0	212
5:30 PM	0	104	6	0	110	9	0	8	0	17	4	91	0	0	95	0	0	0	0	0	222
5:45 PM	0	90	6	0	96	6	0	3	0	9	1	88	0	0	89	0	0	0	0	0	194
Total	0	350	28	0	378	39	0	22	0	61	20	402	0	0	422	0	0	0	0	0	861
Grand Total	0	607	49	0	656	60	0	34	2	96	31	735	0	0	766	1	0	0	0	1	1519
Approach %	0.0	92.5	7.5	0.0		62.5	0.0	35.4	2.1		4.0	96.0	0.0	0.0		100.0	0.0	0.0	0.0		
Total %	0.0	40.0	3.2	0.0	43.2	3.9	0.0	2.2	0.1	6.3	2.0	48.4	0.0	0.0	50.4	0.1	0.0	0.0	0.0	0.1	
Exiting Leg Total	795					82					642					0					1519

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:45 PM	0	86	7	0	93	8	0	0	0	8	2	91	0	0	93	0	0	0	0	0	194
5:00 PM	0	73	9	0	82	13	0	6	0	19	7	125	0	0	132	0	0	0	0	0	233
5:15 PM	0	83	7	0	90	11	0	5	0	16	8	98	0	0	106	0	0	0	0	0	212
5:30 PM	0	104	6	0	110	9	0	8	0	17	4	91	0	0	95	0	0	0	0	0	222
Total Volume	0	346	29	0	375	41	0	19	0	60	21	405	0	0	426	0	0	0	0	0	861
% Approach Total	0.0	92.3	7.7	0.0		68.3	0.0	31.7	0.0		4.9	95.1	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.832	0.806	0.000	0.852	0.788	0.000	0.594	0.000	0.789	0.656	0.810	0.000	0.000	0.807	0.000	0.000	0.000	0.000	0.000	0.924
Entering Leg	0	346	29	0	375	41	0	19	0	60	21	405	0	0	426	0	0	0	0	0	861
Exiting Leg	446					50					365					0					861
Total	821					110					791					0					1722

PDI File #: **196718 A**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Raynes Avenue W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Light Goods Vehicle

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
4:00 PM	0	11	3	0	14	2	0	0	0	2	0	12	0	0	12	0	0	0	0	0	28	
4:15 PM	0	7	0	0	7	0	0	2	0	2	1	9	0	0	10	0	0	0	0	0	19	
4:30 PM	0	14	2	0	16	1	0	0	0	1	0	18	0	0	18	0	0	0	0	0	35	
4:45 PM	0	4	1	0	5	1	0	1	0	2	0	8	0	0	8	0	0	0	0	0	15	
Total	0	36	6	0	42	4	0	3	0	7	1	47	0	0	48	0	0	0	0	0	97	
5:00 PM	0	7	1	0	8	2	0	0	0	2	0	11	0	0	11	0	0	0	0	0	21	
5:15 PM	0	6	2	0	8	0	0	0	0	0	1	7	0	0	8	0	0	0	0	0	16	
5:30 PM	0	3	0	0	3	2	0	2	0	4	0	13	0	0	13	0	0	0	0	0	20	
5:45 PM	0	5	1	0	6	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	13	
Total	0	21	4	0	25	4	0	2	0	6	1	38	0	0	39	0	0	0	0	0	70	
Grand Total	0	57	10	0	67	8	0	5	0	13	2	85	0	0	87	0	0	0	0	0	167	
Approach %	0.0	85.1	14.9	0.0		61.5	0.0	38.5	0.0		2.3	97.7	0.0	0.0		0.0	0.0	0.0	0.0			
Total %	0.0	34.1	6.0	0.0	40.1	4.8	0.0	3.0	0.0	7.8	1.2	50.9	0.0	0.0	52.1	0.0	0.0	0.0	0.0	0.0		
Exiting Leg Total						93					12					62					0	167

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
4:00 PM	0	11	3	0	14	2	0	0	0	2	0	12	0	0	12	0	0	0	0	0	28	
4:15 PM	0	7	0	0	7	0	0	2	0	2	1	9	0	0	10	0	0	0	0	0	19	
4:30 PM	0	14	2	0	16	1	0	0	0	1	0	18	0	0	18	0	0	0	0	0	35	
4:45 PM	0	4	1	0	5	1	0	1	0	2	0	8	0	0	8	0	0	0	0	0	15	
Total Volume	0	36	6	0	42	4	0	3	0	7	1	47	0	0	48	0	0	0	0	0	97	
% Approach Total	0.0	85.7	14.3	0.0		57.1	0.0	42.9	0.0		2.1	97.9	0.0	0.0		0.0	0.0	0.0	0.0			
PHF	0.000	0.643	0.500	0.000	0.656	0.500	0.000	0.375	0.000	0.875	0.250	0.653	0.000	0.000	0.667	0.000	0.000	0.000	0.000	0.000	0.693	
Entering Leg	0	36	6	0	42	4	0	3	0	7	1	47	0	0	48	0	0	0	0	0	97	
Exiting Leg						51					7					39					0	97
Total						93					14					87					0	194

PDI File #: **196718 A**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Raynes Avenue W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Buses

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
Grand Total	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	4
Approach %	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	2					0					2					0					4

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
% Approach Total	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.250
Entering Leg	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
Exiting Leg	1					0					2					0					3
Total	3					0					3					0					6

PDI File #: **196718 A**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Raynes Avenue W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Single-Unit Trucks

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Total	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	6
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
Total	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
Grand Total	0	2	0	0	2	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	7
Approach %	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	28.6	0.0	0.0	28.6	0.0	0.0	0.0	0.0	0.0	0.0	71.4	0.0	0.0	71.4	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total						5					0					2					7

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Total Volume	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	6
% Approach Total	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.500
Entering Leg	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	6
Exiting Leg						4					0					2					6
Total						6					0					6					12

PDI File #: **196718 A**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Raynes Avenue W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Articulated Trucks

	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Maplewood Avenue					Raynes Avenue					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0					0					0					0					0
Total	0					0					0					0					0

PDI File #: 196718 A
 Location: N: Maplewood Avenue S: Maplewood Avenue
 Location: E: Raynes Avenue W: Driveway
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Bicycles (on Roadway and Crosswalks)

	Maplewood Avenue							Raynes Avenue							Maplewood Avenue							Driveway							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:45 PM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	
Total	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	
Grand Total	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	50.0	0.0	0.0	0.0	50.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	1							0							1							0							2

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Maplewood Avenue							Raynes Avenue							Maplewood Avenue							Driveway							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:45 PM	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	
Total Volume	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.250	
Entering Leg	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	
Exiting Leg	1							0							1							0							2
Total	1							1							2							0							4

PDI File #: 196718 A
 Location: N: Maplewood Avenue S: Maplewood Avenue
 Location: E: Raynes Avenue W: Driveway
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Pedestrians

	Maplewood Avenue							Raynes Avenue							Maplewood Avenue							Driveway							Total						
	from North							from East							from South							from West													
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total							
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	2
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	2
Total	0	0	0	0	0	0	0	0	0	0	0	3	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	8
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	0	0	0	0	0	0	0	0	0	0	2	2	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3
Grand Total	0	0	0	0	0	0	0	0	0	0	0	3	4	7	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	3	0	3	11
Approach %	0	0	0	0	0	0	0	0	0	0	0	42.9	57.1		0	0	0	0	0	0	100						0	0	0	0	0	100	0		
Total %	0	0	0	0	0	0	0	0	0	0	0	27.3	36.4	63.6	0	0	0	0	0	0	9.09	9.09					0	0	0	0	0	27.3	0	27.3	
Exiting Leg Total	0							7							1							3							11						

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue							Raynes Avenue							Maplewood Avenue							Driveway							Total						
	from North							from East							from South							from West													
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total							
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	2
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	2
Total Volume	0	0	0	0	0	0	0	0	0	0	0	3	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	8
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	60.0	40.0		0.0	0.0	0.0	0.0	0.0	0.0							0.0	0.0	0.0	0.0	0.0	100.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000		0.000	0.000	0.000	0.000	0.750	0.250	0.625	0.000	0.000	0.000	0.000	0.000	0.000	0.000						0.000	0.000	0.000	0.000	0.750	0.000	0.750		1.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	3	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	8
Exiting Leg	0							5							0							3							8						
Total	0							10							0							6							16						

PDI File #: **196718 B**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Kennebunk Savings Bank Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Cars and Heavy Vehicles (Combined)

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	79	0	0	79	3	4	0	7	2	81	0	83	169
4:15 PM	73	1	0	74	5	2	0	7	0	101	0	101	182
4:30 PM	71	0	0	71	1	2	0	3	2	100	0	102	176
4:45 PM	92	0	0	92	0	1	0	1	1	97	0	98	191
Total	315	1	0	316	9	9	0	18	5	379	0	384	718
5:00 PM	89	0	0	89	1	0	0	1	1	146	0	147	237
5:15 PM	92	1	0	93	3	2	0	5	1	105	0	106	204
5:30 PM	119	0	0	119	0	2	0	2	0	109	0	109	230
5:45 PM	102	1	0	103	5	0	1	6	1	95	0	96	205
Total	402	2	0	404	9	4	1	14	3	455	0	458	876
Grand Total	717	3	0	720	18	13	1	32	8	834	0	842	1594
Approach %	99.6	0.4	0.0		56.3	40.6	3.1		1.0	99.0	0.0		
Total %	45.0	0.2	0.0	45.2	1.1	0.8	0.1	2.0	0.5	52.3	0.0	52.8	
Exiting Leg Total				852				12				730	1594
Cars	713	3	0	716	18	13	1	32	8	827	0	835	1583
% Cars	99.4	100.0	0.0	99.4	100.0	100.0	100.0	100.0	100.0	99.2	0.0	99.2	99.3
Exiting Leg Total				845				12				726	1583
Heavy Vehicles	4	0	0	4	0	0	0	0	0	7	0	7	11
% Heavy Vehicles	0.6	0.0	0.0	0.6	0.0	0.0	0.0	0.0	0.0	0.8	0.0	0.8	0.7
Exiting Leg Total				7				0				4	11

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
5:00 PM	89	0	0	89	1	0	0	1	1	146	0	147	237
5:15 PM	92	1	0	93	3	2	0	5	1	105	0	106	204
5:30 PM	119	0	0	119	0	2	0	2	0	109	0	109	230
5:45 PM	102	1	0	103	5	0	1	6	1	95	0	96	205
Total Volume	402	2	0	404	9	4	1	14	3	455	0	458	876
% Approach Total	99.5	0.5	0.0		64.3	28.6	7.1		0.7	99.3	0.0		
PHF	0.845	0.500	0.000	0.849	0.450	0.500	0.250	0.583	0.750	0.779	0.000	0.779	0.924
Cars	402	2	0	404	9	4	1	14	3	453	0	456	874
Cars %	100.0	100.0	0.0	100.0	100.0	100.0	100.0	100.0	100.0	99.6	0.0	99.6	99.8
Heavy Vehicles	0	0	0	0	0	0	0	0	0	2	0	2	2
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.4	0.0	0.4	0.2
Cars Enter Leg	402	2	0	404	9	4	1	14	3	453	0	456	874
Heavy Enter Leg	0	0	0	0	0	0	0	0	0	2	0	2	2
Total Entering Leg	402	2	0	404	9	4	1	14	3	455	0	458	876
Cars Exiting Leg				462				6				406	874
Heavy Exiting Leg				2				0				0	2
Total Exiting Leg				464				6				406	876

PDI File #: **196718 B**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Kennebunk Savings Bank Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Cars-Combined (Motorcycles, Cars, Light Goods)

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	77	0	0	77	3	4	0	7	2	79	0	81	165
4:15 PM	71	1	0	72	5	2	0	7	0	100	0	100	179
4:30 PM	71	0	0	71	1	2	0	3	2	100	0	102	176
4:45 PM	92	0	0	92	0	1	0	1	1	95	0	96	189
Total	311	1	0	312	9	9	0	18	5	374	0	379	709
5:00 PM	89	0	0	89	1	0	0	1	1	145	0	146	236
5:15 PM	92	1	0	93	3	2	0	5	1	105	0	106	204
5:30 PM	119	0	0	119	0	2	0	2	0	109	0	109	230
5:45 PM	102	1	0	103	5	0	1	6	1	94	0	95	204
Total	402	2	0	404	9	4	1	14	3	453	0	456	874
Grand Total	713	3	0	716	18	13	1	32	8	827	0	835	1583
Approach %	99.6	0.4	0.0		56.3	40.6	3.1		1.0	99.0	0.0		
Total %	45.0	0.2	0.0	45.2	1.1	0.8	0.1	2.0	0.5	52.2	0.0	52.7	
Exiting Leg Total				845				12				726	1583

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
5:00 PM	89	0	0	89	1	0	0	1	1	145	0	146	236
5:15 PM	92	1	0	93	3	2	0	5	1	105	0	106	204
5:30 PM	119	0	0	119	0	2	0	2	0	109	0	109	230
5:45 PM	102	1	0	103	5	0	1	6	1	94	0	95	204
Total Volume	402	2	0	404	9	4	1	14	3	453	0	456	874
% Approach Total	99.5	0.5	0.0		64.3	28.6	7.1		0.7	99.3	0.0		
PHF	0.845	0.500	0.000	0.849	0.450	0.500	0.250	0.583	0.750	0.781	0.000	0.781	0.926
Entering Leg	402	2	0	404	9	4	1	14	3	453	0	456	874
Exiting Leg				462				6				406	874
Total				866				20				862	1748

PDI File #: **196718 B**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Kennebunk Savings Bank Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Class: **Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)**

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	2	0	0	2	0	0	0	0	0	2	0	2	4
4:15 PM	2	0	0	2	0	0	0	0	0	1	0	1	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	2	0	2	2
Total	4	0	0	4	0	0	0	0	0	5	0	5	9
5:00 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
Total	0	0	0	0	0	0	0	0	0	2	0	2	2
Grand Total	4	0	0	4	0	0	0	0	0	7	0	7	11
Approach %	100.0	0.0	0.0		0.0	0.0	0.0		0.0	100.0	0.0		
Total %	36.4	0.0	0.0	36.4	0.0	0.0	0.0	0.0	0.0	63.6	0.0	63.6	
Exiting Leg Total	7				0				4				11
Buses	2	0	0	2	0	0	0	0	0	2	0	2	4
% Buses	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	28.6	0.0	28.6	36.4
Exiting Leg Total	2				0				2				4
Single-Unit Trucks	2	0	0	2	0	0	0	0	0	5	0	5	7
% Single-Unit	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	71.4	0.0	71.4	63.6
Exiting Leg Total	5				0				2				7
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	2	0	0	2	0	0	0	0	0	2	0	2	4
4:15 PM	2	0	0	2	0	0	0	0	0	1	0	1	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	2	0	2	2
Total Volume	4	0	0	4	0	0	0	0	0	5	0	5	9
% Approach Total	100.0	0.0	0.0		0.0	0.0	0.0		0.0	100.0	0.0		
PHF	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.625	0.000	0.625	0.563
Buses	2	0	0	2	0	0	0	0	0	1	0	1	3
Buses %	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	20.0	0.0	20.0	33.3
Single-Unit Trucks	2	0	0	2	0	0	0	0	0	4	0	4	6
Single-Unit %	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	80.0	0.0	80.0	66.7
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Buses	2	0	0	2	0	0	0	0	0	1	0	1	3
Single-Unit Trucks	2	0	0	2	0	0	0	0	0	4	0	4	6
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	4	0	0	4	0	0	0	0	0	5	0	5	9
Buses	1				0				2				3
Single-Unit Trucks	4				0				2				6
Articulated Trucks	0				0				0				0
Total Exiting Leg	5				0				4				9

PDI File #: **196718 B**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Kennebunk Savings Bank Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	65	0	0	65	3	3	0	6	2	67	0	69	140
4:15 PM	59	0	0	59	5	1	0	6	0	89	0	89	154
4:30 PM	57	0	0	57	1	2	0	3	2	82	0	84	144
4:45 PM	87	0	0	87	0	1	0	1	0	88	0	88	176
Total	268	0	0	268	9	7	0	16	4	326	0	330	614
5:00 PM	82	0	0	82	0	0	0	0	1	135	0	136	218
5:15 PM	86	1	0	87	3	0	0	3	0	98	0	98	188
5:30 PM	110	0	0	110	0	2	0	2	0	96	0	96	208
5:45 PM	94	1	0	95	5	0	1	6	1	88	0	89	190
Total	372	2	0	374	8	2	1	11	2	417	0	419	804
Grand Total	640	2	0	642	17	9	1	27	6	743	0	749	1418
Approach %	99.7	0.3	0.0		63.0	33.3	3.7		0.8	99.2	0.0		
Total %	45.1	0.1	0.0	45.3	1.2	0.6	0.1	1.9	0.4	52.4	0.0	52.8	
Exiting Leg Total				760				9				649	1418

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
5:00 PM	82	0	0	82	0	0	0	0	1	135	0	136	218
5:15 PM	86	1	0	87	3	0	0	3	0	98	0	98	188
5:30 PM	110	0	0	110	0	2	0	2	0	96	0	96	208
5:45 PM	94	1	0	95	5	0	1	6	1	88	0	89	190
Total Volume	372	2	0	374	8	2	1	11	2	417	0	419	804
% Approach Total	99.5	0.5	0.0		72.7	18.2	9.1		0.5	99.5	0.0		
PHF	0.845	0.500	0.000	0.850	0.400	0.250	0.250	0.458	0.500	0.772	0.000	0.770	0.922
Entering Leg	372	2	0	374	8	2	1	11	2	417	0	419	804
Exiting Leg				425				5				374	804
Total				799				16				793	1608

PDI File #: **196718 B**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Kennebunk Savings Bank Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Class: **Light Goods Vehicle**

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	12	0	0	12	0	1	0	1	0	12	0	12	25
4:15 PM	12	1	0	13	0	1	0	1	0	11	0	11	25
4:30 PM	14	0	0	14	0	0	0	0	0	18	0	18	32
4:45 PM	5	0	0	5	0	0	0	0	1	7	0	8	13
Total	43	1	0	44	0	2	0	2	1	48	0	49	95
5:00 PM	7	0	0	7	1	0	0	1	0	10	0	10	18
5:15 PM	6	0	0	6	0	2	0	2	1	7	0	8	16
5:30 PM	9	0	0	9	0	0	0	0	0	13	0	13	22
5:45 PM	8	0	0	8	0	0	0	0	0	6	0	6	14
Total	30	0	0	30	1	2	0	3	1	36	0	37	70
Grand Total	73	1	0	74	1	4	0	5	2	84	0	86	165
Approach %	98.6	1.4	0.0		20.0	80.0	0.0		2.3	97.7	0.0		
Total %	44.2	0.6	0.0	44.8	0.6	2.4	0.0	3.0	1.2	50.9	0.0	52.1	
Exiting Leg Total				85				3				77	165

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	12	0	0	12	0	1	0	1	0	12	0	12	25
4:15 PM	12	1	0	13	0	1	0	1	0	11	0	11	25
4:30 PM	14	0	0	14	0	0	0	0	0	18	0	18	32
4:45 PM	5	0	0	5	0	0	0	0	1	7	0	8	13
Total Volume	43	1	0	44	0	2	0	2	1	48	0	49	95
% Approach Total	97.7	2.3	0.0		0.0	100.0	0.0		2.0	98.0	0.0		
PHF	0.768	0.250	0.000	0.786	0.000	0.500	0.000	0.500	0.250	0.667	0.000	0.681	0.742
Entering Leg	43	1	0	44	0	2	0	2	1	48	0	49	95
Exiting Leg				48				2				45	95
Total				92				4				94	190

PDI File #: **196718 B**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Kennebunk Savings Bank Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Buses

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total	
	from North				from East				from South					
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total		
4:00 PM	1	0	0	1	0	0	0	0	0	0	1	0	1	2
4:15 PM	1	0	0	1	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	2	0	0	2	0	0	0	0	0	0	1	0	1	3
5:00 PM	0	0	0	0	0	0	0	0	0	0	1	0	1	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	1	0	1	1
Grand Total	2	0	0	2	0	0	0	0	0	0	2	0	2	4
Approach %	100.0	0.0	0.0		0.0	0.0	0.0		0.0	100.0	0.0			
Total %	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	50.0	0.0	50.0		
Exiting Leg Total				2				0					2	4

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total	
	from North				from East				from South					
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total		
4:00 PM	1	0	0	1	0	0	0	0	0	0	1	0	1	2
4:15 PM	1	0	0	1	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	2	0	0	2	0	0	0	0	0	0	1	0	1	3
% Approach Total	100.0	0.0	0.0		0.0	0.0	0.0		0.0	100.0	0.0			
PHF	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.250	0.375
Entering Leg	2	0	0	2	0	0	0	0	0	0	1	0	1	3
Exiting Leg				1				0					2	3
Total				3				0					3	6

PDI File #: **196718 B**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Kennebunk Savings Bank Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Single-Unit Trucks

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total	
	from North				from East				from South					
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total		
4:00 PM	1	0	0	1	0	0	0	0	0	0	1	0	1	2
4:15 PM	1	0	0	1	0	0	0	0	0	0	1	0	1	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	2	0	2	2
Total	2	0	0	2	0	0	0	0	0	0	4	0	4	6
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	1	0	1	1
Total	0	0	0	0	0	0	0	0	0	0	1	0	1	1
Grand Total	2	0	0	2	0	0	0	0	0	0	5	0	5	7
Approach %	100.0	0.0	0.0		0.0	0.0	0.0		0.0	100.0	0.0			
Total %	28.6	0.0	0.0	28.6	0.0	0.0	0.0	0.0	0.0	71.4	0.0	71.4		
Exiting Leg Total				5				0					2	7

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total	
	from North				from East				from South					
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total		
4:00 PM	1	0	0	1	0	0	0	0	0	0	1	0	1	2
4:15 PM	1	0	0	1	0	0	0	0	0	0	1	0	1	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	2	0	2	2
Total Volume	2	0	0	2	0	0	0	0	0	0	4	0	4	6
% Approach Total	100.0	0.0	0.0		0.0	0.0	0.0		0.0	100.0	0.0			
PHF	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.500	0.750
Entering Leg	2	0	0	2	0	0	0	0	0	0	4	0	4	6
Exiting Leg				4				0					2	6
Total				6				0					6	12

PDI File #: **196718 B**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Kennebunk Savings Bank Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Articulated Trucks

	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total	
	from North				from East				from South					
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
Exiting Leg Total				0				0					0	0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Maplewood Avenue				Kennebunk Savings Bank Driveway				Maplewood Avenue				Total	
	from North				from East				from South					
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000		0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg				0				0					0	0
Total				0				0					0	0

PDI File #: **196718 B**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Kennebunk Savings Bank Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Bicycles (on Roadway and Crosswalks)

	Maplewood Avenue						Kennebunk Savings Bank Driveway						Maplewood Avenue						Total
	from North						from East						from South						
	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	U-Turn	CW-WB	CW-EB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	1	0	0	0	0	1	0	0	0	0	1	1	0	1	0	0	0	1	3
Total	1	0	0	0	0	1	0	0	0	0	1	1	0	1	0	0	0	1	3
Grand Total	1	0	0	0	0	1	0	0	0	0	1	1	0	1	0	0	0	1	3
Approach %	100.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	100.0		0.0	100.0	0.0	0.0	0.0		
Total %	33.3	0.0	0.0	0.0	0.0	33.3	0.0	0.0	0.0	0.0	33.3	33.3	0.0	33.3	0.0	0.0	0.0	33.3	
Exiting Leg Total	1						1						1						3

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue						Kennebunk Savings Bank Driveway						Maplewood Avenue						Total
	from North						from East						from South						
	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	U-Turn	CW-WB	CW-EB	Total	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	1	0	0	0	0	1	0	0	0	0	1	1	0	1	0	0	0	1	3
Total Volume	1	0	0	0	0	1	0	0	0	0	1	1	0	1	0	0	0	1	3
% Approach Total	100.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	100.0		0.0	100.0	0.0	0.0	0.0		
PHF	0.250	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.250	0.250	0.000	0.250	0.000	0.000	0.000	0.250	0.250
Entering Leg	1	0	0	0	0	1	0	0	0	0	1	1	0	1	0	0	0	1	3
Exiting Leg	1						1						1						3
Total	2						2						2						6

PDI File #: **196718 B**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Kennebunk Savings Bank Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Pedestrians

	Maplewood Avenue						Kennebunk Savings Bank Driveway						Maplewood Avenue						Total
	from North						from East						from South						
	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	U-Turn	CW-WB	CW-EB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	3
4:15 PM	0	0	0	1	0	1	0	0	0	1	1	2	0	0	0	0	0	0	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	4	4	0	0	0	0	0	0	4
4:45 PM	0	0	0	0	0	0	0	0	0	0	4	4	0	0	0	0	0	0	4
Total	0	0	0	1	0	1	0	0	0	4	9	13	0	0	0	0	0	0	14
5:00 PM	0	0	0	0	0	0	0	0	0	0	2	2	0	0	0	0	0	0	2
5:15 PM	0	0	0	0	0	0	0	0	0	0	2	2	0	0	0	0	0	0	2
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	4	4	0	0	0	0	0	0	4
Grand Total	0	0	0	1	0	1	0	0	0	4	13	17	0	0	0	0	0	0	18
Approach %	0	0	0	100	0		0	0	0	23.529	76.471		0	0	0	0	0		
Total %	0	0	0	5.5556	0	5.5556	0	0	0	22.222	72.222	94.444	0	0	0	0	0	0	
Exiting Leg Total	1						17						0						18

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Maplewood Avenue						Kennebunk Savings Bank Driveway						Maplewood Avenue						Total
	from North						from East						from South						
	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	U-Turn	CW-WB	CW-EB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	3
4:15 PM	0	0	0	1	0	1	0	0	0	1	1	2	0	0	0	0	0	0	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	4	4	0	0	0	0	0	0	4
4:45 PM	0	0	0	0	0	0	0	0	0	0	4	4	0	0	0	0	0	0	4
Total Volume	0	0	0	1	0	1	0	0	0	4	9	13	0	0	0	0	0	0	14
% Approach Total	0.0	0.0	0.0	100.0	0.0		0.0	0.0	0.0	30.8	69.2		0.0	0.0	0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.333	0.563	0.813	0.000	0.000	0.000	0.000	0.000	0.000	0.875
Entering Leg	0	0	0	1	0	1	0	0	0	4	9	13	0	0	0	0	0	0	14
Exiting Leg	1						13						0						14
Total	2						26						0						28

PDI File #: **196718 C**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Vaughan Street W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Cars and Heavy Vehicles (Combined)

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	79	3	0	82	0	0	7	0	7	4	84	0	0	88	0	0	0	0	0	177
4:15 PM	0	76	1	0	77	1	0	3	0	4	7	100	0	0	107	0	0	0	0	0	188
4:30 PM	0	72	1	0	73	3	0	4	0	7	7	99	0	0	106	0	0	0	0	0	186
4:45 PM	0	94	1	0	95	2	0	1	0	3	3	97	0	0	100	0	0	0	0	0	198
Total	0	321	6	0	327	6	0	15	0	21	21	380	0	0	401	0	0	0	0	0	749
5:00 PM	0	85	4	0	89	3	0	5	0	8	3	143	0	1	147	0	0	0	0	0	244
5:15 PM	0	90	3	1	94	2	0	7	0	9	3	104	0	0	107	0	0	0	0	0	210
5:30 PM	0	119	2	0	121	4	0	3	0	7	4	104	0	0	108	0	0	0	0	0	236
5:45 PM	0	99	3	0	102	1	0	2	0	3	8	95	0	0	103	0	0	0	0	0	208
Total	0	393	12	1	406	10	0	17	0	27	18	446	0	1	465	0	0	0	0	0	898
Grand Total	0	714	18	1	733	16	0	32	0	48	39	826	0	1	866	0	0	0	0	0	1647
Approach %	0.0	97.4	2.5	0.1		33.3	0.0	66.7	0.0		4.5	95.4	0.0	0.1		0.0	0.0	0.0	0.0		
Total %	0.0	43.4	1.1	0.1	44.5	1.0	0.0	1.9	0.0	2.9	2.4	50.2	0.0	0.1	52.6	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	843					57					747					0					1647
Cars	0	710	18	1	729	16	0	32	0	48	39	819	0	1	859	0	0	0	0	0	1636
% Cars	0.0	99.4	100.0	100.0	99.5	100.0	0.0	100.0	0.0	100.0	100.0	99.2	0.0	100.0	99.2	0.0	0.0	0.0	0.0	0.0	99.3
Exiting Leg Total	836					57					743					0					1636
Heavy Vehicles	0	4	0	0	4	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	11
% Heavy Vehicles	0.0	0.6	0.0	0.0	0.5	0.0	0.0	0.0	0.0	0.0	0.0	0.8	0.0	0.0	0.8	0.0	0.0	0.0	0.0	0.0	0.7
Exiting Leg Total	7					0					4					0					11

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
5:00 PM	0	85	4	0	89	3	0	5	0	8	3	143	0	1	147	0	0	0	0	0	244
5:15 PM	0	90	3	1	94	2	0	7	0	9	3	104	0	0	107	0	0	0	0	0	210
5:30 PM	0	119	2	0	121	4	0	3	0	7	4	104	0	0	108	0	0	0	0	0	236
5:45 PM	0	99	3	0	102	1	0	2	0	3	8	95	0	0	103	0	0	0	0	0	208
Total Volume	0	393	12	1	406	10	0	17	0	27	18	446	0	1	465	0	0	0	0	0	898
% Approach Total	0.0	96.8	3.0	0.2		37.0	0.0	63.0	0.0		3.9	95.9	0.0	0.2		0.0	0.0	0.0	0.0		
PHF	0.000	0.826	0.750	0.250	0.839	0.625	0.000	0.607	0.000	0.750	0.563	0.780	0.000	0.250	0.791	0.000	0.000	0.000	0.000	0.000	0.920
Cars	0	393	12	1	406	10	0	17	0	27	18	444	0	1	463	0	0	0	0	0	896
Cars %	0.0	100.0	100.0	100.0	100.0	100.0	0.0	100.0	0.0	100.0	100.0	99.6	0.0	100.0	99.6	0.0	0.0	0.0	0.0	0.0	99.8
Heavy Vehicles	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.4	0.0	0.0	0.4	0.0	0.0	0.0	0.0	0.0	0.2
Cars Enter Leg	0	393	12	1	406	10	0	17	0	27	18	444	0	1	463	0	0	0	0	0	896
Heavy Enter Leg	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Total Entering Leg	0	393	12	1	406	10	0	17	0	27	18	446	0	1	465	0	0	0	0	0	898
Cars Exiting Leg	455					30					411					0					896
Heavy Exiting Leg	2					0					0					0					2
Total Exiting Leg	457					30					411					0					898

PDI File #: **196718 C**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Vaughan Street W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Cars-Combined (Motorcycles, Cars, Light Goods)

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	77	3	0	80	0	0	7	0	7	4	82	0	0	86	0	0	0	0	0	173
4:15 PM	0	74	1	0	75	1	0	3	0	4	7	99	0	0	106	0	0	0	0	0	185
4:30 PM	0	72	1	0	73	3	0	4	0	7	7	99	0	0	106	0	0	0	0	0	186
4:45 PM	0	94	1	0	95	2	0	1	0	3	3	95	0	0	98	0	0	0	0	0	196
Total	0	317	6	0	323	6	0	15	0	21	21	375	0	0	396	0	0	0	0	0	740
5:00 PM	0	85	4	0	89	3	0	5	0	8	3	142	0	1	146	0	0	0	0	0	243
5:15 PM	0	90	3	1	94	2	0	7	0	9	3	104	0	0	107	0	0	0	0	0	210
5:30 PM	0	119	2	0	121	4	0	3	0	7	4	104	0	0	108	0	0	0	0	0	236
5:45 PM	0	99	3	0	102	1	0	2	0	3	8	94	0	0	102	0	0	0	0	0	207
Total	0	393	12	1	406	10	0	17	0	27	18	444	0	1	463	0	0	0	0	0	896
Grand Total	0	710	18	1	729	16	0	32	0	48	39	819	0	1	859	0	0	0	0	0	1636
Approach %	0.0	97.4	2.5	0.1		33.3	0.0	66.7	0.0		4.5	95.3	0.0	0.1		0.0	0.0	0.0	0.0		
Total %	0.0	43.4	1.1	0.1	44.6	1.0	0.0	2.0	0.0	2.9	2.4	50.1	0.0	0.1	52.5	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	836					57					743					0					1636

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
5:00 PM	0	85	4	0	89	3	0	5	0	8	3	142	0	1	146	0	0	0	0	0	243
5:15 PM	0	90	3	1	94	2	0	7	0	9	3	104	0	0	107	0	0	0	0	0	210
5:30 PM	0	119	2	0	121	4	0	3	0	7	4	104	0	0	108	0	0	0	0	0	236
5:45 PM	0	99	3	0	102	1	0	2	0	3	8	94	0	0	102	0	0	0	0	0	207
Total Volume	0	393	12	1	406	10	0	17	0	27	18	444	0	1	463	0	0	0	0	0	896
% Approach Total	0.0	96.8	3.0	0.2		37.0	0.0	63.0	0.0		3.9	95.9	0.0	0.2		0.0	0.0	0.0	0.0		
PHF	0.000	0.826	0.750	0.250	0.839	0.625	0.000	0.607	0.000	0.750	0.563	0.782	0.000	0.250	0.793	0.000	0.000	0.000	0.000	0.000	0.922
Entering Leg	0	393	12	1	406	10	0	17	0	27	18	444	0	1	463	0	0	0	0	0	896
Exiting Leg	455					30					411					0					896
Total	861					57					874					0					1792

PDI File #: **196718 C**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Vaughan Street W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	4
4:15 PM	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Total	0	4	0	0	4	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	9
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
Total	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Grand Total	0	4	0	0	4	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	11
Approach %	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total %	0.0	36.4	0.0	0.0	36.4	0.0	0.0	0.0	0.0	0.0	0.0	63.6	0.0	0.0	63.6	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	7					0					4					0					11
Buses	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	4
% Buses	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	28.6	0.0	0.0	28.6	0.0	0.0	0.0	0.0	0.0	36.4
Exiting Leg Total	2					0					2					0					4
Single-Unit Trucks	0	2	0	0	2	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	7
% Single-Unit	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	71.4	0.0	0.0	71.4	0.0	0.0	0.0	0.0	0.0	63.6
Exiting Leg Total	5					0					2					0					7
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	4
4:15 PM	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Total Volume	0	4	0	0	4	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	9
% Approach Total	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0	
PHF	0.000	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.625	0.000	0.000	0.625	0.000	0.000	0.000	0.000	0.000	0.563
Buses	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
Buses %	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	20.0	0.0	0.0	20.0	0.0	0.0	0.0	0.0	0.0	33.3
Single-Unit Trucks	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	6
Single-Unit %	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	80.0	0.0	0.0	80.0	0.0	0.0	0.0	0.0	0.0	66.7
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Buses	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
Single-Unit Trucks	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	6
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	0	4	0	0	4	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	9
Buses	1					0					2					0					3
Single-Unit Trucks	4					0					2					0					6
Articulated Trucks	0					0					0					0					0
Total Exiting Leg	5					0					4					0					9

PDI File #: **196718 C**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Vaughan Street W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Cars

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	64	3	0	67	0	0	7	0	7	3	70	0	0	73	0	0	0	0	0	147
4:15 PM	0	61	1	0	62	1	0	3	0	4	7	88	0	0	95	0	0	0	0	0	161
4:30 PM	0	59	0	0	59	3	0	4	0	7	6	83	0	0	89	0	0	0	0	0	155
4:45 PM	0	89	1	0	90	2	0	0	0	2	3	87	0	0	90	0	0	0	0	0	182
Total	0	273	5	0	278	6	0	14	0	20	19	328	0	0	347	0	0	0	0	0	645
5:00 PM	0	78	4	0	82	2	0	4	0	6	3	133	0	1	137	0	0	0	0	0	225
5:15 PM	0	83	3	1	87	2	0	6	0	8	3	97	0	0	100	0	0	0	0	0	195
5:30 PM	0	110	2	0	112	3	0	3	0	6	4	92	0	0	96	0	0	0	0	0	214
5:45 PM	0	92	2	0	94	1	0	2	0	3	8	89	0	0	97	0	0	0	0	0	194
Total	0	363	11	1	375	8	0	15	0	23	18	411	0	1	430	0	0	0	0	0	828
Grand Total	0	636	16	1	653	14	0	29	0	43	37	739	0	1	777	0	0	0	0	0	1473
Approach %	0.0	97.4	2.5	0.2		32.6	0.0	67.4	0.0		4.8	95.1	0.0	0.1		0.0	0.0	0.0	0.0		
Total %	0.0	43.2	1.1	0.1	44.3	1.0	0.0	2.0	0.0	2.9	2.5	50.2	0.0	0.1	52.7	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	754					53					666					0					1473

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
5:00 PM	0	78	4	0	82	2	0	4	0	6	3	133	0	1	137	0	0	0	0	0	225
5:15 PM	0	83	3	1	87	2	0	6	0	8	3	97	0	0	100	0	0	0	0	0	195
5:30 PM	0	110	2	0	112	3	0	3	0	6	4	92	0	0	96	0	0	0	0	0	214
5:45 PM	0	92	2	0	94	1	0	2	0	3	8	89	0	0	97	0	0	0	0	0	194
Total Volume	0	363	11	1	375	8	0	15	0	23	18	411	0	1	430	0	0	0	0	0	828
% Approach Total	0.0	96.8	2.9	0.3		34.8	0.0	65.2	0.0		4.2	95.6	0.0	0.2		0.0	0.0	0.0	0.0		
PHF	0.000	0.825	0.688	0.250	0.837	0.667	0.000	0.625	0.000	0.719	0.563	0.773	0.000	0.250	0.785	0.000	0.000	0.000	0.000	0.000	0.920
Entering Leg	0	363	11	1	375	8	0	15	0	23	18	411	0	1	430	0	0	0	0	0	828
Exiting Leg	420					29					379					0					828
Total	795					52					809					0					1656

PDI File #: **196718 C**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Vaughan Street W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Light Goods Vehicle

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	0	13	0	0	13	0	0	0	0	0	1	12	0	0	13	0	0	0	0	0	0	0	0	0	0	26
4:15 PM	0	13	0	0	13	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	0	0	0	0	24
4:30 PM	0	13	1	0	14	0	0	0	0	0	1	16	0	0	17	0	0	0	0	0	0	0	0	0	0	31
4:45 PM	0	5	0	0	5	0	0	1	0	1	0	8	0	0	8	0	0	0	0	0	0	0	0	0	0	14
Total	0	44	1	0	45	0	0	1	0	1	2	47	0	0	49	0	0	0	0	0	0	0	0	0	0	95
5:00 PM	0	7	0	0	7	1	0	1	0	2	0	9	0	0	9	0	0	0	0	0	0	0	0	0	0	18
5:15 PM	0	7	0	0	7	0	0	1	0	1	0	7	0	0	7	0	0	0	0	0	0	0	0	0	0	15
5:30 PM	0	9	0	0	9	1	0	0	0	1	0	12	0	0	12	0	0	0	0	0	0	0	0	0	0	22
5:45 PM	0	7	1	0	8	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	0	0	0	0	13
Total	0	30	1	0	31	2	0	2	0	4	0	33	0	0	33	0	0	0	0	0	0	0	0	0	0	68
Grand Total	0	74	2	0	76	2	0	3	0	5	2	80	0	0	82	0	0	0	0	0	0	0	0	0	0	163
Approach %	0.0	97.4	2.6	0.0		40.0	0.0	60.0	0.0		2.4	97.6	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	45.4	1.2	0.0	46.6	1.2	0.0	1.8	0.0	3.1	1.2	49.1	0.0	0.0	50.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	82					4					77					0					163					

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	0	13	0	0	13	0	0	0	0	0	1	12	0	0	13	0	0	0	0	0	0	0	0	0	0	26
4:15 PM	0	13	0	0	13	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	0	0	0	0	24
4:30 PM	0	13	1	0	14	0	0	0	0	0	1	16	0	0	17	0	0	0	0	0	0	0	0	0	0	31
4:45 PM	0	5	0	0	5	0	0	1	0	1	0	8	0	0	8	0	0	0	0	0	0	0	0	0	0	14
Total Volume	0	44	1	0	45	0	0	1	0	1	2	47	0	0	49	0	0	0	0	0	0	0	0	0	0	95
% Approach Total	0.0	97.8	2.2	0.0		0.0	0.0	100.0	0.0		4.1	95.9	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.846	0.250	0.000	0.804	0.000	0.000	0.250	0.000	0.250	0.500	0.734	0.000	0.000	0.721	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.766
Entering Leg	0	44	1	0	45	0	0	1	0	1	2	47	0	0	49	0	0	0	0	0	0	0	0	0	0	95
Exiting Leg	47					3					45					0					95					
Total	92					4					94					0					190					

PDI File #: **196718 C**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Vaughan Street W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Buses

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
4:15 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	3
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Grand Total	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	4
Approach %	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	2					0					2					0					4					

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
4:15 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	3
% Approach Total	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.375
Entering Leg	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	3
Exiting Leg	1					0					2					0					3					
Total	3					0					3					0					6					

PDI File #: **196718 C**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Vaughan Street W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Single-Unit Trucks

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
4:15 PM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Total	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	6
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
Total	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
Grand Total	0	2	0	0	2	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	7
Approach %	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	28.6	0.0	0.0	28.6	0.0	0.0	0.0	0.0	0.0	0.0	71.4	0.0	0.0	71.4	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total						5					0					2					7

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
4:15 PM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	2
Total Volume	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	6
% Approach Total	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.750
Entering Leg	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	6
Exiting Leg						4					0					2					6
Total						6					0					6					12

PDI File #: **196718 C**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Vaughan Street W: Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Articulated Trucks

	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Maplewood Avenue					Vaughan Street					Maplewood Avenue					Driveway					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0					0					0					0					0
Total	0					0					0					0					0

PDI File #: 196718 C
 Location: N: Maplewood Avenue S: Maplewood Avenue
 Location: E: Vaughan Street W: Driveway
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Bicycles (on Roadway and Crosswalks)

	Maplewood Avenue							Vaughan Street							Maplewood Avenue							Driveway							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:45 PM	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Grand Total	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Approach %	0.0	100.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0		66.7	33.3	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Total %	0.0	25.0	0.0	0.0	0.0	0.0	25.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		50.0	25.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	1							2							1							0							4

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue							Vaughan Street							Maplewood Avenue							Driveway							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:45 PM	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total Volume	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
% Approach Total	0.0	100.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0		66.7	33.3	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0	
PHF	0.000	0.250	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.250	0.000	0.000	0.000	0.000	0.375	0.000	0.000	0.000	0.000	0.000	0.000	0.333	
Entering Leg	0	1	0	0	0	0	1	0	0	0	0	0	0	0	2	1	0	0	0	0	0	3	0	0	0	0	0	0	
Exiting Leg	1							2							1							0							4
Total	2							2							4							0							8

PDI File #: 196718 C
 Location: N: Maplewood Avenue S: Maplewood Avenue
 Location: E: Vaughan Street W: Driveway
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Pedestrians

	Maplewood Avenue								Vaughan Street								Maplewood Avenue								Driveway								Total
	from North								from East								from South								from West								
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total		Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total		Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total		Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	3	0	3		0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	4	
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	1	2		0	0	0	0	0	0	0	0	0	0	0	0	2	0	2		4	
4:30 PM	0	0	0	0	0	1	1	0	0	0	0	0	4	4		0	0	0	0	0	0	0	0	0	0	0	0	1	0	1		6	
4:45 PM	0	0	0	0	0	1	1	0	0	0	0	0	5	5		0	0	0	0	1	0	1		0	0	0	0	3	0	3		10	
Total	0	0	0	0	0	2	2	0	0	0	0	4	10	14		0	0	0	0	1	0	1		0	0	0	0	6	1	7		24	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	2		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		2	
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	2		0	0	0	0	0	0	0	0	0	0	0	0	1	0	1		3	
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	1	2	3		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		3	
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0		0	0	0	0	0	0	0	0	0	0	0	0	0	2	2		2	
Total	0	0	0	0	0	0	0	0	0	0	0	1	6	7		0	0	0	0	0	0	0	0	0	0	0	0	1	2	3		10	
Grand Total	0	0	0	0	0	2	2	0	0	0	0	5	16	21		0	0	0	0	1	0	1		0	0	0	0	7	3	10		34	
Approach %	0	0	0	0	0	100		0	0	0	0	23.8	76.2		0	0	0	0	100	0		0	0	0	0	70	30						
Total %	0	0	0	0	0	5.88	5.88	0	0	0	0	14.7	47.1	61.8		0	0	0	0	2.94	0	2.94		0	0	0	0	20.6	8.82	29.4			
Exiting Leg Total	2							21							1							10							34				

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue								Vaughan Street								Maplewood Avenue								Driveway								Total
	from North								from East								from South								from West								
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total		Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total		Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total		Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	3	0	3		0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	4	
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	1	2		0	0	0	0	0	0	0	0	0	0	0	0	2	0	2		4	
4:30 PM	0	0	0	0	0	1	1	0	0	0	0	0	4	4		0	0	0	0	0	0	0	0	0	0	0	0	1	0	1		6	
4:45 PM	0	0	0	0	0	1	1	0	0	0	0	0	5	5		0	0	0	0	1	0	1		0	0	0	0	3	0	3		10	
Total Volume	0	0	0	0	0	2	2	0	0	0	0	4	10	14		0	0	0	0	1	0	1		0	0	0	0	6	1	7		24	
% Approach Total	0.0	0.0	0.0	0.0	0.0	100.0		0.0	0.0	0.0	0.0	28.6	71.4		0.0	0.0	0.0	0.0	100.0	0.0		0.0	0.0	0.0	0.0	85.7	14.3						
PHF	0.000	0.000	0.000	0.000	0.000	0.500	0.500	0.000	0.000	0.000	0.000	0.333	0.500	0.700		0.000	0.000	0.000	0.000	0.250	0.000	0.250		0.000	0.000	0.000	0.000	0.500	0.250	0.583	0.600		
Entering Leg	0	0	0	0	0	2	2	0	0	0	0	4	10	14		0	0	0	0	1	0	1		0	0	0	0	6	1	7		24	
Exiting Leg	2							14							1							7							24				
Total	4							28							2							14							48				

PDI File #: **196718 D**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars and Heavy Vehicles (Combined)

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	13	57	11	0	81	10	9	29	0	48	43	66	1	0	110	1	18	12	0	31	270
4:15 PM	14	57	12	0	83	11	13	25	0	49	39	78	3	0	120	2	14	17	0	33	285
4:30 PM	13	57	7	0	77	8	18	24	0	50	50	81	4	0	135	3	7	17	0	27	289
4:45 PM	11	70	12	0	93	8	12	43	0	63	31	76	3	0	110	3	14	16	0	33	299
Total	51	241	42	0	334	37	52	121	0	210	163	301	11	0	475	9	53	62	0	124	1143
5:00 PM	10	71	7	0	88	13	27	37	0	77	45	99	2	0	146	1	21	36	0	58	369
5:15 PM	11	77	8	0	96	15	14	34	0	63	39	79	1	0	119	1	21	12	0	34	312
5:30 PM	10	95	19	0	124	13	22	63	0	98	37	82	2	0	121	0	23	13	0	36	379
5:45 PM	9	81	10	0	100	8	18	35	0	61	41	83	0	0	124	4	8	12	0	24	309
Total	40	324	44	0	408	49	81	169	0	299	162	343	5	0	510	6	73	73	0	152	1369
Grand Total	91	565	86	0	742	86	133	290	0	509	325	644	16	0	985	15	126	135	0	276	2512
Approach %	12.3	76.1	11.6	0.0		16.9	26.1	57.0	0.0		33.0	65.4	1.6	0.0		5.4	45.7	48.9	0.0		
Total %	3.6	22.5	3.4	0.0	29.5	3.4	5.3	11.5	0.0	20.3	12.9	25.6	0.6	0.0	39.2	0.6	5.0	5.4	0.0	11.0	
Exiting Leg Total	865					537					870					240					2512
Cars	90	562	86	0	738	86	133	284	0	503	318	638	14	0	970	15	125	134	0	274	2485
% Cars	98.9	99.5	100.0	0.0	99.5	100.0	100.0	97.9	0.0	98.8	97.8	99.1	87.5	0.0	98.5	100.0	99.2	99.3	0.0	99.3	98.9
Exiting Leg Total	858					529					861					237					2485
Heavy Vehicles	1	3	0	0	4	0	0	6	0	6	7	6	2	0	15	0	1	1	0	2	27
% Heavy Vehicles	1.1	0.5	0.0	0.0	0.5	0.0	0.0	2.1	0.0	1.2	2.2	0.9	12.5	0.0	1.5	0.0	0.8	0.7	0.0	0.7	1.1
Exiting Leg Total	7					8					9					3					27

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
5:00 PM	10	71	7	0	88	13	27	37	0	77	45	99	2	0	146	1	21	36	0	58	369
5:15 PM	11	77	8	0	96	15	14	34	0	63	39	79	1	0	119	1	21	12	0	34	312
5:30 PM	10	95	19	0	124	13	22	63	0	98	37	82	2	0	121	0	23	13	0	36	379
5:45 PM	9	81	10	0	100	8	18	35	0	61	41	83	0	0	124	4	8	12	0	24	309
Total Volume	40	324	44	0	408	49	81	169	0	299	162	343	5	0	510	6	73	73	0	152	1369
% Approach Total	9.8	79.4	10.8	0.0		16.4	27.1	56.5	0.0		31.8	67.3	1.0	0.0		3.9	48.0	48.0	0.0		
PHF	0.909	0.853	0.579	0.000	0.823	0.817	0.750	0.671	0.000	0.763	0.900	0.866	0.625	0.000	0.873	0.375	0.793	0.507	0.000	0.655	0.903
Cars	40	324	44	0	408	49	81	166	0	296	158	341	5	0	504	6	73	73	0	152	1360
Cars %	100.0	100.0	100.0	0.0	100.0	100.0	100.0	98.2	0.0	99.0	97.5	99.4	100.0	0.0	98.8	100.0	100.0	100.0	0.0	100.0	99.3
Heavy Vehicles	0	0	0	0	0	0	0	3	0	3	4	2	0	0	6	0	0	0	0	0	0
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1.8	0.0	1.0	2.5	0.6	0.0	0.0	1.2	0.0	0.0	0.0	0.0	0.0	0.7
Cars Enter Leg	40	324	44	0	408	49	81	166	0	296	158	341	5	0	504	6	73	73	0	152	1360
Heavy Enter Leg	0	0	0	0	0	0	0	3	0	3	4	2	0	0	6	0	0	0	0	0	9
Total Entering Leg	40	324	44	0	408	49	81	169	0	299	162	343	5	0	510	6	73	73	0	152	1369
Cars Exiting Leg	463					275					496					126					1360
Heavy Exiting Leg	2					4					3					0					9
Total Exiting Leg	465					279					499					126					1369

PDI File #: **196718 D**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars-Combined (Motorcycles, Cars, Light Goods)

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	12	56	11	0	79	10	9	28	0	47	42	64	1	0	107	1	18	12	0	31	264
4:15 PM	14	55	12	0	81	11	13	25	0	49	38	78	2	0	118	2	14	16	0	32	280
4:30 PM	13	57	7	0	77	8	18	23	0	49	50	81	4	0	135	3	7	17	0	27	288
4:45 PM	11	70	12	0	93	8	12	42	0	62	30	74	2	0	106	3	13	16	0	32	293
Total	50	238	42	0	330	37	52	118	0	207	160	297	9	0	466	9	52	61	0	122	1125
5:00 PM	10	71	7	0	88	13	27	36	0	76	44	98	2	0	144	1	21	36	0	58	366
5:15 PM	11	77	8	0	96	15	14	34	0	63	38	79	1	0	118	1	21	12	0	34	311
5:30 PM	10	95	19	0	124	13	22	61	0	96	37	82	2	0	121	0	23	13	0	36	377
5:45 PM	9	81	10	0	100	8	18	35	0	61	39	82	0	0	121	4	8	12	0	24	306
Total	40	324	44	0	408	49	81	166	0	296	158	341	5	0	504	6	73	73	0	152	1360
Grand Total	90	562	86	0	738	86	133	284	0	503	318	638	14	0	970	15	125	134	0	274	2485
Approach %	12.2	76.2	11.7	0.0		17.1	26.4	56.5	0.0		32.8	65.8	1.4	0.0		5.5	45.6	48.9	0.0		
Total %	3.6	22.6	3.5	0.0	29.7	3.5	5.4	11.4	0.0	20.2	12.8	25.7	0.6	0.0	39.0	0.6	5.0	5.4	0.0	11.0	
Exiting Leg Total	858					529					861					237					2485

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
5:00 PM	10	71	7	0	88	13	27	36	0	76	44	98	2	0	144	1	21	36	0	58	366
5:15 PM	11	77	8	0	96	15	14	34	0	63	38	79	1	0	118	1	21	12	0	34	311
5:30 PM	10	95	19	0	124	13	22	61	0	96	37	82	2	0	121	0	23	13	0	36	377
5:45 PM	9	81	10	0	100	8	18	35	0	61	39	82	0	0	121	4	8	12	0	24	306
Total Volume	40	324	44	0	408	49	81	166	0	296	158	341	5	0	504	6	73	73	0	152	1360
% Approach Total	9.8	79.4	10.8	0.0		16.6	27.4	56.1	0.0		31.3	67.7	1.0	0.0		3.9	48.0	48.0	0.0		
PHF	0.909	0.853	0.579	0.000	0.823	0.817	0.750	0.680	0.000	0.771	0.898	0.870	0.625	0.000	0.875	0.375	0.793	0.507	0.000	0.655	0.902
Entering Leg	40	324	44	0	408	49	81	166	0	296	158	341	5	0	504	6	73	73	0	152	1360
Exiting Leg	463					275					496					126					1360
Total	871					571					1000					278					2720

PDI File #: **196718 D**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class: **Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	1	1	0	0	2	0	0	1	0	1	1	2	0	0	3	0	0	0	0	0	6
4:15 PM	0	2	0	0	2	0	0	0	0	0	1	0	1	0	2	0	0	1	0	1	5
4:30 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1
4:45 PM	0	0	0	0	0	0	0	1	0	1	1	2	1	0	4	0	1	0	0	1	6
Total	1	3	0	0	4	0	0	3	0	3	3	4	2	0	9	0	1	1	0	2	18
5:00 PM	0	0	0	0	0	0	0	1	0	1	1	1	0	0	2	0	0	0	0	0	3
5:15 PM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	1
5:30 PM	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	2
5:45 PM	0	0	0	0	0	0	0	0	0	0	2	1	0	0	3	0	0	0	0	0	3
Total	0	0	0	0	0	0	0	3	0	3	4	2	0	0	6	0	0	0	0	0	9
Grand Total	1	3	0	0	4	0	0	6	0	6	7	6	2	0	15	0	1	1	0	2	27
Approach %	25.0	75.0	0.0	0.0		0.0	0.0	100.0	0.0		46.7	40.0	13.3	0.0		0.0	50.0	50.0	0.0		
Total %	3.7	11.1	0.0	0.0	14.8	0.0	0.0	22.2	0.0	22.2	25.9	22.2	7.4	0.0	55.6	0.0	3.7	3.7	0.0	7.4	
Exiting Leg Total	7					8					9					3					27
Buses	0	2	0	0	2	0	0	6	0	6	7	2	0	0	9	0	0	0	0	0	17
% Buses	0.0	66.7	0.0	0.0	50.0	0.0	0.0	100.0	0.0	100.0	100.0	33.3	0.0	0.0	60.0	0.0	0.0	0.0	0.0	0.0	63.0
Exiting Leg Total	2					7					8					0					17
Single-Unit Trucks	1	1	0	0	2	0	0	0	0	0	0	4	2	0	6	0	1	1	0	2	10
% Single-Unit	100.0	33.3	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	66.7	100.0	0.0	40.0	0.0	100.0	100.0	0.0	100.0	37.0
Exiting Leg Total	5					1					1					3					10
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	1	1	0	0	2	0	0	1	0	1	1	2	0	0	3	0	0	0	0	0	6
4:15 PM	0	2	0	0	2	0	0	0	0	0	1	0	1	0	2	0	0	1	0	1	5
4:30 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1
4:45 PM	0	0	0	0	0	0	0	1	0	1	1	2	1	0	4	0	1	0	0	1	6
Total Volume	1	3	0	0	4	0	0	3	0	3	3	4	2	0	9	0	1	1	0	2	18
% Approach Total	25.0	75.0	0.0	0.0		0.0	0.0	100.0	0.0		33.3	44.4	22.2	0.0		0.0	50.0	50.0	0.0		
PHF	0.250	0.375	0.000	0.000	0.500	0.000	0.000	0.750	0.000	0.750	0.750	0.500	0.500	0.000	0.563	0.000	0.250	0.250	0.000	0.500	0.750
Buses	0	2	0	0	2	0	0	3	0	3	3	1	0	0	4	0	0	0	0	0	9
Buses %	0.0	66.7	0.0	0.0	50.0	0.0	0.0	100.0	0.0	100.0	100.0	25.0	0.0	0.0	44.4	0.0	0.0	0.0	0.0	0.0	50.0
Single-Unit Trucks	1	1	0	0	2	0	0	0	0	0	0	3	2	0	5	0	1	1	0	2	9
Single-Unit %	100.0	33.3	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	75.0	100.0	0.0	55.6	0.0	100.0	100.0	0.0	100.0	50.0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Buses	0	2	0	0	2	0	0	3	0	3	3	1	0	0	4	0	0	0	0	0	9
Single-Unit Trucks	1	1	0	0	2	0	0	0	0	0	0	3	2	0	5	0	1	1	0	2	9
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	1	3	0	0	4	0	0	3	0	3	3	4	2	0	9	0	1	1	0	2	18
Buses	1					3					5					0					9
Single-Unit Trucks	4					1					3					3					9
Articulated Trucks	0					0					0					0					0
Total Exiting Leg	5					4					6					3					18

PDI File #: **196718 D**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	11	52	8	0	71	7	7	25	0	39	39	54	1	0	94	1	15	12	0	28	232					
4:15 PM	11	47	9	0	67	10	11	22	0	43	33	67	2	0	102	2	11	16	0	29	241					
4:30 PM	12	44	7	0	63	6	17	23	0	46	45	68	4	0	117	2	7	15	0	24	250					
4:45 PM	11	66	10	0	87	6	11	36	0	53	26	69	2	0	97	3	11	15	0	29	266					
Total	45	209	34	0	288	29	46	106	0	181	143	258	9	0	410	8	44	58	0	110	989					
5:00 PM	9	64	7	0	80	12	24	30	0	66	40	92	2	0	134	1	19	35	0	55	335					
5:15 PM	10	70	7	0	87	15	11	31	0	57	34	75	1	0	110	1	19	9	0	29	283					
5:30 PM	9	89	18	0	116	12	19	57	0	88	36	75	2	0	113	0	20	9	0	29	346					
5:45 PM	8	75	10	0	93	7	17	30	0	54	38	77	0	0	115	3	6	12	0	21	283					
Total	36	298	42	0	376	46	71	148	0	265	148	319	5	0	472	5	64	65	0	134	1247					
Grand Total	81	507	76	0	664	75	117	254	0	446	291	577	14	0	882	13	108	123	0	244	2236					
Approach %	12.2	76.4	11.4	0.0		16.8	26.2	57.0	0.0		33.0	65.4	1.6	0.0		5.3	44.3	50.4	0.0							
Total %	3.6	22.7	3.4	0.0	29.7	3.4	5.2	11.4	0.0	19.9	13.0	25.8	0.6	0.0	39.4	0.6	4.8	5.5	0.0	10.9						
Exiting Leg Total						775					475					774					212					2236

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
5:00 PM	9	64	7	0	80	12	24	30	0	66	40	92	2	0	134	1	19	35	0	55	335					
5:15 PM	10	70	7	0	87	15	11	31	0	57	34	75	1	0	110	1	19	9	0	29	283					
5:30 PM	9	89	18	0	116	12	19	57	0	88	36	75	2	0	113	0	20	9	0	29	346					
5:45 PM	8	75	10	0	93	7	17	30	0	54	38	77	0	0	115	3	6	12	0	21	283					
Total Volume	36	298	42	0	376	46	71	148	0	265	148	319	5	0	472	5	64	65	0	134	1247					
% Approach Total	9.6	79.3	11.2	0.0		17.4	26.8	55.8	0.0		31.4	67.6	1.1	0.0		3.7	47.8	48.5	0.0							
PHF	0.900	0.837	0.583	0.000	0.810	0.767	0.740	0.649	0.000	0.753	0.925	0.867	0.625	0.000	0.881	0.417	0.800	0.464	0.000	0.609	0.901					
Entering Leg	36	298	42	0	376	46	71	148	0	265	148	319	5	0	472	5	64	65	0	134	1247					
Exiting Leg						430					254					451					112	1247				
Total						806					519					923					246					2494

PDI File #: **196718 D**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Light Goods Vehicle

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	1	4	3	0	8	3	2	3	0	8	3	10	0	0	13	0	3	0	0	3	32					
4:15 PM	3	8	3	0	14	1	2	3	0	6	5	11	0	0	16	0	3	0	0	3	39					
4:30 PM	1	13	0	0	14	2	1	0	0	3	5	13	0	0	18	1	0	2	0	3	38					
4:45 PM	0	4	2	0	6	2	1	6	0	9	4	5	0	0	9	0	2	1	0	3	27					
Total	5	29	8	0	42	8	6	12	0	26	17	39	0	0	56	1	8	3	0	12	136					
5:00 PM	1	7	0	0	8	1	3	6	0	10	4	6	0	0	10	0	2	1	0	3	31					
5:15 PM	1	7	1	0	9	0	3	3	0	6	4	4	0	0	8	0	2	3	0	5	28					
5:30 PM	1	6	1	0	8	1	3	4	0	8	1	7	0	0	8	0	3	4	0	7	31					
5:45 PM	1	6	0	0	7	1	1	5	0	7	1	5	0	0	6	1	2	0	0	3	23					
Total	4	26	2	0	32	3	10	18	0	31	10	22	0	0	32	1	9	8	0	18	113					
Grand Total	9	55	10	0	74	11	16	30	0	57	27	61	0	0	88	2	17	11	0	30	249					
Approach %	12.2	74.3	13.5	0.0		19.3	28.1	52.6	0.0		30.7	69.3	0.0	0.0		6.7	56.7	36.7	0.0							
Total %	3.6	22.1	4.0	0.0	29.7	4.4	6.4	12.0	0.0	22.9	10.8	24.5	0.0	0.0	35.3	0.8	6.8	4.4	0.0	12.0						
Exiting Leg Total						83					54					87					25					249

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	1	4	3	0	8	3	2	3	0	8	3	10	0	0	13	0	3	0	0	3	32					
4:15 PM	3	8	3	0	14	1	2	3	0	6	5	11	0	0	16	0	3	0	0	3	39					
4:30 PM	1	13	0	0	14	2	1	0	0	3	5	13	0	0	18	1	0	2	0	3	38					
4:45 PM	0	4	2	0	6	2	1	6	0	9	4	5	0	0	9	0	2	1	0	3	27					
Total Volume	5	29	8	0	42	8	6	12	0	26	17	39	0	0	56	1	8	3	0	12	136					
% Approach Total	11.9	69.0	19.0	0.0		30.8	23.1	46.2	0.0		30.4	69.6	0.0	0.0		8.3	66.7	25.0	0.0							
PHF	0.417	0.558	0.667	0.000	0.750	0.667	0.750	0.500	0.000	0.722	0.850	0.750	0.000	0.000	0.778	0.250	0.667	0.375	0.000	1.000	0.872					
Entering Leg	5	29	8	0	42	8	6	12	0	26	17	39	0	0	56	1	8	3	0	12	136					
Exiting Leg						50					42					11					136					
Total						92					59					98					23					272

PDI File #: **196718 D**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Buses

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	1	0	0	1	0	0	1	0	1	1	1	0	0	2	0	0	0	0	0	4
4:15 PM	0	1	0	0	1	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	2
4:30 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1
4:45 PM	0	0	0	0	0	0	0	1	0	1	1	0	0	0	1	0	0	0	0	0	2
Total	0	2	0	0	2	0	0	3	0	3	3	1	0	0	4	0	0	0	0	0	9
5:00 PM	0	0	0	0	0	0	0	1	0	1	1	1	0	0	2	0	0	0	0	0	3
5:15 PM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	1
5:30 PM	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	2
5:45 PM	0	0	0	0	0	0	0	0	0	0	2	0	0	0	2	0	0	0	0	0	2
Total	0	0	0	0	0	0	0	3	0	3	4	1	0	0	5	0	0	0	0	0	8
Grand Total	0	2	0	0	2	0	0	6	0	6	7	2	0	0	9	0	0	0	0	0	17
Approach %	0.0	100.0	0.0	0.0		0.0	0.0	100.0	0.0		77.8	22.2	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	11.8	0.0	0.0	11.8	0.0	0.0	35.3	0.0	35.3	41.2	11.8	0.0	0.0	52.9	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	2					7					8					0					17

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	1	0	0	1	0	0	1	0	1	1	1	0	0	2	0	0	0	0	0	4
4:15 PM	0	1	0	0	1	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	2
4:30 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1
4:45 PM	0	0	0	0	0	0	0	1	0	1	1	0	0	0	1	0	0	0	0	0	2
Total Volume	0	2	0	0	2	0	0	3	0	3	3	1	0	0	4	0	0	0	0	0	9
% Approach Total	0.0	100.0	0.0	0.0		0.0	0.0	100.0	0.0		75.0	25.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.500	0.000	0.000	0.500	0.000	0.000	0.750	0.000	0.750	0.750	0.250	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.563
Entering Leg	0	2	0	0	2	0	0	3	0	3	3	1	0	0	4	0	0	0	0	0	9
Exiting Leg	1					3					5					0					9
Total	3					6					9					0					18

PDI File #: **196718 D**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Single-Unit Trucks

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	1	0	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
4:15 PM	0	1	0	0	1	0	0	0	0	0	0	0	1	0	1	0	0	1	0	1	0	0	1	0	1	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	1	0	3	0	1	0	0	1	0	1	0	0	1	4
Total	1	1	0	0	2	0	0	0	0	0	0	3	2	0	5	0	1	1	0	2	0	1	1	0	2	9
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Grand Total	1	1	0	0	2	0	0	0	0	0	0	4	2	0	6	0	1	1	0	2	0	1	1	0	2	10
Approach %	50.0	50.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	66.7	33.3	0.0		0.0	50.0	50.0	0.0							
Total %	10.0	10.0	0.0	0.0	20.0	0.0	0.0	0.0	0.0	0.0	0.0	40.0	20.0	0.0	60.0	0.0	10.0	10.0	0.0	20.0						
Exiting Leg Total						5					1					1					3		10			

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	1	0	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
4:15 PM	0	1	0	0	1	0	0	0	0	0	0	0	1	0	1	0	0	1	0	1	0	0	1	0	1	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	2	1	0	3	0	1	0	0	1	0	1	0	0	1	4
Total Volume	1	1	0	0	2	0	0	0	0	0	0	3	2	0	5	0	1	1	0	2	0	1	1	0	2	9
% Approach Total	50.0	50.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	60.0	40.0	0.0		0.0	50.0	50.0	0.0							
PHF	0.250	0.250	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.375	0.500	0.000	0.417	0.000	0.250	0.250	0.000	0.500						0.563
Entering Leg	1	1	0	0	2	0	0	0	0	0	0	3	2	0	5	0	1	1	0	2						9
Exiting Leg						4					1					1					3		9			
Total						6					1					6					5		18			

PDI File #: **196718 D**
 Location: **N: Maplewood Avenue S: Maplewood Avenue**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Articulated Trucks

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total				
	from North					from East					from South					from West									
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total					
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0				

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue					Deer Street					Maplewood Avenue					Deer Street					Total				
	from North					from East					from South					from West									
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total					
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0					0					0					0					0				
Total	0					0					0					0					0				

PDI File #: 196718 D
 Location: N: Maplewood Avenue S: Maplewood Avenue
 Location: E: Deer Street W: Deer Street
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Pedestrians

	Maplewood Avenue								Deer Street								Maplewood Avenue								Deer Street								Total
	from North								from East								from South								from West								
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total		Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total		Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total		Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	0	0	1	1	0	0	0	0	1	0	1	0	0	0	0	1	1	2	0	0	0	0	1	1	2	6				
4:15 PM	0	0	0	0	0	2	2	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4				
4:30 PM	0	0	0	0	0	1	1	0	0	0	0	0	5	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6				
4:45 PM	0	0	0	0	1	0	1	0	0	0	0	0	5	5	0	0	0	0	1	2	3	0	0	0	0	2	0	2	11				
Total	0	0	0	0	1	4	5	0	0	0	0	1	12	13	0	0	0	0	2	3	5	0	0	0	0	3	1	4	27				
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1				
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	1	0	1	0	0	0	0	1	0	1	3				
5:30 PM	0	0	0	0	1	0	1	0	0	0	0	1	1	2	0	0	0	0	2	1	3	0	0	0	0	0	0	0	6				
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	0	0	0	0	0	2	2	4				
Total	0	0	0	0	1	0	1	0	0	0	0	1	3	4	0	0	0	0	3	3	6	0	0	0	0	1	2	3	14				
Grand Total	0	0	0	0	2	4	6	0	0	0	0	2	15	17	0	0	0	0	5	6	11	0	0	0	0	4	3	7	41				
Approach %	0	0	0	0	33.3	66.7		0	0	0	0	11.8	88.2		0	0	0	0	45.5	54.5		0	0	0	0	57.1	42.9						
Total %	0	0	0	0	4.88	9.76	14.6	0	0	0	0	4.88	36.6	41.5	0	0	0	0	12.2	14.6	26.8	0	0	0	0	9.76	7.32	17.1					
Exiting Leg Total	6							17							11							7							41				

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maplewood Avenue								Deer Street								Maplewood Avenue								Deer Street								Total
	from North								from East								from South								from West								
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total		Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total		Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total		Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	0	0	1	1	0	0	0	0	1	0	1	0	0	0	0	1	1	2	0	0	0	0	1	1	2	6				
4:15 PM	0	0	0	0	0	2	2	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4				
4:30 PM	0	0	0	0	0	1	1	0	0	0	0	0	5	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6				
4:45 PM	0	0	0	0	1	0	1	0	0	0	0	0	5	5	0	0	0	0	1	2	3	0	0	0	0	2	0	2	11				
Total Volume	0	0	0	0	1	4	5	0	0	0	0	1	12	13	0	0	0	0	2	3	5	0	0	0	0	3	1	4	27				
% Approach Total	0.0	0.0	0.0	0.0	20.0	80.0		0.0	0.0	0.0	0.0	7.7	92.3		0.0	0.0	0.0	0.0	40.0	60.0		0.0	0.0	0.0	0.0	75.0	25.0						
PHF	0.000	0.000	0.000	0.000	0.250	0.500	0.625	0.000	0.000	0.000	0.000	0.250	0.600	0.650	0.000	0.000	0.000	0.000	0.500	0.375	0.417	0.000	0.000	0.000	0.000	0.375	0.250	0.500	0.614				
Entering Leg	0	0	0	0	1	4	5	0	0	0	0	1	12	13	0	0	0	0	2	3	5	0	0	0	0	3	1	4	27				
Exiting Leg	5							13							5							4							27				
Total	10							26							10							8							54				

PDI File #: **196718 E**
 Location: **N: Kennebunk Savings Bank Driveway**
 Location: **E: Vaughan Street W: Vaughan Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Cars and Heavy Vehicles (Combined)

	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	2	2	0	4	0	4	0	4	7	0	0	7	15
4:15 PM	1	0	0	1	1	2	0	3	6	2	0	8	12
4:30 PM	0	0	0	0	1	7	0	8	8	0	0	8	16
4:45 PM	2	0	0	2	0	2	1	3	4	0	0	4	9
Total	5	2	0	7	2	15	1	18	25	2	0	27	52
5:00 PM	1	0	0	1	1	7	0	8	7	0	0	7	16
5:15 PM	0	0	0	0	0	9	0	9	5	1	0	6	15
5:30 PM	2	1	0	3	0	5	0	5	5	1	0	6	14
5:45 PM	1	1	0	2	0	2	0	2	9	2	0	11	15
Total	4	2	0	6	1	23	0	24	26	4	0	30	60
Grand Total	9	4	0	13	3	38	1	42	51	6	0	57	112
Approach %	69.2	30.8	0.0		7.1	90.5	2.4		89.5	10.5	0.0		
Total %	8.0	3.6	0.0	11.6	2.7	33.9	0.9	37.5	45.5	5.4	0.0	50.9	
Exiting Leg Total				9				56				47	112
Cars	9	4	0	13	3	38	1	42	51	6	0	57	112
% Cars	100.0	100.0	0.0	100.0	100.0	100.0	100.0	100.0	100.0	100.0	0.0	100.0	100.0
Exiting Leg Total				9				56				47	112
Heavy Vehicles	0	0	0	0	0	0	0	0	0	0	0	0	0
% Heavy Vehicles	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total				0				0				0	0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
5:00 PM	1	0	0	1	1	7	0	8	7	0	0	7	16
5:15 PM	0	0	0	0	0	9	0	9	5	1	0	6	15
5:30 PM	2	1	0	3	0	5	0	5	5	1	0	6	14
5:45 PM	1	1	0	2	0	2	0	2	9	2	0	11	15
Total Volume	4	2	0	6	1	23	0	24	26	4	0	30	60
% Approach Total	66.7	33.3	0.0		4.2	95.8	0.0		86.7	13.3	0.0		
PHF	0.500	0.500	0.000	0.500	0.250	0.639	0.000	0.667	0.722	0.500	0.000	0.682	0.938
Cars	4	2	0	6	1	23	0	24	26	4	0	30	60
Cars %	100.0	100.0	0.0	100.0	100.0	100.0	0.0	100.0	100.0	100.0	0.0	100.0	100.0
Heavy Vehicles	0	0	0	0	0	0	0	0	0	0	0	0	0
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Cars Enter Leg	4	2	0	6	1	23	0	24	26	4	0	30	60
Heavy Enter Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	4	2	0	6	1	23	0	24	26	4	0	30	60
Cars Exiting Leg				5				28				27	60
Heavy Exiting Leg				0				0				0	0
Total Exiting Leg				5				28				27	60

PDI File #: **196718 E**
 Location: **N: Kennebunk Savings Bank Driveway**
 Location: **E: Vaughan Street W: Vaughan Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Class: **Cars-Combined (Motorcycles, Cars, Light Goods)**

	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	2	2	0	4	0	4	0	4	7	0	0	7	15
4:15 PM	1	0	0	1	1	2	0	3	6	2	0	8	12
4:30 PM	0	0	0	0	1	7	0	8	8	0	0	8	16
4:45 PM	2	0	0	2	0	2	1	3	4	0	0	4	9
Total	5	2	0	7	2	15	1	18	25	2	0	27	52
5:00 PM	1	0	0	1	1	7	0	8	7	0	0	7	16
5:15 PM	0	0	0	0	0	9	0	9	5	1	0	6	15
5:30 PM	2	1	0	3	0	5	0	5	5	1	0	6	14
5:45 PM	1	1	0	2	0	2	0	2	9	2	0	11	15
Total	4	2	0	6	1	23	0	24	26	4	0	30	60
Grand Total	9	4	0	13	3	38	1	42	51	6	0	57	112
Approach %	69.2	30.8	0.0		7.1	90.5	2.4		89.5	10.5	0.0		
Total %	8.0	3.6	0.0	11.6	2.7	33.9	0.9	37.5	45.5	5.4	0.0	50.9	
Exiting Leg Total				9				56				47	112

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
5:00 PM	1	0	0	1	1	7	0	8	7	0	0	7	16
5:15 PM	0	0	0	0	0	9	0	9	5	1	0	6	15
5:30 PM	2	1	0	3	0	5	0	5	5	1	0	6	14
5:45 PM	1	1	0	2	0	2	0	2	9	2	0	11	15
Total Volume	4	2	0	6	1	23	0	24	26	4	0	30	60
% Approach Total	66.7	33.3	0.0		4.2	95.8	0.0		86.7	13.3	0.0		
PHF	0.500	0.500	0.000	0.500	0.250	0.639	0.000	0.667	0.722	0.500	0.000	0.682	0.938
Entering Leg	4	2	0	6	1	23	0	24	26	4	0	30	60
Exiting Leg				5				28				27	60
Total				11				52				57	120

PDI File #: **196718 E**
 Location: **N: Kennebunk Savings Bank Driveway**
 Location: **E: Vaughan Street W: Vaughan Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Class: Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)

	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0
Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
% Buses	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
% Single-Unit	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
Buses %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Buses	0				0				0				0
Single-Unit Trucks	0				0				0				0
Articulated Trucks	0				0				0				0
Total Exiting Leg	0				0				0				0

PDI File #: **196718 E**
 Location: **N: Kennebunk Savings Bank Driveway**
 Location: **E: Vaughan Street W: Vaughan Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Cars

	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	2	2	0	4	0	4	0	4	6	0	0	6	14
4:15 PM	1	0	0	1	1	2	0	3	6	2	0	8	12
4:30 PM	0	0	0	0	1	7	0	8	6	0	0	6	14
4:45 PM	1	0	0	1	0	2	0	2	4	0	0	4	7
Total	4	2	0	6	2	15	0	17	22	2	0	24	47
5:00 PM	1	0	0	1	0	5	0	5	7	0	0	7	13
5:15 PM	0	0	0	0	0	8	0	8	5	1	0	6	14
5:30 PM	2	1	0	3	0	4	0	4	5	1	0	6	13
5:45 PM	1	1	0	2	0	2	0	2	9	2	0	11	15
Total	4	2	0	6	0	19	0	19	26	4	0	30	55
Grand Total	8	4	0	12	2	34	0	36	48	6	0	54	102
Approach %	66.7	33.3	0.0		5.6	94.4	0.0		88.9	11.1	0.0		
Total %	7.8	3.9	0.0	11.8	2.0	33.3	0.0	35.3	47.1	5.9	0.0	52.9	
Exiting Leg Total				8				52				42	102

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
5:00 PM	1	0	0	1	0	5	0	5	7	0	0	7	13
5:15 PM	0	0	0	0	0	8	0	8	5	1	0	6	14
5:30 PM	2	1	0	3	0	4	0	4	5	1	0	6	13
5:45 PM	1	1	0	2	0	2	0	2	9	2	0	11	15
Total Volume	4	2	0	6	0	19	0	19	26	4	0	30	55
% Approach Total	66.7	33.3	0.0		0.0	100.0	0.0		86.7	13.3	0.0		
PHF	0.500	0.500	0.000	0.500	0.000	0.594	0.000	0.594	0.722	0.500	0.000	0.682	0.917
Entering Leg	4	2	0	6	0	19	0	19	26	4	0	30	55
Exiting Leg				4				28				23	55
Total				10				47				53	110

PDI File #: **196718 E**
 Location: **N: Kennebunk Savings Bank Driveway**
 Location: **E: Vaughan Street W: Vaughan Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Class:

Light Goods Vehicle

	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	1	0	0	1	1
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	2	0	0	2	2
4:45 PM	1	0	0	1	0	0	1	1	0	0	0	0	2
Total	1	0	0	1	0	0	1	1	3	0	0	3	5
5:00 PM	0	0	0	0	1	2	0	3	0	0	0	0	3
5:15 PM	0	0	0	0	0	1	0	1	0	0	0	0	1
5:30 PM	0	0	0	0	0	1	0	1	0	0	0	0	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	1	4	0	5	0	0	0	0	5
Grand Total	1	0	0	1	1	4	1	6	3	0	0	3	10
Approach %	100.0	0.0	0.0		16.7	66.7	16.7		100.0	0.0	0.0		
Total %	10.0	0.0	0.0	10.0	10.0	40.0	10.0	60.0	30.0	0.0	0.0	30.0	
Exiting Leg Total				1				4				5	10

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:30 PM	0	0	0	0	0	0	0	0	2	0	0	2	2
4:45 PM	1	0	0	1	0	0	1	1	0	0	0	0	2
5:00 PM	0	0	0	0	1	2	0	3	0	0	0	0	3
5:15 PM	0	0	0	0	0	1	0	1	0	0	0	0	1
Total Volume	1	0	0	1	1	3	1	5	2	0	0	2	8
% Approach Total	100.0	0.0	0.0		20.0	60.0	20.0		100.0	0.0	0.0		
PHF	0.250	0.000	0.000	0.250	0.250	0.375	0.250	0.417	0.250	0.000	0.000	0.250	0.667
Entering Leg	1	0	0	1	1	3	1	5	2	0	0	2	8
Exiting Leg				1				3				4	8
Total				2				8				6	16

PDI File #: **196718 E**
 Location: **N: Kennebunk Savings Bank Driveway**
 Location: **E: Vaughan Street W: Vaughan Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Buses

	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total	
	from North				from East				from West					
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
Exiting Leg Total	0				0				0				0	

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total	
	from North				from East				from West					
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000		0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0				0				0				0	
Total	0				0				0				0	

PDI File #: **196718 E**
 Location: **N: Kennebunk Savings Bank Driveway**
 Location: **E: Vaughan Street W: Vaughan Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Single-Unit Trucks

	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total	
	from North				from East				from West					
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
Exiting Leg Total				0				0					0	0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total	
	from North				from East				from West					
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000		0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg				0				0					0	0
Total				0				0					0	0

PDI File #: **196718 E**
 Location: **N: Kennebunk Savings Bank Driveway**
 Location: **E: Vaughan Street W: Vaughan Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Articulated Trucks

	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total	
	from North				from East				from West					
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
Exiting Leg Total	0				0				0				0	

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Kennebunk Savings Bank Driveway				Vaughan Street				Vaughan Street				Total	
	from North				from East				from West					
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000		0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0				0				0				0	
Total	0				0				0				0	

PDI File #: 196718 E
 Location: N: Kennebunk Savings Bank Driveway
 Location: E: Vaughan Street W: Vaughan Street
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Bicycles (on Roadway and Crosswalks)

	Kennebunk Savings Bank Driveway						Vaughan Street						Vaughan Street						Total
	from North						from East						from West						
	Right	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	U-Turn	CW-SB	CW-NB	Total	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
Total	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	2
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	2
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0			
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	100.0		
Exiting Leg Total	0						2						0						2

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Kennebunk Savings Bank Driveway						Vaughan Street						Vaughan Street						Total
	from North						from East						from West						
	Right	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	U-Turn	CW-SB	CW-NB	Total	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	2
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.500		0.500
Entering Leg	0						0						2						2
Exiting Leg	0						2						0						2
Total	0						2						2						4

PDI File #: 196718 E
 Location: N: Kennebunk Savings Bank Driveway
 Location: E: Vaughan Street W: Vaughan Street
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Pedestrians

	Kennebunk Savings Bank Driveway						Vaughan Street						Vaughan Street						Total
	from North						from East						from West						
	Right	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	U-Turn	CW-SB	CW-NB	Total	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	1	1	0	0	0	1	0	1	0	0	0	0	0	0	2
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	1	1	0	0	0	1	0	1	0	0	0	0	0	0	2
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
5:45 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	2
Grand Total	0	0	0	1	2	3	0	0	0	1	0	1	0	0	0	0	0	0	4
Approach %	0	0	0	33.333	66.667		0	0	0	100	0		0	0	0	0	0		
Total %	0	0	0	25	50	75	0	0	0	25	0	25	0	0	0	0	0	0	
Exiting Leg Total	3						1						0						4

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Kennebunk Savings Bank Driveway						Vaughan Street						Vaughan Street						Total
	from North						from East						from West						
	Right	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	U-Turn	CW-SB	CW-NB	Total	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	1	1	0	0	0	1	0	1	0	0	0	0	0	0	2
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	1	1	0	0	0	1	0	1	0	0	0	0	0	0	2
% Approach Total	0.0	0.0	0.0	0.0	100.0		0.0	0.0	0.0	100.0	0.0		0.0	0.0	0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.250	0.250	0.000	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.250
Entering Leg	0	0	0	0	1	1	0	0	0	1	0	1	0	0	0	0	0	0	2
Exiting Leg	1						1						0						2
Total	2						2						0						4

PDI File #: **196718 F**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **E: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Cars and Heavy Vehicles (Combined)

	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	2	5	0	7	2	2	0	4	5	5	0	10	21
4:15 PM	2	0	0	2	5	1	0	6	3	4	0	7	15
4:30 PM	6	2	0	8	3	2	0	5	2	7	1	10	23
4:45 PM	2	2	0	4	4	0	0	4	3	3	0	6	14
Total	12	9	0	21	14	5	0	19	13	19	1	33	73
5:00 PM	7	4	0	11	5	1	0	6	3	4	0	7	24
5:15 PM	4	4	0	8	2	4	0	6	1	3	0	4	18
5:30 PM	4	0	1	5	6	1	0	7	4	3	0	7	19
5:45 PM	2	1	0	3	7	0	0	7	3	5	0	8	18
Total	17	9	1	27	20	6	0	26	11	15	0	26	79
Grand Total	29	18	1	48	34	11	0	45	24	34	1	59	152
Approach %	60.4	37.5	2.1		75.6	24.4	0.0		40.7	57.6	1.7		
Total %	19.1	11.8	0.7	31.6	22.4	7.2	0.0	29.6	15.8	22.4	0.7	38.8	
Exiting Leg Total				69				42				41	152
Cars	29	18	1	48	34	11	0	45	24	34	1	59	152
% Cars	100.0	100.0	100.0	100.0	100.0	100.0	0.0	100.0	100.0	100.0	100.0	100.0	100.0
Exiting Leg Total				69				42				41	152
Heavy Vehicles	0	0	0	0	0	0	0	0	0	0	0	0	0
% Heavy Vehicles	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total				0				0				0	0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:30 PM	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:30 PM	6	2	0	8	3	2	0	5	2	7	1	10	23
4:45 PM	2	2	0	4	4	0	0	4	3	3	0	6	14
5:00 PM	7	4	0	11	5	1	0	6	3	4	0	7	24
5:15 PM	4	4	0	8	2	4	0	6	1	3	0	4	18
Total Volume	19	12	0	31	14	7	0	21	9	17	1	27	79
% Approach Total	61.3	38.7	0.0		66.7	33.3	0.0		33.3	63.0	3.7		
PHF	0.679	0.750	0.000	0.705	0.700	0.438	0.000	0.875	0.750	0.607	0.250	0.675	0.823
Cars	19	12	0	31	14	7	0	21	9	17	1	27	79
Cars %	100.0	100.0	0.0	100.0	100.0	100.0	0.0	100.0	100.0	100.0	100.0	100.0	100.0
Heavy Vehicles	0	0	0	0	0	0	0	0	0	0	0	0	0
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Cars Enter Leg	19	12	0	31	14	7	0	21	9	17	1	27	79
Heavy Enter Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	19	12	0	31	14	7	0	21	9	17	1	27	79
Cars Exiting Leg				31				21				27	79
Heavy Exiting Leg				0				0				0	0
Total Exiting Leg				31				21				27	79

PDI File #: **196718 F**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **E: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Class: **Cars-Combined (Motorcycles, Cars, Light Goods)**

	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	2	5	0	7	2	2	0	4	5	5	0	10	21
4:15 PM	2	0	0	2	5	1	0	6	3	4	0	7	15
4:30 PM	6	2	0	8	3	2	0	5	2	7	1	10	23
4:45 PM	2	2	0	4	4	0	0	4	3	3	0	6	14
Total	12	9	0	21	14	5	0	19	13	19	1	33	73
5:00 PM	7	4	0	11	5	1	0	6	3	4	0	7	24
5:15 PM	4	4	0	8	2	4	0	6	1	3	0	4	18
5:30 PM	4	0	1	5	6	1	0	7	4	3	0	7	19
5:45 PM	2	1	0	3	7	0	0	7	3	5	0	8	18
Total	17	9	1	27	20	6	0	26	11	15	0	26	79
Grand Total	29	18	1	48	34	11	0	45	24	34	1	59	152
Approach %	60.4	37.5	2.1		75.6	24.4	0.0		40.7	57.6	1.7		
Total %	19.1	11.8	0.7	31.6	22.4	7.2	0.0	29.6	15.8	22.4	0.7	38.8	
Exiting Leg Total				69				42				41	152

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:30 PM	6	2	0	8	3	2	0	5	2	7	1	10	23
4:45 PM	2	2	0	4	4	0	0	4	3	3	0	6	14
5:00 PM	7	4	0	11	5	1	0	6	3	4	0	7	24
5:15 PM	4	4	0	8	2	4	0	6	1	3	0	4	18
Total Volume	19	12	0	31	14	7	0	21	9	17	1	27	79
% Approach Total	61.3	38.7	0.0		66.7	33.3	0.0		33.3	63.0	3.7		
PHF	0.679	0.750	0.000	0.705	0.700	0.438	0.000	0.875	0.750	0.607	0.250	0.675	0.823
Entering Leg	19	12	0	31	14	7	0	21	9	17	1	27	79
Exiting Leg				31				21				27	79
Total				62				42				54	158

PDI File #: **196718 F**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **E: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Class: **Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)**

	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0
Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
% Buses	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
% Single-Unit	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
Buses %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Buses	0				0				0				0
Single-Unit Trucks	0				0				0				0
Articulated Trucks	0				0				0				0
Total Exiting Leg	0				0				0				0

PDI File #: **196718 F**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **E: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Cars

	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	2	1	0	3	2	2	0	4	5	4	0	9	16
4:15 PM	2	0	0	2	5	1	0	6	3	4	0	7	15
4:30 PM	6	1	0	7	2	2	0	4	1	6	1	8	19
4:45 PM	1	1	0	2	3	0	0	3	2	3	0	5	10
Total	11	3	0	14	12	5	0	17	11	17	1	29	60
5:00 PM	5	2	0	7	5	0	0	5	3	4	0	7	19
5:15 PM	4	3	0	7	1	3	0	4	1	3	0	4	15
5:30 PM	4	0	1	5	5	0	0	5	4	2	0	6	16
5:45 PM	2	0	0	2	6	0	0	6	2	5	0	7	15
Total	15	5	1	21	17	3	0	20	10	14	0	24	65
Grand Total	26	8	1	35	29	8	0	37	21	31	1	53	125
Approach %	74.3	22.9	2.9		78.4	21.6	0.0		39.6	58.5	1.9		
Total %	20.8	6.4	0.8	28.0	23.2	6.4	0.0	29.6	16.8	24.8	0.8	42.4	
Exiting Leg Total				61				29				35	125

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
5:00 PM	5	2	0	7	5	0	0	5	3	4	0	7	19
5:15 PM	4	3	0	7	1	3	0	4	1	3	0	4	15
5:30 PM	4	0	1	5	5	0	0	5	4	2	0	6	16
5:45 PM	2	0	0	2	6	0	0	6	2	5	0	7	15
Total Volume	15	5	1	21	17	3	0	20	10	14	0	24	65
% Approach Total	71.4	23.8	4.8		85.0	15.0	0.0		41.7	58.3	0.0		
PHF	0.750	0.417	0.250	0.750	0.708	0.250	0.000	0.833	0.625	0.700	0.000	0.857	0.855
Entering Leg	15	5	1	21	17	3	0	20	10	14	0	24	65
Exiting Leg				32				15				18	65
Total				53				35				42	130

PDI File #: **196718 F**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **E: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Light Goods Vehicle

	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	4	0	4	0	0	0	0	0	1	0	1	5
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	1	0	1	1	0	0	1	1	1	0	2	4
4:45 PM	1	1	0	2	1	0	0	1	1	0	0	1	4
Total	1	6	0	7	2	0	0	2	2	2	0	4	13
5:00 PM	2	2	0	4	0	1	0	1	0	0	0	0	5
5:15 PM	0	1	0	1	1	1	0	2	0	0	0	0	3
5:30 PM	0	0	0	0	1	1	0	2	0	1	0	1	3
5:45 PM	0	1	0	1	1	0	0	1	1	0	0	1	3
Total	2	4	0	6	3	3	0	6	1	1	0	2	14
Grand Total	3	10	0	13	5	3	0	8	3	3	0	6	27
Approach %	23.1	76.9	0.0		62.5	37.5	0.0		50.0	50.0	0.0		
Total %	11.1	37.0	0.0	48.1	18.5	11.1	0.0	29.6	11.1	11.1	0.0	22.2	
Exiting Leg Total				8				13				6	27

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:30 PM	0	1	0	1	1	0	0	1	1	1	0	2	4
4:45 PM	1	1	0	2	1	0	0	1	1	0	0	1	4
5:00 PM	2	2	0	4	0	1	0	1	0	0	0	0	5
5:15 PM	0	1	0	1	1	1	0	2	0	0	0	0	3
Total Volume	3	5	0	8	3	2	0	5	2	1	0	3	16
% Approach Total	37.5	62.5	0.0		60.0	40.0	0.0		66.7	33.3	0.0		
PHF	0.375	0.625	0.000	0.500	0.750	0.500	0.000	0.625	0.500	0.250	0.000	0.375	0.800
Entering Leg	3	5	0	8	3	2	0	5	2	1	0	3	16
Exiting Leg				4				7				5	16
Total				12				12				8	32

PDI File #: **196718 F**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **E: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Buses

	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	0				0				0				0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0				0				0				0
Total	0				0				0				0

PDI File #: **196718 F**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **E: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Single-Unit Trucks

	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	0				0				0				0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0				0				0				0
Total	0				0				0				0

PDI File #: **196718 F**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **E: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Articulated Trucks

	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	0				0				0				0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Vaughan Street				Green Street				Vaughan Street				Total
	from North				from East				from South				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0				0				0				0
Total	0				0				0				0

PDI File #: 196718 F
 Location: N: Vaughan Street S: Vaughan Street
 Location: E: Green Street
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



46 Morton Street, Framingham, MA 01702
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 Email: datarequests@pdilic.com

Bicycles (on Roadway and Crosswalks)

	Vaughan Street						Green Street						Vaughan Street						Total
	from North						from East						from South						
	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	U-Turn	CW-WB	CW-EB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1
5:45 PM	0	1	0	0	0	1	0	0	0	0	0	0	0	1	0	0	0	1	2
Total	0	1	0	0	0	1	0	0	0	0	0	0	0	2	0	0	0	2	3
Grand Total	0	1	0	0	0	1	0	0	0	0	0	0	0	2	0	0	0	2	3
Approach %	0.0	100.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0	0.0		
Total %	0.0	33.3	0.0	0.0	0.0	33.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	66.7	0.0	0.0	0.0	66.7	
Exiting Leg Total	2						1						0						3

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Vaughan Street						Green Street						Vaughan Street						Total
	from North						from East						from South						
	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	U-Turn	CW-WB	CW-EB	Total	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1
5:45 PM	0	1	0	0	0	1	0	0	0	0	0	0	0	1	0	0	0	1	2
Total Volume	0	1	0	0	0	1	0	0	0	0	0	0	0	2	0	0	0	2	3
% Approach Total	0.0	100.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0	0.0		
PHF	0.000	0.250	0.000	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.000	0.500	0.375
Entering Leg	0	1	0	0	0	1	0	0	0	0	0	0	0	2	0	0	0	2	3
Exiting Leg	2						1						0						3
Total	3						1						2						6

PDI File #: **196718 F**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **E: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Pedestrians

	Vaughan Street						Green Street						Vaughan Street						Total
	from North						from East						from South						
	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	U-Turn	CW-WB	CW-EB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	1	0	1	0	0	0	1	0	1	0	0	0	0	0	0	2
4:30 PM	0	0	0	0	3	3	0	0	0	0	4	4	0	0	0	0	0	0	7
4:45 PM	0	0	0	0	1	1	0	0	0	0	2	2	0	0	0	0	0	0	3
Total	0	0	0	1	4	5	0	0	0	1	6	7	0	0	0	0	0	0	12
5:00 PM	0	0	0	0	2	2	0	0	0	1	2	3	0	0	0	5	0	5	10
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	2	2	0	0	0	0	1	1	3
5:45 PM	0	0	0	0	1	1	0	0	0	0	3	3	0	0	0	0	0	0	4
Total	0	0	0	0	3	3	0	0	0	1	7	8	0	0	0	5	1	6	17
Grand Total	0	0	0	1	7	8	0	0	0	2	13	15	0	0	0	5	1	6	29
Approach %	0	0	0	12.5	87.5		0	0	0	13.333	86.667		0	0	0	83.333	16.667		
Total %	0	0	0	3.4483	24.138	27.586	0	0	0	6.8966	44.828	51.724	0	0	0	17.241	3.4483	20.69	
Exiting Leg Total	8						15						6						29

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:15 PM	Vaughan Street						Green Street						Vaughan Street						Total
	from North						from East						from South						
	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	U-Turn	CW-WB	CW-EB	Total	
4:15 PM	0	0	0	1	0	1	0	0	0	1	0	1	0	0	0	0	0	0	2
4:30 PM	0	0	0	0	3	3	0	0	0	0	4	4	0	0	0	0	0	0	7
4:45 PM	0	0	0	0	1	1	0	0	0	0	2	2	0	0	0	0	0	0	3
5:00 PM	0	0	0	0	2	2	0	0	0	1	2	3	0	0	0	5	0	5	10
Total Volume	0	0	0	1	6	7	0	0	0	2	8	10	0	0	0	5	0	5	22
% Approach Total	0.0	0.0	0.0	14.3	85.7		0.0	0.0	0.0	20.0	80.0		0.0	0.0	0.0	100.0	0.0		
PHF	0.000	0.000	0.000	0.250	0.500	0.583	0.000	0.000	0.000	0.500	0.500	0.625	0.000	0.000	0.000	0.250	0.000	0.250	0.550
Entering Leg	0	0	0	1	6	7	0	0	0	2	8	10	0	0	0	5	0	5	22
Exiting Leg	7						10						5						22
Total	14						20						10						44

PDI File #: **196718 G**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **W: Office Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Cars and Heavy Vehicles (Combined)

	Vaughan Street				Vaughan Street				Office Driveway				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	1	7	0	8	6	1	0	7	0	0	0	0	15
4:15 PM	0	2	0	2	9	0	0	9	0	1	0	1	12
4:30 PM	0	5	0	5	9	0	1	10	1	0	0	1	16
4:45 PM	2	4	1	7	5	0	0	5	0	2	0	2	14
Total	3	18	1	22	29	1	1	31	1	3	0	4	57
5:00 PM	6	6	0	12	8	1	2	11	1	7	0	8	31
5:15 PM	4	10	0	14	5	0	0	5	0	2	0	2	21
5:30 PM	2	5	0	7	8	1	0	9	0	2	0	2	18
5:45 PM	4	2	0	6	10	2	0	12	1	1	0	2	20
Total	16	23	0	39	31	4	2	37	2	12	0	14	90
Grand Total	19	41	1	61	60	5	3	68	3	15	0	18	147
Approach %	31.1	67.2	1.6		88.2	7.4	4.4		16.7	83.3	0.0		
Total %	12.9	27.9	0.7	41.5	40.8	3.4	2.0	46.3	2.0	10.2	0.0	12.2	
Exiting Leg Total				76				47				24	147
Cars	19	41	1	61	60	5	3	68	3	15	0	18	147
% Cars	100.0	100.0	100.0	100.0	100.0	100.0	100.0	100.0	100.0	100.0	0.0	100.0	100.0
Exiting Leg Total				76				47				24	147
Heavy Vehicles	0	0	0	0	0	0	0	0	0	0	0	0	0
% Heavy Vehicles	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total				0				0				0	0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Vaughan Street				Vaughan Street				Office Driveway				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
5:00 PM	6	6	0	12	8	1	2	11	1	7	0	8	31
5:15 PM	4	10	0	14	5	0	0	5	0	2	0	2	21
5:30 PM	2	5	0	7	8	1	0	9	0	2	0	2	18
5:45 PM	4	2	0	6	10	2	0	12	1	1	0	2	20
Total Volume	16	23	0	39	31	4	2	37	2	12	0	14	90
% Approach Total	41.0	59.0	0.0		83.8	10.8	5.4		14.3	85.7	0.0		
PHF	0.667	0.575	0.000	0.696	0.775	0.500	0.250	0.771	0.500	0.429	0.000	0.438	0.726
Cars	16	23	0	39	31	4	2	37	2	12	0	14	90
Cars %	100.0	100.0	0.0	100.0	100.0	100.0	100.0	100.0	100.0	100.0	0.0	100.0	100.0
Heavy Vehicles	0	0	0	0	0	0	0	0	0	0	0	0	0
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Cars Enter Leg	16	23	0	39	31	4	2	37	2	12	0	14	90
Heavy Enter Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	16	23	0	39	31	4	2	37	2	12	0	14	90
Cars Exiting Leg				43				27				20	90
Heavy Exiting Leg				0				0				0	0
Total Exiting Leg				43				27				20	90

PDI File #: **196718 G**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **W: Office Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Class: **Cars-Combined (Motorcycles, Cars, Light Goods)**

	Vaughan Street				Vaughan Street				Office Driveway				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	1	7	0	8	6	1	0	7	0	0	0	0	15
4:15 PM	0	2	0	2	9	0	0	9	0	1	0	1	12
4:30 PM	0	5	0	5	9	0	1	10	1	0	0	1	16
4:45 PM	2	4	1	7	5	0	0	5	0	2	0	2	14
Total	3	18	1	22	29	1	1	31	1	3	0	4	57
5:00 PM	6	6	0	12	8	1	2	11	1	7	0	8	31
5:15 PM	4	10	0	14	5	0	0	5	0	2	0	2	21
5:30 PM	2	5	0	7	8	1	0	9	0	2	0	2	18
5:45 PM	4	2	0	6	10	2	0	12	1	1	0	2	20
Total	16	23	0	39	31	4	2	37	2	12	0	14	90
Grand Total	19	41	1	61	60	5	3	68	3	15	0	18	147
Approach %	31.1	67.2	1.6		88.2	7.4	4.4		16.7	83.3	0.0		
Total %	12.9	27.9	0.7	41.5	40.8	3.4	2.0	46.3	2.0	10.2	0.0	12.2	
Exiting Leg Total				76				47				24	147

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Vaughan Street				Vaughan Street				Office Driveway				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
5:00 PM	6	6	0	12	8	1	2	11	1	7	0	8	31
5:15 PM	4	10	0	14	5	0	0	5	0	2	0	2	21
5:30 PM	2	5	0	7	8	1	0	9	0	2	0	2	18
5:45 PM	4	2	0	6	10	2	0	12	1	1	0	2	20
Total Volume	16	23	0	39	31	4	2	37	2	12	0	14	90
% Approach Total	41.0	59.0	0.0		83.8	10.8	5.4		14.3	85.7	0.0		
PHF	0.667	0.575	0.000	0.696	0.775	0.500	0.250	0.771	0.500	0.429	0.000	0.438	0.726
Entering Leg	16	23	0	39	31	4	2	37	2	12	0	14	90
Exiting Leg				43				27				20	90
Total				82				64				34	180

PDI File #: **196718 G**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **W: Office Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Class: **Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)**

	Vaughan Street				Vaughan Street				Office Driveway				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0
Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
% Buses	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
% Single-Unit	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Vaughan Street				Vaughan Street				Office Driveway				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
Buses %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Buses	0				0				0				0
Single-Unit Trucks	0				0				0				0
Articulated Trucks	0				0				0				0
Total Exiting Leg	0				0				0				0

PDI File #: **196718 G**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **W: Office Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Cars

	Vaughan Street				Vaughan Street				Office Driveway				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	1	3	0	4	5	1	0	6	0	0	0	0	10
4:15 PM	0	2	0	2	9	0	0	9	0	1	0	1	12
4:30 PM	0	4	0	4	7	0	1	8	1	0	0	1	13
4:45 PM	2	2	1	5	4	0	0	4	0	2	0	2	11
Total	3	11	1	15	25	1	1	27	1	3	0	4	46
5:00 PM	6	3	0	9	8	1	2	11	1	7	0	8	28
5:15 PM	4	9	0	13	4	0	0	4	0	2	0	2	19
5:30 PM	2	5	0	7	7	0	0	7	0	1	0	1	15
5:45 PM	3	1	0	4	9	2	0	11	1	1	0	2	17
Total	15	18	0	33	28	3	2	33	2	11	0	13	79
Grand Total	18	29	1	48	53	4	3	60	3	14	0	17	125
Approach %	37.5	60.4	2.1		88.3	6.7	5.0		17.6	82.4	0.0		
Total %	14.4	23.2	0.8	38.4	42.4	3.2	2.4	48.0	2.4	11.2	0.0	13.6	
Exiting Leg Total				68				35				22	125

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Vaughan Street				Vaughan Street				Office Driveway				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
5:00 PM	6	3	0	9	8	1	2	11	1	7	0	8	28
5:15 PM	4	9	0	13	4	0	0	4	0	2	0	2	19
5:30 PM	2	5	0	7	7	0	0	7	0	1	0	1	15
5:45 PM	3	1	0	4	9	2	0	11	1	1	0	2	17
Total Volume	15	18	0	33	28	3	2	33	2	11	0	13	79
% Approach Total	45.5	54.5	0.0		84.8	9.1	6.1		15.4	84.6	0.0		
PHF	0.625	0.500	0.000	0.635	0.778	0.375	0.250	0.750	0.500	0.393	0.000	0.406	0.705
Entering Leg	15	18	0	33	28	3	2	33	2	11	0	13	79
Exiting Leg				39				22				18	79
Total				72				55				31	158

PDI File #: **196718 G**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **W: Office Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Class:

Light Goods Vehicle

	Vaughan Street				Vaughan Street				Office Driveway				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	0	4	0	4	1	0	0	1	0	0	0	0	5
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	1	0	1	2	0	0	2	0	0	0	0	3
4:45 PM	0	2	0	2	1	0	0	1	0	0	0	0	3
Total	0	7	0	7	4	0	0	4	0	0	0	0	11
5:00 PM	0	3	0	3	0	0	0	0	0	0	0	0	3
5:15 PM	0	1	0	1	1	0	0	1	0	0	0	0	2
5:30 PM	0	0	0	0	1	1	0	2	0	1	0	1	3
5:45 PM	1	1	0	2	1	0	0	1	0	0	0	0	3
Total	1	5	0	6	3	1	0	4	0	1	0	1	11
Grand Total	1	12	0	13	7	1	0	8	0	1	0	1	22
Approach %	7.7	92.3	0.0		87.5	12.5	0.0		0.0	100.0	0.0		
Total %	4.5	54.5	0.0	59.1	31.8	4.5	0.0	36.4	0.0	4.5	0.0	4.5	
Exiting Leg Total				8				12				2	22

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Vaughan Street				Vaughan Street				Office Driveway				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	0	4	0	4	1	0	0	1	0	0	0	0	5
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	1	0	1	2	0	0	2	0	0	0	0	3
4:45 PM	0	2	0	2	1	0	0	1	0	0	0	0	3
Total Volume	0	7	0	7	4	0	0	4	0	0	0	0	11
% Approach Total	0.0	100.0	0.0		100.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.438	0.000	0.438	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.550
Entering Leg	0	7	0	7	4	0	0	4	0	0	0	0	11
Exiting Leg				4				7				0	11
Total				11				11				0	22

PDI File #: **196718 G**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **W: Office Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Class: **Buses**

	Vaughan Street				Vaughan Street				Office Driveway				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	0				0				0				0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Vaughan Street				Vaughan Street				Office Driveway				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0				0				0				0
Total	0				0				0				0

PDI File #: **196718 G**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **W: Office Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



**PRECISION
D A T A
INDUSTRIES, LLC**

46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Class:

Single-Unit Trucks

	Vaughan Street				Vaughan Street				Office Driveway				Total	
	from North				from South				from West					
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
Exiting Leg Total				0				0					0	0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Vaughan Street				Vaughan Street				Office Driveway				Total	
	from North				from South				from West					
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000		0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg				0				0					0	0
Total				0				0					0	0

PDI File #: **196718 G**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **W: Office Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Articulated Trucks

	Vaughan Street				Vaughan Street				Office Driveway				Total	
	from North				from South				from West					
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
Exiting Leg Total				0				0					0	0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Vaughan Street				Vaughan Street				Office Driveway				Total	
	from North				from South				from West					
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000		0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg				0				0					0	0
Total				0				0					0	0

PDI File #: 196718 G
 Location: N: Vaughan Street S: Vaughan Street
 Location: W: Office Driveway
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Bicycles (on Roadway and Crosswalks)

	Vaughan Street						Vaughan Street						Office Driveway						Total
	from North						from South						from West						
	Right	Thru	U-Turn	CW-EB	CW-WB	Total	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	1	0	0	0	0	1	0	0	0	0	0	0	1
5:45 PM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	1	0	0	0	1	1	0	0	0	0	1	0	0	0	0	0	0	2
Grand Total	0	1	0	0	0	1	1	0	0	0	0	1	0	0	0	0	0	0	2
Approach %	0.0	100.0	0.0	0.0	0.0		100.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0		
Total %	0.0	50.0	0.0	0.0	0.0	50.0	50.0	0.0	0.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	1						1						0						2

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Vaughan Street						Vaughan Street						Office Driveway						Total
	from North						from South						from West						
	Right	Thru	U-Turn	CW-EB	CW-WB	Total	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Left	U-Turn	CW-NB	CW-SB	Total	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	1	0	0	0	0	1	0	0	0	0	0	0	1
5:45 PM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Total Volume	0	1	0	0	0	1	1	0	0	0	0	1	0	0	0	0	0	0	2
% Approach Total	0.0	100.0	0.0	0.0	0.0		100.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0		
PHF	0.000	0.250	0.000	0.000	0.000	0.250	0.250	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.500
Entering Leg	0	1	0	0	0	1	1	0	0	0	0	1	0	0	0	0	0	0	2
Exiting Leg	1						1						0						2
Total	2						2						0						4

PDI File #: **196718 G**
 Location: **N: Vaughan Street S: Vaughan Street**
 Location: **W: Office Driveway**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Pedestrians

	Vaughan Street						Vaughan Street						Office Driveway						Total	
	from North						from South						from West							
	Right	Thru	U-Turn	CW-EB	CW-WB	Total	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	2	1	3	0	0	0	0	0	0	0	0	0	1	1	2	5	
4:15 PM	0	0	0	0	0	0	0	0	0	0	2	2	0	0	0	0	1	1	3	
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	1	5	5	
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	3	3	
Total	0	0	0	2	1	3	0	0	0	0	2	2	0	0	0	5	6	11	16	
5:00 PM	0	0	0	5	0	5	0	0	0	0	0	0	0	0	0	4	1	5	10	
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:30 PM	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	3	1	4	6	
5:45 PM	0	0	0	9	2	11	0	0	0	0	0	0	0	0	0	5	0	5	16	
Total	0	0	0	16	2	18	0	0	0	0	0	0	0	0	0	12	2	14	32	
Grand Total	0	0	0	18	3	21	0	0	0	0	2	2	0	0	0	17	8	25	48	
Approach %	0	0	0	85.714	14.286		0	0	0	0	100		0	0	0	68	32			
Total %	0	0	0	37.5	6.25	43.75	0	0	0	0	4.1667	4.1667	0	0	0	35.417	16.667	52.083		
Exiting Leg Total																			25	48

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Vaughan Street						Vaughan Street						Office Driveway						Total	
	from North						from South						from West							
	Right	Thru	U-Turn	CW-EB	CW-WB	Total	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Left	U-Turn	CW-NB	CW-SB	Total		
5:00 PM	0	0	0	5	0	5	0	0	0	0	0	0	0	0	0	4	1	5	10	
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:30 PM	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	3	1	4	6	
5:45 PM	0	0	0	9	2	11	0	0	0	0	0	0	0	0	0	5	0	5	16	
Total Volume	0	0	0	16	2	18	0	0	0	0	0	0	0	0	0	12	2	14	32	
% Approach Total	0.0	0.0	0.0	88.9	11.1		0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	85.7	14.3			
PHF	0.000	0.000	0.000	0.444	0.250	0.409	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.600	0.500	0.700	0.500	
Entering Leg	0	0	0	16	2	18	0	0	0	0	0	0	0	0	0	12	2	14	32	
Exiting Leg																			14	32
Total																			28	64

PDI File #: **196718 H**
 Location: **N: Russell Street**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Cars and Heavy Vehicles (Combined)

	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	48	2	0	50	4	8	0	12	5	55	0	60	122
4:15 PM	40	3	0	43	4	16	1	21	8	50	0	58	122
4:30 PM	51	7	0	58	5	9	0	14	9	50	0	59	131
4:45 PM	52	3	0	55	6	18	0	24	15	36	0	51	130
Total	191	15	0	206	19	51	1	71	37	191	0	228	505
5:00 PM	76	6	0	82	7	9	0	16	8	63	0	71	169
5:15 PM	65	0	1	66	3	16	0	19	10	51	0	61	146
5:30 PM	86	2	0	88	3	16	0	19	15	54	0	69	176
5:45 PM	79	2	0	81	2	11	0	13	9	46	1	56	150
Total	306	10	1	317	15	52	0	67	42	214	1	257	641
Grand Total	497	25	1	523	34	103	1	138	79	405	1	485	1146
Approach %	95.0	4.8	0.2		24.6	74.6	0.7		16.3	83.5	0.2		
Total %	43.4	2.2	0.1	45.6	3.0	9.0	0.1	12.0	6.9	35.3	0.1	42.3	
Exiting Leg Total				440				105				601	1146
Cars	488	25	1	514	34	103	1	138	79	398	1	478	1130
% Cars	98.2	100.0	100.0	98.3	100.0	100.0	100.0	100.0	100.0	98.3	100.0	98.6	98.6
Exiting Leg Total				433				105				592	1130
Heavy Vehicles	9	0	0	9	0	0	0	0	0	7	0	7	16
% Heavy Vehicles	1.8	0.0	0.0	1.7	0.0	0.0	0.0	0.0	0.0	1.7	0.0	1.4	1.4
Exiting Leg Total				7				0				9	16

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
5:00 PM	76	6	0	82	7	9	0	16	8	63	0	71	169
5:15 PM	65	0	1	66	3	16	0	19	10	51	0	61	146
5:30 PM	86	2	0	88	3	16	0	19	15	54	0	69	176
5:45 PM	79	2	0	81	2	11	0	13	9	46	1	56	150
Total Volume	306	10	1	317	15	52	0	67	42	214	1	257	641
% Approach Total	96.5	3.2	0.3		22.4	77.6	0.0		16.3	83.3	0.4		
PHF	0.890	0.417	0.250	0.901	0.536	0.813	0.000	0.882	0.700	0.849	0.250	0.905	0.911
Cars	301	10	1	312	15	52	0	67	42	210	1	253	632
Cars %	98.4	100.0	100.0	98.4	100.0	100.0	0.0	100.0	100.0	98.1	100.0	98.4	98.6
Heavy Vehicles	5	0	0	5	0	0	0	0	0	4	0	4	9
Heavy Vehicles %	1.6	0.0	0.0	1.6	0.0	0.0	0.0	0.0	0.0	1.9	0.0	1.6	1.4
Cars Enter Leg	301	10	1	312	15	52	0	67	42	210	1	253	632
Heavy Enter Leg	5	0	0	5	0	0	0	0	0	4	0	4	9
Total Entering Leg	306	10	1	317	15	52	0	67	42	214	1	257	641
Cars Exiting Leg				226				52				354	632
Heavy Exiting Leg				4				0				5	9
Total Exiting Leg				230				52				359	641

PDI File #: **196718 H**
 Location: **N: Russell Street**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Cars-Combined (Motorcycles, Cars, Light Goods)

Class:

	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	47	2	0	49	4	8	0	12	5	54	0	59	120
4:15 PM	38	3	0	41	4	16	1	21	8	49	0	57	119
4:30 PM	51	7	0	58	5	9	0	14	9	50	0	59	131
4:45 PM	51	3	0	54	6	18	0	24	15	35	0	50	128
Total	187	15	0	202	19	51	1	71	37	188	0	225	498
5:00 PM	75	6	0	81	7	9	0	16	8	62	0	70	167
5:15 PM	65	0	1	66	3	16	0	19	10	50	0	60	145
5:30 PM	83	2	0	85	3	16	0	19	15	54	0	69	173
5:45 PM	78	2	0	80	2	11	0	13	9	44	1	54	147
Total	301	10	1	312	15	52	0	67	42	210	1	253	632
Grand Total	488	25	1	514	34	103	1	138	79	398	1	478	1130
Approach %	94.9	4.9	0.2		24.6	74.6	0.7		16.5	83.3	0.2		
Total %	43.2	2.2	0.1	45.5	3.0	9.1	0.1	12.2	7.0	35.2	0.1	42.3	
Exiting Leg Total				433				105				592	1130

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
5:00 PM	75	6	0	81	7	9	0	16	8	62	0	70	167
5:15 PM	65	0	1	66	3	16	0	19	10	50	0	60	145
5:30 PM	83	2	0	85	3	16	0	19	15	54	0	69	173
5:45 PM	78	2	0	80	2	11	0	13	9	44	1	54	147
Total Volume	301	10	1	312	15	52	0	67	42	210	1	253	632
% Approach Total	96.5	3.2	0.3		22.4	77.6	0.0		16.6	83.0	0.4		
PHF	0.907	0.417	0.250	0.918	0.536	0.813	0.000	0.882	0.700	0.847	0.250	0.904	0.913
Entering Leg	301	10	1	312	15	52	0	67	42	210	1	253	632
Exiting Leg				226				52				354	632
Total				538				119				607	1264

PDI File #: **196718 H**
 Location: **N: Russell Street**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Class: **Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)**

	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	1	0	0	1	0	0	0	0	0	1	0	1	2
4:15 PM	2	0	0	2	0	0	0	0	0	1	0	1	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	1	0	0	1	0	0	0	0	0	1	0	1	2
Total	4	0	0	4	0	0	0	0	0	3	0	3	7
5:00 PM	1	0	0	1	0	0	0	0	0	1	0	1	2
5:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
5:30 PM	3	0	0	3	0	0	0	0	0	0	0	0	3
5:45 PM	1	0	0	1	0	0	0	0	0	2	0	2	3
Total	5	0	0	5	0	0	0	0	0	4	0	4	9
Grand Total	9	0	0	9	0	0	0	0	0	7	0	7	16
Approach %	100.0	0.0	0.0		0.0	0.0	0.0		0.0	100.0	0.0		
Total %	56.3	0.0	0.0	56.3	0.0	0.0	0.0	0.0	0.0	43.8	0.0	43.8	
Exiting Leg Total				7				0				9	16
Buses	9	0	0	9	0	0	0	0	0	7	0	7	16
% Buses	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	100.0	100.0
Exiting Leg Total				7				0				9	16
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
% Single-Unit	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total				0				0				0	0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total				0				0				0	0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
5:00 PM	1	0	0	1	0	0	0	0	0	1	0	1	2
5:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
5:30 PM	3	0	0	3	0	0	0	0	0	0	0	0	3
5:45 PM	1	0	0	1	0	0	0	0	0	2	0	2	3
Total Volume	5	0	0	5	0	0	0	0	0	4	0	4	9
% Approach Total	100.0	0.0	0.0		0.0	0.0	0.0		0.0	100.0	0.0		
PHF	0.417	0.000	0.000	0.417	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.500	0.750
Buses	5	0	0	5	0	0	0	0	0	4	0	4	9
Buses %	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	100.0	100.0
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Buses	5	0	0	5	0	0	0	0	0	4	0	4	9
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	5	0	0	5	0	0	0	0	0	4	0	4	9
Buses				4				0				5	9
Single-Unit Trucks				0				0				0	0
Articulated Trucks				0				0				0	0
Total Exiting Leg				4				0				5	9

PDI File #: **196718 H**
 Location: **N: Russell Street**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Cars

	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	42	1	0	43	3	6	0	9	4	47	0	51	103
4:15 PM	31	3	0	34	4	16	1	21	7	41	0	48	103
4:30 PM	48	6	0	54	4	6	0	10	9	46	0	55	119
4:45 PM	45	2	0	47	6	16	0	22	12	31	0	43	112
Total	166	12	0	178	17	44	1	62	32	165	0	197	437
5:00 PM	67	6	0	73	7	8	0	15	7	58	0	65	153
5:15 PM	57	0	1	58	3	16	0	19	10	45	0	55	132
5:30 PM	75	2	0	77	3	15	0	18	15	48	0	63	158
5:45 PM	72	2	0	74	2	10	0	12	9	41	1	51	137
Total	271	10	1	282	15	49	0	64	41	192	1	234	580
Grand Total	437	22	1	460	32	93	1	126	73	357	1	431	1017
Approach %	95.0	4.8	0.2		25.4	73.8	0.8		16.9	82.8	0.2		
Total %	43.0	2.2	0.1	45.2	3.1	9.1	0.1	12.4	7.2	35.1	0.1	42.4	
Exiting Leg Total				390				96				531	1017

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
5:00 PM	67	6	0	73	7	8	0	15	7	58	0	65	153
5:15 PM	57	0	1	58	3	16	0	19	10	45	0	55	132
5:30 PM	75	2	0	77	3	15	0	18	15	48	0	63	158
5:45 PM	72	2	0	74	2	10	0	12	9	41	1	51	137
Total Volume	271	10	1	282	15	49	0	64	41	192	1	234	580
% Approach Total	96.1	3.5	0.4		23.4	76.6	0.0		17.5	82.1	0.4		
PHF	0.903	0.417	0.250	0.916	0.536	0.766	0.000	0.842	0.683	0.828	0.250	0.900	0.918
Entering Leg	271	10	1	282	15	49	0	64	41	192	1	234	580
Exiting Leg				208				51				321	580
Total				490				115				555	1160

PDI File #: **196718 H**
 Location: **N: Russell Street**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Class:

Light Goods Vehicle

	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	5	1	0	6	1	2	0	3	1	7	0	8	17
4:15 PM	7	0	0	7	0	0	0	0	1	8	0	9	16
4:30 PM	3	1	0	4	1	3	0	4	0	4	0	4	12
4:45 PM	6	1	0	7	0	2	0	2	3	4	0	7	16
Total	21	3	0	24	2	7	0	9	5	23	0	28	61
5:00 PM	8	0	0	8	0	1	0	1	1	4	0	5	14
5:15 PM	8	0	0	8	0	0	0	0	0	5	0	5	13
5:30 PM	8	0	0	8	0	1	0	1	0	6	0	6	15
5:45 PM	6	0	0	6	0	1	0	1	0	3	0	3	10
Total	30	0	0	30	0	3	0	3	1	18	0	19	52
Grand Total	51	3	0	54	2	10	0	12	6	41	0	47	113
Approach %	94.4	5.6	0.0		16.7	83.3	0.0		12.8	87.2	0.0		
Total %	45.1	2.7	0.0	47.8	1.8	8.8	0.0	10.6	5.3	36.3	0.0	41.6	
Exiting Leg Total				43				9				61	113

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	5	1	0	6	1	2	0	3	1	7	0	8	17
4:15 PM	7	0	0	7	0	0	0	0	1	8	0	9	16
4:30 PM	3	1	0	4	1	3	0	4	0	4	0	4	12
4:45 PM	6	1	0	7	0	2	0	2	3	4	0	7	16
Total Volume	21	3	0	24	2	7	0	9	5	23	0	28	61
% Approach Total	87.5	12.5	0.0		22.2	77.8	0.0		17.9	82.1	0.0		
PHF	0.750	0.750	0.000	0.857	0.500	0.583	0.000	0.563	0.417	0.719	0.000	0.778	0.897
Entering Leg	21	3	0	24	2	7	0	9	5	23	0	28	61
Exiting Leg				25				8				28	61
Total				49				17				56	122

PDI File #: **196718 H**
 Location: **N: Russell Street**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Buses

	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	1	0	0	1	0	0	0	0	0	1	0	1	2
4:15 PM	2	0	0	2	0	0	0	0	0	1	0	1	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	1	0	0	1	0	0	0	0	0	1	0	1	2
Total	4	0	0	4	0	0	0	0	0	3	0	3	7
5:00 PM	1	0	0	1	0	0	0	0	0	1	0	1	2
5:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
5:30 PM	3	0	0	3	0	0	0	0	0	0	0	0	3
5:45 PM	1	0	0	1	0	0	0	0	0	2	0	2	3
Total	5	0	0	5	0	0	0	0	0	4	0	4	9
Grand Total	9	0	0	9	0	0	0	0	0	7	0	7	16
Approach %	100.0	0.0	0.0		0.0	0.0	0.0		0.0	100.0	0.0		
Total %	56.3	0.0	0.0	56.3	0.0	0.0	0.0	0.0	0.0	43.8	0.0	43.8	
Exiting Leg Total				7				0				9	16

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
5:00 PM	1	0	0	1	0	0	0	0	0	1	0	1	2
5:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
5:30 PM	3	0	0	3	0	0	0	0	0	0	0	0	3
5:45 PM	1	0	0	1	0	0	0	0	0	2	0	2	3
Total Volume	5	0	0	5	0	0	0	0	0	4	0	4	9
% Approach Total	100.0	0.0	0.0		0.0	0.0	0.0		0.0	100.0	0.0		
PHF	0.417	0.000	0.000	0.417	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.500	0.750
Entering Leg	5	0	0	5	0	0	0	0	0	4	0	4	9
Exiting Leg				4				0				5	9
Total				9				0				9	18

PDI File #: **196718 H**
 Location: **N: Russell Street**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Single-Unit Trucks

	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total				0				0					0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Russell Street				Deer Street				Deer Street				Total
	from North				from East				from West				
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg				0				0					0
Total				0				0					0

PDI File #: **196718 H**
 Location: **N: Russell Street**
 Location: **E: Deer Street W: Deer Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Articulated Trucks

	Russell Street				Deer Street				Deer Street				Total	
	from North				from East				from West					
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
Exiting Leg Total	0				0				0				0	

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Russell Street				Deer Street				Deer Street				Total	
	from North				from East				from West					
	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000		0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0				0				0				0	
Total	0				0				0				0	

PDI File #: 196718 H
 Location: N: Russell Street
 Location: E: Deer Street W: Deer Street
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Bicycles (on Roadway and Crosswalks)

	Russell Street							Deer Street						Deer Street						Total	
	from North							from East						from West							
	Right	Left	U-Turn	CW-EB	CW-WB	Total		Right	Thru	U-Turn	CW-SB	CW-NB	Total	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0							0						0						0	

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Russell Street							Deer Street						Deer Street						Total	
	from North							from East						from West							
	Right	Left	U-Turn	CW-EB	CW-WB	Total		Right	Thru	U-Turn	CW-SB	CW-NB	Total	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0							0						0						0	
Total	0							0						0						0	

PDI File #: 196718 H
 Location: N: Russell Street
 Location: E: Deer Street W: Deer Street
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



Pedestrians

	Russell Street						Deer Street						Deer Street						Total	
	from North						from East						from West							
	Right	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	U-Turn	CW-SB	CW-NB	Total	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	0	0	0	1	2	3	0	0	0	0	0	0	0	3
4:30 PM	0	0	0	0	1	1	0	0	0	0	1	1	0	0	0	0	0	0	0	2
4:45 PM	0	0	0	0	1	1	0	0	0	2	3	5	0	0	0	1	0	1	1	7
Total	0	0	0	0	2	2	0	0	0	4	6	10	0	0	0	1	0	1	1	13
5:00 PM	0	0	0	0	1	1	0	0	0	2	6	8	0	0	0	1	1	2	2	11
5:15 PM	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0	0	0	0	2
5:30 PM	0	0	0	0	0	0	0	0	0	0	2	2	0	0	0	0	0	0	0	2
5:45 PM	0	0	0	0	1	1	0	0	0	1	1	2	0	0	0	0	0	0	0	3
Total	0	0	0	0	2	2	0	0	0	4	10	14	0	0	0	1	1	2	2	18
Grand Total	0	0	0	0	4	4	0	0	0	8	16	24	0	0	0	2	1	3	3	31
Approach %	0	0	0	0	100		0	0	0	33.333	66.667		0	0	0	66.667	33.333			
Total %	0	0	0	0	12.903	12.903	0	0	0	25.806	51.613	77.419	0	0	0	6.4516	3.2258	9.6774		
Exiting Leg Total	4						24						3						31	

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:15 PM	Russell Street						Deer Street						Deer Street						Total	
	from North						from East						from West							
	Right	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	U-Turn	CW-SB	CW-NB	Total	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:15 PM	0	0	0	0	0	0	0	0	0	1	2	3	0	0	0	0	0	0	0	3
4:30 PM	0	0	0	0	1	1	0	0	0	0	1	1	0	0	0	0	0	0	0	2
4:45 PM	0	0	0	0	1	1	0	0	0	2	3	5	0	0	0	1	0	1	1	7
5:00 PM	0	0	0	0	1	1	0	0	0	2	6	8	0	0	0	1	1	2	2	11
Total Volume	0	0	0	0	3	3	0	0	0	5	12	17	0	0	0	2	1	3	3	23
% Approach Total	0.0	0.0	0.0	0.0	100.0		0.0	0.0	0.0	29.4	70.6		0.0	0.0	0.0	66.7	33.3			
PHF	0.000	0.000	0.000	0.000	0.750	0.750	0.000	0.000	0.000	0.625	0.500	0.531	0.000	0.000	0.000	0.500	0.250	0.375	0.523	
Entering Leg	0	0	0	0	3	3	0	0	0	5	12	17	0	0	0	2	1	3	3	23
Exiting Leg	3						17						3						23	
Total	6						34						6						46	

PDI File #: **196718 I**
 Location: **N: Russell Street S: Russell Street**
 Location: **W: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Cars and Heavy Vehicles (Combined)

	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	2	46	0	48	59	2	0	61	1	9	0	10	119
4:15 PM	8	46	0	54	51	1	0	52	2	4	0	6	112
4:30 PM	4	55	0	59	51	1	0	52	3	9	0	12	123
4:45 PM	4	55	0	59	47	2	0	49	2	7	0	9	117
Total	18	202	0	220	208	6	0	214	8	29	0	37	471
5:00 PM	5	76	0	81	70	1	0	71	3	10	0	13	165
5:15 PM	2	64	0	66	58	1	0	59	0	5	0	5	130
5:30 PM	5	93	0	98	60	1	0	61	1	4	0	5	164
5:45 PM	11	73	0	84	48	1	0	49	1	6	0	7	140
Total	23	306	0	329	236	4	0	240	5	25	0	30	599
Grand Total	41	508	0	549	444	10	0	454	13	54	0	67	1070
Approach %	7.5	92.5	0.0		97.8	2.2	0.0		19.4	80.6	0.0		
Total %	3.8	47.5	0.0	51.3	41.5	0.9	0.0	42.4	1.2	5.0	0.0	6.3	
Exiting Leg Total				498				521				51	1070
Cars	41	499	0	540	437	10	0	447	13	54	0	67	1054
% Cars	100.0	98.2	0.0	98.4	98.4	100.0	0.0	98.5	100.0	100.0	0.0	100.0	98.5
Exiting Leg Total				491				512				51	1054
Heavy Vehicles	0	9	0	9	7	0	0	7	0	0	0	0	16
% Heavy Vehicles	0.0	1.8	0.0	1.6	1.6	0.0	0.0	1.5	0.0	0.0	0.0	0.0	1.5
Exiting Leg Total				7				9				0	16

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
5:00 PM	5	76	0	81	70	1	0	71	3	10	0	13	165
5:15 PM	2	64	0	66	58	1	0	59	0	5	0	5	130
5:30 PM	5	93	0	98	60	1	0	61	1	4	0	5	164
5:45 PM	11	73	0	84	48	1	0	49	1	6	0	7	140
Total Volume	23	306	0	329	236	4	0	240	5	25	0	30	599
% Approach Total	7.0	93.0	0.0		98.3	1.7	0.0		16.7	83.3	0.0		
PHF	0.523	0.823	0.000	0.839	0.843	1.000	0.000	0.845	0.417	0.625	0.000	0.577	0.908
Cars	23	301	0	324	232	4	0	236	5	25	0	30	590
Cars %	100.0	98.4	0.0	98.5	98.3	100.0	0.0	98.3	100.0	100.0	0.0	100.0	98.5
Heavy Vehicles	0	5	0	5	4	0	0	4	0	0	0	0	9
Heavy Vehicles %	0.0	1.6	0.0	1.5	1.7	0.0	0.0	1.7	0.0	0.0	0.0	0.0	1.5
Cars Enter Leg	23	301	0	324	232	4	0	236	5	25	0	30	590
Heavy Enter Leg	0	5	0	5	4	0	0	4	0	0	0	0	9
Total Entering Leg	23	306	0	329	236	4	0	240	5	25	0	30	599
Cars Exiting Leg				257				306				27	590
Heavy Exiting Leg				4				5				0	9
Total Exiting Leg				261				311				27	599

PDI File #: **196718 I**
 Location: **N: Russell Street S: Russell Street**
 Location: **W: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Cars-Combined (Motorcycles, Cars, Light Goods)

Class:

	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	2	45	0	47	58	2	0	60	1	9	0	10	117
4:15 PM	8	44	0	52	50	1	0	51	2	4	0	6	109
4:30 PM	4	55	0	59	51	1	0	52	3	9	0	12	123
4:45 PM	4	54	0	58	46	2	0	48	2	7	0	9	115
Total	18	198	0	216	205	6	0	211	8	29	0	37	464
5:00 PM	5	75	0	80	69	1	0	70	3	10	0	13	163
5:15 PM	2	64	0	66	57	1	0	58	0	5	0	5	129
5:30 PM	5	90	0	95	60	1	0	61	1	4	0	5	161
5:45 PM	11	72	0	83	46	1	0	47	1	6	0	7	137
Total	23	301	0	324	232	4	0	236	5	25	0	30	590
Grand Total	41	499	0	540	437	10	0	447	13	54	0	67	1054
Approach %	7.6	92.4	0.0		97.8	2.2	0.0		19.4	80.6	0.0		
Total %	3.9	47.3	0.0	51.2	41.5	0.9	0.0	42.4	1.2	5.1	0.0	6.4	
Exiting Leg Total				491				512				51	1054

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
5:00 PM	5	75	0	80	69	1	0	70	3	10	0	13	163
5:15 PM	2	64	0	66	57	1	0	58	0	5	0	5	129
5:30 PM	5	90	0	95	60	1	0	61	1	4	0	5	161
5:45 PM	11	72	0	83	46	1	0	47	1	6	0	7	137
Total Volume	23	301	0	324	232	4	0	236	5	25	0	30	590
% Approach Total	7.1	92.9	0.0		98.3	1.7	0.0		16.7	83.3	0.0		
PHF	0.523	0.836	0.000	0.853	0.841	1.000	0.000	0.843	0.417	0.625	0.000	0.577	0.905
Entering Leg	23	301	0	324	232	4	0	236	5	25	0	30	590
Exiting Leg				257				306				27	590
Total				581				542				57	1180

PDI File #: **196718 I**
 Location: **N: Russell Street S: Russell Street**
 Location: **W: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Class: Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)

	Russell Street				Russell Street				Green Street				Total	
	from North				from South				from West					
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total		
4:00 PM	0	1	0	1	1	0	0	1	0	0	0	0	2	
4:15 PM	0	2	0	2	1	0	0	1	0	0	0	0	3	
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:45 PM	0	1	0	1	1	0	0	1	0	0	0	0	2	
Total	0	4	0	4	3	0	0	3	0	0	0	0	7	
5:00 PM	0	1	0	1	1	0	0	1	0	0	0	0	2	
5:15 PM	0	0	0	0	1	0	0	1	0	0	0	0	1	
5:30 PM	0	3	0	3	0	0	0	0	0	0	0	0	3	
5:45 PM	0	1	0	1	2	0	0	2	0	0	0	0	3	
Total	0	5	0	5	4	0	0	4	0	0	0	0	9	
Grand Total	0	9	0	9	7	0	0	7	0	0	0	0	16	
Approach %	0.0	100.0	0.0		100.0	0.0	0.0		0.0	0.0	0.0			
Total %	0.0	56.3	0.0	56.3	43.8	0.0	0.0	43.8	0.0	0.0	0.0	0.0		
Exiting Leg Total				7				9					0	16
Buses	0	9	0	9	7	0	0	7	0	0	0	0	0	16
% Buses	0.0	100.0	0.0	100.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0
Exiting Leg Total				7				9					0	16
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Single-Unit	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total				0				0					0	0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total				0				0					0	0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Russell Street				Russell Street				Green Street				Total	
	from North				from South				from West					
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total		
5:00 PM	0	1	0	1	1	0	0	1	0	0	0	0	2	
5:15 PM	0	0	0	0	1	0	0	1	0	0	0	0	1	
5:30 PM	0	3	0	3	0	0	0	0	0	0	0	0	3	
5:45 PM	0	1	0	1	2	0	0	2	0	0	0	0	3	
Total Volume	0	5	0	5	4	0	0	4	0	0	0	0	9	
% Approach Total	0.0	100.0	0.0		100.0	0.0	0.0		0.0	0.0	0.0			
PHF	0.000	0.417	0.000	0.417	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.750	
Buses	0	5	0	5	4	0	0	4	0	0	0	0	9	
Buses %	0.0	100.0	0.0	100.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	100.0	
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Buses	0	5	0	5	4	0	0	4	0	0	0	0	9	
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total Entering Leg	0	5	0	5	4	0	0	4	0	0	0	0	9	
Buses				4				5					0	9
Single-Unit Trucks				0				0					0	0
Articulated Trucks				0				0					0	0
Total Exiting Leg				4				5					0	9

PDI File #: **196718 I**
 Location: **N: Russell Street S: Russell Street**
 Location: **W: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars

	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	1	40	0	41	50	2	0	52	1	6	0	7	100
4:15 PM	8	38	0	46	41	1	0	42	1	4	0	5	93
4:30 PM	3	52	0	55	46	1	0	47	2	7	0	9	111
4:45 PM	3	47	0	50	41	2	0	43	1	4	0	5	98
Total	15	177	0	192	178	6	0	184	5	21	0	26	402
5:00 PM	2	68	0	70	64	1	0	65	3	6	0	9	144
5:15 PM	1	54	0	55	53	1	0	54	0	3	0	3	112
5:30 PM	3	83	0	86	54	1	0	55	1	4	0	5	146
5:45 PM	10	65	0	75	42	1	0	43	1	4	0	5	123
Total	16	270	0	286	213	4	0	217	5	17	0	22	525
Grand Total	31	447	0	478	391	10	0	401	10	38	0	48	927
Approach %	6.5	93.5	0.0		97.5	2.5	0.0		20.8	79.2	0.0		
Total %	3.3	48.2	0.0	51.6	42.2	1.1	0.0	43.3	1.1	4.1	0.0	5.2	
Exiting Leg Total				429				457				41	927

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
5:00 PM	2	68	0	70	64	1	0	65	3	6	0	9	144
5:15 PM	1	54	0	55	53	1	0	54	0	3	0	3	112
5:30 PM	3	83	0	86	54	1	0	55	1	4	0	5	146
5:45 PM	10	65	0	75	42	1	0	43	1	4	0	5	123
Total Volume	16	270	0	286	213	4	0	217	5	17	0	22	525
% Approach Total	5.6	94.4	0.0		98.2	1.8	0.0		22.7	77.3	0.0		
PHF	0.400	0.813	0.000	0.831	0.832	1.000	0.000	0.835	0.417	0.708	0.000	0.611	0.899
Entering Leg	16	270	0	286	213	4	0	217	5	17	0	22	525
Exiting Leg				230				275				20	525
Total				516				492				42	1050

PDI File #: **196718 I**
 Location: **N: Russell Street S: Russell Street**
 Location: **W: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Class:

Light Goods Vehicle

	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	1	5	0	6	8	0	0	8	0	3	0	3	17
4:15 PM	0	6	0	6	9	0	0	9	1	0	0	1	16
4:30 PM	1	3	0	4	5	0	0	5	1	2	0	3	12
4:45 PM	1	7	0	8	5	0	0	5	1	3	0	4	17
Total	3	21	0	24	27	0	0	27	3	8	0	11	62
5:00 PM	3	7	0	10	5	0	0	5	0	4	0	4	19
5:15 PM	1	10	0	11	4	0	0	4	0	2	0	2	17
5:30 PM	2	7	0	9	6	0	0	6	0	0	0	0	15
5:45 PM	1	7	0	8	4	0	0	4	0	2	0	2	14
Total	7	31	0	38	19	0	0	19	0	8	0	8	65
Grand Total	10	52	0	62	46	0	0	46	3	16	0	19	127
Approach %	16.1	83.9	0.0		100.0	0.0	0.0		15.8	84.2	0.0		
Total %	7.9	40.9	0.0	48.8	36.2	0.0	0.0	36.2	2.4	12.6	0.0	15.0	
Exiting Leg Total				62				55				10	127

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:45 PM	1	7	0	8	5	0	0	5	1	3	0	4	17
5:00 PM	3	7	0	10	5	0	0	5	0	4	0	4	19
5:15 PM	1	10	0	11	4	0	0	4	0	2	0	2	17
5:30 PM	2	7	0	9	6	0	0	6	0	0	0	0	15
Total Volume	7	31	0	38	20	0	0	20	1	9	0	10	68
% Approach Total	18.4	81.6	0.0		100.0	0.0	0.0		10.0	90.0	0.0		
PHF	0.583	0.775	0.000	0.864	0.833	0.000	0.000	0.833	0.250	0.563	0.000	0.625	0.895
Entering Leg	7	31	0	38	20	0	0	20	1	9	0	10	68
Exiting Leg				29				32				7	68
Total				67				52				17	136

PDI File #: **196718 I**
 Location: **N: Russell Street S: Russell Street**
 Location: **W: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Class:

Buses

	Russell Street				Russell Street				Green Street				Total	
	from North				from South				from West					
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total		
4:00 PM	0	1	0	1	1	0	0	1	0	0	0	0	2	
4:15 PM	0	2	0	2	1	0	0	1	0	0	0	0	3	
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:45 PM	0	1	0	1	1	0	0	1	0	0	0	0	2	
Total	0	4	0	4	3	0	0	3	0	0	0	0	7	
5:00 PM	0	1	0	1	1	0	0	1	0	0	0	0	2	
5:15 PM	0	0	0	0	1	0	0	1	0	0	0	0	1	
5:30 PM	0	3	0	3	0	0	0	0	0	0	0	0	3	
5:45 PM	0	1	0	1	2	0	0	2	0	0	0	0	3	
Total	0	5	0	5	4	0	0	4	0	0	0	0	9	
Grand Total	0	9	0	9	7	0	0	7	0	0	0	0	16	
Approach %	0.0	100.0	0.0		100.0	0.0	0.0		0.0	0.0	0.0			
Total %	0.0	56.3	0.0	56.3	43.8	0.0	0.0	43.8	0.0	0.0	0.0	0.0		
Exiting Leg Total				7				9					0	16

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
5:00 PM	0	1	0	1	1	0	0	1	0	0	0	0	2
5:15 PM	0	0	0	0	1	0	0	1	0	0	0	0	1
5:30 PM	0	3	0	3	0	0	0	0	0	0	0	0	3
5:45 PM	0	1	0	1	2	0	0	2	0	0	0	0	3
Total Volume	0	5	0	5	4	0	0	4	0	0	0	0	9
% Approach Total	0.0	100.0	0.0		100.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.417	0.000	0.417	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.750
Entering Leg	0	5	0	5	4	0	0	4	0	0	0	0	9
Exiting Leg				4				5					9
Total				9				9					18

PDI File #: **196718 I**
 Location: **N: Russell Street S: Russell Street**
 Location: **W: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Single-Unit Trucks

	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	0				0				0				0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0				0				0				0
Total	0				0				0				0

PDI File #: **196718 I**
 Location: **N: Russell Street S: Russell Street**
 Location: **W: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Articulated Trucks

	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	0				0				0				0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Russell Street				Russell Street				Green Street				Total
	from North				from South				from West				
	Right	Thru	U-Turn	Total	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0				0				0				0
Total	0				0				0				0

PDI File #: 196718 I
 Location: N: Russell Street S: Russell Street
 Location: W: Green Street
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Bicycles (on Roadway and Crosswalks)

	Russell Street						Russell Street						Green Street						Total
	from North						from South						from West						
	Right	Thru	U-Turn	CW-EB	CW-WB	Total	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0						0						0						0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Russell Street						Russell Street						Green Street						Total
	from North						from South						from West						
	Right	Thru	U-Turn	CW-EB	CW-WB	Total	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0						0						0						0
Total	0						0						0						0

PDI File #: **196718 I**
 Location: **N: Russell Street S: Russell Street**
 Location: **W: Green Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Pedestrians

	Russell Street						Russell Street						Green Street						Total	
	from North						from South						from West							
	Right	Thru	U-Turn	CW-EB	CW-WB	Total	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	1	
4:30 PM	0	0	0	1	1	2	0	0	0	0	2	2	0	0	0	0	1	1	5	
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	0	0	1	1	2	0	0	0	1	2	3	0	0	0	0	1	1	6	
5:00 PM	0	0	0	1	1	2	0	0	0	0	2	2	0	0	0	0	1	1	5	
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:45 PM	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	1	1	2	4	
Total	0	0	0	2	2	4	0	0	0	0	2	2	0	0	0	1	2	3	9	
Grand Total	0	0	0	3	3	6	0	0	0	1	4	5	0	0	0	1	3	4	15	
Approach %	0	0	0	50	50		0	0	0	20	80		0	0	0	25	75			
Total %	0	0	0	20	20	40	0	0	0	6.6667	26.667	33.333	0	0	0	6.6667	20	26.667		
Exiting Leg Total																			4	15

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:15 PM	Russell Street						Russell Street						Green Street						Total	
	from North						from South						from West							
	Right	Thru	U-Turn	CW-EB	CW-WB	Total	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Left	U-Turn	CW-NB	CW-SB	Total		
4:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	1	
4:30 PM	0	0	0	1	1	2	0	0	0	0	2	2	0	0	0	0	1	1	5	
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:00 PM	0	0	0	1	1	2	0	0	0	0	2	2	0	0	0	0	1	1	5	
Total Volume	0	0	0	2	2	4	0	0	0	1	4	5	0	0	0	0	2	2	11	
% Approach Total	0.0	0.0	0.0	50.0	50.0		0.0	0.0	0.0	20.0	80.0		0.0	0.0	0.0	0.0	100.0			
PHF	0.000	0.000	0.000	0.500	0.500	0.500	0.000	0.000	0.000	0.250	0.500	0.625	0.000	0.000	0.000	0.000	0.500	0.500	0.550	
Entering Leg	0	0	0	2	2	4	0	0	0	1	4	5	0	0	0	0	2	2	11	
Exiting Leg																			2	11
Total																			4	22

PDI File #: **196718 J**
 Location: **S: Russell Street**
 Location: **E: Market Street W: Market Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Cars and Heavy Vehicles (Combined)

	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	72	0	0	72	2	62	0	64	48	39	0	87	223
4:15 PM	78	0	0	78	0	57	0	57	54	53	0	107	242
4:30 PM	88	0	0	88	4	54	0	58	62	60	1	123	269
4:45 PM	86	0	0	86	2	53	0	55	55	71	0	126	267
Total	324	0	0	324	8	226	0	234	219	223	1	443	1001
5:00 PM	132	0	0	132	3	78	0	81	81	63	0	144	357
5:15 PM	84	0	0	84	0	64	0	64	69	59	0	128	276
5:30 PM	78	0	0	78	3	54	0	57	95	84	0	179	314
5:45 PM	81	0	0	81	0	60	0	60	84	69	0	153	294
Total	375	0	0	375	6	256	0	262	329	275	0	604	1241
Grand Total	699	0	0	699	14	482	0	496	548	498	1	1047	2242
Approach %	100.0	0.0	0.0		2.8	97.2	0.0		52.3	47.6	0.1		
Total %	31.2	0.0	0.0	31.2	0.6	21.5	0.0	22.1	24.4	22.2	0.0	46.7	
Exiting Leg Total				512				548				1182	2242
Cars	697	0	0	697	14	475	0	489	539	495	1	1035	2221
% Cars	99.7	0.0	0.0	99.7	100.0	98.5	0.0	98.6	98.4	99.4	100.0	98.9	99.1
Exiting Leg Total				509				539				1173	2221
Heavy Vehicles	2	0	0	2	0	7	0	7	9	3	0	12	21
% Heavy Vehicles	0.3	0.0	0.0	0.3	0.0	1.5	0.0	1.4	1.6	0.6	0.0	1.1	0.9
Exiting Leg Total				3				9				9	21

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
5:00 PM	132	0	0	132	3	78	0	81	81	63	0	144	357
5:15 PM	84	0	0	84	0	64	0	64	69	59	0	128	276
5:30 PM	78	0	0	78	3	54	0	57	95	84	0	179	314
5:45 PM	81	0	0	81	0	60	0	60	84	69	0	153	294
Total Volume	375	0	0	375	6	256	0	262	329	275	0	604	1241
% Approach Total	100.0	0.0	0.0		2.3	97.7	0.0		54.5	45.5	0.0		
PHF	0.710	0.000	0.000	0.710	0.500	0.821	0.000	0.809	0.866	0.818	0.000	0.844	0.869
Cars	375	0	0	375	6	252	0	258	324	273	0	597	1230
Cars %	100.0	0.0	0.0	100.0	100.0	98.4	0.0	98.5	98.5	99.3	0.0	98.8	99.1
Heavy Vehicles	0	0	0	0	0	4	0	4	5	2	0	7	11
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	1.6	0.0	1.5	1.5	0.7	0.0	1.2	0.9
Cars Enter Leg	375	0	0	375	6	252	0	258	324	273	0	597	1230
Heavy Enter Leg	0	0	0	0	0	4	0	4	5	2	0	7	11
Total Entering Leg	375	0	0	375	6	256	0	262	329	275	0	604	1241
Cars Exiting Leg				279				324				627	1230
Heavy Exiting Leg				2				5				4	11
Total Exiting Leg				281				329				631	1241

PDI File #: **196718 J**
 Location: **S: Russell Street**
 Location: **E: Market Street W: Market Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Class: **Cars-Combined (Motorcycles, Cars, Light Goods)**

	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	72	0	0	72	2	61	0	63	47	39	0	86	221
4:15 PM	76	0	0	76	0	56	0	56	52	53	0	105	237
4:30 PM	88	0	0	88	4	54	0	58	62	59	1	122	268
4:45 PM	86	0	0	86	2	52	0	54	54	71	0	125	265
Total	322	0	0	322	8	223	0	231	215	222	1	438	991
5:00 PM	132	0	0	132	3	77	0	80	80	63	0	143	355
5:15 PM	84	0	0	84	0	63	0	63	69	58	0	127	274
5:30 PM	78	0	0	78	3	54	0	57	92	84	0	176	311
5:45 PM	81	0	0	81	0	58	0	58	83	68	0	151	290
Total	375	0	0	375	6	252	0	258	324	273	0	597	1230
Grand Total	697	0	0	697	14	475	0	489	539	495	1	1035	2221
Approach %	100.0	0.0	0.0		2.9	97.1	0.0		52.1	47.8	0.1		
Total %	31.4	0.0	0.0	31.4	0.6	21.4	0.0	22.0	24.3	22.3	0.0	46.6	
Exiting Leg Total				509				539				1173	2221

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
5:00 PM	132	0	0	132	3	77	0	80	80	63	0	143	355
5:15 PM	84	0	0	84	0	63	0	63	69	58	0	127	274
5:30 PM	78	0	0	78	3	54	0	57	92	84	0	176	311
5:45 PM	81	0	0	81	0	58	0	58	83	68	0	151	290
Total Volume	375	0	0	375	6	252	0	258	324	273	0	597	1230
% Approach Total	100.0	0.0	0.0		2.3	97.7	0.0		54.3	45.7	0.0		
PHF	0.710	0.000	0.000	0.710	0.500	0.818	0.000	0.806	0.880	0.813	0.000	0.848	0.866
Entering Leg	375	0	0	375	6	252	0	258	324	273	0	597	1230
Exiting Leg				279				324				627	1230
Total				654				582				1224	2460

PDI File #: **196718 J**
 Location: **S: Russell Street**
 Location: **E: Market Street W: Market Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Class: **Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)**

	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	1	0	1	1	0	0	1	2
4:15 PM	2	0	0	2	0	1	0	1	2	0	0	2	5
4:30 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
4:45 PM	0	0	0	0	0	1	0	1	1	0	0	1	2
Total	2	0	0	2	0	3	0	3	4	1	0	5	10
5:00 PM	0	0	0	0	0	1	0	1	1	0	0	1	2
5:15 PM	0	0	0	0	0	1	0	1	0	1	0	1	2
5:30 PM	0	0	0	0	0	0	0	0	3	0	0	3	3
5:45 PM	0	0	0	0	0	2	0	2	1	1	0	2	4
Total	0	0	0	0	0	4	0	4	5	2	0	7	11
Grand Total	2	0	0	2	0	7	0	7	9	3	0	12	21
Approach %	100.0	0.0	0.0		0.0	100.0	0.0		75.0	25.0	0.0		
Total %	9.5	0.0	0.0	9.5	0.0	33.3	0.0	33.3	42.9	14.3	0.0	57.1	
Exiting Leg Total	3				9				9				21
Buses	1	0	0	1	0	7	0	7	9	0	0	9	17
% Buses	50.0	0.0	0.0	50.0	0.0	100.0	0.0	100.0	100.0	0.0	0.0	75.0	81.0
Exiting Leg Total	0				9				8				17
Single-Unit Trucks	1	0	0	1	0	0	0	0	0	3	0	3	4
% Single-Unit	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	25.0	19.0
Exiting Leg Total	3				0				1				4
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0				0				0				0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
5:00 PM	0	0	0	0	0	1	0	1	1	0	0	1	2
5:15 PM	0	0	0	0	0	1	0	1	0	1	0	1	2
5:30 PM	0	0	0	0	0	0	0	0	3	0	0	3	3
5:45 PM	0	0	0	0	0	2	0	2	1	1	0	2	4
Total Volume	0	0	0	0	0	4	0	4	5	2	0	7	11
% Approach Total	0.0	0.0	0.0		0.0	100.0	0.0		71.4	28.6	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.500	0.417	0.500	0.000	0.583	0.688
Buses	0	0	0	0	0	4	0	4	5	0	0	5	9
Buses %	0.0	0.0	0.0	0.0	0.0	100.0	0.0	100.0	100.0	0.0	0.0	71.4	81.8
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	2	0	2	2
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	28.6	18.2
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Buses	0	0	0	0	0	4	0	4	5	0	0	5	9
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	2	0	2	2
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	0	0	0	0	0	4	0	4	5	2	0	7	11
Buses	0				5				4				9
Single-Unit Trucks	2				0				0				2
Articulated Trucks	0				0				0				0
Total Exiting Leg	2				5				4				11

PDI File #: **196718 J**
 Location: **S: Russell Street**
 Location: **E: Market Street W: Market Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Cars

	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	67	0	0	67	1	53	0	54	42	37	0	79	200
4:15 PM	67	0	0	67	0	47	0	47	46	49	0	95	209
4:30 PM	82	0	0	82	4	48	0	52	59	53	1	113	247
4:45 PM	75	0	0	75	2	43	0	45	48	64	0	112	232
Total	291	0	0	291	7	191	0	198	195	203	1	399	888
5:00 PM	117	0	0	117	2	69	0	71	69	54	0	123	311
5:15 PM	82	0	0	82	0	56	0	56	58	54	0	112	250
5:30 PM	78	0	0	78	3	49	0	52	84	82	0	166	296
5:45 PM	80	0	0	80	0	50	0	50	75	68	0	143	273
Total	357	0	0	357	5	224	0	229	286	258	0	544	1130
Grand Total	648	0	0	648	12	415	0	427	481	461	1	943	2018
Approach %	100.0	0.0	0.0		2.8	97.2	0.0		51.0	48.9	0.1		
Total %	32.1	0.0	0.0	32.1	0.6	20.6	0.0	21.2	23.8	22.8	0.0	46.7	
Exiting Leg Total				473				481				1064	2018

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
5:00 PM	117	0	0	117	2	69	0	71	69	54	0	123	311
5:15 PM	82	0	0	82	0	56	0	56	58	54	0	112	250
5:30 PM	78	0	0	78	3	49	0	52	84	82	0	166	296
5:45 PM	80	0	0	80	0	50	0	50	75	68	0	143	273
Total Volume	357	0	0	357	5	224	0	229	286	258	0	544	1130
% Approach Total	100.0	0.0	0.0		2.2	97.8	0.0		52.6	47.4	0.0		
PHF	0.763	0.000	0.000	0.763	0.417	0.812	0.000	0.806	0.851	0.787	0.000	0.819	0.908
Entering Leg	357	0	0	357	5	224	0	229	286	258	0	544	1130
Exiting Leg				263				286				581	1130
Total				620				515				1125	2260

PDI File #: **196718 J**
 Location: **S: Russell Street**
 Location: **E: Market Street W: Market Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Class:

Light Goods Vehicle

	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	5	0	0	5	1	8	0	9	5	2	0	7	21
4:15 PM	9	0	0	9	0	9	0	9	6	4	0	10	28
4:30 PM	6	0	0	6	0	6	0	6	3	6	0	9	21
4:45 PM	11	0	0	11	0	9	0	9	6	7	0	13	33
Total	31	0	0	31	1	32	0	33	20	19	0	39	103
5:00 PM	15	0	0	15	1	8	0	9	11	9	0	20	44
5:15 PM	2	0	0	2	0	7	0	7	11	4	0	15	24
5:30 PM	0	0	0	0	0	5	0	5	8	2	0	10	15
5:45 PM	1	0	0	1	0	8	0	8	8	0	0	8	17
Total	18	0	0	18	1	28	0	29	38	15	0	53	100
Grand Total	49	0	0	49	2	60	0	62	58	34	0	92	203
Approach %	100.0	0.0	0.0		3.2	96.8	0.0		63.0	37.0	0.0		
Total %	24.1	0.0	0.0	24.1	1.0	29.6	0.0	30.5	28.6	16.7	0.0	45.3	
Exiting Leg Total				36				58				109	203

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:15 PM	9	0	0	9	0	9	0	9	6	4	0	10	28
4:30 PM	6	0	0	6	0	6	0	6	3	6	0	9	21
4:45 PM	11	0	0	11	0	9	0	9	6	7	0	13	33
5:00 PM	15	0	0	15	1	8	0	9	11	9	0	20	44
Total Volume	41	0	0	41	1	32	0	33	26	26	0	52	126
% Approach Total	100.0	0.0	0.0		3.0	97.0	0.0		50.0	50.0	0.0		
PHF	0.683	0.000	0.000	0.683	0.250	0.889	0.000	0.917	0.591	0.722	0.000	0.650	0.716
Entering Leg	41	0	0	41	1	32	0	33	26	26	0	52	126
Exiting Leg				27				26				73	126
Total				68				59				125	252

PDI File #: **196718 J**
 Location: **S: Russell Street**
 Location: **E: Market Street W: Market Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdillc.com

Buses

	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	1	0	1	1	0	0	1	2
4:15 PM	1	0	0	1	0	1	0	1	2	0	0	2	4
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	1	0	1	1	0	0	1	2
Total	1	0	0	1	0	3	0	3	4	0	0	4	8
5:00 PM	0	0	0	0	0	1	0	1	1	0	0	1	2
5:15 PM	0	0	0	0	0	1	0	1	0	0	0	0	1
5:30 PM	0	0	0	0	0	0	0	0	3	0	0	3	3
5:45 PM	0	0	0	0	0	2	0	2	1	0	0	1	3
Total	0	0	0	0	0	4	0	4	5	0	0	5	9
Grand Total	1	0	0	1	0	7	0	7	9	0	0	9	17
Approach %	100.0	0.0	0.0		0.0	100.0	0.0		100.0	0.0	0.0		
Total %	5.9	0.0	0.0	5.9	0.0	41.2	0.0	41.2	52.9	0.0	0.0	52.9	
Exiting Leg Total	0				9				8				17

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

5:00 PM	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
5:00 PM	0	0	0	0	0	1	0	1	1	0	0	1	2
5:15 PM	0	0	0	0	0	1	0	1	0	0	0	0	1
5:30 PM	0	0	0	0	0	0	0	0	3	0	0	3	3
5:45 PM	0	0	0	0	0	2	0	2	1	0	0	1	3
Total Volume	0	0	0	0	0	4	0	4	5	0	0	5	9
% Approach Total	0.0	0.0	0.0		0.0	100.0	0.0		100.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.500	0.417	0.000	0.000	0.417	0.750
Entering Leg	0				4				5				9
Exiting Leg	0				5				4				9
Total	0				9				9				18

PDI File #: **196718 J**
 Location: **S: Russell Street**
 Location: **E: Market Street W: Market Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Single-Unit Trucks

	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	1	0	0	1	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	1	0	0	1	0	0	0	0	0	1	0	1	2
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
Total	0	0	0	0	0	0	0	0	0	2	0	2	2
Grand Total	1	0	0	1	0	0	0	0	0	3	0	3	4
Approach %	100.0	0.0	0.0		0.0	0.0	0.0		0.0	100.0	0.0		
Total %	25.0	0.0	0.0	25.0	0.0	0.0	0.0	0.0	0.0	75.0	0.0	75.0	
Exiting Leg Total				3				0				1	4

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	1	0	0	1	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	1	0	0	1	0	0	0	0	0	1	0	1	2
% Approach Total	100.0	0.0	0.0		0.0	0.0	0.0		0.0	100.0	0.0		
PHF	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.250	0.500
Entering Leg	1	0	0	1	0	0	0	0	0	1	0	1	2
Exiting Leg				1				0				1	2
Total				2				0				2	4

PDI File #: **196718 J**
 Location: **S: Russell Street**
 Location: **E: Market Street W: Market Street**
 City, State: **Portsmouth, NH**
 Client: **Tighe & Bond/ M. Santos**
 Site Code: **200076019**
 Count Date: **Thursday, January 31, 2019**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



46 Morton Street, Framingham, MA 01702
 Office: 508-875-0100 Fax: 508-875-0118
 Email: datarequests@pdilic.com

Articulated Trucks

	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	0				0				0				0

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Market Street				Russell Street				Market Street				Total
	from East				from South				from West				
	Thru	Left	U-Turn	Total	Right	Left	U-Turn	Total	Right	Thru	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0		0.0	0.0	0.0		0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0				0				0				0
Total	0				0				0				0

PDI File #: 196718 J
 Location: S: Russell Street
 Location: E: Market Street W: Market Street
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



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Bicycles (on Roadway and Crosswalks)

	Market Street						Russell Street						Market Street						Total
	from East						from South						from West						
	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Approach %	100.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0		
Total %	100.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	0						0						0						1

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Market Street						Russell Street						Market Street						Total
	from East						from South						from West						
	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
% Approach Total	100.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0		
PHF	0.250	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.250
Entering Leg	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Exiting Leg	0						0						0						1
Total	1						0						0						2

PDI File #: 196718 J
 Location: S: Russell Street
 Location: E: Market Street W: Market Street
 City, State: Portsmouth, NH
 Client: Tighe & Bond/ M. Santos
 Site Code: 200076019
 Count Date: Thursday, January 31, 2019
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



46 Morton Street, Framingham, MA 01702
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 Email: datarequests@pdilic.com

Pedestrians

	Market Street						Russell Street						Market Street						Total
	from East						from South						from West						
	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	1	1	0	0	0	1	0	1	0	0	0	0	0	0	2
5:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Grand Total	0	0	0	0	2	2	0	0	0	1	0	1	0	0	0	0	0	0	3
Approach %	0	0	0	0	100		0	0	0	100	0		0	0	0	0	0		
Total %	0	0	0	0	66.667	66.667	0	0	0	33.333	0	33.333	0	0	0	0	0	0	
Exiting Leg Total	2						1						0						3

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:15 PM	Market Street						Russell Street						Market Street						Total
	from East						from South						from West						
	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	U-Turn	CW-NB	CW-SB	Total	
4:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Total Volume	0	0	0	0	2	2	0	0	0	1	0	1	0	0	0	0	0	0	3
% Approach Total	0.0	0.0	0.0	0.0	100.0		0.0	0.0	0.0	100.0	0.0		0.0	0.0	0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.500	0.500	0.000	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.750
Entering Leg	0	0	0	0	2	2	0	0	0	1	0	1	0	0	0	0	0	0	3
Exiting Leg	2						1						0						3
Total	4						2						0						6

Seasonal Adjustment Factors

Group 4 Peak Adjustment Factor

Year	2014	2015	2016
Adj. Factor	1.25	1.179865	1.151118

Average 1.19

<u>GROUP</u>	<u>COUNTER</u>	<u>TOWN</u>	<u>LOCATION</u>
04	02051003	BOW	NH 3A south of Robinson Rd
04	02089001	CHICHESTE	NH 28 (Suncook Valley Rd) north of Bear Hill Rd
04	02091001	CLAREMON	NH 12/103 east of Vermont SL
04	62099056	CONCORD	NH 106 (Sheep Davis Rd) at Loudon TL (north of Ashby Rd)
04	72099278	CONCORD	US 3 (Fisherville Rd) north of Sewalls Falls Rd
04	02125001	DOVER	Dover Point Rd south of Thornwood Ln
04	02133021	DURHAM	US 4 east of NH 108
04	82197076	HAMPTON	US 1 (Lafayette Rd) south of Ramp to NH 101
04	02229022	HUDSON	Circumferential Hwy east of Nashua TL
04	02253025	LEBANON	0
04	02255001	LEE	NH 125 (Calef Hwy) north of Pinkham Rd
04	02287001	MARLBOR	(NH 12 at Swanzey TL
04	02297001	MERRIMAC	US 3 (Daniel Webster Hwy) north of Hilton Dr
04	02303001	MILFORD	NH 101A at Amherst TL (west of Overlook Dr)
04	02315051	NASHUA	NH 111 (Bridge / Ferry St) at Hudson TL
04	02339001	NEWPORT	NH 10 1 mile south of Croydon TL (north of Corbin Rd)
04	02345001	NORTH HA	US 1 (Lafayette Rd) north of North Rd
04	62387052	RINDGE	US 202 at Jaffrey TL (north of County Rd)
04	62389040	ROCHESTE	NH 16 (Spaulding TPK) between Exit 12-13
04	02445001	TEMPLE	NH 101 at Wilton TL (west of Old County Farm Rd)
04	02489001	WINDHAM	NH 28 at Derry TL (north of Northland Rd)

Trip Generation

Land Use	Size	Units	Daily			AM Peak Hour			PM Peak Hour		
			Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total
Office (710)	70	ksf	376	376	752	106	15	121	25	117	142
Retail (820)	4	ksf	337	337	674	47	41	88	28	28	56
TOTAL			713	713	1426	153	56	209	53	145	198
Office Trip Generation (710)											
Transit Trips	1.50%		6	6	12	2	0	2	0	2	2
Walk/Bike Trips	8.0%		<u>30</u>	<u>30</u>	60	<u>8</u>	<u>1</u>	9	<u>2</u>	<u>9</u>	11
Total New Vehicle Trips			340	340	680	96	14	110	23	106	129
Retail Trip Generation (710)											
Transit Trips	1.50%		5	5	10	1	1	2	0	0	0
Walk/Bike Trips	8.0%		<u>27</u>	<u>27</u>	54	<u>4</u>	<u>3</u>	7	<u>2</u>	<u>2</u>	4
Total New Vehicle Trips			305	305	610	42	37	79	26	26	52
Net New Trip Generation			645	645	1290	138	51	189	49	132	181

ITE Trip Generation 10, Office, Weekday Daily

Query Filter

DATA SOURCE:

SEARCH BY LAND USE CODE:

LAND USE CATEGORY:

LAND USE :

INDEPENDENT VARIABLE (IV):

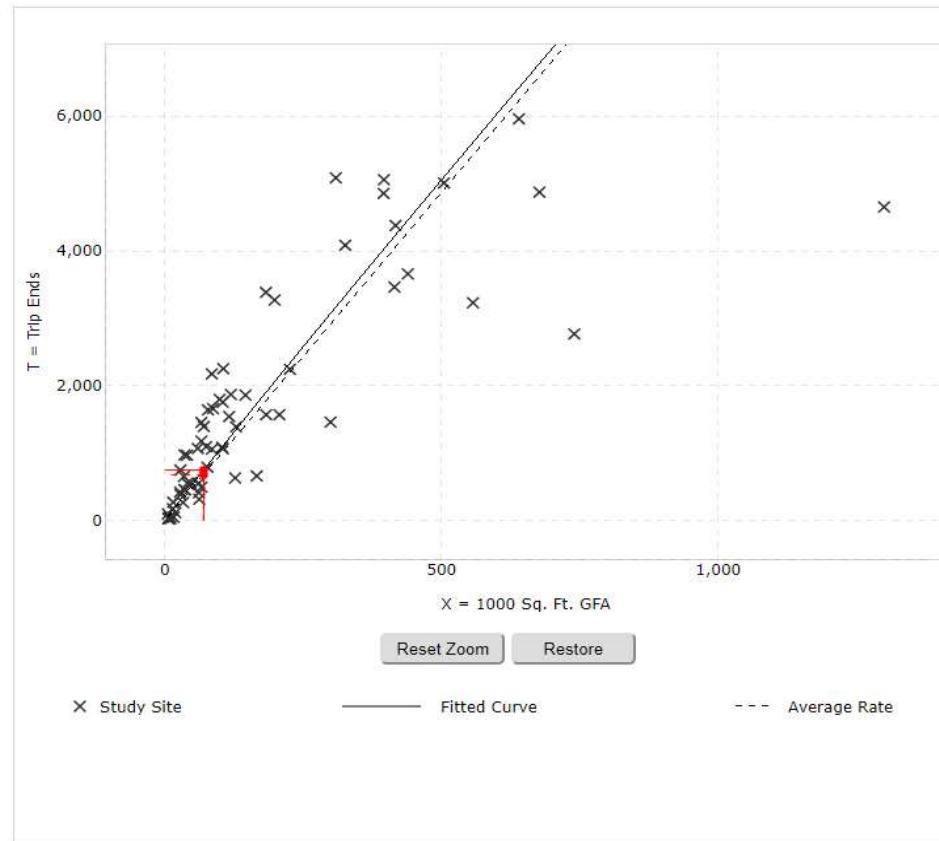
TIME PERIOD:

SETTING/LOCATION:

TRIP TYPE:

ENTER IV VALUE TO CALCULATE TRIPS:

Data Plot and Equation

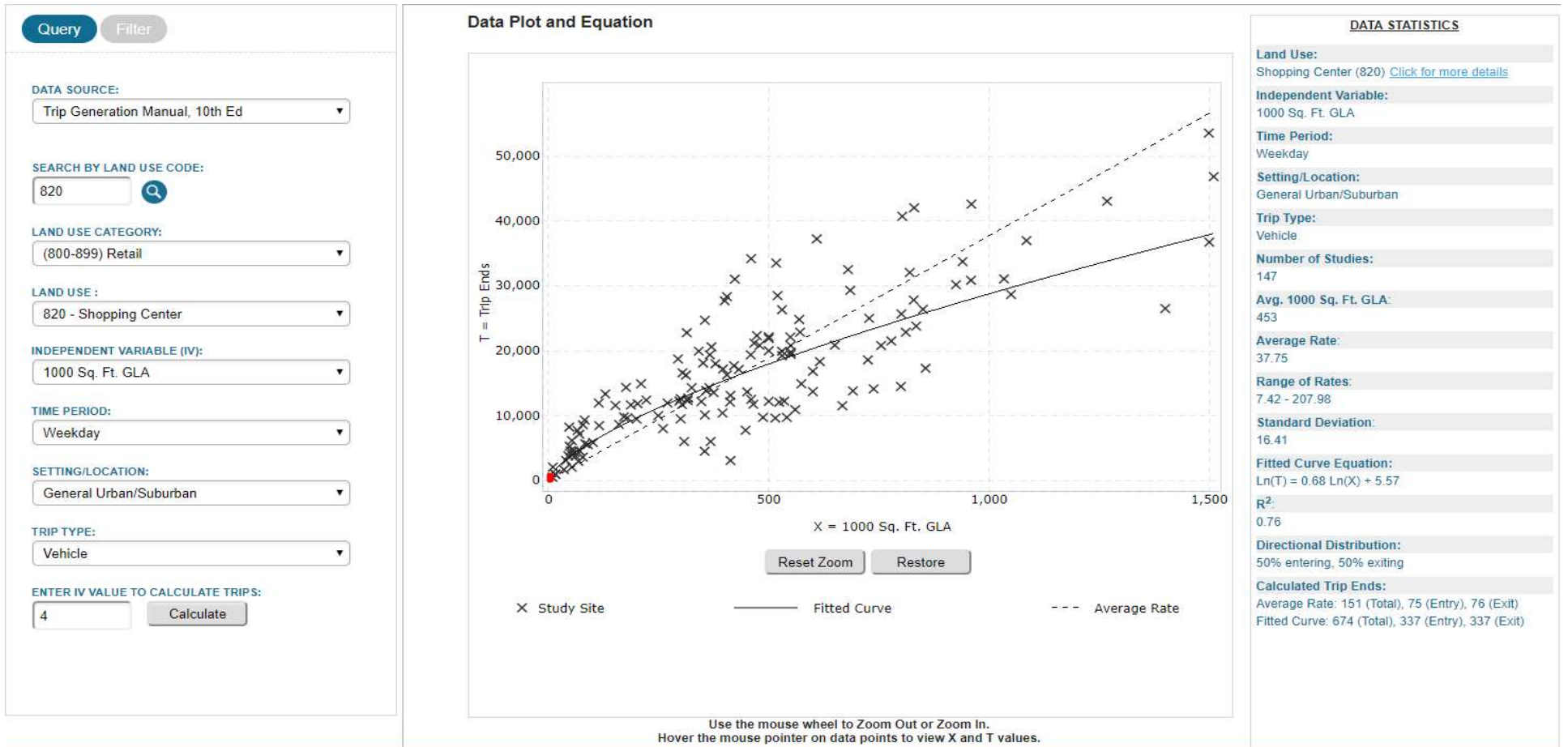


Use the mouse wheel to Zoom Out or Zoom In.
 Hover the mouse pointer on data points to view X and T values.

DATA STATISTICS

Land Use:	General Office Building (710) Click for more details
Independent Variable:	1000 Sq. Ft. GFA
Time Period:	Weekday
Setting/Location:	General Urban/Suburban
Trip Type:	Vehicle
Number of Studies:	66
Avg. 1000 Sq. Ft. GFA:	171
Average Rate:	9.74
Range of Rates:	2.71 - 27.56
Standard Deviation:	5.15
Fitted Curve Equation:	$\ln(T) = 0.97 \ln(X) + 2.50$
R²:	0.83
Directional Distribution:	50% entering, 50% exiting
Calculated Trip Ends:	Average Rate: 682 (Total), 341 (Entry), 341 (Exit) Fitted Curve: 751 (Total), 375 (Entry), 376 (Exit)

ITE Trip Generation 10, Retail, Weekday Daily



ITE Trip Generation 10, Office, Weekday AM Peak Hour

Query Filter

DATA SOURCE:
 Trip Generation Manual, 10th Ed

SEARCH BY LAND USE CODE:
 710

LAND USE CATEGORY:
 (700-799) Office

LAND USE:
 710 - General Office Building

INDEPENDENT VARIABLE (IV):
 1000 Sq. Ft. GFA

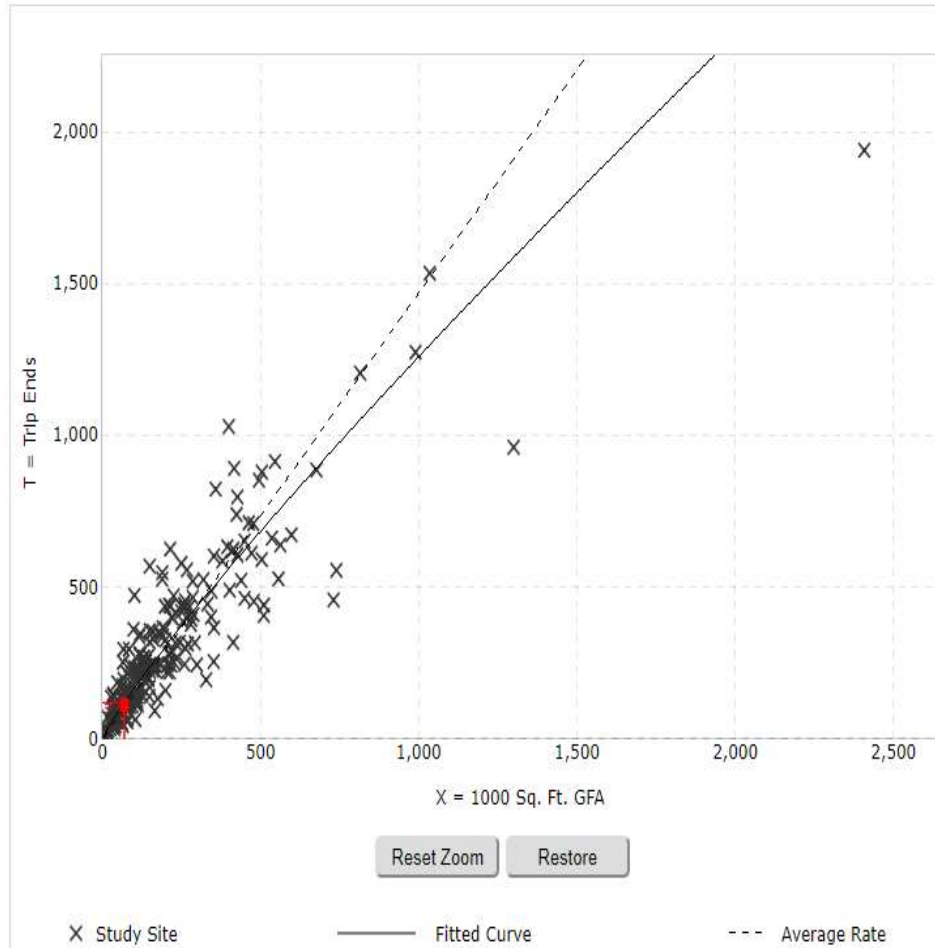
TIME PERIOD:
 Weekday, AM Peak Hour of Generator

SETTING/LOCATION:
 General Urban/Suburban

TRIP TYPE:
 Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:
 70 Calculate

Data Plot and Equation



DATA STATISTICS

Land Use:	General Office Building (710) Click for more details
Independent Variable:	1000 Sq. Ft. GFA
Time Period:	Weekday AM Peak Hour of Generator
Setting/Location:	General Urban/Suburban
Trip Type:	Vehicle
Number of Studies:	228
Avg. 1000 Sq. Ft. GFA:	209
Average Rate:	1.47
Range of Rates:	0.57 - 4.93
Standard Deviation:	0.60
Fitted Curve Equation:	$\ln(T) = 0.88 \ln(X) + 1.06$
R²:	0.84
Directional Distribution:	88% entering, 12% exiting
Calculated Trip Ends:	Average Rate: 103 (Total), 91 (Entry), 12 (Exit) Fitted Curve: 121 (Total), 106 (Entry), 15 (Exit)

ITE Trip Generation 10, Retail, Weekday AM Peak Hour

Query Filter

DATA SOURCE:
 Trip Generation Manual, 10th Ed

SEARCH BY LAND USE CODE:
 820

LAND USE CATEGORY:
 (800-899) Retail

LAND USE:
 820 - Shopping Center

INDEPENDENT VARIABLE (IV):
 1000 Sq. Ft. GLA

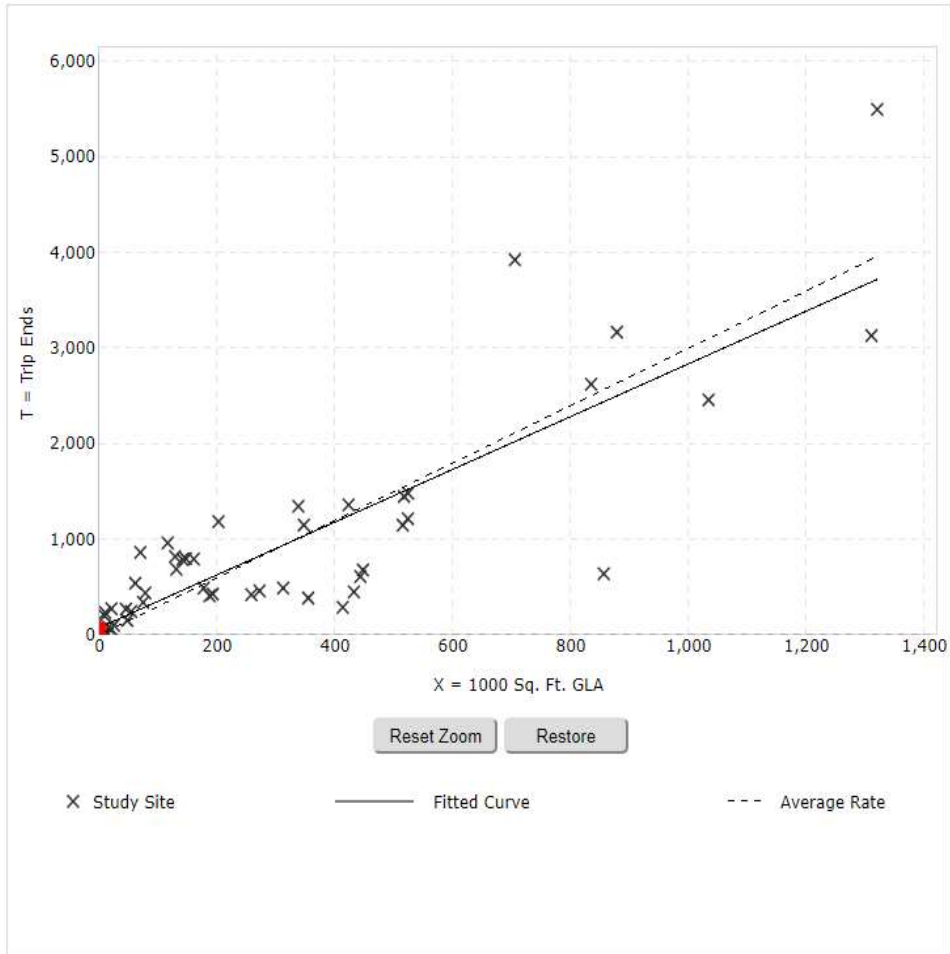
TIME PERIOD:
 Weekday, AM Peak Hour of Generator

SETTING/LOCATION:
 General Urban/Suburban

TRIP TYPE:
 Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:
 4 Calculate

Data Plot and Equation



DATA STATISTICS

Land Use:	Shopping Center (820) Click for more details
Independent Variable:	1000 Sq. Ft. GLA
Time Period:	Weekday AM Peak Hour of Generator
Setting/Location:	General Urban/Suburban
Trip Type:	Vehicle
Number of Studies:	47
Avg. 1000 Sq. Ft. GLA:	323
Average Rate:	3.00
Range of Rates:	0.70 - 23.74
Standard Deviation:	1.85
Fitted Curve Equation:	$T = 2.76(X) + 77.28$
R ² :	0.71
Directional Distribution:	54% entering, 46% exiting
Calculated Trip Ends:	Average Rate: 12 (Total), 6 (Entry), 6 (Exit) Fitted Curve: 88 (Total), 47 (Entry), 41 (Exit)

ITE Trip Generation 10, Office, Weekday PM Peak Hour

Query **Filter**

DATA SOURCE:
Trip Generation Manual, 10th Ed

SEARCH BY LAND USE CODE:
710

LAND USE CATEGORY:
(700-799) Office

LAND USE:
710 - General Office Building

INDEPENDENT VARIABLE (IV):
1000 Sq. Ft. GFA

TIME PERIOD:
Weekday, PM Peak Hour of Generator

SETTING/LOCATION:
General Urban/Suburban

TRIP TYPE:
Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:
70 **Calculate**

Data Plot and Equation

Use the mouse wheel to Zoom Out or Zoom In.
Hover the mouse pointer on data points to view X and T values.

DATA STATISTICS

Land Use:
General Office Building (710) [Click for more details](#)

Independent Variable:
1000 Sq. Ft. GFA

Time Period:
Weekday
PM Peak Hour of Generator

Setting/Location:
General Urban/Suburban

Trip Type:
Vehicle

Number of Studies:
243

Avg. 1000 Sq. Ft. GFA:
205

Average Rate:
1.42

Range of Rates:
0.49 - 6.20

Standard Deviation:
0.61

Fitted Curve Equation:
 $T = 1.10(X) + 65.39$

R²:
0.82

Directional Distribution:
18% entering, 82% exiting

Calculated Trip Ends:
Average Rate: 99 (Total), 17 (Entry), 82 (Exit)
Fitted Curve: 142 (Total), 25 (Entry), 117 (Exit)

ITE Trip Generation 10, Retail, Weekday PM Peak Hour

Query Filter

DATA SOURCE:
 Trip Generation Manual, 10th Ed

SEARCH BY LAND USE CODE:
 820

LAND USE CATEGORY:
 (800-899) Retail

LAND USE :
 820 - Shopping Center

INDEPENDENT VARIABLE (IV):
 1000 Sq. Ft. GLA

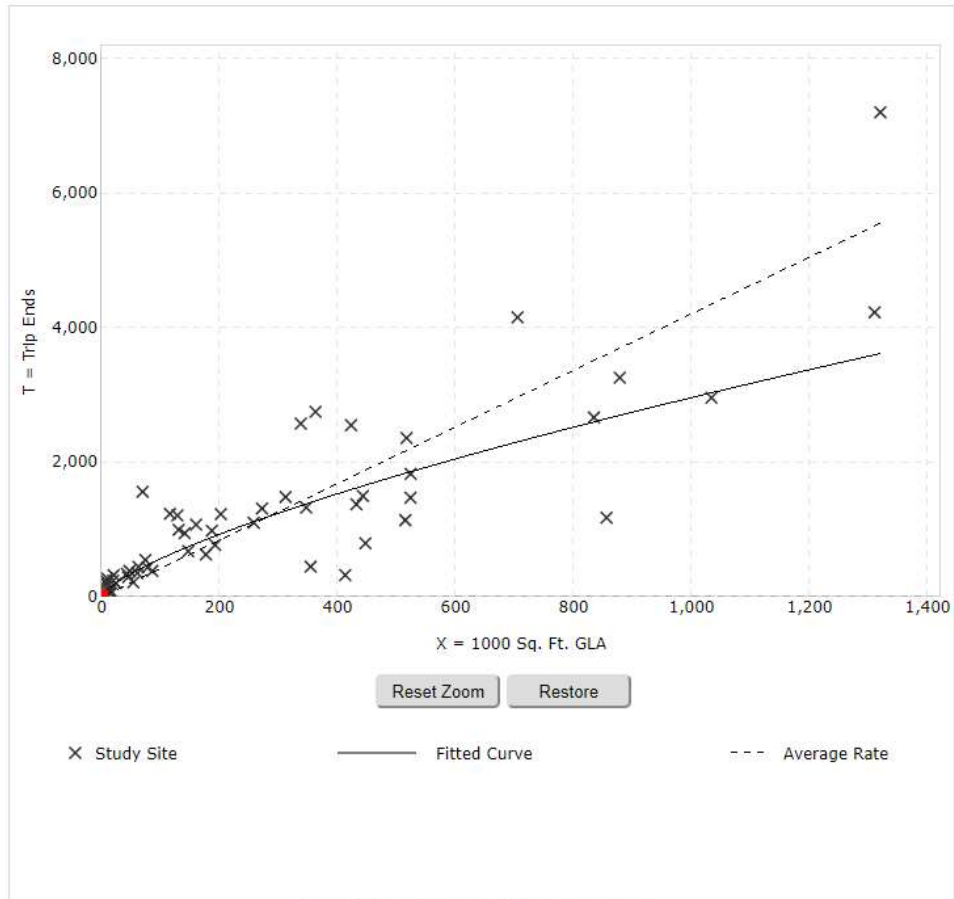
TIME PERIOD:
 Weekday, PM Peak Hour of Generator

SETTING/LOCATION:
 General Urban/Suburban

TRIP TYPE:
 Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:
 4 Calculate

Data Plot and Equation



Use the mouse wheel to Zoom Out or Zoom In.
 Hover the mouse pointer on data points to view X and T values.

DATA STATISTICS

Land Use:	Shopping Center (820) Click for more details
Independent Variable:	1000 Sq. Ft. GLA
Time Period:	Weekday PM Peak Hour of Generator
Setting/Location:	General Urban/Suburban
Trip Type:	Vehicle
Number of Studies:	53
Avg. 1000 Sq. Ft. GLA:	298
Average Rate:	4.21
Range of Rates:	0.78 - 27.27
Standard Deviation:	2.47
Fitted Curve Equation:	$\ln(T) = 0.72 \ln(X) + 3.02$
R²:	0.76
Directional Distribution:	50% entering, 50% exiting
Calculated Trip Ends:	Average Rate: 17 (Total), 8 (Entry), 9 (Exit) Fitted Curve: 56 (Total), 28 (Entry), 28 (Exit)

Trip Distribution

Deer Street Associates Development Trip Distribution

The distribution of the retail-based site-generated traffic volumes for the Deer Street Development was based upon average traffic volumes at five (5) gateway locations into the Downtown Portsmouth roadway network: Maplewood Avenue, Market Street, Congress Street, Islington Street, and Middle Street. The resulting primary trip distribution is shown in Table 8.

Table 8 – Retail-Based Trip Distribution Summary

Direction	Entering %	Exiting %
Maplewood Ave to/from Northwest	30%	30%
Market St to/from Northwest	15%	15%
Congress St to/from Northeast	20%	20%
Islington St to/from Southwest	10%	10%
<u>Middle St to/from South</u>	<u>25%</u>	<u>25%</u>
Total	100%	100%

The distribution of the residential-based site-generated traffic volumes for the Deer Street Development was based upon Journey to Work data obtained from the United States Census Bureau, 2000. The resulting primary trip distribution is shown in Table 9.

Table 9 – Residential-Based Trip Distribution Summary

Direction	Entering %	Exiting %
Maplewood Ave to/from Northwest	55%	55%
Market St to/from Northwest	10%	10%
Congress St to/from Northeast	15%	15%
Islington St to/from Southwest	5%	5%
<u>Middle St to/from South</u>	<u>15%</u>	<u>15%</u>
Total	100%	100%

The distribution of the office-based site-generated traffic volumes for the Deer Street Development was based upon Journey to Home data obtained from the United States Census Bureau, 2000. The resulting primary trip distribution is shown in Table 10.

Table 10 – Office-Based Trip Distribution Summary

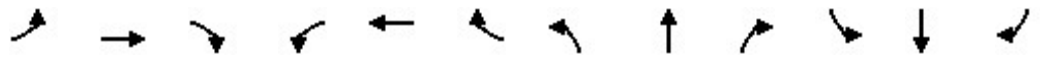
Direction	Entering %	Exiting %
Maplewood Ave to/from Northwest	60%	60%
Market St to/from Northwest	20%	20%
Congress St to/from Northeast	10%	10%
Islington St to/from Southwest	5%	5%
<u>Middle St to/from South</u>	<u>5%</u>	<u>5%</u>
Total	100%	100%

The resulting site-generated traffic-volume networks for the Deer Street Development during the weekday morning, weekday evening, and Saturday peak periods are presented in Figure 14 (A through C).

Capacity Analysis Worksheets

Lanes, Volumes, Timings
3: Maplewood Ave & Deer St

K0076-19 111 Maplewood Ave, Portsmouth HH
2020 No Build

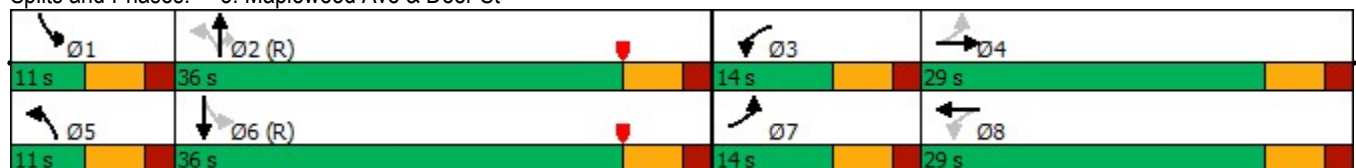


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	230	136	69	287	138	88	48	453	218	70	382	149
Future Volume (vph)	230	136	69	287	138	88	48	453	218	70	382	149
Peak Hour Factor	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Shared Lane Traffic (%)												
Lane Group Flow (vph)	348	311	0	378	298	0	55	521	251	85	648	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Perm	pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2		2	6		
Detector Phase	7	4		3	8		5	2	2	1	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	10.0	10.0	5.0	10.0	
Minimum Split (s)	11.0	29.0		11.0	29.0		11.0	28.0	28.0	11.0	25.0	
Total Split (s)	14.0	29.0		14.0	29.0		11.0	36.0	36.0	11.0	36.0	
Total Split (%)	15.6%	32.2%		15.6%	32.2%		12.2%	40.0%	40.0%	12.2%	40.0%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag	Lag	Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes	Yes	Yes	Yes	
Recall Mode	None	None		None	None		None	C-Max	C-Max	None	C-Max	
v/c Ratio	1.32	0.78		1.39	0.72		0.28	0.73	0.32	0.30	0.93	
Control Delay	194.9	44.1		221.2	39.1		17.2	33.5	4.2	16.1	49.7	
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay	194.9	44.1		221.2	39.1		17.2	33.5	4.2	16.1	49.7	
Queue Length 50th (ft)	~181	153		~212	139		16	270	0	24	~394	
Queue Length 95th (ft)	#165	153		#258	171		37	#429	45	48	#537	
Internal Link Dist (ft)		283			373			505			151	
Turn Bay Length (ft)												
Base Capacity (vph)	263	487		272	503		193	709	796	284	698	
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	
Reduced v/c Ratio	1.32	0.64		1.39	0.59		0.28	0.73	0.32	0.30	0.93	























Intersection Summary

Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 41 (46%), Referenced to phase 2:NBTL and 6:SBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 3: Maplewood Ave & Deer St



HCM Signalized Intersection Capacity Analysis K0076-19 111 Maplewood Ave, Portsmouth HH
 3: Maplewood Ave & Deer St 2020 No Build

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	230	136	69	287	138	88	48	453	218	70	382	149
Future Volume (vph)	230	136	69	287	138	88	48	453	218	70	382	149
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	10	13	13	12	14	14	11	11	13	11	11	11
Total Lost time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Frt	1.00	0.95		1.00	0.94		1.00	1.00	0.85	1.00	0.96	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00	1.00	0.95	1.00	
Satd. Flow (prot)	1652	1827		1770	1871		1711	1801	1636	1711	1725	
Flt Permitted	0.32	1.00		0.30	1.00		0.12	1.00	1.00	0.24	1.00	
Satd. Flow (perm)	564	1827		554	1871		210	1801	1636	432	1725	
Peak-hour factor, PHF	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Adj. Flow (vph)	348	206	105	378	182	116	55	521	251	85	466	182
RTOR Reduction (vph)	0	21	0	0	27	0	0	0	155	0	14	0
Lane Group Flow (vph)	348	290	0	378	271	0	55	521	96	85	634	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Perm	pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2		2	6		
Actuated Green, G (s)	26.6	18.6		26.6	18.6		39.1	34.3	34.3	39.7	34.6	
Effective Green, g (s)	26.6	18.6		26.6	18.6		39.1	34.3	34.3	39.7	34.6	
Actuated g/C Ratio	0.30	0.21		0.30	0.21		0.43	0.38	0.38	0.44	0.38	
Clearance Time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Vehicle Extension (s)	3.0	3.0		3.0	4.0		3.0	3.0	3.0	3.0	3.0	
Lane Grp Cap (vph)	263	377		271	386		171	686	623	263	663	
v/s Ratio Prot	0.12	0.16		c0.12	0.14		0.02	0.29		c0.02	c0.37	
v/s Ratio Perm	0.27			c0.29			0.12		0.06	0.12		
v/c Ratio	1.32	0.77		1.39	0.70		0.32	0.76	0.15	0.32	0.96	
Uniform Delay, d1	30.1	33.7		29.8	33.1		18.6	24.3	18.3	16.6	27.0	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Incremental Delay, d2	169.5	9.1		198.7	6.1		1.1	7.7	0.5	0.7	25.7	
Delay (s)	199.6	42.7		228.5	39.2		19.7	32.0	18.8	17.3	52.6	
Level of Service	F	D		F	D		B	C	B	B	D	
Approach Delay (s)		125.6			145.1			27.2			48.5	
Approach LOS		F			F			C			D	
Intersection Summary												
HCM 2000 Control Delay			82.5				HCM 2000 Level of Service			F		
HCM 2000 Volume to Capacity ratio			1.09									
Actuated Cycle Length (s)			90.0				Sum of lost time (s)			24.0		
Intersection Capacity Utilization			80.6%				ICU Level of Service			D		
Analysis Period (min)			15									
c Critical Lane Group												

Intersection						
Int Delay, s/veh	1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↶	↷		↶	
Traffic Vol, veh/h	4	53	39	1	2	4
Future Vol, veh/h	4	53	39	1	2	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	68	68	67	67	50	50
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	6	78	58	1	4	8

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	59	0	-	0	149 59
Stage 1	-	-	-	-	59 -
Stage 2	-	-	-	-	90 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1545	-	-	-	843 1007
Stage 1	-	-	-	-	964 -
Stage 2	-	-	-	-	934 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1545	-	-	-	840 1007
Mov Cap-2 Maneuver	-	-	-	-	840 -
Stage 1	-	-	-	-	960 -
Stage 2	-	-	-	-	934 -

Approach	EB	WB	SB
HCM Control Delay, s	0.5	0	8.9
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1545	-	-	-	944
HCM Lane V/C Ratio	0.004	-	-	-	0.013
HCM Control Delay (s)	7.3	0	-	-	8.9
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection						
Int Delay, s/veh	18.1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	346	52	65	50	44	525
Future Vol, veh/h	346	52	65	50	44	525
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	88	88	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	384	58	74	57	49	583

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	131	0	-	0	929 103
Stage 1	-	-	-	-	103 -
Stage 2	-	-	-	-	826 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1454	-	-	-	297 952
Stage 1	-	-	-	-	921 -
Stage 2	-	-	-	-	430 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1454	-	-	-	216 952
Mov Cap-2 Maneuver	-	-	-	-	216 -
Stage 1	-	-	-	-	670 -
Stage 2	-	-	-	-	430 -

Approach	EB	WB	SB
HCM Control Delay, s	7.3	0	29.4
HCM LOS			D

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1454	-	-	-	753
HCM Lane V/C Ratio	0.264	-	-	-	0.84
HCM Control Delay (s)	8.4	0	-	-	29.4
HCM Lane LOS	A	A	-	-	D
HCM 95th %tile Q(veh)	1.1	-	-	-	9.6

Intersection						
Int Delay, s/veh	0.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W	R	T	R	L	T
Traffic Vol, veh/h	4	9	679	3	2	617
Future Vol, veh/h	4	9	679	3	2	617
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	58	58	78	78	85	85
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	7	16	871	4	2	726

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1603	873	0	0	875
Stage 1	873	-	-	-	-
Stage 2	730	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	116	349	-	-	771
Stage 1	409	-	-	-	-
Stage 2	477	-	-	-	-
Platoon blocked, %					
Mov Cap-1 Maneuver	116	349	-	-	771
Mov Cap-2 Maneuver	116	-	-	-	-
Stage 1	407	-	-	-	-
Stage 2	477	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	23.6	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	216	771
HCM Lane V/C Ratio	-	-	0.104	0.003
HCM Control Delay (s)	-	-	23.6	9.7
HCM Lane LOS	-	-	C	A
HCM 95th %tile Q(veh)	-	-	0.3	0

Intersection						
Int Delay, s/veh	121.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↘	↗		↑	↑	↗
Traffic Vol, veh/h	434	7	0	442	318	519
Future Vol, veh/h	434	7	0	442	318	519
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	71	71	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	536	9	0	623	379	618

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1002	379	-	0	-	0
Stage 1	379	-	-	-	-	-
Stage 2	623	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	~ 269	668	0	-	-	-
Stage 1	692	-	0	-	-	-
Stage 2	~ 535	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	~ 269	668	-	-	-	-
Mov Cap-2 Maneuver	~ 269	-	-	-	-	-
Stage 1	692	-	-	-	-	-
Stage 2	~ 535	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	482.5	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	-	269	668	-	-
HCM Lane V/C Ratio	-	1.992	0.013	-	-
HCM Control Delay (s)	-	490.1	10.5	-	-
HCM Lane LOS	-	F	B	-	-
HCM 95th %tile Q(veh)	-	38.6	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection						
Int Delay, s/veh	3.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	29	64	663	25	52	580
Future Vol, veh/h	29	64	663	25	52	580
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	36	80	829	31	58	652

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1613	845	0	0	860
Stage 1	845	-	-	-	-
Stage 2	768	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	115	363	-	-	781
Stage 1	421	-	-	-	-
Stage 2	458	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	102	363	-	-	781
Mov Cap-2 Maneuver	102	-	-	-	-
Stage 1	372	-	-	-	-
Stage 2	458	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	44.5	0	0.8
HCM LOS	E		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	202	781
HCM Lane V/C Ratio	-	-	0.575	0.075
HCM Control Delay (s)	-	-	44.5	10
HCM Lane LOS	-	-	E	A
HCM 95th %tile Q(veh)	-	-	3.1	0.2

Intersection						
Int Delay, s/veh	1.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	32	12	668	33	14	607
Future Vol, veh/h	32	12	668	33	14	607
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	79	79	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	43	16	846	42	17	723

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1624	867	0	0	888
Stage 1	867	-	-	-	-
Stage 2	757	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	113	352	-	-	763
Stage 1	411	-	-	-	-
Stage 2	463	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	109	352	-	-	763
Mov Cap-2 Maneuver	109	-	-	-	-
Stage 1	396	-	-	-	-
Stage 2	463	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	51.3	0	0.2
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	134	763
HCM Lane V/C Ratio	-	-	0.438	0.022
HCM Control Delay (s)	-	-	51.3	9.8
HCM Lane LOS	-	-	F	A
HCM 95th %tile Q(veh)	-	-	1.9	0.1

Intersection						
Int Delay, s/veh	1.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			W	W	
Traffic Vol, veh/h	12	2	4	72	62	16
Future Vol, veh/h	12	2	4	72	62	16
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	44	44	77	77	70	70
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	27	5	5	94	89	23

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	205	101	112	0	0
Stage 1	101	-	-	-	-
Stage 2	104	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	783	954	1478	-	-
Stage 1	923	-	-	-	-
Stage 2	920	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	780	954	1478	-	-
Mov Cap-2 Maneuver	780	-	-	-	-
Stage 1	919	-	-	-	-
Stage 2	920	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	9.7	0.4	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1478	-	801	-	-
HCM Lane V/C Ratio	0.004	-	0.04	-	-
HCM Control Delay (s)	7.4	0	9.7	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0.1	-	-

Intersection						
Int Delay, s/veh	1.9					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	42	7	7	409	491	45
Future Vol, veh/h	42	7	7	409	491	45
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	58	58	85	85	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	72	12	8	481	585	54

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1109	612	639	0	-	0
Stage 1	612	-	-	-	-	-
Stage 2	497	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	232	493	945	-	-	-
Stage 1	541	-	-	-	-	-
Stage 2	611	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	229	493	945	-	-	-
Mov Cap-2 Maneuver	229	-	-	-	-	-
Stage 1	535	-	-	-	-	-
Stage 2	611	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	26.8	0.1	0
HCM LOS	D		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	945	-	248	-	-
HCM Lane V/C Ratio	0.009	-	0.341	-	-
HCM Control Delay (s)	8.8	0	26.8	-	-
HCM Lane LOS	A	A	D	-	-
HCM 95th %tile Q(veh)	0	-	1.4	-	-

Intersection						
Int Delay, s/veh	3.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W	W	T	T	T	T
Traffic Vol, veh/h	7	42	31	13	28	32
Future Vol, veh/h	7	42	31	13	28	32
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	81	81	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	8	45	38	16	46	52

Major/Minor	Minor1	Major1	Major2	Major2	Major2
Conflicting Flow All	190	46	0	0	54
Stage 1	46	-	-	-	-
Stage 2	144	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	799	1023	-	-	1551
Stage 1	976	-	-	-	-
Stage 2	883	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	774	1023	-	-	1551
Mov Cap-2 Maneuver	774	-	-	-	-
Stage 1	946	-	-	-	-
Stage 2	883	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	8.9	0	3.4
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	978	1551
HCM Lane V/C Ratio	-	-	0.054	0.03
HCM Control Delay (s)	-	-	8.9	7.4
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0.1

Lanes, Volumes, Timings
3: Maplewood Ave & Deer St

K0076-19 111 Maplewood Ave, Portsmouth HH
2020 Build



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	278	152	85	287	141	88	51	459	218	70	393	159
Future Volume (vph)	278	152	85	287	141	88	51	459	218	70	393	159
Peak Hour Factor	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Shared Lane Traffic (%)												
Lane Group Flow (vph)	421	359	0	378	302	0	59	528	251	85	673	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Perm	pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2		2	6		
Detector Phase	7	4		3	8		5	2	2	1	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	10.0	10.0	5.0	10.0	
Minimum Split (s)	11.0	29.0		11.0	29.0		11.0	28.0	28.0	11.0	25.0	
Total Split (s)	14.0	29.0		14.0	29.0		11.0	36.0	36.0	11.0	36.0	
Total Split (%)	15.6%	32.2%		15.6%	32.2%		12.2%	40.0%	40.0%	12.2%	40.0%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag	Lag	Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes	Yes	Yes	Yes	
Recall Mode	None	None		None	None		None	Max	Max	None	Max	
v/c Ratio	1.40	0.82		1.37	0.67		0.32	0.81	0.34	0.37	1.06	
Control Delay	224.5	44.6		213.2	34.2		18.6	38.5	4.4	18.7	82.0	
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay	224.5	44.6		213.2	34.2		18.6	38.5	4.4	18.7	82.0	
Queue Length 50th (ft)	~273	172		~213	134		18	272	0	26	~430	
Queue Length 95th (ft)	#266	177		#285	174		39	#438	45	48	#566	
Internal Link Dist (ft)		283			373			505			151	
Turn Bay Length (ft)												
Base Capacity (vph)	300	523		275	540		184	648	749	231	635	
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	
Reduced v/c Ratio	1.40	0.69		1.37	0.56		0.32	0.81	0.34	0.37	1.06	

Intersection Summary

Cycle Length: 90

Actuated Cycle Length: 84.3

Natural Cycle: 130

Control Type: Actuated-Uncoordinated

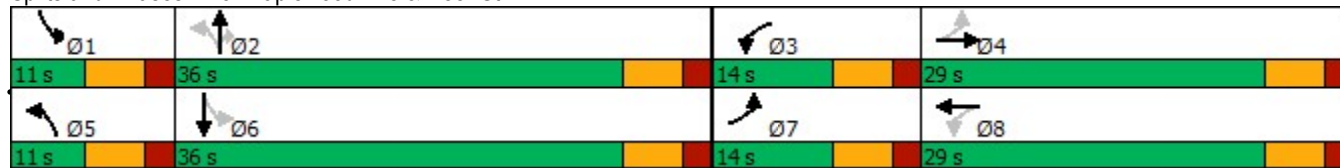
~ Volume exceeds capacity, queue is theoretically infinite.

Queue shown is maximum after two cycles.























95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

Splits and Phases: 3: Maplewood Ave & Deer St



HCM Signalized Intersection Capacity Analysis K0076-19 111 Maplewood Ave, Portsmouth HH
 3: Maplewood Ave & Deer St 2020 Build

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	278	152	85	287	141	88	51	459	218	70	393	159
Future Volume (vph)	278	152	85	287	141	88	51	459	218	70	393	159
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	10	13	13	12	14	14	11	11	13	11	11	11
Total Lost time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Frt	1.00	0.95		1.00	0.94		1.00	1.00	0.85	1.00	0.96	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00	1.00	0.95	1.00	
Satd. Flow (prot)	1652	1821		1770	1872		1711	1801	1636	1711	1723	
Flt Permitted	0.36	1.00		0.25	1.00		0.13	1.00	1.00	0.21	1.00	
Satd. Flow (perm)	620	1821		465	1872		238	1801	1636	372	1723	
Peak-hour factor, PHF	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Adj. Flow (vph)	421	230	129	378	186	116	59	528	251	85	479	194
RTOR Reduction (vph)	0	23	0	0	26	0	0	0	162	0	15	0
Lane Group Flow (vph)	421	336	0	378	276	0	59	528	89	85	658	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Perm	pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2		2	6		
Actuated Green, G (s)	27.4	19.3		27.4	19.3		34.1	30.3	30.3	34.1	30.3	
Effective Green, g (s)	27.4	19.3		27.4	19.3		34.1	30.3	30.3	34.1	30.3	
Actuated g/C Ratio	0.32	0.23		0.32	0.23		0.40	0.35	0.35	0.40	0.35	
Clearance Time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Vehicle Extension (s)	3.0	3.0		3.0	4.0		3.0	3.0	3.0	3.0	3.0	
Lane Grp Cap (vph)	296	411		272	422		160	638	579	207	610	
v/s Ratio Prot	c0.13	0.18		0.13	0.15		0.02	0.29		c0.02	c0.38	
v/s Ratio Perm	c0.32			0.31			0.13		0.05	0.14		
v/c Ratio	1.42	0.82		1.39	0.65		0.37	0.83	0.15	0.41	1.08	
Uniform Delay, d1	27.6	31.4		26.7	30.1		20.2	25.2	18.8	18.1	27.6	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Incremental Delay, d2	208.7	11.9		196.4	4.0		1.4	11.8	0.6	1.3	59.2	
Delay (s)	236.3	43.3		223.1	34.1		21.6	37.0	19.4	19.4	86.8	
Level of Service	F	D		F	C		C	D	B	B	F	
Approach Delay (s)		147.5			139.1			30.6			79.3	
Approach LOS		F			F			C			E	
Intersection Summary												
HCM 2000 Control Delay			96.7				HCM 2000 Level of Service			F		
HCM 2000 Volume to Capacity ratio			1.19									
Actuated Cycle Length (s)			85.5				Sum of lost time (s)			24.0		
Intersection Capacity Utilization			83.6%				ICU Level of Service			E		
Analysis Period (min)			15									
c Critical Lane Group												

Intersection						
Int Delay, s/veh	2.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	15	88	50	18	14	19
Future Vol, veh/h	15	88	50	18	14	19
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	68	68	67	67	50	50
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	22	129	75	27	28	38

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	102	0	-	0	262 89
Stage 1	-	-	-	-	89 -
Stage 2	-	-	-	-	173 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1490	-	-	-	727 969
Stage 1	-	-	-	-	934 -
Stage 2	-	-	-	-	857 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1490	-	-	-	715 969
Mov Cap-2 Maneuver	-	-	-	-	715 -
Stage 1	-	-	-	-	919 -
Stage 2	-	-	-	-	857 -

Approach	EB	WB	SB
HCM Control Delay, s	1.1	0	9.6
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1490	-	-	-	842
HCM Lane V/C Ratio	0.015	-	-	-	0.078
HCM Control Delay (s)	7.5	0	-	-	9.6
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.3

Intersection						
Int Delay, s/veh	19.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↶	↷		↶	↷
Traffic Vol, veh/h	362	52	65	50	44	528
Future Vol, veh/h	362	52	65	50	44	528
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	88	88	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	402	58	74	57	49	587

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	131	0	-	0	965 103
Stage 1	-	-	-	-	103 -
Stage 2	-	-	-	-	862 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1454	-	-	-	283 952
Stage 1	-	-	-	-	921 -
Stage 2	-	-	-	-	414 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1454	-	-	-	202 952
Mov Cap-2 Maneuver	-	-	-	-	202 -
Stage 1	-	-	-	-	658 -
Stage 2	-	-	-	-	414 -

Approach	EB	WB	SB
HCM Control Delay, s	7.4	0	31.7
HCM LOS			D

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1454	-	-	-	741
HCM Lane V/C Ratio	0.277	-	-	-	0.858
HCM Control Delay (s)	8.4	0	-	-	31.7
HCM Lane LOS	A	A	-	-	D
HCM 95th %tile Q(veh)	1.1	-	-	-	10.2

Intersection						
Int Delay, s/veh	139.7					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↘	↗		↑	↑	↗
Traffic Vol, veh/h	460	7	0	442	318	529
Future Vol, veh/h	460	7	0	442	318	529
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	71	71	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	568	9	0	623	379	630

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1002	379	-	0	-	0
Stage 1	379	-	-	-	-	-
Stage 2	623	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	~ 269	668	0	-	-	-
Stage 1	692	-	0	-	-	-
Stage 2	~ 535	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	~ 269	668	-	-	-	-
Mov Cap-2 Maneuver	~ 269	-	-	-	-	-
Stage 1	692	-	-	-	-	-
Stage 2	~ 535	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	534.7	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	-	269	668	-	-
HCM Lane V/C Ratio	-	2.111	0.013	-	-
HCM Control Delay (s)	-	542.7	10.5	-	-
HCM Lane LOS	-	F	B	-	-
HCM 95th %tile Q(veh)	-	42.4	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection						
Int Delay, s/veh	8.8					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	32	105	711	25	71	590
Future Vol, veh/h	32	105	711	25	71	590
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	40	131	889	31	80	663

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1728	905	0	0	920
Stage 1	905	-	-	-	-
Stage 2	823	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	97	335	-	-	742
Stage 1	395	-	-	-	-
Stage 2	431	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	80	335	-	-	742
Mov Cap-2 Maneuver	80	-	-	-	-
Stage 1	327	-	-	-	-
Stage 2	431	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	89.5	0	1.1
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	192	742
HCM Lane V/C Ratio	-	-	0.892	0.108
HCM Control Delay (s)	-	-	89.5	10.4
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	6.8	0.4

Intersection						
Int Delay, s/veh	4.7					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	47	21	713	41	16	617
Future Vol, veh/h	47	21	713	41	16	617
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	79	79	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	63	28	903	52	19	735

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1702	929	0	0	955
Stage 1	929	-	-	-	-
Stage 2	773	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	101	324	-	-	720
Stage 1	385	-	-	-	-
Stage 2	455	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	96	324	-	-	720
Mov Cap-2 Maneuver	96	-	-	-	-
Stage 1	368	-	-	-	-
Stage 2	455	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	90	0	0.3
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	123	720
HCM Lane V/C Ratio	-	-	0.737	0.026
HCM Control Delay (s)	-	-	90	10.1
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	4.2	0.1

Intersection						
Int Delay, s/veh	3.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	32	21	13	84	78	19
Future Vol, veh/h	32	21	13	84	78	19
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	44	44	77	77	70	70
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	73	48	17	109	111	27

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	268	125	138	0	0
Stage 1	125	-	-	-	-
Stage 2	143	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	721	926	1446	-	-
Stage 1	901	-	-	-	-
Stage 2	884	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	712	926	1446	-	-
Mov Cap-2 Maneuver	712	-	-	-	-
Stage 1	889	-	-	-	-
Stage 2	884	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10.4	1	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1446	-	784	-	-
HCM Lane V/C Ratio	0.012	-	0.154	-	-
HCM Control Delay (s)	7.5	0	10.4	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0.5	-	-

Intersection						
Int Delay, s/veh	2.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	53	7	7	425	494	51
Future Vol, veh/h	53	7	7	425	494	51
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	58	58	85	85	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	91	12	8	500	588	61

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1135	619	649	0	-	0
Stage 1	619	-	-	-	-	-
Stage 2	516	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	224	489	937	-	-	-
Stage 1	537	-	-	-	-	-
Stage 2	599	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	221	489	937	-	-	-
Mov Cap-2 Maneuver	221	-	-	-	-	-
Stage 1	531	-	-	-	-	-
Stage 2	599	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	31.6	0.1	0
HCM LOS	D		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	937	-	236	-	-
HCM Lane V/C Ratio	0.009	-	0.438	-	-
HCM Control Delay (s)	8.9	0	31.6	-	-
HCM Lane LOS	A	A	D	-	-
HCM 95th %tile Q(veh)	0	-	2.1	-	-

Intersection						
Int Delay, s/veh	3.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	7	48	49	13	39	59
Future Vol, veh/h	7	48	49	13	39	59
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	81	81	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	8	52	60	16	64	97

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	293	68	0	0	76
Stage 1	68	-	-	-	-
Stage 2	225	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	698	995	-	-	1523
Stage 1	955	-	-	-	-
Stage 2	812	-	-	-	-
Platoon blocked, %					
Mov Cap-1 Maneuver	667	995	-	-	1523
Mov Cap-2 Maneuver	667	-	-	-	-
Stage 1	913	-	-	-	-
Stage 2	812	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.1	0	3
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	936	1523
HCM Lane V/C Ratio	-	-	0.063	0.042
HCM Control Delay (s)	-	-	9.1	7.5
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0.1

Lanes, Volumes, Timings
3: Maplewood Ave & Deer St

K0076-19 111 Maplewood Ave, Portsmouth HH
2030 No Build

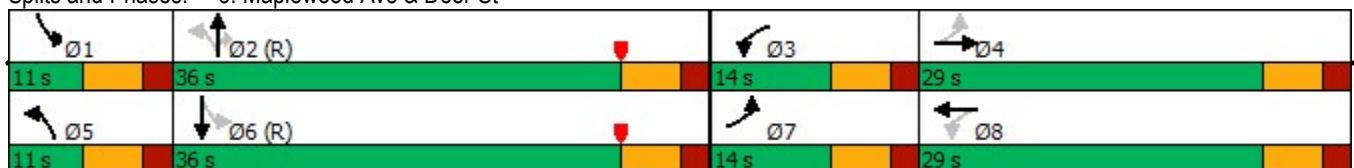


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	239	145	70	308	148	94	49	496	238	75	423	154
Future Volume (vph)	239	145	70	308	148	94	49	496	238	75	423	154
Peak Hour Factor	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Shared Lane Traffic (%)												
Lane Group Flow (vph)	362	326	0	405	319	0	56	570	274	91	704	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Perm	pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2		2	6		
Detector Phase	7	4		3	8		5	2	2	1	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	10.0	10.0	5.0	10.0	
Minimum Split (s)	11.0	29.0		11.0	29.0		11.0	28.0	28.0	11.0	25.0	
Total Split (s)	14.0	29.0		14.0	29.0		11.0	36.0	36.0	11.0	36.0	
Total Split (%)	15.6%	32.2%		15.6%	32.2%		12.2%	40.0%	40.0%	12.2%	40.0%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag	Lag	Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes	Yes	Yes	Yes	
Recall Mode	None	None		None	None		None	C-Max	C-Max	None	C-Max	
v/c Ratio	1.41	0.79		1.51	0.75		0.30	0.82	0.34	0.38	1.02	
Control Delay	233.3	44.9		271.3	40.5		17.7	38.7	4.2	18.3	69.6	
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay	233.3	44.9		271.3	40.5		17.7	38.7	4.2	18.3	69.6	
Queue Length 50th (ft)	~205	161		~247	151		16	313	0	27	~470	
Queue Length 95th (ft)	#194	162		#298	185		38	#492	46	51	#604	
Internal Link Dist (ft)		283			373			505			151	
Turn Bay Length (ft)												
Base Capacity (vph)	256	487		268	503		189	699	803	242	691	
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	
Reduced v/c Ratio	1.41	0.67		1.51	0.63		0.30	0.82	0.34	0.38	1.02	

Intersection Summary

Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 41 (46%), Referenced to phase 2:NBTL and 6:SBTL, Start of Yellow
 Natural Cycle: 120
 Control Type: Actuated-Coordinated
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 3: Maplewood Ave & Deer St



HCM Signalized Intersection Capacity Analysis K0076-19 111 Maplewood Ave, Portsmouth HH
 3: Maplewood Ave & Deer St 2030 No Build

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	239	145	70	308	148	94	49	496	238	75	423	154
Future Volume (vph)	239	145	70	308	148	94	49	496	238	75	423	154
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	10	13	13	12	14	14	11	11	13	11	11	11
Total Lost time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Frt	1.00	0.95		1.00	0.94		1.00	1.00	0.85	1.00	0.96	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00	1.00	0.95	1.00	
Satd. Flow (prot)	1652	1831		1770	1871		1711	1801	1636	1711	1729	
Flt Permitted	0.29	1.00		0.28	1.00		0.12	1.00	1.00	0.18	1.00	
Satd. Flow (perm)	511	1831		522	1871		213	1801	1636	327	1729	
Peak-hour factor, PHF	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Adj. Flow (vph)	362	220	106	405	195	124	56	570	274	91	516	188
RTOR Reduction (vph)	0	20	0	0	27	0	0	0	171	0	14	0
Lane Group Flow (vph)	362	306	0	405	292	0	56	570	103	91	690	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Perm	pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2		2	6		
Actuated Green, G (s)	27.2	19.2		27.2	19.2		38.5	33.8	33.8	39.1	34.1	
Effective Green, g (s)	27.2	19.2		27.2	19.2		38.5	33.8	33.8	39.1	34.1	
Actuated g/C Ratio	0.30	0.21		0.30	0.21		0.43	0.38	0.38	0.43	0.38	
Clearance Time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Vehicle Extension (s)	3.0	3.0		3.0	4.0		3.0	3.0	3.0	3.0	3.0	
Lane Grp Cap (vph)	255	390		268	399		169	676	614	218	655	
v/s Ratio Prot	0.13	0.17		c0.13	0.16		0.02	0.32		c0.02	c0.40	
v/s Ratio Perm	0.30			c0.32			0.12		0.06	0.16		
v/c Ratio	1.42	0.78		1.51	0.73		0.33	0.84	0.17	0.42	1.05	
Uniform Delay, d1	29.5	33.4		29.4	33.0		20.3	25.7	18.7	17.8	27.9	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Incremental Delay, d2	210.3	9.9		248.4	7.2		1.2	12.2	0.6	1.3	50.2	
Delay (s)	239.8	43.3		277.8	40.3		21.4	37.9	19.3	19.1	78.2	
Level of Service	F	D		F	D		C	D	B	B	E	
Approach Delay (s)		146.7			173.1			31.2			71.4	
Approach LOS		F			F			C			E	
Intersection Summary												
HCM 2000 Control Delay			100.1				HCM 2000 Level of Service			F		
HCM 2000 Volume to Capacity ratio			1.20									
Actuated Cycle Length (s)			90.0				Sum of lost time (s)			24.0		
Intersection Capacity Utilization			84.8%				ICU Level of Service			E		
Analysis Period (min)			15									
c Critical Lane Group												

Intersection						
Int Delay, s/veh	1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↶	↷		↶	↷
Traffic Vol, veh/h	4	46	42	1	2	4
Future Vol, veh/h	4	46	42	1	2	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	68	68	67	67	50	50
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	6	68	63	1	4	8

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	64	0	-	0	144
Stage 1	-	-	-	-	64
Stage 2	-	-	-	-	80
Critical Hdwy	4.12	-	-	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	2.218	-	-	-	3.518
Pot Cap-1 Maneuver	1538	-	-	-	849
Stage 1	-	-	-	-	959
Stage 2	-	-	-	-	943
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1538	-	-	-	846
Mov Cap-2 Maneuver	-	-	-	-	846
Stage 1	-	-	-	-	955
Stage 2	-	-	-	-	943

Approach	EB	WB	SB
HCM Control Delay, s	0.6	0	8.9
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1538	-	-	-	943
HCM Lane V/C Ratio	0.004	-	-	-	0.013
HCM Control Delay (s)	7.4	0	-	-	8.9
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection						
Int Delay, s/veh	24.9					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	373	57	72	52	45	563
Future Vol, veh/h	373	57	72	52	45	563
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	88	88	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	414	63	82	59	50	626

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	141	0	-	0	1003 112
Stage 1	-	-	-	-	112 -
Stage 2	-	-	-	-	891 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1442	-	-	-	268 941
Stage 1	-	-	-	-	913 -
Stage 2	-	-	-	-	401 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1442	-	-	-	188 941
Mov Cap-2 Maneuver	-	-	-	-	188 -
Stage 1	-	-	-	-	641 -
Stage 2	-	-	-	-	401 -

Approach	EB	WB	SB
HCM Control Delay, s	7.4	0	42.5
HCM LOS			E

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1442	-	-	-	726
HCM Lane V/C Ratio	0.287	-	-	-	0.931
HCM Control Delay (s)	8.5	0	-	-	42.5
HCM Lane LOS	A	A	-	-	E
HCM 95th %tile Q(veh)	1.2	-	-	-	13.1

Intersection						
Int Delay, s/veh	0.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	4	9	736	3	2	667
Future Vol, veh/h	4	9	736	3	2	667
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	58	58	78	78	85	85
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	7	17	944	4	2	785

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1735	946	0	0	948
Stage 1	946	-	-	-	-
Stage 2	789	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	96	317	-	-	724
Stage 1	377	-	-	-	-
Stage 2	448	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	96	317	-	-	724
Mov Cap-2 Maneuver	96	-	-	-	-
Stage 1	375	-	-	-	-
Stage 2	448	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	26.6	0	0
HCM LOS	D		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	191	724
HCM Lane V/C Ratio	-	-	0.126	0.003
HCM Control Delay (s)	-	-	26.6	10
HCM Lane LOS	-	-	D	A
HCM 95th %tile Q(veh)	-	-	0.4	0

Intersection						
Int Delay, s/veh	173.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↘	↗		↑	↑	↗
Traffic Vol, veh/h	466	8	0	489	353	560
Future Vol, veh/h	466	8	0	489	353	560
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	71	71	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	575	10	0	689	420	667

Major/Minor	Minor2	Major1	Major2		
Conflicting Flow All	1109	420	-	0	-
Stage 1	420	-	-	-	-
Stage 2	689	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-
Pot Cap-1 Maneuver	~ 232	633	0	-	-
Stage 1	663	-	0	-	-
Stage 2	~ 498	-	0	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	~ 232	633	-	-	-
Mov Cap-2 Maneuver	~ 232	-	-	-	-
Stage 1	663	-	-	-	-
Stage 2	~ 498	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	699.7	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	-	232	633	-	-
HCM Lane V/C Ratio	-	2.48	0.016	-	-
HCM Control Delay (s)	-	711.5	10.8	-	-
HCM Lane LOS	-	F	B	-	-
HCM 95th %tile Q(veh)	-	47.5	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection						
Int Delay, s/veh	5.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	32	69	718	28	56	627
Future Vol, veh/h	32	69	718	28	56	627
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	40	86	898	35	63	704

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1746	916	0	0	933
Stage 1	916	-	-	-	-
Stage 2	830	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	95	330	-	-	734
Stage 1	390	-	-	-	-
Stage 2	428	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	82	330	-	-	734
Mov Cap-2 Maneuver	82	-	-	-	-
Stage 1	335	-	-	-	-
Stage 2	428	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	71.4	0	0.8
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	169	734
HCM Lane V/C Ratio	-	-	0.747	0.086
HCM Control Delay (s)	-	-	71.4	10.4
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	4.7	0.3

Intersection						
Int Delay, s/veh	2.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	34	13	724	35	15	656
Future Vol, veh/h	34	13	724	35	15	656
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	79	79	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	45	17	916	44	18	781

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1755	938	0	0	960
Stage 1	938	-	-	-	-
Stage 2	817	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	94	321	-	-	717
Stage 1	381	-	-	-	-
Stage 2	434	-	-	-	-
Platoon blocked, %					
Mov Cap-1 Maneuver	90	321	-	-	717
Mov Cap-2 Maneuver	90	-	-	-	-
Stage 1	364	-	-	-	-
Stage 2	434	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	71.9	0	0.2
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	112	717
HCM Lane V/C Ratio	-	-	0.56	0.025
HCM Control Delay (s)	-	-	71.9	10.1
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	2.7	0.1

Intersection						
Int Delay, s/veh	1.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	13	2	4	76	65	18
Future Vol, veh/h	13	2	4	76	65	18
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	44	44	77	77	70	70
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	30	5	5	99	93	26

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	215	106	119	0	0
Stage 1	106	-	-	-	-
Stage 2	109	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	773	948	1469	-	-
Stage 1	918	-	-	-	-
Stage 2	916	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	770	948	1469	-	-
Mov Cap-2 Maneuver	770	-	-	-	-
Stage 1	914	-	-	-	-
Stage 2	916	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	9.8	0.4	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1469	-	790	-	-
HCM Lane V/C Ratio	0.004	-	0.043	-	-
HCM Control Delay (s)	7.5	0	9.8	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0.1	-	-

Intersection						
Int Delay, s/veh	2.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	45	8	8	439	529	48
Future Vol, veh/h	45	8	8	439	529	48
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	58	58	85	85	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	78	14	9	516	630	57

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1193	659	687	0	-	0
Stage 1	659	-	-	-	-	-
Stage 2	534	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	206	464	907	-	-	-
Stage 1	515	-	-	-	-	-
Stage 2	588	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	203	464	907	-	-	-
Mov Cap-2 Maneuver	203	-	-	-	-	-
Stage 1	508	-	-	-	-	-
Stage 2	588	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	32.1	0.2	0
HCM LOS	D		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	907	-	222	-	-
HCM Lane V/C Ratio	0.01	-	0.412	-	-
HCM Control Delay (s)	9	0	32.1	-	-
HCM Lane LOS	A	A	D	-	-
HCM 95th %tile Q(veh)	0	-	1.9	-	-

Intersection						
Int Delay, s/veh	3.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	8	45	33	14	29	34
Future Vol, veh/h	8	45	33	14	29	34
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	81	81	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	48	41	17	48	56

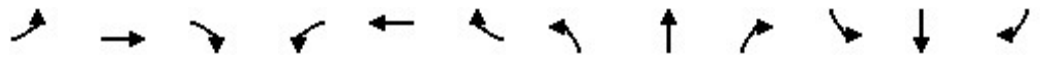
Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	202	50	0	0	58
Stage 1	50	-	-	-	-
Stage 2	152	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	787	1018	-	-	1546
Stage 1	972	-	-	-	-
Stage 2	876	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	762	1018	-	-	1546
Mov Cap-2 Maneuver	762	-	-	-	-
Stage 1	941	-	-	-	-
Stage 2	876	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	8.9	0	3.4
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	969	1546
HCM Lane V/C Ratio	-	-	0.059	0.031
HCM Control Delay (s)	-	-	8.9	7.4
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0.1

Lanes, Volumes, Timings
3: Maplewood Ave & Deer St

K0076-19 111 Maplewood Ave, Portsmouth HH
2030 Build

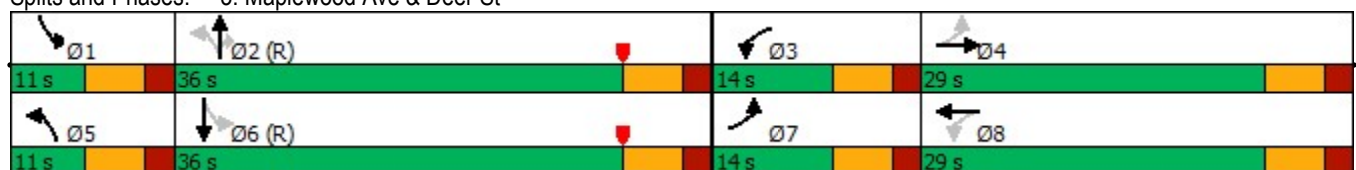


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	287	161	86	308	151	94	52	502	238	75	434	164
Future Volume (vph)	287	161	86	308	151	94	52	502	238	75	434	164
Peak Hour Factor	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Shared Lane Traffic (%)												
Lane Group Flow (vph)	435	374	0	405	323	0	60	577	274	91	729	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Perm	pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2		2	6		
Detector Phase	7	4		3	8		5	2	2	1	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	10.0	10.0	5.0	10.0	
Minimum Split (s)	11.0	29.0		11.0	29.0		11.0	28.0	28.0	11.0	25.0	
Total Split (s)	14.0	29.0		14.0	29.0		11.0	36.0	36.0	11.0	36.0	
Total Split (%)	15.6%	32.2%		15.6%	32.2%		12.2%	40.0%	40.0%	12.2%	40.0%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag	Lag	Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes	Yes	Yes	Yes	
Recall Mode	None	None		None	None		None	C-Max	C-Max	None	C-Max	
v/c Ratio	1.60	0.85		1.61	0.71		0.33	0.85	0.35	0.43	1.09	
Control Delay	308.5	48.6		313.0	37.3		18.9	41.8	4.2	20.7	91.7	
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay	308.5	48.6		313.0	37.3		18.9	41.8	4.2	20.7	91.7	
Queue Length 50th (ft)	~261	183		~260	148		18	318	0	29	~512	
Queue Length 95th (ft)	#253	186		#340	188		40	#501	46	51	#633	
Internal Link Dist (ft)		283			373			505			151	
Turn Bay Length (ft)												
Base Capacity (vph)	272	487		252	502		183	680	788	212	669	
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	
Reduced v/c Ratio	1.60	0.77		1.61	0.64		0.33	0.85	0.35	0.43	1.09	























Intersection Summary

Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 41 (46%), Referenced to phase 2:NBTL and 6:SBTL, Start of Yellow
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 3: Maplewood Ave & Deer St



HCM Signalized Intersection Capacity Analysis K0076-19 111 Maplewood Ave, Portsmouth HH
 3: Maplewood Ave & Deer St 2030 Build

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	287	161	86	308	151	94	52	502	238	75	434	164
Future Volume (vph)	287	161	86	308	151	94	52	502	238	75	434	164
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	10	13	13	12	14	14	11	11	13	11	11	11
Total Lost time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Frt	1.00	0.95		1.00	0.94		1.00	1.00	0.85	1.00	0.96	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00	1.00	0.95	1.00	
Satd. Flow (prot)	1652	1824		1770	1873		1711	1801	1636	1711	1727	
Flt Permitted	0.32	1.00		0.22	1.00		0.12	1.00	1.00	0.16	1.00	
Satd. Flow (perm)	548	1824		414	1873		220	1801	1636	288	1727	
Peak-hour factor, PHF	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Adj. Flow (vph)	435	244	130	405	199	124	60	577	274	91	529	200
RTOR Reduction (vph)	0	22	0	0	25	0	0	0	174	0	15	0
Lane Group Flow (vph)	435	352	0	405	298	0	60	577	100	91	714	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Perm	pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2		2	6		
Actuated Green, G (s)	28.7	20.7		28.7	20.7		37.1	32.8	32.8	37.5	33.0	
Effective Green, g (s)	28.7	20.7		28.7	20.7		37.1	32.8	32.8	37.5	33.0	
Actuated g/C Ratio	0.32	0.23		0.32	0.23		0.41	0.36	0.36	0.42	0.37	
Clearance Time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Vehicle Extension (s)	3.0	3.0		3.0	4.0		3.0	3.0	3.0	3.0	3.0	
Lane Grp Cap (vph)	272	419		252	430		161	656	596	191	633	
v/s Ratio Prot	0.14	0.19		c0.14	0.16		0.02	0.32		c0.02	c0.41	
v/s Ratio Perm	0.37			c0.37			0.14		0.06	0.17		
v/c Ratio	1.60	0.84		1.61	0.69		0.37	0.88	0.17	0.48	1.13	
Uniform Delay, d1	29.0	33.1		28.0	31.7		20.9	26.8	19.4	19.0	28.5	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Incremental Delay, d2	286.3	13.7		291.0	5.1		1.5	15.6	0.6	1.9	76.7	
Delay (s)	315.3	46.8		318.9	36.9		22.3	42.3	20.0	20.9	105.2	
Level of Service	F	D		F	D		C	D	B	C	F	
Approach Delay (s)		191.2			193.8			34.3			95.9	
Approach LOS		F			F			C			F	
Intersection Summary												
HCM 2000 Control Delay			124.1				HCM 2000 Level of Service			F		
HCM 2000 Volume to Capacity ratio			1.29									
Actuated Cycle Length (s)			90.0				Sum of lost time (s)			24.0		
Intersection Capacity Utilization			87.8%				ICU Level of Service			E		
Analysis Period (min)			15									
c Critical Lane Group												

Intersection						
Int Delay, s/veh	2.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↖	↗		↘	
Traffic Vol, veh/h	15	91	53	18	14	19
Future Vol, veh/h	15	91	53	18	14	19
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	68	68	67	67	50	50
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	22	134	79	27	28	38

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	106	0	-	0	271 93
Stage 1	-	-	-	-	93 -
Stage 2	-	-	-	-	178 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1485	-	-	-	718 964
Stage 1	-	-	-	-	931 -
Stage 2	-	-	-	-	853 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1485	-	-	-	707 964
Mov Cap-2 Maneuver	-	-	-	-	707 -
Stage 1	-	-	-	-	916 -
Stage 2	-	-	-	-	853 -

Approach	EB	WB	SB
HCM Control Delay, s	1.1	0	9.7
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1485	-	-	-	835
HCM Lane V/C Ratio	0.015	-	-	-	0.079
HCM Control Delay (s)	7.5	0	-	-	9.7
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.3

Intersection						
Int Delay, s/veh	27.1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	389	57	72	52	45	566
Future Vol, veh/h	389	57	72	52	45	566
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	88	88	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	432	63	82	59	50	629

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	141	0	-	0	1039 112
Stage 1	-	-	-	-	112 -
Stage 2	-	-	-	-	927 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1442	-	-	-	255 941
Stage 1	-	-	-	-	913 -
Stage 2	-	-	-	-	385 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1442	-	-	-	176 941
Mov Cap-2 Maneuver	-	-	-	-	176 -
Stage 1	-	-	-	-	629 -
Stage 2	-	-	-	-	385 -

Approach	EB	WB	SB
HCM Control Delay, s	7.5	0	47
HCM LOS			E

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1442	-	-	-	713
HCM Lane V/C Ratio	0.3	-	-	-	0.952
HCM Control Delay (s)	8.6	0	-	-	47
HCM Lane LOS	A	A	-	-	E
HCM 95th %tile Q(veh)	1.3	-	-	-	14

Intersection						
Int Delay, s/veh	195.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↘	↗		↑	↑	↗
Traffic Vol, veh/h	492	8	0	489	353	570
Future Vol, veh/h	492	8	0	489	353	570
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	71	71	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	607	10	0	689	420	679

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1109	420	-	0	-	0
Stage 1	420	-	-	-	-	-
Stage 2	689	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	~ 232	633	0	-	-	-
Stage 1	663	-	0	-	-	-
Stage 2	~ 498	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	~ 232	633	-	-	-	-
Mov Cap-2 Maneuver	~ 232	-	-	-	-	-
Stage 1	663	-	-	-	-	-
Stage 2	~ 498	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s/\$	760.8	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	-	232	633	-	-
HCM Lane V/C Ratio	-	2.618	0.016	-	-
HCM Control Delay (s)	-	\$ 773	10.8	-	-
HCM Lane LOS	-	F	B	-	-
HCM 95th %tile Q(veh)	-	51.4	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection						
Int Delay, s/veh	15.7					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	35	110	766	28	75	637
Future Vol, veh/h	35	110	766	28	75	637
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	44	138	958	35	84	716

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1860	976	0	0	993
Stage 1	976	-	-	-	-
Stage 2	884	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	81	305	-	-	696
Stage 1	365	-	-	-	-
Stage 2	404	-	-	-	-
Platoon blocked, %					
Mov Cap-1 Maneuver	65	305	-	-	696
Mov Cap-2 Maneuver	65	-	-	-	-
Stage 1	292	-	-	-	-
Stage 2	404	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	165.8	0	1.1
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	161	696
HCM Lane V/C Ratio	-	-	1.126	0.121
HCM Control Delay (s)	-	-	165.8	10.9
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	9.6	0.4

Intersection						
Int Delay, s/veh	7.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	49	22	769	44	17	666
Future Vol, veh/h	49	22	769	44	17	666
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	79	79	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	65	29	973	56	20	793

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1834	1001	0	0	1029
Stage 1	1001	-	-	-	-
Stage 2	833	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	84	295	-	-	675
Stage 1	355	-	-	-	-
Stage 2	427	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	80	295	-	-	675
Mov Cap-2 Maneuver	80	-	-	-	-
Stage 1	336	-	-	-	-
Stage 2	427	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	143.4	0	0.3
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	103	675
HCM Lane V/C Ratio	-	-	0.919	0.03
HCM Control Delay (s)	-	-	143.4	10.5
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	5.5	0.1

Intersection						
Int Delay, s/veh	3.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	33	21	13	88	81	21
Future Vol, veh/h	33	21	13	88	81	21
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	44	44	77	77	70	70
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	75	48	17	114	116	30

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	279	131	146	0	0
Stage 1	131	-	-	-	-
Stage 2	148	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	711	919	1436	-	-
Stage 1	895	-	-	-	-
Stage 2	880	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	702	919	1436	-	-
Mov Cap-2 Maneuver	702	-	-	-	-
Stage 1	883	-	-	-	-
Stage 2	880	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10.5	1	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1436	-	773	-	-
HCM Lane V/C Ratio	0.012	-	0.159	-	-
HCM Control Delay (s)	7.5	0	10.5	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0.6	-	-

Intersection						
Int Delay, s/veh	3.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	56	8	8	455	532	54
Future Vol, veh/h	56	8	8	455	532	54
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	58	58	85	85	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	97	14	9	535	633	64

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	1218	665	697	0	0
Stage 1	665	-	-	-	-
Stage 2	553	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	199	460	899	-	-
Stage 1	511	-	-	-	-
Stage 2	576	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	196	460	899	-	-
Mov Cap-2 Maneuver	196	-	-	-	-
Stage 1	504	-	-	-	-
Stage 2	576	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	39.4	0.2	0
HCM LOS	E		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	899	-	211	-	-
HCM Lane V/C Ratio	0.01	-	0.523	-	-
HCM Control Delay (s)	9	0	39.4	-	-
HCM Lane LOS	A	A	E	-	-
HCM 95th %tile Q(veh)	0	-	2.7	-	-

Intersection						
Int Delay, s/veh	2.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	8	51	51	14	10	61
Future Vol, veh/h	8	51	51	14	10	61
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	81	81	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	55	63	17	16	100

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	204	72	0	0	80	0
Stage 1	72	-	-	-	-	-
Stage 2	132	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	784	990	-	-	1518	-
Stage 1	951	-	-	-	-	-
Stage 2	894	-	-	-	-	-
Platoon blocked, %			-	-		
Mov Cap-1 Maneuver	775	990	-	-	1518	-
Mov Cap-2 Maneuver	775	-	-	-	-	-
Stage 1	941	-	-	-	-	-
Stage 2	894	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9	0	1
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	954	1518
HCM Lane V/C Ratio	-	-	0.066	0.011
HCM Control Delay (s)	-	-	9	7.4
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0

APPENDIX H

111 Maplewood Avenue Traffic Evaluation
Response to Comments

Responses to TAC Traffic Comments Proposed Office Building at 111 Maplewood Avenue Portsmouth, NH

To: Eric Eby, PE
Parking and Transportation Engineer
Department of Public Works
City of Portsmouth, NH

FROM: Vinod Kalikiri, PE, PTOE

DATE: April 18, 2019

Tighe & Bond prepared a detailed traffic evaluation dated March 18, 2019 for the above referenced project as part of the Site Review and Subdivision submittal to the City of Portsmouth Technical Advisory Committee (TAC). This memorandum includes revised analysis based on feedback provided by the City Traffic Engineer on the original traffic study.

Specifically, the following revisions were made to the original analysis and the underlying analytical assumptions.

- Traffic diversion assumptions related to the US Route 1 Bypass Bridge project were removed from the No-Build and Build analysis.
- Future conditions traffic associated with the Deer Street Associates (DSA) development and the Harbor Corp Redevelopment, as well as any traffic improvements by the two projects within the study area were assumed to be in place only in the 2030 analysis.
- Trip distribution assumptions for the commercial component of the project were revised to be consistent with the corresponding assumptions included in the DSA traffic study.
- In addition, newly available permanent traffic count station data from NHDOT were reviewed to confirm if the seasonal adjustment factor used in the original study was too high. The seasonal adjustment factor was not revised based on the review of the new permanent count station data.

Revised capacity analysis summary tabulation is presented in Tables 1 and 2 for signalized and unsignalized study intersections, respectively. Also included in the attachment to this memorandum are revised traffic volume networks and Synchro analysis worksheets resulting from the above outlined revisions.

Overall, while the revisions to the analytical assumptions changed some of the traffic volumes, the overall finding of the original study that certain movements at the study locations are expected to be constrained with or without the project related traffic remains valid. A review of the analysis results indicated that the exclusive pedestrian phase at the intersection significantly contributes to the reduced capacity. As part of the Maplewood Avenue corridor road diet project, newer signal timing may be implemented by the City's signal design consultant which may be better suited for the future conditions. Since new signal timings are not yet available, analysis of the 2020 No-Build and Build conditions were based on existing timings provided by the City. It is unclear if the road diet project will also include replacement of the exclusive pedestrian phase with a concurrent phase. Signal timing changes and/or phasing changes as part of the road diet project has the potential to provide some capacity enhancement at the intersection in the short term. As discussed in the original study, signal

phasing and geometric improvements are also proposed by other private development projects in the longer term, which will provide additional capacity at the intersection.

Compared to the area roadway traffic volumes, the additional traffic estimated for the project at the various study intersections, including the Maplewood Avenue/Deer Street signalized intersection, is nominal. The Site Plans show the elimination of one of the unsignalized curb cuts for the east parcel, which promotes access management. Further, as shown in the Site Plans, the project will implement significant enhancements to the pedestrian accommodations around the Site. The limited additional traffic estimated for the project do not warrant any significant capacity enhancements at study intersections. The proponent will continue to work with the City staff during the project review to further refine the proposed pedestrian and streetscape enhancements to the area.

\\tighebond.com\data\Data\Projects\K\K0076 The Kane Company - General Proposals\0076-019 Maplewood\Traffic\Memos\2019-04-16 Traffic Responses.docx

TABLE 1: Signalized Intersection Operations Summary

Intersection / Lane Group	2020 No Build					2020 Build					2030 No Build					2030 Build				
	V/C	Del	LOS	50 th Q	95 th Q	V/C	Del	LOS	50 th Q	95 th Q	V/C	Del	LOS	50 th Q	95 th Q	V/C	Del	LOS	50 th Q	95 th Q
Maplewood Ave / Deer St																				
Deer St EBL/T/R	1.14	>120	F	~274	#274	>1.2	>120	F	~465	#430	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA
Deer St EBL	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	>1.2	>120	F	~205	#194	>1.2	>120	F	~261	#253
Deer St EBT/R	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	0.78	43	D	161	162	0.84	47	D	183	186
Deer St WBL	>1.2	>120	F	~264	#340	>1.2	>120	F	~306	#381	>1.2	>120	F	~280	#328	>1.2	>120	F	~335	#405
Deer St WBT/R	0.41	36	D	109	146	0.42	36	D	113	149	0.73	40	D	151	185	0.69	37	D	148	188
Maplewood Ave NBL	0.04	12	B	5	16	0.05	12	B	7	19	0.33	21	C	16	38	0.37	22	C	18	40
Maplewood Ave NBT	0.50	18	B	210	296	0.51	18	B	220	309	0.84	38	D	313	#492	0.89	44	D	327	#514
Maplewood Ave NBR	0.14	13	B	0	33	0.14	13	B	2	35	0.17	19	B	0	46	0.17	20	B	0	46
Maplewood Ave SBL	0.14	9	A	15	28	0.14	9	A	15	28	0.42	19	B	27	51	0.49	21	C	29	51
Maplewood Ave SBT/R	0.48	11	B	178	220	0.49	12	B	183	226	1.02	67	E	~439	#573	1.07	84	F	~463	#585
<i>Overall Intersection</i>	0.77	60	E			0.88	111	F			>1.2	106	F			>1.2	>120	F		

LOS level-of-service
 Del Average intersection delay, measured in seconds
 v/c Volume to capacity ratio
 50th Q and 95th Q Percentile queues measured in feet
 # 95th percentile volume exceeds capacity, queue may be longer
 ~ Volume exceeds capacity. Queues are shown after two signal cycles

TABLE 2: Unsignalized Intersection Operations Summary

Intersection / Lane Group	2020 No Build				2020 Build				2030 No Build				2030 Build			
	V/C	Del	LOS	95 th Q	V/C	Del	LOS	95 th Q	V/C	Del	LOS	95 th Q	V/C	Del	LOS	95 th Q
Maplewood Ave / Raynes Ave:																
Maplewood Ave SBL/T	0.1	10	A	0.2	0.1	10	A	0.3	0.1	10	B	0.3	0.1	11	B	0.4
Raynes Ave WBL/R	0.4	26	D	1.9	0.5	32	D	2.9	0.7	70	F	4.6	0.9	107	F	7.0
Maplewood Ave / Kennebunk Bank Driveway:																
Maplewood Ave SBL/T	0.0	9	A	0.0	NA	NA	NA	NA	0.0	10	A	0.0	NA	NA	NA	NA
Kennebunk Bank WBL/R	0.1	18	C	0.2	NA	NA	NA	NA	0.1	27	D	0.4	NA	NA	NA	NA
Maplewood Ave / Vaughan St:																
Maplewood Ave SBL/T	0.0	9	A	0.1	0.0	10	A	0.1	0.0	10	B	0.1	0.0	11	B	0.1
Vaughan St WBL/R	0.3	30	D	1.2	0.6	47	E	2.9	0.5	68	F	2.6	1.0	>120	F	6.5
Vaughan St / Kennebunk Bank Driveway:																
Vaughan St EBL/T	0.0	7	A	0.0	0.0	8	A	0.0	0.0	7	A	0.0	0.0	8	A	0.0
Kennebunk Bank SBL/R	0.0	9	A	0.0	0.1	10	A	0.2	0.0	9	A	0.0	0.1	10	A	0.2
Vaughan St / Green St:																
Vaughan St SBL/T	0.1	7	A	0.1	0.0	8	A	0.1	0.0	7	A	0.1	0.0	8	A	0.1
Green St WBL/R	0.2	9	A	0.2	0.1	9	A	0.2	0.1	9	A	0.2	0.1	9	A	0.2
Vaughan St / Site Driveway:																
Vaughan St NBL/T	0.0	7	A	0.0	0.0	8	A	0.1	0.0	8	A	0.0	0.0	8	A	0.1
Site Driveway EBL/R	0.0	10	A	0.1	0.1	10.2	B	0.5	0.0	10	A	0.1	0.2	10	B	0.5
Deer St / Russell St:																
Deer St EBL/T	0.2	8	A	0.7	0.2	8	A	0.8	0.3	9	A	1.2	0.3	9	A	1.3
Russell St SBL/R	0.5	13	B	2.8	0.5	14	B	3.4	1.0	47	E	14.3	1.0	58	F	17.0
Green St / Russell St:																
Russell St NBL/T	0.0	9	A	0.0	0.0	9	A	0.0	0.0	9	A	0.0	0.0	9	A	0.0
Green St EBL	0.2	19	C	1.0	0.3	22	C	1.3	0.4	32	D	1.9	0.5	40	E	2.7
Russell St / Market St:																
Russell St EBL	>1.2	>120	F	24.4	>1.2	>120	F	27.9	>1.2	>120	F	47.5	>1.2	>120	F	51.2
Russell St EBR	0.0	11	B	0.0	0.0	11	B	0.0	0.0	11	B	0.0	0.0	11	B	0.0

LOS level-of-service
 Del Average intersection delay, measured in seconds
 v/c Volume to capacity ratio
 95th Q Percentile queues measured in vehicles



Legend



Study Area Location

Proposed Office Building
111 Maplewood Avenue, Portsmouth NH

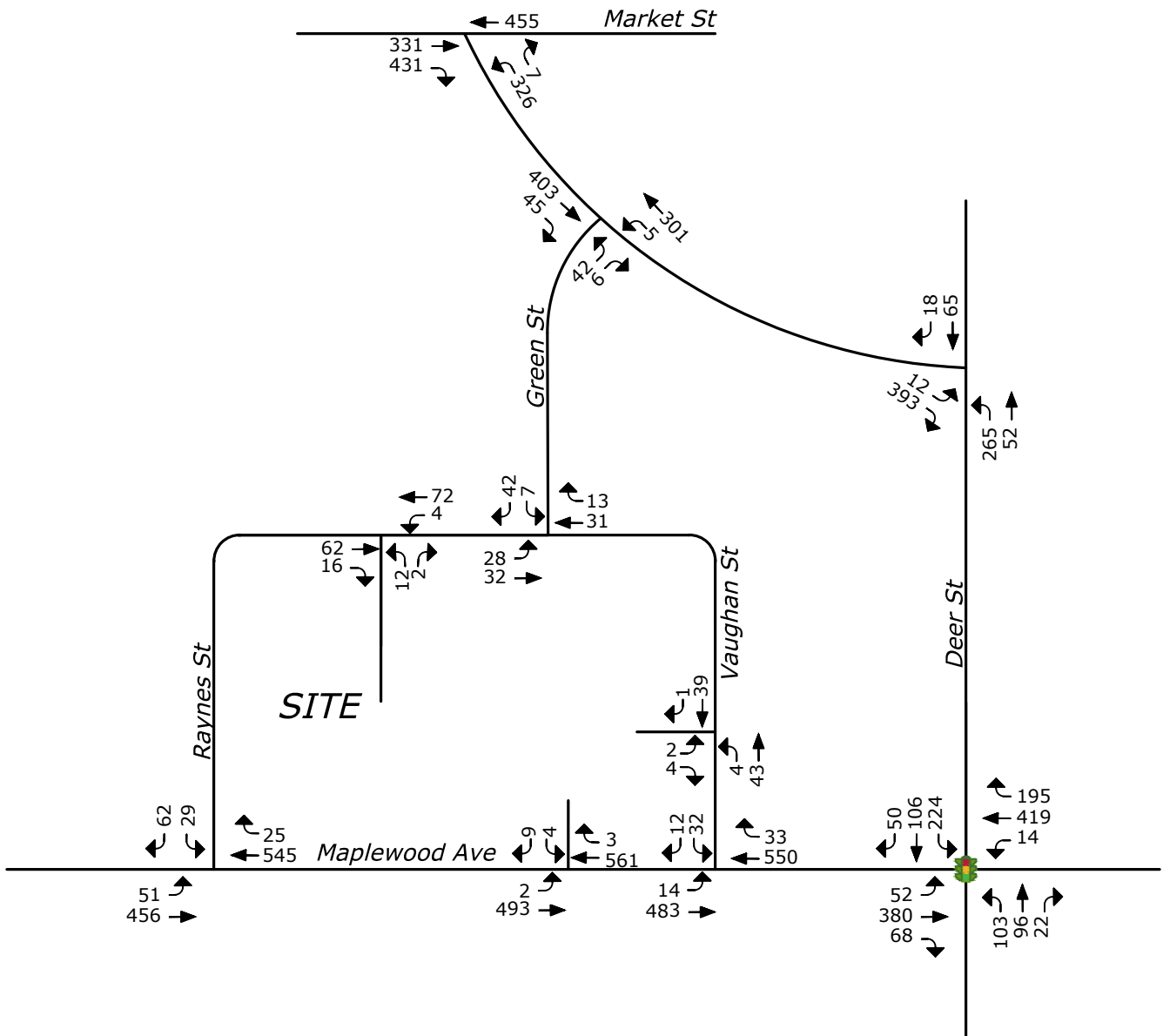
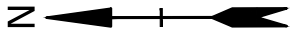
Study Area

DATE: 03/18/2019

SCALE: 1" = 200'

FIGURE 1


Tighe & Bond
www.tighebond.com

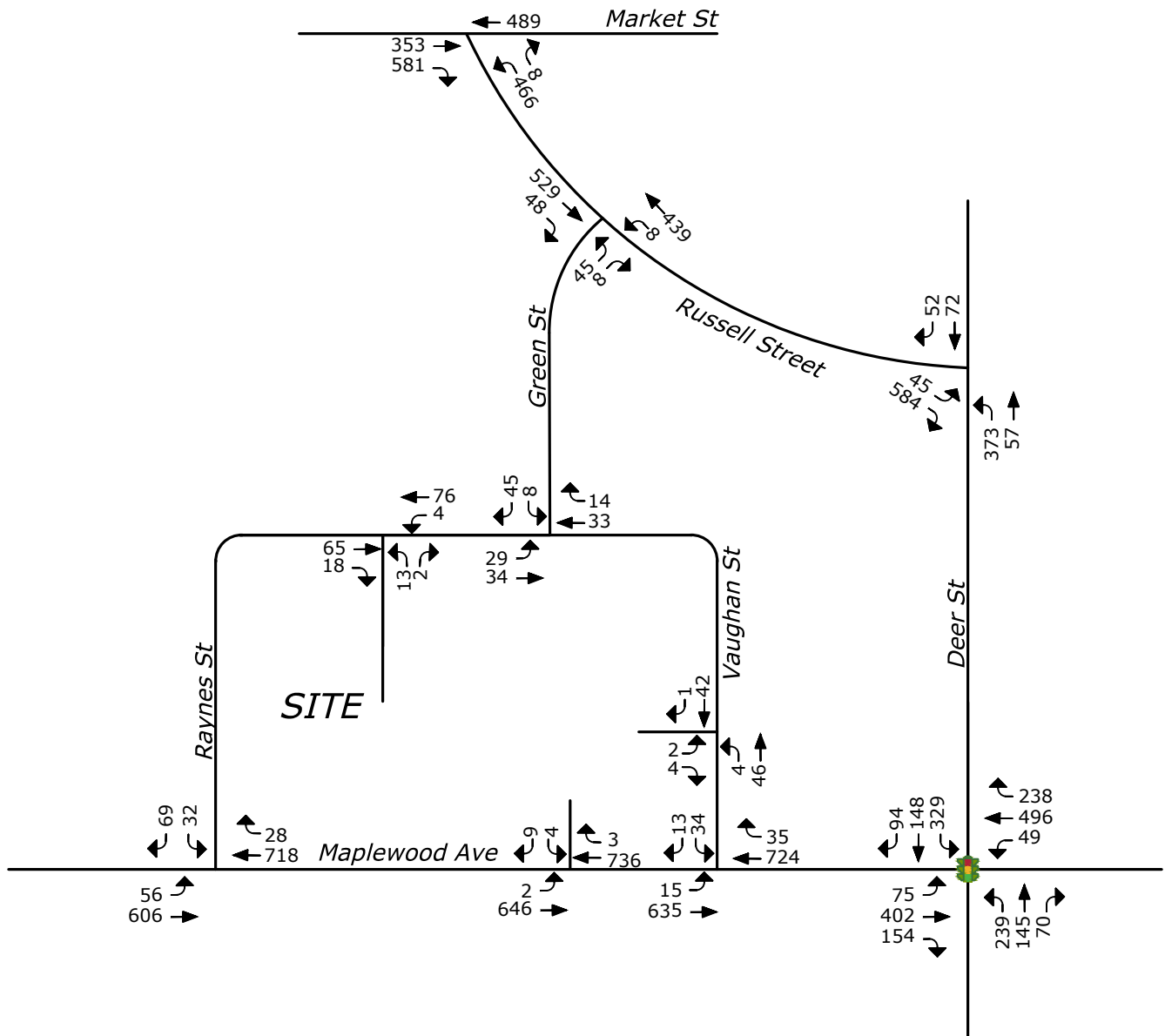
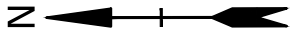


LEGEND



TRAFFIC SIGNAL


Proposed Office Building 111 Maplewood Avenue, Portsmouth NH	
2020 No Build Peak Hour Traffic Volumes	
DATE: 03/18/2019	 www.tighebond.com
SCALE: No Scale	
FIGURE 2	

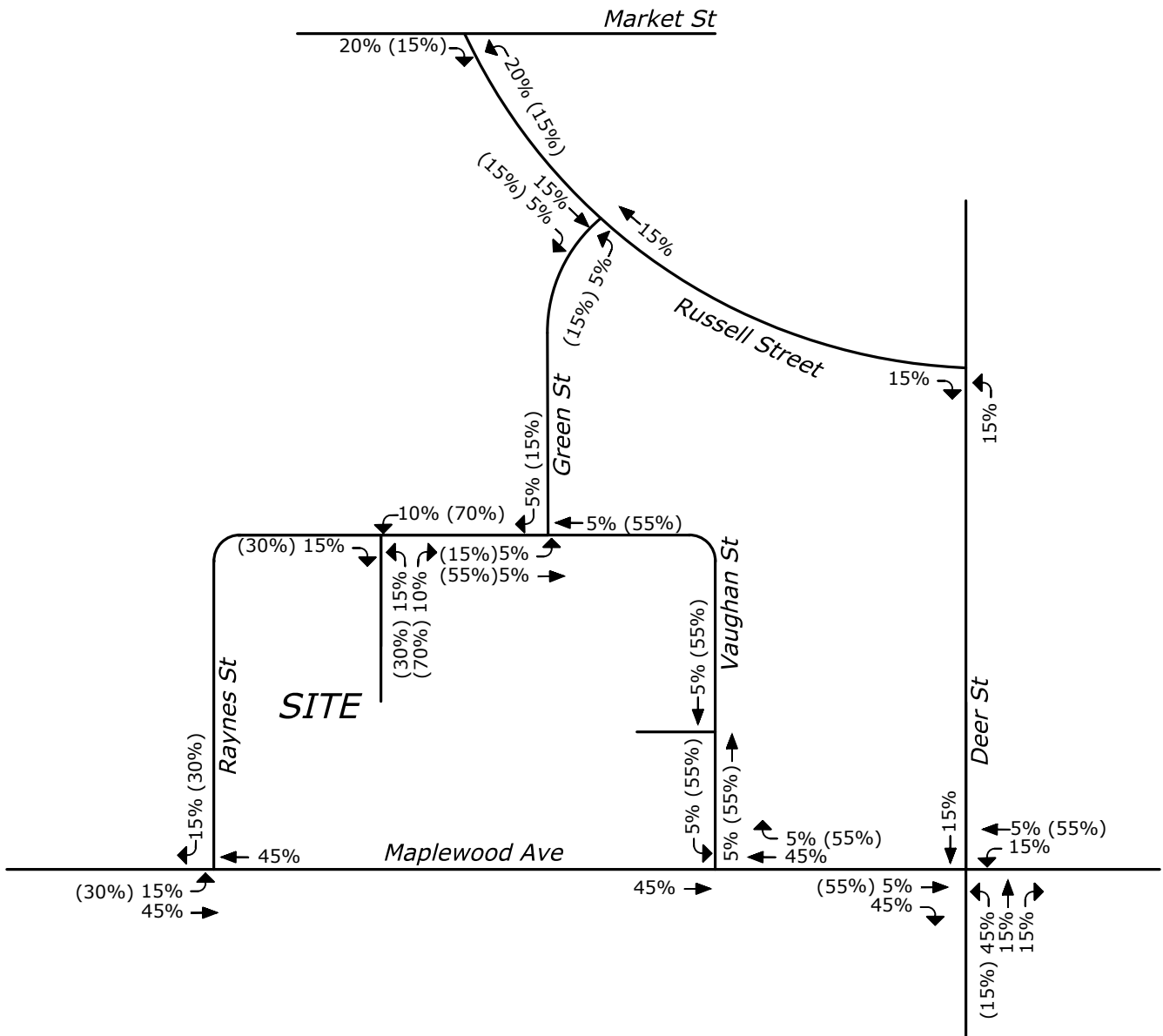
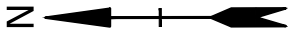


LEGEND




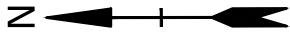
TRAFFIC SIGNAL

Proposed Office Building 111 Maplewood Avenue, Portsmouth NH	
2030 No Build Peak Hour Traffic Volumes	
DATE: 03/18/2019	 www.tighebond.com
SCALE: No Scale	
FIGURE 3	



Apr 11, 2019-11:44am Plotted By: YMayboroda Tighe & Bond, Inc. C:\Users\YMayboroda\appdata\local\temp\AcPublish_5272\Diversion figure - Future_April2019.dwg

Proposed Office Building	
111 Maplewood Avenue, Portsmouth NH	
Trip Distribution	
DATE: 03/18/2019	 www.tighebond.com
SCALE: No Scale	
FIGURE 4	

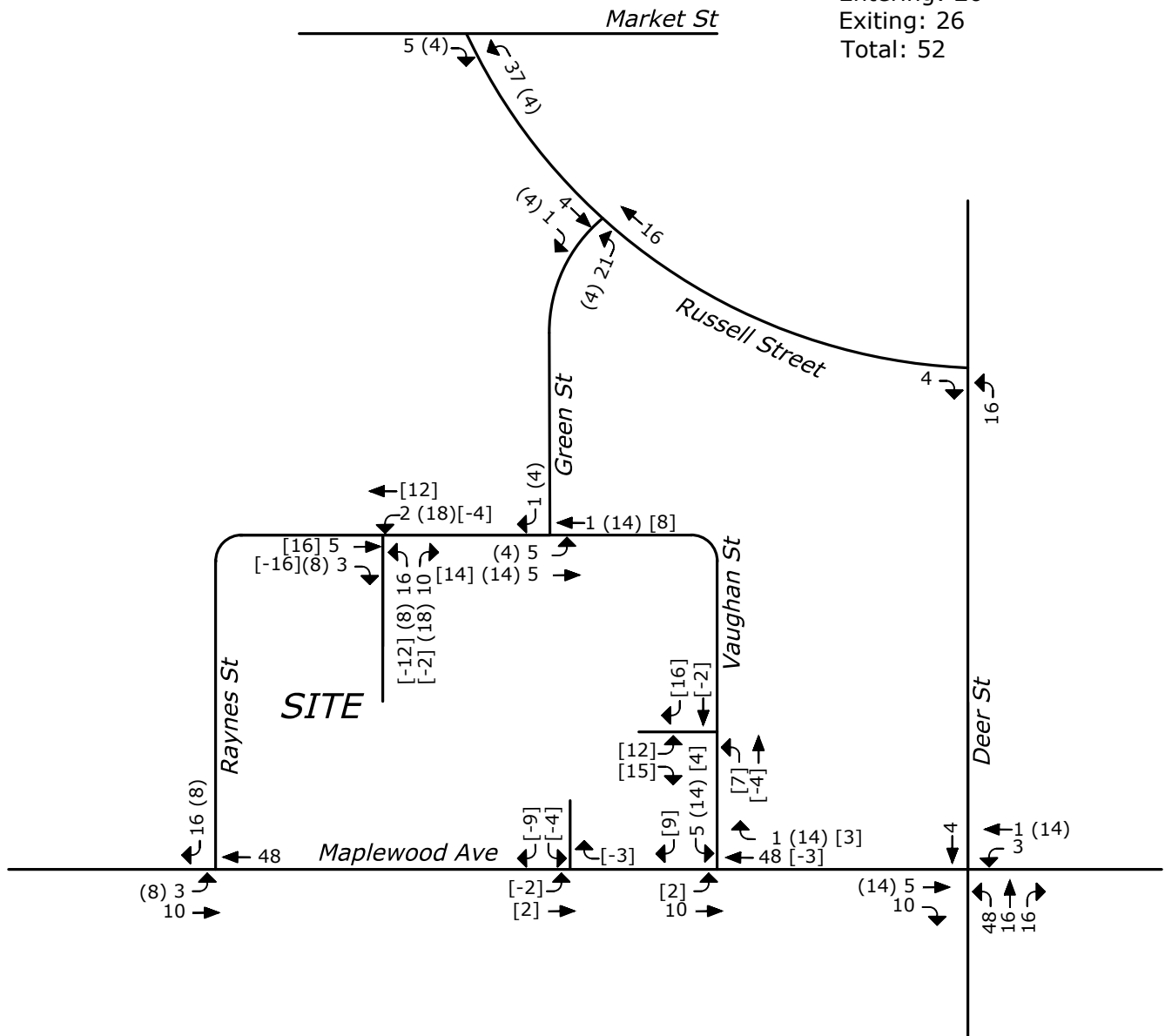


Office Generated Trips

Entering: 23
 Exiting: 106
 Total: 129

Retail Generated Trips

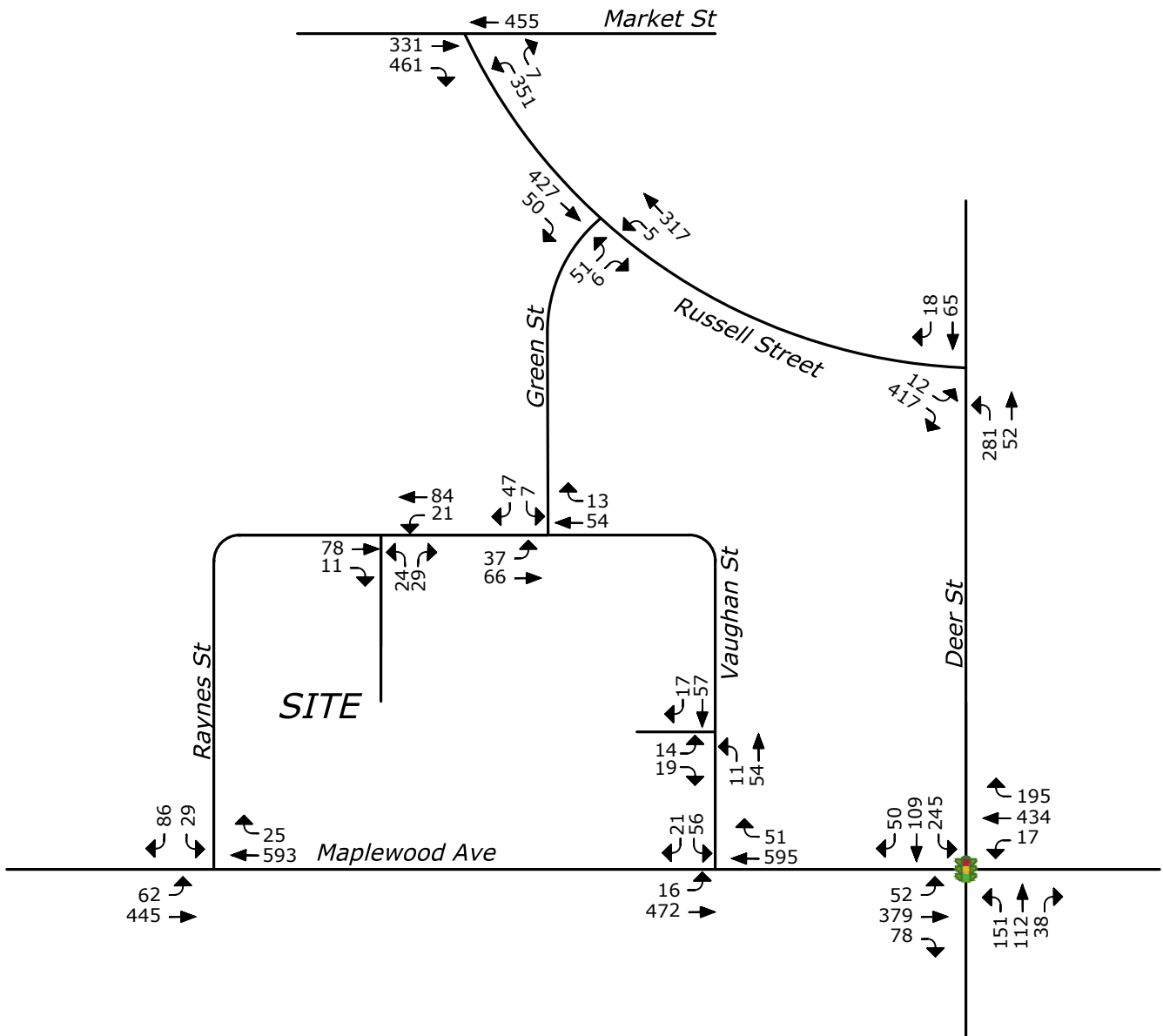
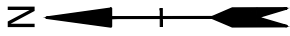
Entering: 26
 Exiting: 26
 Total: 52



LEGEND

- XX Office Trips
- (XX) Retail Trips
- [XX] Driveway Redistribution


<p>Proposed Office Building 111 Maplewood Avenue, Portsmouth NH</p>	
<p>Site Generated Trips</p>	
<p>DATE: 03/18/2019</p> <p>SCALE: No Scale</p> <p>FIGURE 5</p>	<p>www.tighebond.com</p>

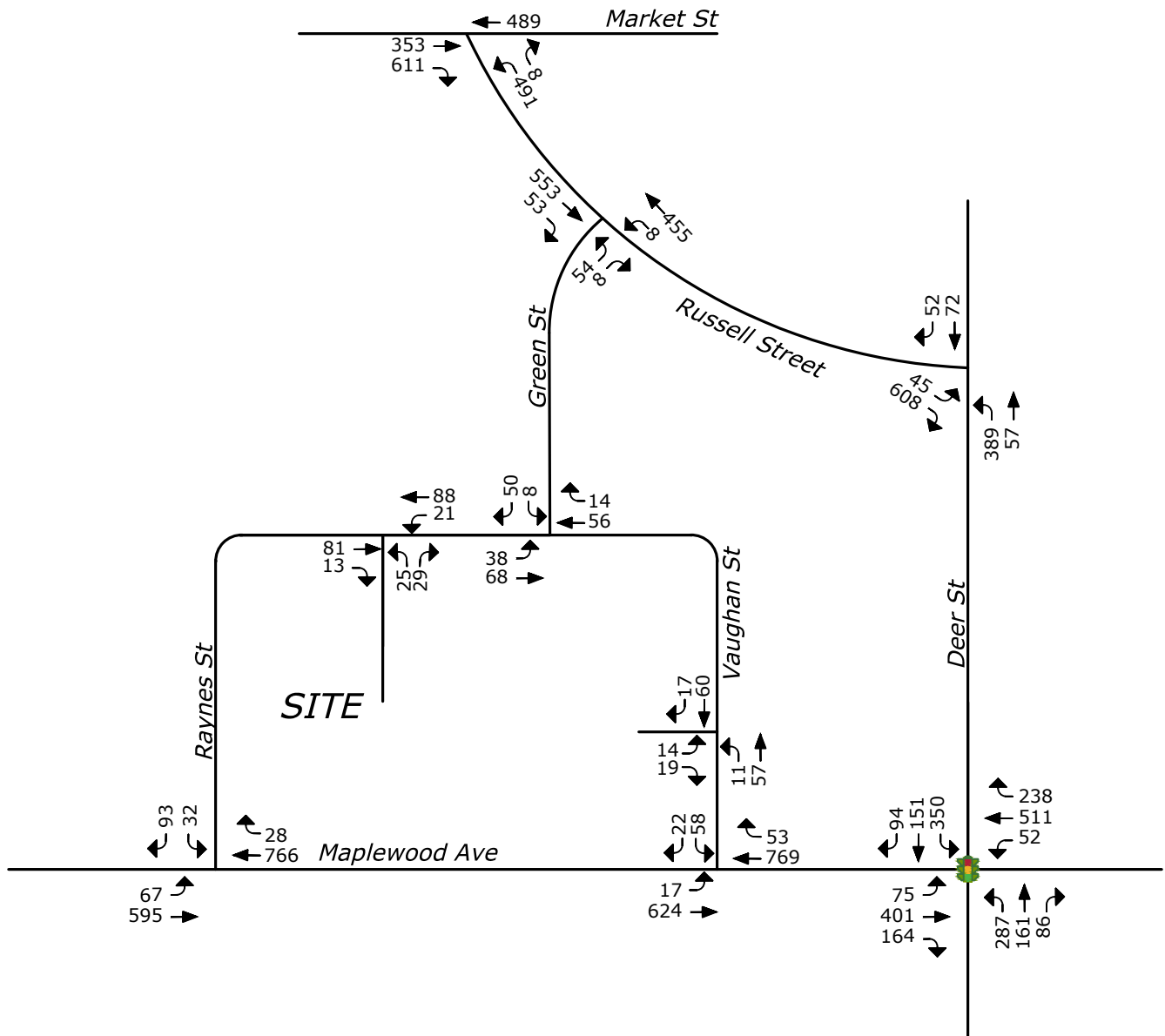
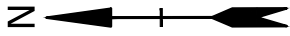


LEGEND



TRAFFIC SIGNAL

Proposed Office Building 111 Maplewood Avenue, Portsmouth NH	
2020 Build Peak Hour Traffic Volumes	
DATE: 03/18/2019	 www.tighebond.com
SCALE: No Scale	
FIGURE 6	



LEGEND



TRAFFIC SIGNAL

Proposed Office Building 111 Maplewood Avenue, Portsmouth NH	
2030 Build Peak Hour Traffic Volumes	
DATE: 03/18/2019	 www.tighebond.com
SCALE: No Scale	
FIGURE 7	

Capacity Analysis Worksheets

Lanes, Volumes, Timings
3: Maplewood Ave & Deer St

K0076-19 111 Maplewood Ave, Portsmouth HH
2020 No Build

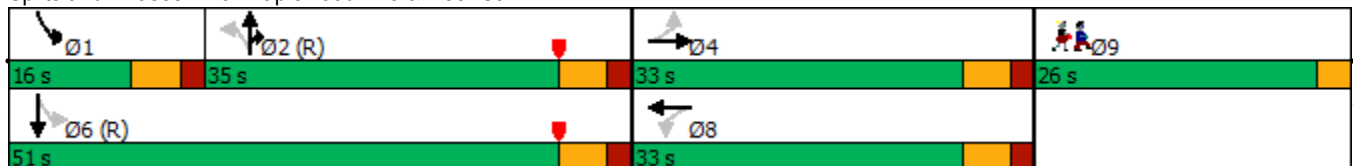


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕		↖	↗		↖	↑	↗	↖	↗	
Traffic Volume (vph)	103	96	22	224	106	50	14	419	195	52	380	68
Future Volume (vph)	103	96	22	224	106	50	14	419	195	52	380	68
Peak Hour Factor	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	334	0	295	205	0	16	482	224	63	546	0
Turn Type	Perm	NA		Perm	NA		Perm	NA	Prot	pm+pt	NA	
Protected Phases		4			8			2	2	1	6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		2	2	2	1	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		10.0	10.0	10.0	5.0	10.0	
Minimum Split (s)	11.0	11.0		11.0	11.0		16.0	16.0	16.0	11.0	16.0	
Total Split (s)	33.0	33.0		33.0	33.0		35.0	35.0	35.0	16.0	51.0	
Total Split (%)	30.0%	30.0%		30.0%	30.0%		31.8%	31.8%	31.8%	14.5%	46.4%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)		6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lead/Lag							Lag	Lag	Lag	Lead		
Lead-Lag Optimize?							Yes	Yes	Yes	Yes		
Recall Mode	Max	Max		None	None		C-Max	C-Max	C-Max	None	C-Max	
v/c Ratio		1.14		1.27	0.43		0.04	0.49	0.22	0.13	0.48	
Control Delay		134.2		187.5	34.5		13.4	18.3	2.4	7.9	11.5	
Queue Delay		0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay		134.2		187.5	34.5		13.4	18.3	2.4	7.9	11.5	
Queue Length 50th (ft)		~274		~264	109		5	210	0	15	178	
Queue Length 95th (ft)		#274		#340	146		16	296	33	28	220	
Internal Link Dist (ft)		283			373			505			151	
Turn Bay Length (ft)												
Base Capacity (vph)		293		232	480		454	988	998	503	1138	
Starvation Cap Reductn		0		0	0		0	0	0	0	0	
Spillback Cap Reductn		0		0	0		0	0	0	0	0	
Storage Cap Reductn		0		0	0		0	0	0	0	0	
Reduced v/c Ratio		1.14		1.27	0.43		0.04	0.49	0.22	0.13	0.48	

Intersection Summary






















Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 6 (5%), Referenced to phase 2:NBTL and 6:SBTL, Start of Yellow
 Natural Cycle: 110
 Control Type: Actuated-Coordinated
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 3: Maplewood Ave & Deer St



Lane Group	Ø9
Lane Configurations	
Traffic Volume (vph)	
Future Volume (vph)	
Peak Hour Factor	
Shared Lane Traffic (%)	
Lane Group Flow (vph)	
Turn Type	
Protected Phases	9
Permitted Phases	
Detector Phase	
Switch Phase	
Minimum Initial (s)	1.0
Minimum Split (s)	26.0
Total Split (s)	26.0
Total Split (%)	24%
Yellow Time (s)	3.0
All-Red Time (s)	0.0
Lost Time Adjust (s)	
Total Lost Time (s)	
Lead/Lag	
Lead-Lag Optimize?	
Recall Mode	None
v/c Ratio	
Control Delay	
Queue Delay	
Total Delay	
Queue Length 50th (ft)	
Queue Length 95th (ft)	
Internal Link Dist (ft)	
Turn Bay Length (ft)	
Base Capacity (vph)	
Starvation Cap Reductn	
Spillback Cap Reductn	
Storage Cap Reductn	
Reduced v/c Ratio	
Intersection Summary	

HCM Signalized Intersection Capacity Analysis K0076-19 111 Maplewood Ave, Portsmouth HH
 3: Maplewood Ave & Deer St 2020 No Build

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	103	96	22	224	106	50	14	419	195	52	380	68
Future Volume (vph)	103	96	22	224	106	50	14	419	195	52	380	68
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	10	13	13	12	14	14	11	11	13	11	11	11
Total Lost time (s)		6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lane Util. Factor		1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Frt		0.99		1.00	0.95		1.00	1.00	0.85	1.00	0.98	
Flt Protected		0.98		0.95	1.00		0.95	1.00	1.00	0.95	1.00	
Satd. Flow (prot)		1856		1770	1891		1711	1801	1636	1711	1760	
Flt Permitted		0.62		0.51	1.00		0.46	1.00	1.00	0.35	1.00	
Satd. Flow (perm)		1181		947	1891		829	1801	1636	629	1760	
Peak-hour factor, PHF	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Adj. Flow (vph)	156	145	33	295	139	66	16	482	224	63	463	83
RTOR Reduction (vph)	0	4	0	0	16	0	0	0	103	0	4	0
Lane Group Flow (vph)	0	330	0	295	189	0	16	482	121	63	542	0
Turn Type	Perm	NA		Perm	NA		Perm	NA	Prot	pm+pt	NA	
Protected Phases		4		8	8		2	2	2	1	6	
Permitted Phases	4			8			2			6		
Actuated Green, G (s)		27.0		27.0	27.0		59.2	59.2	59.2	71.0	71.0	
Effective Green, g (s)		27.0		27.0	27.0		59.2	59.2	59.2	71.0	71.0	
Actuated g/C Ratio		0.25		0.25	0.25		0.54	0.54	0.54	0.65	0.65	
Clearance Time (s)		6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Vehicle Extension (s)		3.0		4.0	4.0		3.0	3.0	3.0	3.0	3.0	
Lane Grp Cap (vph)		289		232	464		446	969	880	463	1136	
v/s Ratio Prot					0.10			0.27	0.07	0.01	c0.31	
v/s Ratio Perm		0.28		c0.31			0.02			0.08		
v/c Ratio		1.14		1.27	0.41		0.04	0.50	0.14	0.14	0.48	
Uniform Delay, d1		41.5		41.5	34.8		12.0	16.0	12.7	8.9	10.0	
Progression Factor		1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Incremental Delay, d2		97.2		151.5	0.8		0.2	1.8	0.3	0.1	1.4	
Delay (s)		138.7		193.0	35.6		12.1	17.8	13.0	9.0	11.4	
Level of Service		F		F	D		B	B	B	A	B	
Approach Delay (s)		138.7			128.5			16.2			11.2	
Approach LOS		F			F			B			B	
Intersection Summary												
HCM 2000 Control Delay			59.6			HCM 2000 Level of Service			E			
HCM 2000 Volume to Capacity ratio			0.77									
Actuated Cycle Length (s)			110.0			Sum of lost time (s)			21.0			
Intersection Capacity Utilization			77.0%			ICU Level of Service			D			
Analysis Period (min)			15									
c Critical Lane Group												

Intersection						
Int Delay, s/veh	1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	4	43	39	1	2	4
Future Vol, veh/h	4	43	39	1	2	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	68	68	67	67	50	50
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	6	63	58	1	4	8

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	59	0	-	0	134 59
Stage 1	-	-	-	-	59 -
Stage 2	-	-	-	-	75 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1545	-	-	-	860 1007
Stage 1	-	-	-	-	964 -
Stage 2	-	-	-	-	948 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1545	-	-	-	857 1007
Mov Cap-2 Maneuver	-	-	-	-	857 -
Stage 1	-	-	-	-	960 -
Stage 2	-	-	-	-	948 -

Approach	EB	WB	SB
HCM Control Delay, s	0.6	0	8.8
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1545	-	-	-	951
HCM Lane V/C Ratio	0.004	-	-	-	0.013
HCM Control Delay (s)	7.3	0	-	-	8.8
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection						
Int Delay, s/veh	9					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	265	52	65	18	12	393
Future Vol, veh/h	265	52	65	18	12	393
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	88	88	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	294	58	74	20	13	437

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	94	0	-	0	730 84
Stage 1	-	-	-	-	84 -
Stage 2	-	-	-	-	646 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1500	-	-	-	389 975
Stage 1	-	-	-	-	939 -
Stage 2	-	-	-	-	522 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1500	-	-	-	310 975
Mov Cap-2 Maneuver	-	-	-	-	310 -
Stage 1	-	-	-	-	749 -
Stage 2	-	-	-	-	522 -

Approach	EB	WB	SB
HCM Control Delay, s	6.7	0	12.6
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1500	-	-	-	917
HCM Lane V/C Ratio	0.196	-	-	-	0.491
HCM Control Delay (s)	8	0	-	-	12.6
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0.7	-	-	-	2.8

Intersection						
Int Delay, s/veh	0.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W	R	T	R	L	T
Traffic Vol, veh/h	4	9	561	3	2	493
Future Vol, veh/h	4	9	561	3	2	493
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	58	58	78	78	85	85
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	7	16	719	4	2	580

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	1305	721	0	0	723	0
Stage 1	721	-	-	-	-	-
Stage 2	584	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	177	427	-	-	879	-
Stage 1	482	-	-	-	-	-
Stage 2	557	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	176	427	-	-	879	-
Mov Cap-2 Maneuver	176	-	-	-	-	-
Stage 1	481	-	-	-	-	-
Stage 2	557	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	18.1	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	297	879
HCM Lane V/C Ratio	-	-	0.075	0.003
HCM Control Delay (s)	-	-	18.1	9.1
HCM Lane LOS	-	-	C	A
HCM 95th %tile Q(veh)	-	-	0.2	0

Intersection						
Int Delay, s/veh	63.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↘	↗		↑	↑	↗
Traffic Vol, veh/h	326	6	0	455	331	431
Future Vol, veh/h	326	6	0	455	331	431
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	71	71	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	402	7	0	641	394	513

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1035	394	-	0	-	0
Stage 1	394	-	-	-	-	-
Stage 2	641	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	~ 257	655	0	-	-	-
Stage 1	681	-	0	-	-	-
Stage 2	525	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	~ 257	655	-	-	-	-
Mov Cap-2 Maneuver	~ 257	-	-	-	-	-
Stage 1	681	-	-	-	-	-
Stage 2	525	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	302.5	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	-	257	655	-	-
HCM Lane V/C Ratio	-	1.566	0.011	-	-
HCM Control Delay (s)	-	307.9	10.6	-	-
HCM Lane LOS	-	F	B	-	-
HCM 95th %tile Q(veh)	-	24.4	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection						
Int Delay, s/veh	2.5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	29	62	545	25	51	456
Future Vol, veh/h	29	62	545	25	51	456
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	36	78	681	31	57	512

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1323	697	0	0	712
Stage 1	697	-	-	-	-
Stage 2	626	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	172	441	-	-	888
Stage 1	494	-	-	-	-
Stage 2	533	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	157	441	-	-	888
Mov Cap-2 Maneuver	157	-	-	-	-
Stage 1	450	-	-	-	-
Stage 2	533	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	26.4	0	0.9
HCM LOS	D		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	280	888
HCM Lane V/C Ratio	-	-	0.406	0.065
HCM Control Delay (s)	-	-	26.4	9.3
HCM Lane LOS	-	-	D	A
HCM 95th %tile Q(veh)	-	-	1.9	0.2

Intersection						
Int Delay, s/veh	1.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	32	12	550	33	14	483
Future Vol, veh/h	32	12	550	33	14	483
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	79	79	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	43	16	696	42	17	575

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1326	717	0	0	738
Stage 1	717	-	-	-	-
Stage 2	609	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	172	430	-	-	868
Stage 1	484	-	-	-	-
Stage 2	543	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	167	430	-	-	868
Mov Cap-2 Maneuver	167	-	-	-	-
Stage 1	470	-	-	-	-
Stage 2	543	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	30.3	0	0.3
HCM LOS	D		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	200	868
HCM Lane V/C Ratio	-	-	0.293	0.019
HCM Control Delay (s)	-	-	30.3	9.2
HCM Lane LOS	-	-	D	A
HCM 95th %tile Q(veh)	-	-	1.2	0.1

Intersection						
Int Delay, s/veh	1.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			W	W	
Traffic Vol, veh/h	12	2	4	72	62	16
Future Vol, veh/h	12	2	4	72	62	16
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	44	44	77	77	70	70
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	27	5	5	94	89	23

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	205	101	112	0	0
Stage 1	101	-	-	-	-
Stage 2	104	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	783	954	1478	-	-
Stage 1	923	-	-	-	-
Stage 2	920	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	780	954	1478	-	-
Mov Cap-2 Maneuver	780	-	-	-	-
Stage 1	919	-	-	-	-
Stage 2	920	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	9.7	0.4	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1478	-	801	-	-
HCM Lane V/C Ratio	0.004	-	0.04	-	-
HCM Control Delay (s)	7.4	0	9.7	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0.1	-	-

Intersection						
Int Delay, s/veh	1.7					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			W	W	
Traffic Vol, veh/h	42	7	5	301	403	45
Future Vol, veh/h	42	7	5	301	403	45
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	58	58	85	85	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	72	12	6	354	480	54

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	873	507	534	0	-	0
Stage 1	507	-	-	-	-	-
Stage 2	366	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	321	566	1034	-	-	-
Stage 1	605	-	-	-	-	-
Stage 2	702	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	319	566	1034	-	-	-
Mov Cap-2 Maneuver	319	-	-	-	-	-
Stage 1	601	-	-	-	-	-
Stage 2	702	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	19.1	0.1	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1034	-	340	-	-
HCM Lane V/C Ratio	0.006	-	0.248	-	-
HCM Control Delay (s)	8.5	0	19.1	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0	-	1	-	-

Intersection						
Int Delay, s/veh	3.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	7	42	31	13	28	32
Future Vol, veh/h	7	42	31	13	28	32
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	81	81	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	8	45	38	16	46	52

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	190	46	0	0	54
Stage 1	46	-	-	-	-
Stage 2	144	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	799	1023	-	-	1551
Stage 1	976	-	-	-	-
Stage 2	883	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	774	1023	-	-	1551
Mov Cap-2 Maneuver	774	-	-	-	-
Stage 1	946	-	-	-	-
Stage 2	883	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	8.9	0	3.4
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	978	1551
HCM Lane V/C Ratio	-	-	0.054	0.03
HCM Control Delay (s)	-	-	8.9	7.4
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0.1

Lanes, Volumes, Timings
3: Maplewood Ave & Deer St

K0076-19 111 Maplewood Ave, Portsmouth HH

2020 Build

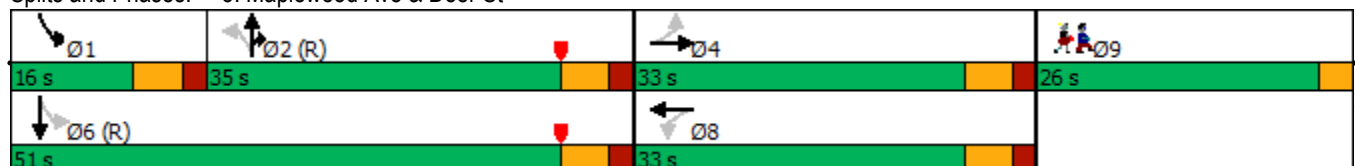


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔		↖	↗		↖	↗	↖	↗	↖	↗
Traffic Volume (vph)	151	112	38	245	109	50	17	434	195	52	379	78
Future Volume (vph)	151	112	38	245	109	50	17	434	195	52	379	78
Peak Hour Factor	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	457	0	322	209	0	20	499	224	63	557	0
Turn Type	Perm	NA		Perm	NA		Perm	NA	Prot	pm+pt	NA	
Protected Phases		4			8			2	2	1	6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		2	2	2	1	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		10.0	10.0	10.0	5.0	10.0	
Minimum Split (s)	11.0	11.0		11.0	11.0		16.0	16.0	16.0	11.0	16.0	
Total Split (s)	33.0	33.0		33.0	33.0		35.0	35.0	35.0	16.0	51.0	
Total Split (%)	30.0%	30.0%		30.0%	30.0%		31.8%	31.8%	31.8%	14.5%	46.4%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)		6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lead/Lag							Lag	Lag	Lag	Lead		
Lead-Lag Optimize?							Yes	Yes	Yes	Yes		
Recall Mode	None	None		None	None		C-Max	C-Max	C-Max	None	C-Max	
v/c Ratio		1.61		1.41	0.44		0.04	0.51	0.22	0.14	0.49	
Control Delay		319.7		240.2	34.9		13.5	18.7	2.6	7.9	11.7	
Queue Delay		0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay		319.7		240.2	34.9		13.5	18.7	2.6	7.9	11.7	
Queue Length 50th (ft)		~465		~306	113		7	220	2	15	183	
Queue Length 95th (ft)		#430		#381	149		19	309	35	28	226	
Internal Link Dist (ft)		283			373			505			151	
Turn Bay Length (ft)												
Base Capacity (vph)		284		229	479		450	988	996	492	1136	
Starvation Cap Reductn		0		0	0		0	0	0	0	0	
Spillback Cap Reductn		0		0	0		0	0	0	0	0	
Storage Cap Reductn		0		0	0		0	0	0	0	0	
Reduced v/c Ratio		1.61		1.41	0.44		0.04	0.51	0.22	0.13	0.49	

Intersection Summary

Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 6 (5%), Referenced to phase 2:NBTL and 6:SBTL, Start of Yellow
 Natural Cycle: 130
 Control Type: Actuated-Coordinated
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 3: Maplewood Ave & Deer St



Lane Group	Ø9
Lane Configurations	
Traffic Volume (vph)	
Future Volume (vph)	
Peak Hour Factor	
Shared Lane Traffic (%)	
Lane Group Flow (vph)	
Turn Type	
Protected Phases	9
Permitted Phases	
Detector Phase	
Switch Phase	
Minimum Initial (s)	1.0
Minimum Split (s)	26.0
Total Split (s)	26.0
Total Split (%)	24%
Yellow Time (s)	3.0
All-Red Time (s)	0.0
Lost Time Adjust (s)	
Total Lost Time (s)	
Lead/Lag	
Lead-Lag Optimize?	
Recall Mode	None
v/c Ratio	
Control Delay	
Queue Delay	
Total Delay	
Queue Length 50th (ft)	
Queue Length 95th (ft)	
Internal Link Dist (ft)	
Turn Bay Length (ft)	
Base Capacity (vph)	
Starvation Cap Reductn	
Spillback Cap Reductn	
Storage Cap Reductn	
Reduced v/c Ratio	
Intersection Summary	

HCM Signalized Intersection Capacity Analysis K0076-19 111 Maplewood Ave, Portsmouth HH
 3: Maplewood Ave & Deer St 2020 Build



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔		↔	↔		↔	↑	↔	↔	↔	↔
Traffic Volume (vph)	151	112	38	245	109	50	17	434	195	52	379	78
Future Volume (vph)	151	112	38	245	109	50	17	434	195	52	379	78
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	10	13	13	12	14	14	11	11	13	11	11	11
Total Lost time (s)		6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lane Util. Factor		1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Frt		0.98		1.00	0.95		1.00	1.00	0.85	1.00	0.97	
Flt Protected		0.98		0.95	1.00		0.95	1.00	1.00	0.95	1.00	
Satd. Flow (prot)		1846		1770	1893		1711	1801	1636	1711	1755	
Flt Permitted		0.60		0.50	1.00		0.46	1.00	1.00	0.34	1.00	
Satd. Flow (perm)		1140		933	1893		820	1801	1636	607	1755	
Peak-hour factor, PHF	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Adj. Flow (vph)	229	170	58	322	143	66	20	499	224	63	462	95
RTOR Reduction (vph)	0	5	0	0	15	0	0	0	101	0	4	0
Lane Group Flow (vph)	0	452	0	322	194	0	20	499	123	63	553	0
Turn Type	Perm	NA		Perm	NA		Perm	NA	Prot	pm+pt	NA	
Protected Phases		4			8			2	2	1	6	
Permitted Phases	4			8			2			6		
Actuated Green, G (s)		27.0		27.0	27.0		59.2	59.2	59.2	71.0	71.0	
Effective Green, g (s)		27.0		27.0	27.0		59.2	59.2	59.2	71.0	71.0	
Actuated g/C Ratio		0.25		0.25	0.25		0.54	0.54	0.54	0.65	0.65	
Clearance Time (s)		6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Vehicle Extension (s)		3.0		3.0	3.0		3.0	3.0	3.0	3.0	3.0	
Lane Grp Cap (vph)		279		229	464		441	969	880	450	1132	
v/s Ratio Prot					0.10			c0.28	0.08	0.01	c0.32	
v/s Ratio Perm		c0.40		0.35			0.02			0.08		
v/c Ratio		1.62		1.41	0.42		0.05	0.51	0.14	0.14	0.49	
Uniform Delay, d1		41.5		41.5	34.9		12.0	16.2	12.7	9.0	10.1	
Progression Factor		1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Incremental Delay, d2		295.7		206.8	0.6		0.2	2.0	0.3	0.1	1.5	
Delay (s)		337.2		248.3	35.5		12.2	18.2	13.0	9.2	11.6	
Level of Service		F		F	D		B	B	B	A	B	
Approach Delay (s)		337.2			164.5			16.5			11.4	
Approach LOS		F			F			B			B	

Intersection Summary		
HCM 2000 Control Delay	110.9	HCM 2000 Level of Service
HCM 2000 Volume to Capacity ratio	0.88	F
Actuated Cycle Length (s)	110.0	Sum of lost time (s)
Intersection Capacity Utilization	83.2%	ICU Level of Service
Analysis Period (min)	15	E
c Critical Lane Group		

Intersection						
Int Delay, s/veh	2.8					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	11	54	57	17	14	19
Future Vol, veh/h	11	54	57	17	14	19
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	68	68	67	67	50	50
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	16	79	85	25	28	38

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	110	0	-	0	209 98
Stage 1	-	-	-	-	98 -
Stage 2	-	-	-	-	111 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1480	-	-	-	779 958
Stage 1	-	-	-	-	926 -
Stage 2	-	-	-	-	914 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1480	-	-	-	770 958
Mov Cap-2 Maneuver	-	-	-	-	770 -
Stage 1	-	-	-	-	916 -
Stage 2	-	-	-	-	914 -

Approach	EB	WB	SB
HCM Control Delay, s	1.3	0	9.5
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1480	-	-	-	868
HCM Lane V/C Ratio	0.011	-	-	-	0.076
HCM Control Delay (s)	7.5	0	-	-	9.5
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.2

Intersection						
Int Delay, s/veh	9.1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	281	52	65	81	12	417
Future Vol, veh/h	281	52	65	81	12	417
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	88	88	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	312	58	74	92	13	463

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	166	0	-	0	802 120
Stage 1	-	-	-	-	120 -
Stage 2	-	-	-	-	682 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1412	-	-	-	353 931
Stage 1	-	-	-	-	905 -
Stage 2	-	-	-	-	502 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1412	-	-	-	273 931
Mov Cap-2 Maneuver	-	-	-	-	273 -
Stage 1	-	-	-	-	699 -
Stage 2	-	-	-	-	502 -

Approach	EB	WB	SB
HCM Control Delay, s	7	0	14
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1412	-	-	-	872
HCM Lane V/C Ratio	0.221	-	-	-	0.547
HCM Control Delay (s)	8.3	0	-	-	14
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0.8	-	-	-	3.4

Intersection						
Int Delay, s/veh	76.8					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↘	↗		↑	↑	↗
Traffic Vol, veh/h	351	7	0	455	331	461
Future Vol, veh/h	351	7	0	455	331	461
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	71	71	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	433	9	0	641	394	549

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1035	394	-	0	-	0
Stage 1	394	-	-	-	-	-
Stage 2	641	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	~ 257	655	0	-	-	-
Stage 1	681	-	0	-	-	-
Stage 2	525	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	~ 257	655	-	-	-	-
Mov Cap-2 Maneuver	~ 257	-	-	-	-	-
Stage 1	681	-	-	-	-	-
Stage 2	525	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	352.2	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	-	257	655	-	-
HCM Lane V/C Ratio	-	1.686	0.013	-	-
HCM Control Delay (s)	-	\$ 359	10.6	-	-
HCM Lane LOS	-	F	B	-	-
HCM 95th %tile Q(veh)	-	27.9	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection						
Int Delay, s/veh	3.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	29	86	593	25	62	445
Future Vol, veh/h	29	86	593	25	62	445
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	36	108	741	31	70	500

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1397	757	0	0	772
Stage 1	757	-	-	-	-
Stage 2	640	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	155	408	-	-	843
Stage 1	463	-	-	-	-
Stage 2	525	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	137	408	-	-	843
Mov Cap-2 Maneuver	137	-	-	-	-
Stage 1	410	-	-	-	-
Stage 2	525	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	32.2	0	1.2
HCM LOS	D		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	272	843
HCM Lane V/C Ratio	-	-	0.528	0.083
HCM Control Delay (s)	-	-	32.2	9.7
HCM Lane LOS	-	-	D	A
HCM 95th %tile Q(veh)	-	-	2.9	0.3

Intersection						
Int Delay, s/veh	3.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	56	21	595	51	16	472
Future Vol, veh/h	56	21	595	51	16	472
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	79	79	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	75	28	753	65	19	562

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1386	786	0	0	818
Stage 1	786	-	-	-	-
Stage 2	600	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	158	392	-	-	810
Stage 1	449	-	-	-	-
Stage 2	548	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	153	392	-	-	810
Mov Cap-2 Maneuver	153	-	-	-	-
Stage 1	434	-	-	-	-
Stage 2	548	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	46.8	0	0.3
HCM LOS	E		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	184	810
HCM Lane V/C Ratio	-	-	0.558	0.024
HCM Control Delay (s)	-	-	46.8	9.6
HCM Lane LOS	-	-	E	A
HCM 95th %tile Q(veh)	-	-	2.9	0.1

Intersection						
Int Delay, s/veh	3.7					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	24	29	21	84	78	11
Future Vol, veh/h	24	29	21	84	78	11
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	44	44	77	77	70	70
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	55	66	27	109	111	16

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	282	119	127	0	0
Stage 1	119	-	-	-	-
Stage 2	163	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	708	933	1459	-	-
Stage 1	906	-	-	-	-
Stage 2	866	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	694	933	1459	-	-
Mov Cap-2 Maneuver	694	-	-	-	-
Stage 1	888	-	-	-	-
Stage 2	866	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10.2	1.5	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1459	-	807	-	-
HCM Lane V/C Ratio	0.019	-	0.149	-	-
HCM Control Delay (s)	7.5	0	10.2	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.1	-	0.5	-	-

Intersection						
Int Delay, s/veh	2.1					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	51	6	6	317	427	50
Future Vol, veh/h	51	6	6	317	427	50
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	58	58	85	85	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	88	10	7	373	508	60

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	925	538	568	0	-	0
Stage 1	538	-	-	-	-	-
Stage 2	387	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	299	543	1004	-	-	-
Stage 1	585	-	-	-	-	-
Stage 2	686	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	296	543	1004	-	-	-
Mov Cap-2 Maneuver	296	-	-	-	-	-
Stage 1	580	-	-	-	-	-
Stage 2	686	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	21.8	0.2	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1004	-	311	-	-
HCM Lane V/C Ratio	0.007	-	0.316	-	-
HCM Control Delay (s)	8.6	0	21.8	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0	-	1.3	-	-

Intersection						
Int Delay, s/veh	3.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	7	47	54	13	37	66
Future Vol, veh/h	7	47	54	13	37	66
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	81	81	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	8	51	67	16	61	108

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	305	75	0	0	83
Stage 1	75	-	-	-	-
Stage 2	230	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	687	986	-	-	1514
Stage 1	948	-	-	-	-
Stage 2	808	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	657	986	-	-	1514
Mov Cap-2 Maneuver	657	-	-	-	-
Stage 1	907	-	-	-	-
Stage 2	808	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.1	0	2.7
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	926	1514
HCM Lane V/C Ratio	-	-	0.063	0.04
HCM Control Delay (s)	-	-	9.1	7.5
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0.1

Lanes, Volumes, Timings
3: Maplewood Ave & Deer St

K0076-19 111 Maplewood Ave, Portsmouth HH
2030 No Build



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	239	145	70	329	148	94	49	496	238	75	402	154
Future Volume (vph)	239	145	70	329	148	94	49	496	238	75	402	154
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	10	13	13	12	14	14	11	11	13	11	11	11
Storage Length (ft)	0		0	0		100	0		0	0		0
Storage Lanes	1		0	1		0	1		1	1		0
Taper Length (ft)	25			25			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		25			25			25				30
Link Distance (ft)		363			453			585				231
Travel Time (s)		9.9			12.4			16.0				5.3
Peak Hour Factor	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Shared Lane Traffic (%)												
Lane Group Flow (vph)	362	326	0	433	319	0	56	570	274	91	678	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Perm	pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2		2	6		
Detector Phase	7	4		3	8		5	2	2	1	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	10.0	10.0	5.0	10.0	
Minimum Split (s)	11.0	29.0		11.0	29.0		11.0	28.0	28.0	11.0	25.0	
Total Split (s)	14.0	29.0		14.0	29.0		11.0	36.0	36.0	11.0	36.0	
Total Split (%)	15.6%	32.2%		15.6%	32.2%		12.2%	40.0%	40.0%	12.2%	40.0%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag	Lag	Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes	Yes	Yes	Yes	
Recall Mode	None	None		None	None		None	C-Max	C-Max	None	C-Max	
v/c Ratio	1.41	0.79		1.62	0.75		0.30	0.82	0.34	0.38	0.98	
Control Delay	233.3	44.9		315.9	40.5		17.7	38.7	4.2	18.3	60.7	
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay	233.3	44.9		315.9	40.5		17.7	38.7	4.2	18.3	60.7	
Queue Length 50th (ft)	~205	161		~280	151		16	313	0	27	~439	
Queue Length 95th (ft)	#194	162		#328	185		38	#492	46	51	#573	
Internal Link Dist (ft)		283			373			505			151	
Turn Bay Length (ft)												
Base Capacity (vph)	256	487		268	503		189	699	803	242	690	
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	
Reduced v/c Ratio	1.41	0.67		1.62	0.63		0.30	0.82	0.34	0.38	0.98	

Intersection Summary

Area Type: Other

Cycle Length: 90

Actuated Cycle Length: 90

Lanes, Volumes, Timings
 3: Maplewood Ave & Deer St

K0076-19 111 Maplewood Ave, Portsmouth HH
 2030 No Build

Offset: 41 (46%), Referenced to phase 2:NBTL and 6:SBTL, Start of Yellow

Natural Cycle: 120

Control Type: Actuated-Coordinated









~ Volume exceeds capacity, queue is theoretically infinite.

Queue shown is maximum after two cycles.

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

Splits and Phases: 3: Maplewood Ave & Deer St

 Ø1 11 s	 Ø2 (R) 36 s	 Ø3 14 s	 Ø4 29 s
 Ø5 11 s	 Ø6 (R) 36 s	 Ø7 14 s	 Ø8 29 s

HCM Signalized Intersection Capacity Analysis K0076-19 111 Maplewood Ave, Portsmouth HH
 3: Maplewood Ave & Deer St 2030 No Build



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	239	145	70	329	148	94	49	496	238	75	402	154
Future Volume (vph)	239	145	70	329	148	94	49	496	238	75	402	154
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	10	13	13	12	14	14	11	11	13	11	11	11
Total Lost time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Frt	1.00	0.95		1.00	0.94		1.00	1.00	0.85	1.00	0.96	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00	1.00	0.95	1.00	
Satd. Flow (prot)	1652	1831		1770	1871		1711	1801	1636	1711	1726	
Flt Permitted	0.29	1.00		0.28	1.00		0.12	1.00	1.00	0.18	1.00	
Satd. Flow (perm)	511	1831		522	1871		213	1801	1636	327	1726	
Peak-hour factor, PHF	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Adj. Flow (vph)	362	220	106	433	195	124	56	570	274	91	490	188
RTOR Reduction (vph)	0	20	0	0	27	0	0	0	171	0	14	0
Lane Group Flow (vph)	362	306	0	433	292	0	56	570	103	91	664	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Perm	pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2		2	6		
Actuated Green, G (s)	27.2	19.2		27.2	19.2		38.5	33.8	33.8	39.1	34.1	
Effective Green, g (s)	27.2	19.2		27.2	19.2		38.5	33.8	33.8	39.1	34.1	
Actuated g/C Ratio	0.30	0.21		0.30	0.21		0.43	0.38	0.38	0.43	0.38	
Clearance Time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Vehicle Extension (s)	3.0	3.0		3.0	4.0		3.0	3.0	3.0	3.0	3.0	
Lane Grp Cap (vph)	255	390		268	399		169	676	614	218	653	
v/s Ratio Prot	0.13	0.17		c0.14	0.16		0.02	0.32		c0.02	c0.38	
v/s Ratio Perm	0.30			c0.34			0.12		0.06	0.16		
v/c Ratio	1.42	0.78		1.62	0.73		0.33	0.84	0.17	0.42	1.02	
Uniform Delay, d1	29.5	33.4		29.4	33.0		19.6	25.7	18.7	17.8	27.9	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Incremental Delay, d2	210.3	9.9		293.7	7.2		1.2	12.2	0.6	1.3	39.4	
Delay (s)	239.8	43.3		323.1	40.3		20.8	37.9	19.3	19.1	67.3	
Level of Service	F	D		F	D		C	D	B	B	E	
Approach Delay (s)		146.7			203.1			31.2			61.6	
Approach LOS		F			F			C			E	

Intersection Summary		
HCM 2000 Control Delay	105.9	HCM 2000 Level of Service
HCM 2000 Volume to Capacity ratio	1.22	F
Actuated Cycle Length (s)	90.0	Sum of lost time (s)
Intersection Capacity Utilization	84.8%	24.0
Analysis Period (min)	15	ICU Level of Service
		E
c Critical Lane Group		

Intersection						
Int Delay, s/veh	1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↶	↷		↶	
Traffic Vol, veh/h	4	46	42	1	2	4
Future Vol, veh/h	4	46	42	1	2	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	68	68	67	67	50	50
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	6	68	63	1	4	8

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	64	0	-	0	144
Stage 1	-	-	-	-	64
Stage 2	-	-	-	-	80
Critical Hdwy	4.12	-	-	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	2.218	-	-	-	3.518
Pot Cap-1 Maneuver	1538	-	-	-	849
Stage 1	-	-	-	-	959
Stage 2	-	-	-	-	943
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1538	-	-	-	846
Mov Cap-2 Maneuver	-	-	-	-	846
Stage 1	-	-	-	-	955
Stage 2	-	-	-	-	943

Approach	EB	WB	SB
HCM Control Delay, s	0.6	0	8.9
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1538	-	-	-	943
HCM Lane V/C Ratio	0.004	-	-	-	0.013
HCM Control Delay (s)	7.4	0	-	-	8.9
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection						
Int Delay, s/veh	27.7					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	373	57	72	52	45	584
Future Vol, veh/h	373	57	72	52	45	584
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	88	88	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	414	63	82	59	50	649

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	141	0	-	0	1003 112
Stage 1	-	-	-	-	112 -
Stage 2	-	-	-	-	891 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1442	-	-	-	268 941
Stage 1	-	-	-	-	913 -
Stage 2	-	-	-	-	401 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1442	-	-	-	188 941
Mov Cap-2 Maneuver	-	-	-	-	188 -
Stage 1	-	-	-	-	641 -
Stage 2	-	-	-	-	401 -

Approach	EB	WB	SB
HCM Control Delay, s	7.4	0	47.1
HCM LOS			E

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1442	-	-	-	731
HCM Lane V/C Ratio	0.287	-	-	-	0.956
HCM Control Delay (s)	8.5	0	-	-	47.1
HCM Lane LOS	A	A	-	-	E
HCM 95th %tile Q(veh)	1.2	-	-	-	14.3

Intersection						
Int Delay, s/veh	0.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W	W	T	T	T	T
Traffic Vol, veh/h	4	9	736	3	2	646
Future Vol, veh/h	4	9	736	3	2	646
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	58	58	78	78	85	85
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	7	16	944	4	2	760

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1710	946	0	0	948
Stage 1	946	-	-	-	-
Stage 2	764	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	100	317	-	-	724
Stage 1	377	-	-	-	-
Stage 2	460	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	100	317	-	-	724
Mov Cap-2 Maneuver	100	-	-	-	-
Stage 1	375	-	-	-	-
Stage 2	460	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	26.5	0	0
HCM LOS	D		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	190	724
HCM Lane V/C Ratio	-	-	0.118	0.003
HCM Control Delay (s)	-	-	26.5	10
HCM Lane LOS	-	-	D	A
HCM 95th %tile Q(veh)	-	-	0.4	0

Intersection

Int Delay, s/veh 171.6

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↘	↗		↑	↑	↗
Traffic Vol, veh/h	466	8	0	489	353	581
Future Vol, veh/h	466	8	0	489	353	581
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	71	71	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	575	10	0	689	420	692

Major/Minor	Minor2	Major1	Major2
Conflicting Flow All	1109	420	0
Stage 1	420	-	-
Stage 2	689	-	-
Critical Hdwy	6.42	6.22	-
Critical Hdwy Stg 1	5.42	-	-
Critical Hdwy Stg 2	5.42	-	-
Follow-up Hdwy	3.518	3.318	-
Pot Cap-1 Maneuver	~ 232	633	0
Stage 1	663	-	0
Stage 2	~ 498	-	0
Platoon blocked, %			-
Mov Cap-1 Maneuver	~ 232	633	-
Mov Cap-2 Maneuver	~ 232	-	-
Stage 1	663	-	-
Stage 2	~ 498	-	-

Approach	EB	NB	SB
HCM Control Delay, s	699.7	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	-	232	633	-	-
HCM Lane V/C Ratio	-	2.48	0.016	-	-
HCM Control Delay (s)	-	711.5	10.8	-	-
HCM Lane LOS	-	F	B	-	-
HCM 95th %tile Q(veh)	-	47.5	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection						
Int Delay, s/veh	5.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	32	69	718	28	56	606
Future Vol, veh/h	32	69	718	28	56	606
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	40	86	898	35	63	681

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1723	916	0	0	933
Stage 1	916	-	-	-	-
Stage 2	807	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	98	330	-	-	734
Stage 1	390	-	-	-	-
Stage 2	439	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	84	330	-	-	734
Mov Cap-2 Maneuver	84	-	-	-	-
Stage 1	336	-	-	-	-
Stage 2	439	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	69.5	0	0.9
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	171	734
HCM Lane V/C Ratio	-	-	0.738	0.086
HCM Control Delay (s)	-	-	69.5	10.4
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	4.6	0.3

Intersection						
Int Delay, s/veh	2.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	34	13	724	35	15	635
Future Vol, veh/h	34	13	724	35	15	635
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	79	79	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	45	17	916	44	18	756

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1730	938	0	0	960
Stage 1	938	-	-	-	-
Stage 2	792	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	97	321	-	-	717
Stage 1	381	-	-	-	-
Stage 2	446	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	93	321	-	-	717
Mov Cap-2 Maneuver	93	-	-	-	-
Stage 1	365	-	-	-	-
Stage 2	446	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	67.7	0	0.2
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	116	717
HCM Lane V/C Ratio	-	-	0.54	0.025
HCM Control Delay (s)	-	-	67.7	10.1
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	2.6	0.1

Intersection						
Int Delay, s/veh	1.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			W	W	
Traffic Vol, veh/h	13	2	4	76	65	18
Future Vol, veh/h	13	2	4	76	65	18
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	44	44	77	77	70	70
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	30	5	5	99	93	26

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	215	106	119	0	-	0
Stage 1	106	-	-	-	-	-
Stage 2	109	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	773	948	1469	-	-	-
Stage 1	918	-	-	-	-	-
Stage 2	916	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	770	948	1469	-	-	-
Mov Cap-2 Maneuver	770	-	-	-	-	-
Stage 1	914	-	-	-	-	-
Stage 2	916	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	9.8	0.4	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1469	-	790	-	-
HCM Lane V/C Ratio	0.004	-	0.043	-	-
HCM Control Delay (s)	7.5	0	9.8	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0.1	-	-

Intersection						
Int Delay, s/veh	2.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	45	8	8	439	529	48
Future Vol, veh/h	45	8	8	439	529	48
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	58	58	85	85	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	78	14	9	516	630	57

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1193	659	687	0	-	0
Stage 1	659	-	-	-	-	-
Stage 2	534	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	206	464	907	-	-	-
Stage 1	515	-	-	-	-	-
Stage 2	588	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	203	464	907	-	-	-
Mov Cap-2 Maneuver	203	-	-	-	-	-
Stage 1	508	-	-	-	-	-
Stage 2	588	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	32.1	0.2	0
HCM LOS	D		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	907	-	222	-	-
HCM Lane V/C Ratio	0.01	-	0.412	-	-
HCM Control Delay (s)	9	0	32.1	-	-
HCM Lane LOS	A	A	D	-	-
HCM 95th %tile Q(veh)	0	-	1.9	-	-

Intersection						
Int Delay, s/veh	3.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	8	45	33	14	29	34
Future Vol, veh/h	8	45	33	14	29	34
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	81	81	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	48	41	17	48	56

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	202	50	0	0	58	0
Stage 1	50	-	-	-	-	-
Stage 2	152	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	787	1018	-	-	1546	-
Stage 1	972	-	-	-	-	-
Stage 2	876	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	762	1018	-	-	1546	-
Mov Cap-2 Maneuver	762	-	-	-	-	-
Stage 1	941	-	-	-	-	-
Stage 2	876	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	8.9	0	3.4
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	969	1546
HCM Lane V/C Ratio	-	-	0.059	0.031
HCM Control Delay (s)	-	-	8.9	7.4
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0.1

Lanes, Volumes, Timings
3: Maplewood Ave & Deer St

K0076-19 111 Maplewood Ave, Portsmouth HH
2030 Build

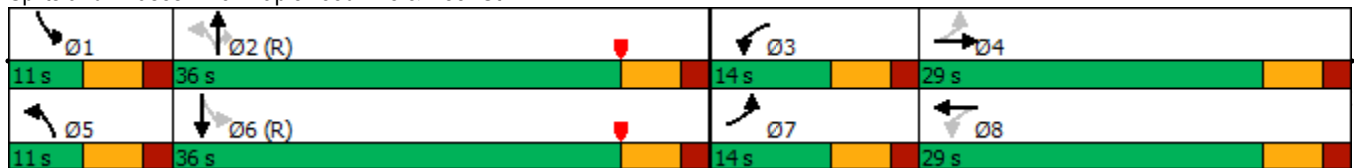


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	287	161	86	350	151	94	52	511	238	75	401	164
Future Volume (vph)	287	161	86	350	151	94	52	511	238	75	401	164
Peak Hour Factor	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Shared Lane Traffic (%)												
Lane Group Flow (vph)	435	374	0	461	323	0	60	587	274	91	689	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Perm	pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2		2	6		
Detector Phase	7	4		3	8		5	2	2	1	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	10.0	10.0	5.0	10.0	
Minimum Split (s)	11.0	29.0		11.0	29.0		11.0	28.0	28.0	11.0	25.0	
Total Split (s)	14.0	29.0		14.0	29.0		11.0	36.0	36.0	11.0	36.0	
Total Split (%)	15.6%	32.2%		15.6%	32.2%		12.2%	40.0%	40.0%	12.2%	40.0%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag	Lag	Lead	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes	Yes	Yes	Yes	
Recall Mode	None	None		None	None		None	C-Max	C-Max	None	C-Max	
v/c Ratio	1.60	0.85		1.83	0.71		0.33	0.86	0.35	0.44	1.03	
Control Delay	308.5	48.6		409.1	37.3		18.9	43.2	4.2	21.4	73.8	
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay	308.5	48.6		409.1	37.3		18.9	43.2	4.2	21.4	73.8	
Queue Length 50th (ft)	~261	183		~335	148		18	327	0	29	~463	
Queue Length 95th (ft)	#253	186		#405	188		40	#514	46	51	#585	
Internal Link Dist (ft)		283			373			505			151	
Turn Bay Length (ft)												
Base Capacity (vph)	272	487		252	502		183	680	788	205	668	
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	
Reduced v/c Ratio	1.60	0.77		1.83	0.64		0.33	0.86	0.35	0.44	1.03	

Intersection Summary

Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 41 (46%), Referenced to phase 2:NBTL and 6:SBTL, Start of Yellow
 Natural Cycle: 140
 Control Type: Actuated-Coordinated
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 3: Maplewood Ave & Deer St



HCM Signalized Intersection Capacity Analysis K0076-19 111 Maplewood Ave, Portsmouth HH
 3: Maplewood Ave & Deer St 2030 Build



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	287	161	86	350	151	94	52	511	238	75	401	164
Future Volume (vph)	287	161	86	350	151	94	52	511	238	75	401	164
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	10	13	13	12	14	14	11	11	13	11	11	11
Total Lost time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Frt	1.00	0.95		1.00	0.94		1.00	1.00	0.85	1.00	0.96	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00	1.00	0.95	1.00	
Satd. Flow (prot)	1652	1824		1770	1873		1711	1801	1636	1711	1722	
Flt Permitted	0.32	1.00		0.22	1.00		0.12	1.00	1.00	0.15	1.00	
Satd. Flow (perm)	548	1824		414	1873		220	1801	1636	269	1722	
Peak-hour factor, PHF	0.66	0.66	0.66	0.76	0.76	0.76	0.87	0.87	0.87	0.82	0.82	0.82
Adj. Flow (vph)	435	244	130	461	199	124	60	587	274	91	489	200
RTOR Reduction (vph)	0	22	0	0	25	0	0	0	174	0	16	0
Lane Group Flow (vph)	435	352	0	461	298	0	60	587	100	91	673	0
Turn Type	pm+pt	NA		pm+pt	NA		pm+pt	NA	Perm	pm+pt	NA	
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4			8			2		2	6		
Actuated Green, G (s)	28.7	20.7		28.7	20.7		37.1	32.8	32.8	37.5	33.0	
Effective Green, g (s)	28.7	20.7		28.7	20.7		37.1	32.8	32.8	37.5	33.0	
Actuated g/C Ratio	0.32	0.23		0.32	0.23		0.41	0.36	0.36	0.42	0.37	
Clearance Time (s)	6.0	6.0		6.0	6.0		6.0	6.0	6.0	6.0	6.0	
Vehicle Extension (s)	3.0	3.0		3.0	4.0		3.0	3.0	3.0	3.0	3.0	
Lane Grp Cap (vph)	272	419		252	430		161	656	596	184	631	
v/s Ratio Prot	0.14	0.19		c0.16	0.16		0.02	0.33		c0.02	c0.39	
v/s Ratio Perm	0.37			c0.42			0.14		0.06	0.18		
v/c Ratio	1.60	0.84		1.83	0.69		0.37	0.89	0.17	0.49	1.07	
Uniform Delay, d1	29.0	33.1		28.0	31.7		20.9	27.0	19.4	19.2	28.5	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00	1.00	
Incremental Delay, d2	286.3	13.7		388.4	5.1		1.5	17.1	0.6	2.1	55.0	
Delay (s)	315.3	46.8		416.3	36.9		22.3	44.1	20.0	21.3	83.5	
Level of Service	F	D		F	D		C	D	B	C	F	
Approach Delay (s)		191.2			260.0			35.5			76.2	
Approach LOS		F			F			D			E	

Intersection Summary		
HCM 2000 Control Delay	136.8	HCM 2000 Level of Service F
HCM 2000 Volume to Capacity ratio	1.36	
Actuated Cycle Length (s)	90.0	Sum of lost time (s) 24.0
Intersection Capacity Utilization	88.4%	ICU Level of Service E
Analysis Period (min)	15	
c Critical Lane Group		

Intersection						
Int Delay, s/veh	2.7					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	11	57	60	17	14	19
Future Vol, veh/h	11	57	60	17	14	19
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	68	68	67	67	50	50
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	16	84	90	25	28	38

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	115	0	-	0	219
Stage 1	-	-	-	-	103
Stage 2	-	-	-	-	116
Critical Hdwy	4.12	-	-	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	2.218	-	-	-	3.518
Pot Cap-1 Maneuver	1474	-	-	-	769
Stage 1	-	-	-	-	921
Stage 2	-	-	-	-	909
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1474	-	-	-	761
Mov Cap-2 Maneuver	-	-	-	-	761
Stage 1	-	-	-	-	911
Stage 2	-	-	-	-	909

Approach	EB	WB	SB
HCM Control Delay, s	1.2	0	9.5
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1474	-	-	-	860
HCM Lane V/C Ratio	0.011	-	-	-	0.077
HCM Control Delay (s)	7.5	0	-	-	9.5
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.2

Intersection						
Int Delay, s/veh	33.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	389	57	72	52	45	608
Future Vol, veh/h	389	57	72	52	45	608
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	88	88	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	432	63	82	59	50	676

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	141	0	-	0	1039 112
Stage 1	-	-	-	-	112 -
Stage 2	-	-	-	-	927 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1442	-	-	-	255 941
Stage 1	-	-	-	-	913 -
Stage 2	-	-	-	-	385 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1442	-	-	-	176 941
Mov Cap-2 Maneuver	-	-	-	-	176 -
Stage 1	-	-	-	-	629 -
Stage 2	-	-	-	-	385 -

Approach	EB	WB	SB
HCM Control Delay, s	7.5	0	57.8
HCM LOS			F

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1442	-	-	-	724
HCM Lane V/C Ratio	0.3	-	-	-	1.002
HCM Control Delay (s)	8.6	0	-	-	57.8
HCM Lane LOS	A	A	-	-	F
HCM 95th %tile Q(veh)	1.3	-	-	-	16.6

Intersection						
Int Delay, s/veh	190.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↘	↗		↑	↑	↗
Traffic Vol, veh/h	491	8	0	489	353	611
Future Vol, veh/h	491	8	0	489	353	611
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	81	81	71	71	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	606	10	0	689	420	727

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1109	420	-	0	-	0
Stage 1	420	-	-	-	-	-
Stage 2	689	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	~ 232	633	0	-	-	-
Stage 1	663	-	0	-	-	-
Stage 2	~ 498	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	~ 232	633	-	-	-	-
Mov Cap-2 Maneuver	~ 232	-	-	-	-	-
Stage 1	663	-	-	-	-	-
Stage 2	~ 498	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	758.4	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	-	232	633	-	-
HCM Lane V/C Ratio	-	2.613	0.016	-	-
HCM Control Delay (s)	-	770.6	10.8	-	-
HCM Lane LOS	-	F	B	-	-
HCM 95th %tile Q(veh)	-	51.2	0	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection						
Int Delay, s/veh	9.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	32	93	766	28	67	595
Future Vol, veh/h	32	93	766	28	67	595
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	89	89
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	40	116	958	35	75	669

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1795	976	0	0	993
Stage 1	976	-	-	-	-
Stage 2	819	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	88	305	-	-	696
Stage 1	365	-	-	-	-
Stage 2	433	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	73	305	-	-	696
Mov Cap-2 Maneuver	73	-	-	-	-
Stage 1	303	-	-	-	-
Stage 2	433	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	106.7	0	1.1
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	168	696
HCM Lane V/C Ratio	-	-	0.93	0.108
HCM Control Delay (s)	-	-	106.7	10.8
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	7	0.4

Intersection						
Int Delay, s/veh	9.7					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	58	22	769	53	17	624
Future Vol, veh/h	58	22	769	53	17	624
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	79	79	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	77	29	973	67	20	743

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1790	1007	0	0	1040
Stage 1	1007	-	-	-	-
Stage 2	783	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	89	292	-	-	669
Stage 1	353	-	-	-	-
Stage 2	450	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	84	292	-	-	669
Mov Cap-2 Maneuver	84	-	-	-	-
Stage 1	335	-	-	-	-
Stage 2	450	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	171.9	0	0.3
HCM LOS	F		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	104	669
HCM Lane V/C Ratio	-	-	1.026	0.03
HCM Control Delay (s)	-	-	171.9	10.5
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	6.5	0.1

Intersection						
Int Delay, s/veh	3.7					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	25	29	21	88	81	13
Future Vol, veh/h	25	29	21	88	81	13
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	44	44	77	77	70	70
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	57	66	27	114	116	19

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	294	126	135	0	-	0
Stage 1	126	-	-	-	-	-
Stage 2	168	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	697	924	1449	-	-	-
Stage 1	900	-	-	-	-	-
Stage 2	862	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	683	924	1449	-	-	-
Mov Cap-2 Maneuver	683	-	-	-	-	-
Stage 1	882	-	-	-	-	-
Stage 2	862	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10.4	1.5	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1449	-	794	-	-
HCM Lane V/C Ratio	0.019	-	0.155	-	-
HCM Control Delay (s)	7.5	0	10.4	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.1	-	0.5	-	-

Intersection						
Int Delay, s/veh	3.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	54	8	8	455	553	53
Future Vol, veh/h	54	8	8	455	553	53
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	58	58	85	85	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	93	14	9	535	658	63

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1243	690	721	0	-	0
Stage 1	690	-	-	-	-	-
Stage 2	553	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	193	445	881	-	-	-
Stage 1	498	-	-	-	-	-
Stage 2	576	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	190	445	881	-	-	-
Mov Cap-2 Maneuver	190	-	-	-	-	-
Stage 1	491	-	-	-	-	-
Stage 2	576	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	40.2	0.2	0
HCM LOS	E		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	881	-	205	-	-
HCM Lane V/C Ratio	0.011	-	0.521	-	-
HCM Control Delay (s)	9.1	0	40.2	-	-
HCM Lane LOS	A	A	E	-	-
HCM 95th %tile Q(veh)	0	-	2.7	-	-

Intersection						
Int Delay, s/veh	3.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	8	50	56	14	38	68
Future Vol, veh/h	8	50	56	14	38	68
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	81	81	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	54	69	17	62	111

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	313	78	0	0	86	0
Stage 1	78	-	-	-	-	-
Stage 2	235	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	680	983	-	-	1510	-
Stage 1	945	-	-	-	-	-
Stage 2	804	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	650	983	-	-	1510	-
Mov Cap-2 Maneuver	650	-	-	-	-	-
Stage 1	903	-	-	-	-	-
Stage 2	804	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.2	0	2.7
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	918	1510
HCM Lane V/C Ratio	-	-	0.068	0.041
HCM Control Delay (s)	-	-	9.2	7.5
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0.1



December 3, 2020

Eben Tormey
Project Manager
XSS Hotels

Will Serve Letter for 1 Raynes Ave Portsmouth, NH 03801

Hi Eben,

Unitil/Northern Utilities Natural Gas Division has reviewed the requested site for natural gas service.

Unitil hereby confirms natural gas is available on Raynes Ave to supply the proposed future development.

If you have any questions, please contact me at 603-534-2379.

Sincerely,

A handwritten signature in blue ink, appearing to read "Dave MacLean", is written over a light blue circular stamp.

Dave MacLean
Senior Business Development Rep



T 603.294.5261

M 603.534.2379

F 603.294.5264

Email macleand@unitil.com



December 3, 2020

Eben Tormey
Project Manager
XSS Hotels

Will Serve Letter for 31 Raynes Ave Portsmouth, NH 03801

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Unitil/Northern Utilities Natural Gas Division has reviewed the requested site for natural gas service.

Unitil hereby confirms natural gas is available on Raynes Ave to supply the proposed future development.

If you have any questions, please contact me at 603-534-2379.

Sincerely,

A handwritten signature in blue ink, appearing to read "Dave MacLean", is written over a light blue circular watermark.

Dave MacLean
Senior Business Development Rep



T 603.294.5261

M 603.534.2379

F 603.294.5264

Email macleand@unitil.com

March 16, 2021

City of Portsmouth Planning Board

GREEN BUILDING STATEMENT

Re: Proposed Mixed-Use Development, Raynes Avenue, Portsmouth, NH

The core and shell of the proposed mixed-use buildings at Raynes Avenue are being designed to meet or exceed current Energy Code requirements. A U.S. Department of Energy "COMcheck" will be submitted with the building permit application.

Currently the State of New Hampshire has adopted the 2015 International Energy Conservation Code with amendments, and the design of the new building will be built to current best practices and will exceed these requirements when appropriate.

- Foundation system: Cast in place concrete with continuous rigid insulation installed to depths required by the energy code. Continuous insulation to be provided under the concrete slab on grade for 2 feet along the exterior wall.
- Exterior walls: Continuous insulation outside the framing system and the continuous air barrier, to provide better overall thermal performance. Exterior skin of building to be a combination of brick, wood siding panels and metal wall panels that provide an air space in front of the insulation to allow for moisture management.
- Exterior windows: Thermally broken aluminum framing for common spaces and vinyl windows at apartment and hotel units. All glazing to be insulated, high-performance type to provide enhanced thermal performance and solar control.
- Roofing system: Light colored membrane roofing system over continuous rigid insulation that exceeds the base energy code requirements.
- HVAC systems: Apartment and hotel units to consist of high-efficiency, variable refrigerant flow, split system heat pumps. Ventilation to be provided by high-efficiency DX gas, dedicated outdoor units to provide fresh air to apartment and hotel units and common spaces. Bathroom exhaust to run through energy recovery heat exchanger to preheat incoming makeup air. Domestic hot water to be provided by high efficiency condensing boilers with variable frequency pumps. Apartment and hotel units to have individual temperature controls and common spaces to have digital controls with occupancy sensors.
- Plumbing: All fixtures to be low flow.
- Lighting: Exterior lighting to be LED cutoff fixtures for energy efficiency and to minimize light pollution. All interior lighting to be LED throughout using less than 1 watt / sf. Occupancy sensors to be utilized as required by code.
- Landscaping: Local species that are drought tolerant to be incorporated into plantings list.

Sincerely,



Christopher J. Lizotte AIA, NCARB, LEED AP
Senior Associate - Architecture and Engineering

Site Plan Review Application Fee

Project: Raynes Avenue

Map/Lot: 123/10,12,13&14

Applicant: North Mill Pond Holdings, LLC

All development

Base fee \$500 \$500.00

Plus \$5.00 per \$1,000 of site costs
Site costs \$850,000 + \$4,250.00

Plus \$10.00 per 1,000 S.F. of site development area
Site development area 105,000 S.F. + \$1,050.00

Fee **\$5,800.00**

Maximum fee: \$15,000.00

Fee received by: _____ Date: _____

Note: Initial application fee may be based on the applicant's estimates of site costs and site development area. Following site plan approval, the application fee will be recalculated based on the approved site plan and site engineer's corresponding site cost estimate as approved by the Department of Public Works, and any additional fee shall be paid prior to the issuance of a building permit.