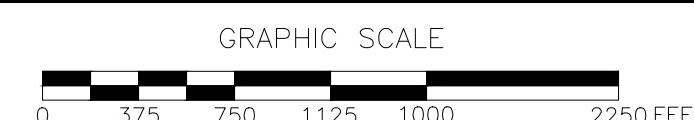
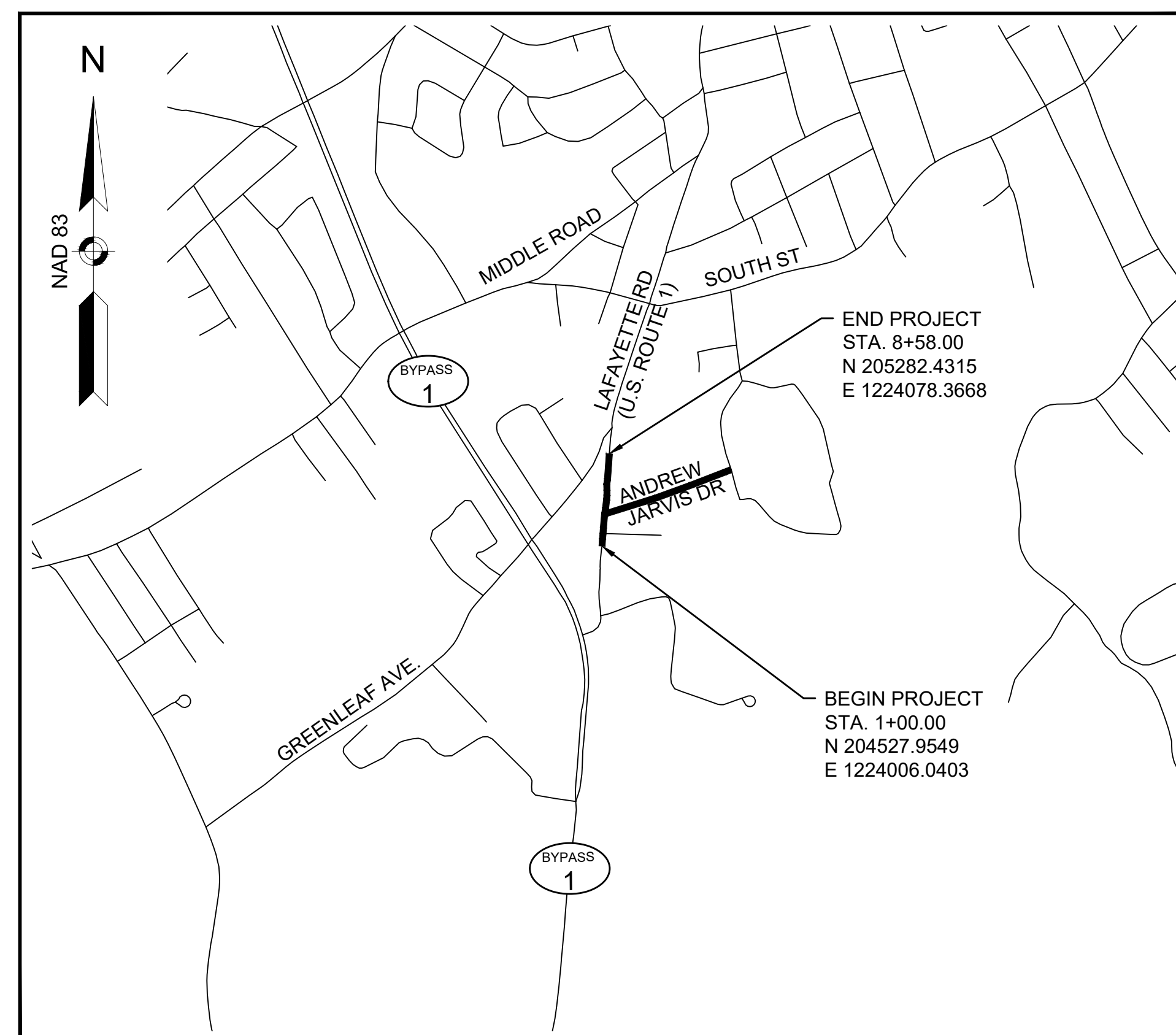


INTERSECTION IMPROVEMENT PROJECT U.S. ROUTE 1 AT ANDREW JARVIS DRIVE

IN THE CITY OF PORTSMOUTH ROCKINGHAM COUNTY STATE OF NEW HAMPSHIRE



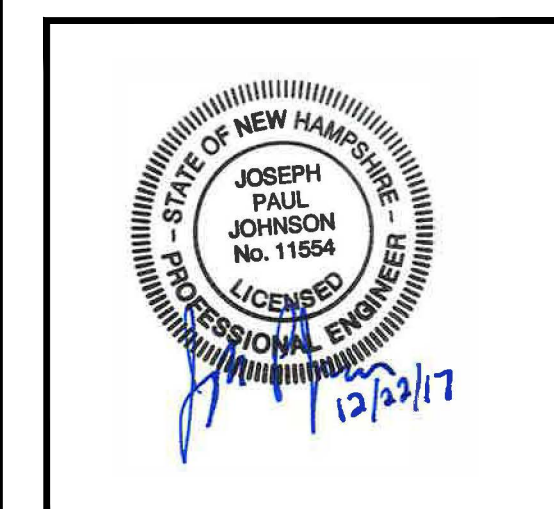
INDEX	
SHEET NO.	
1	TITLE SHEET & INDEX
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5-6	TYPICAL SECTIONS
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LENGTH OF PROJECT = 1593 FEET = 0.30 MILES

PORTSMOUTH BID NO. 35-18

GPI Greenman-Pedersen, Inc.
 Engineers, Architects, Planners, Construction Engineers & Inspectors
 21 Daniel Street, Second Floor, Portsmouth, NH 03801
 Tel: (603) 891-2213 Fax: (978) 658-3044
<http://www.gpinet.com>



PREPARED FOR
 CITY OF PORTSMOUTH
 DEPT. OF PUBLIC WORKS
 680 PEVERLY HILL ROAD
 PORTSMOUTH, NH 03801

**INTERSECTION IMPROVEMENT PROJECT
 U.S. ROUTE 1 AT ANDREW JARVIS DRIVE
 CITY OF PORTSMOUTH
 NEW HAMPSHIRE**

REVISIONS		
NO.	REVISION	DATE

12/22/17

DRAWN/DESIGN BY CLC	CHECKED BY JPJ
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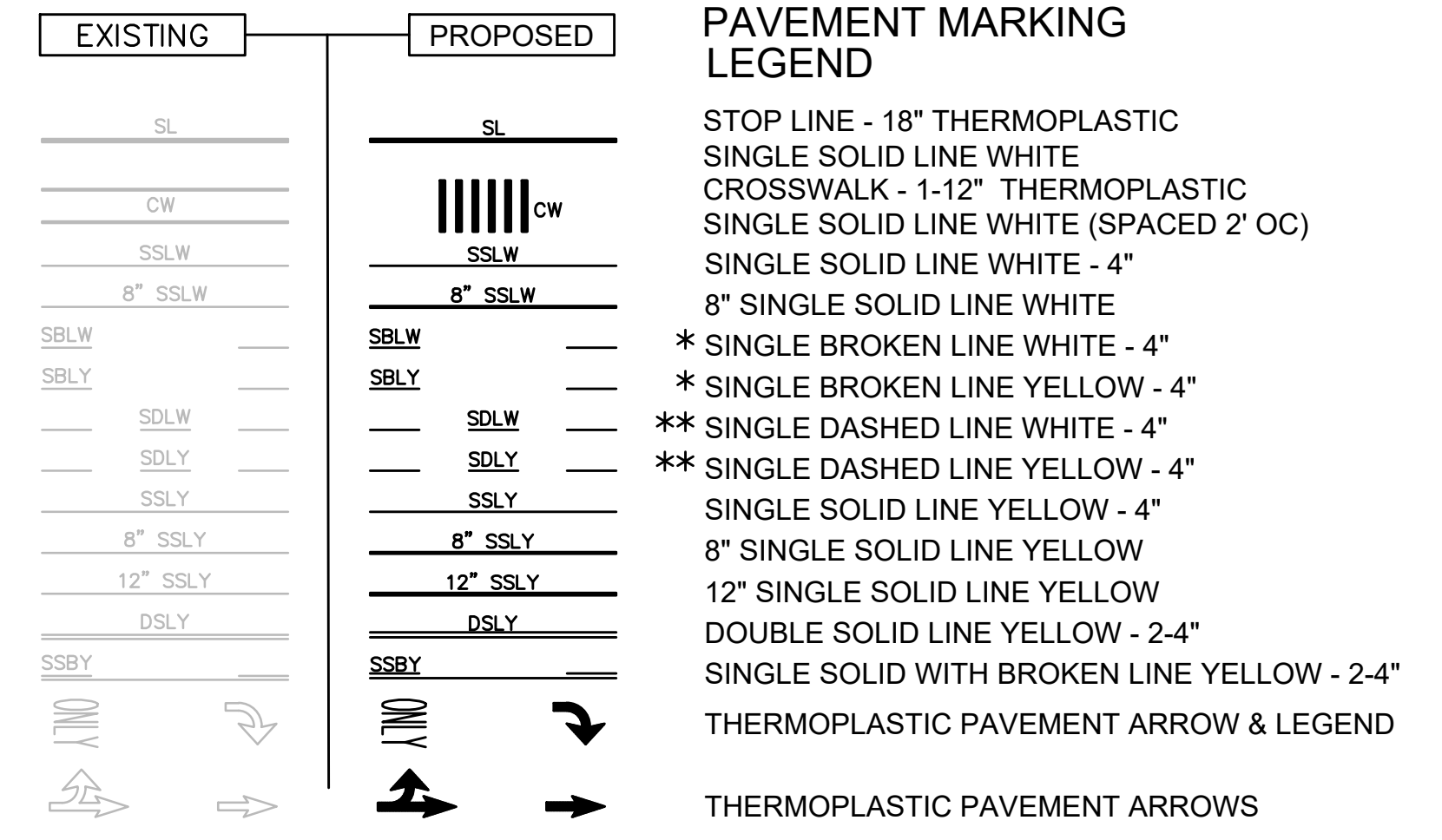
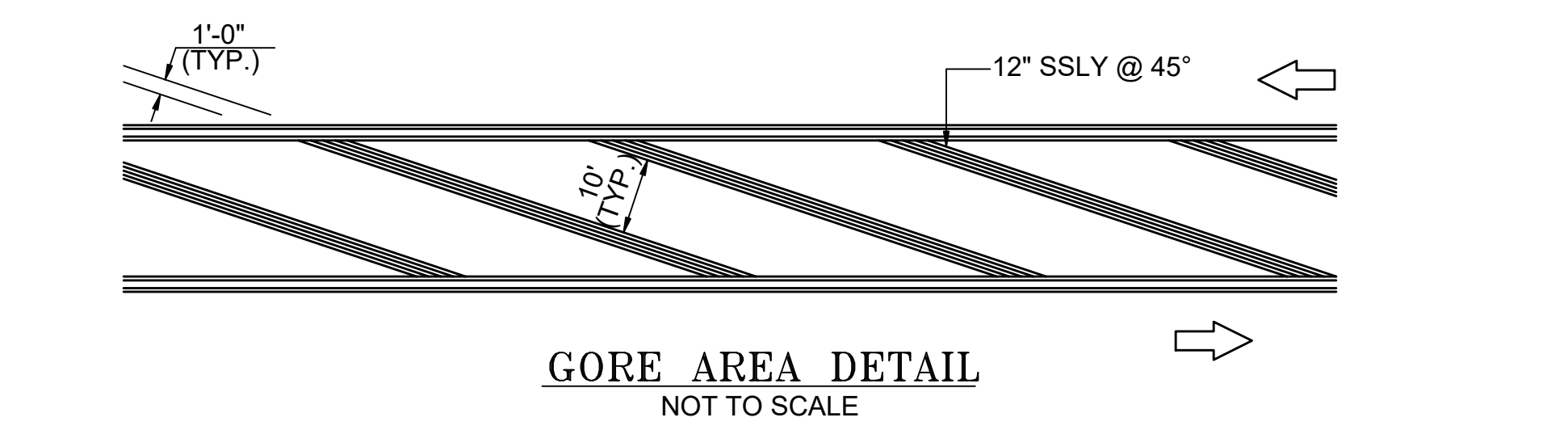
TITLE SHEET &
INDEX

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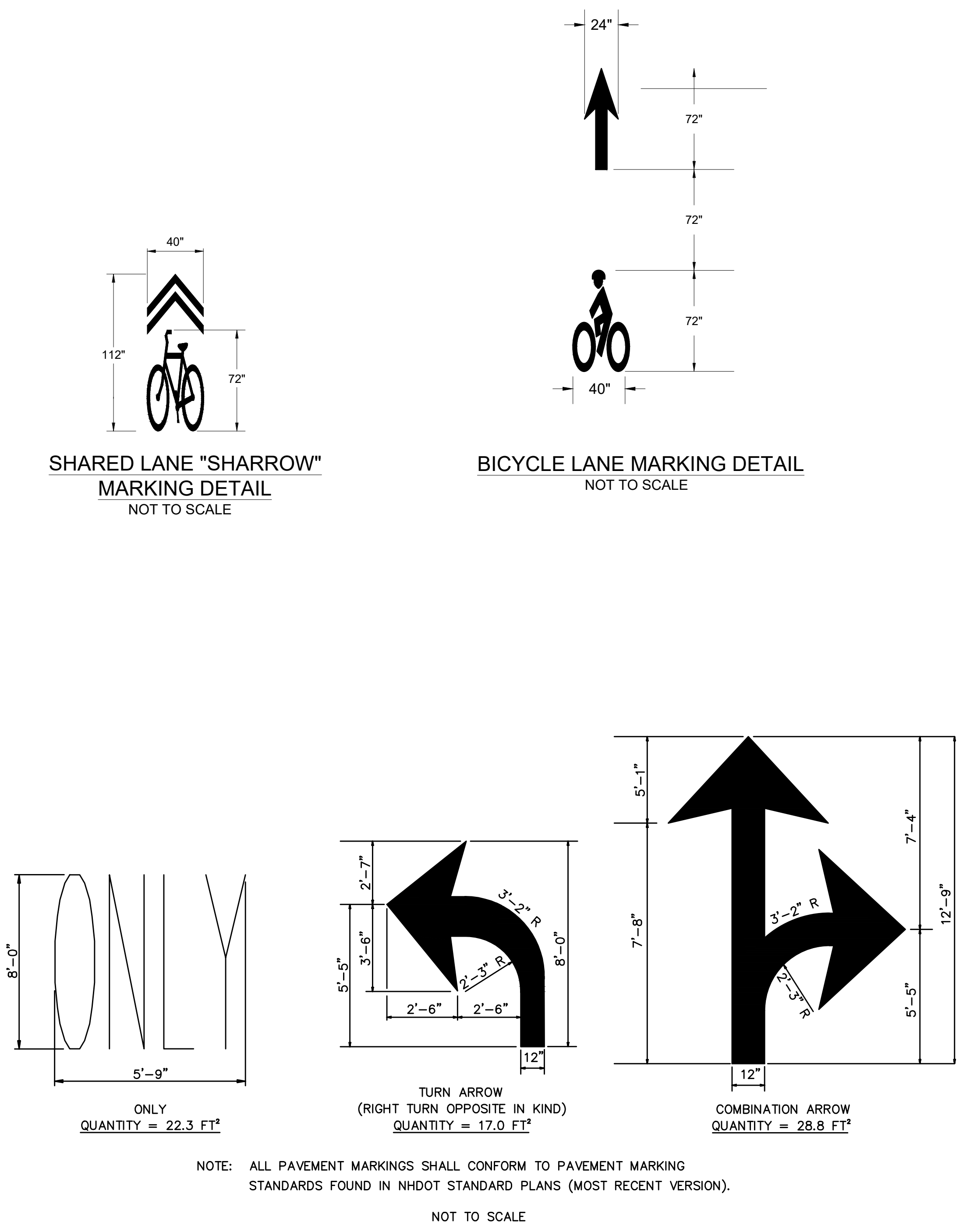
1 OF 33

GENERAL SYMBOLS

EXISTING	PROPOSED	DESCRIPTION
[Symbol]	[Symbol]	JERSEY BARRIER
[Symbol]	[Symbol]	CATCH BASIN
[Symbol]	[Symbol]	CATCH BASIN CURB INLET
[Symbol]	[Symbol]	FLAG POLE
[Symbol]	[Symbol]	GAS PUMP
[Symbol]	[Symbol]	MAIL BOX
[Symbol]	[Symbol]	POST SQUARE
[Symbol]	[Symbol]	POST CIRCULAR
[Symbol]	[Symbol]	WELL
[Symbol]	[Symbol]	ELECTRIC HANDHOLE
[Symbol]	[Symbol]	FENCE GATE POST
[Symbol]	[Symbol]	GAS GATE
[Symbol]	[Symbol]	BORING HOLE
[Symbol]	[Symbol]	MONITORING WELL
[Symbol]	[Symbol]	TEST PIT
[Symbol]	[Symbol]	HYDRANT
[Symbol]	[Symbol]	LIGHT POLE
[Symbol]	[Symbol]	COUNTY BOUND
[Symbol]	[Symbol]	GPS POINT
[Symbol]	[Symbol]	CABLE MANHOLE
[Symbol]	[Symbol]	DRAINAGE MANHOLE
[Symbol]	[Symbol]	ELECTRIC MANHOLE
[Symbol]	[Symbol]	GAS MANHOLE
[Symbol]	[Symbol]	MISC MANHOLE
[Symbol]	[Symbol]	SEWER MANHOLE
[Symbol]	[Symbol]	TELEPHONE MANHOLE
[Symbol]	[Symbol]	WATER MANHOLE
[Symbol]	[Symbol]	GRANITE BOUND
[Symbol]	[Symbol]	MONUMENT
[Symbol]	[Symbol]	STONE BOUND
[Symbol]	[Symbol]	TOWN OR CITY BOUND
[Symbol]	[Symbol]	TRAVERSE OR TRIANGULATION STATION
[Symbol]	[Symbol]	TROLLEY POLE OR GUY POLE
[Symbol]	[Symbol]	TRANSMISSION POLE
[Symbol]	[Symbol]	UTILITY POLE W/ FIREBOX
[Symbol]	[Symbol]	UTILITY POLE WITH DOUBLE LIGHT
[Symbol]	[Symbol]	UTILITY POLE W/ 1 LIGHT
[Symbol]	[Symbol]	UTILITY POLE
[Symbol]	[Symbol]	BUSH
[Symbol]	[Symbol]	TREE
[Symbol]	[Symbol]	STUMP
[Symbol]	[Symbol]	SWAMP / MARSH
[Symbol]	[Symbol]	WATER GATE
[Symbol]	[Symbol]	PARKING METER
[Symbol]	[Symbol]	OVERHEAD CABLE/WIRE
[Symbol]	[Symbol]	CURBING
[Symbol]	[Symbol]	CONTOURS (ON-THE-GROUND SURVEY DATA)
[Symbol]	[Symbol]	CONTOURS (PHOTOGRAMMETRIC DATA)
[Symbol]	[Symbol]	UNDERGROUND DRAIN PIPE (DOUBLE LINE 24 INCH AND OVER)
[Symbol]	[Symbol]	UNDERGROUND ELECTRIC DUCT (DOUBLE LINE 24 INCH AND OVER)
[Symbol]	[Symbol]	UNDERGROUND GAS MAIN (DOUBLE LINE 24 INCH AND OVER)
[Symbol]	[Symbol]	UNDERGROUND SEWER MAIN (DOUBLE LINE 24 INCH AND OVER)
[Symbol]	[Symbol]	UNDERGROUND TELEPHONE DUCT (DOUBLE LINE 24 INCH AND OVER)
[Symbol]	[Symbol]	UNDERGROUND WATER MAIN (DOUBLE LINE 24 INCH AND OVER)
[Symbol]	[Symbol]	BALANCED STONE WALL
[Symbol]	[Symbol]	GUARD RAIL - STEEL POSTS
[Symbol]	[Symbol]	GUARD RAIL - WOOD POSTS
[Symbol]	[Symbol]	CHAIN LINK OR METAL FENCE
[Symbol]	[Symbol]	WOOD FENCE
[Symbol]	[Symbol]	HAY BALES/SILT FENCE/COMPOST FILTER TUBES
[Symbol]	[Symbol]	TREE LINE
[Symbol]	[Symbol]	SAWCUT LINE
[Symbol]	[Symbol]	TOP OR BOTTOM OF SLOPE
[Symbol]	[Symbol]	LIMIT OF EDGE OF PAVEMENT OR COLD PLANE AND OVERLAY
[Symbol]	[Symbol]	BANK OF RIVER OR STREAM
[Symbol]	[Symbol]	BORDER OF WETLAND
[Symbol]	[Symbol]	100 FT WETLAND BUFFER
[Symbol]	[Symbol]	200 FT RIVERFRONT BUFFER
[Symbol]	[Symbol]	STATE HIGHWAY LAYOUT
[Symbol]	[Symbol]	TOWN OR CITY LAYOUT
[Symbol]	[Symbol]	COUNTY LAYOUT
[Symbol]	[Symbol]	RAILROAD SIDELINE
[Symbol]	[Symbol]	TOWN OR CITY BOUNDARY LINE
[Symbol]	[Symbol]	PROPERTY LINE OR APPROXIMATE PROPERTY LINE
[Symbol]	[Symbol]	EASEMENT



* BROKEN LANE LINES TO BE 10' IN LENGTH WITH 30' GAP (TYP.)
 ** DASHED LINES TO BE 2' IN LENGTH WITH 6' GAP (TYP.)



NOTE: ALL PAVEMENT MARKINGS SHALL CONFORM TO PAVEMENT MARKING STANDARDS FOUND IN NHDOT STANDARD PLANS (MOST RECENT VERSION).

GENERAL ABBREVIATIONS

ABAN	ABANDON
ACCP	ASPHALT COATED CORRUGATED METAL PIPE
ADJ	ADJUST
APPROX	APPROXIMATE
BC	BOTTOM OF CURB
BD	BOUND
BIT CONC	BITUMINOUS CONCRETE
BY OTHERS	BY OTHERS
BM	BENCH MARK
BW	BOTTOM OF WALL
CC	CEMENT CONCRETE
CCB	CAPE GOD BERM
CI	CURB INLET
CIP	CAST IRON PIPE
CT	CHANGE IN TYPE
CLR	CLEARANCE
CLF	CHAIN LINK FENCE
CLDI	CEMENT LINED DUCTILE IRON
CMP	CORRUGATED METAL PIPE
CONST	CONSTRUCTION
CP	CENTER POINT
CPP	CORRUGATED PLASTIC PIPE
COND	CONDUIT
DIP	DUCTILE IRON PIPE
DR	DRIVEWAY
ELEV OR EL	ELEVATION
EGP	EDGE OF PAVEMENT
ETW	EDGE OF TRAVELED WAY
EXIST	EXISTING
F&C	FRAME AND COVER
F&G	FRAME AND GRATE
FDN	FOUNDATION
FND	FOUND
FWD	FILLED WITH DEBRIS
GRAN	GRANITE
HMA	HOT MIX ASPHALT
HOR	HORIZONTAL
HP	HIGH POINT
HYD	HYDRANT
INVT	INVERT
LB	LEACHING BASIN
LO	LAYOUT
MAX	MAXIMUM
MIN	MINIMUM
MON	MONUMENT
NOT IN CONTRACT	NOT IN CONTRACT
NGVD	NATIONAL GEODETIC VERTICAL DATUM
N/F	NOW OR FORMERLY
OHW	OVERHEAD WIRE
PC	POINT OF CURVATURE
PCC	POINT OF COMPOUND CURVATURE
PGL	PROFILE GRADE LINE
PI	POINT OF INTERSECTION
POC	POINT ON CURVE
PRC	POINT OF REVERSE CURVATURE
PROP	PROPOSED
PT	POINT OF TANGENCY
PVC	POINT OF VERTICAL CURVATURE
PVCC	POINT OF VERTICAL COMPOUND CURVATURE
PVCP	POLYVINYLCHLORIDE PIPE
PVI	POINT OF VERTICAL INTERSECTION
PVMT	PAVEMENT
PVRC	POINT OF VERTICAL REVERSE CURVATURE
PVT	POINT OF VERTICAL TANGENCY
PWW	PAVED WATERWAY
R	RADIUS
R&D	REMOVE AND DISPOSE
R&R	REMOVE AND RESET
R&S	REMOVE AND STACK
REM	REMOVE
REMOD	REMODEL
RCP	REINFORCED CONCRETE PIPE
RET	RETAIN
ROW	RIGHT OF WAY
RR	RAILROAD
SB	STONE BOUND
SB/DH	STONE BOUND/DRILL HOLE
SHLD	SHOULDER
SHLO	STATE HIGHWAY LAYOUT
SSD	STOPPING SIGHT DISTANCE
STA	STATION
STL	STEEL
TBM	TEMPORARY BENCH MARK
TC	TOP OF CURB
TOS	TOP OF SLOPE
TS	TRAFFIC SIGNAL
TSV & B	TAPPING SLEEVE, VALVE AND BOX
TYP	TYPICAL
TOP OF WALL	TOP OF WALL
UC	UNDER CONSTRUCTION
USGS	U.S. GEOLOGICAL SURVEY
UP	UTILITY POLE
VCP	VITRIFIED CLAY PIPE
VERT	VERTICAL
WCR	WHEELCHAIR RAMP

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NO.	REVISION	DATE

12/22/17
 DRAWN/DESIGN BY: CLS
 CHECKED BY: JPJ

LEGEND & ABBREVIATIONS
 SCALE: N.T.S.

GENERAL NOTES

1. EXISTING CONDITIONS INFORMATION IS BASED ON SURVEY COLLECTED BY: GREENMAN PEDERSEN, INC., 21 DANIEL STREET, PORTSMOUTH, NH 03801, (603) 891-2213.
2. THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH NHDOT STANDARD SPECIFICATIONS DATED 2016, CURRENT STANDARD PLANS, SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS INCLUDED IN THE PROPOSAL. THESE PLANS HAVE BEEN CREATED TO BE USED TOGETHER WITH THE CONTRACT AND SPECIFICATIONS TO CREATE ONE COMPLETE BID AND CONSTRUCTION DOCUMENT.
3. CONTRACTOR SHALL TAKE SPECIAL CARE IN NOT DISTURBING EXISTING MONUMENTS, BOUNDS, AND/OR BENCHMARKS WITHOUT FIRST MAKING PROVISIONS FOR RELOCATION.
4. THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES AND SERVICES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE CITY OF PORTSMOUTH OR GREENMAN-PEDERSEN, INC. THE CONTRACTOR SHALL DETERMINE THE EXACT SIZE AND LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO LOCATE EXACTLY AND TO PRESERVE ANY AND ALL UNDERGROUND UTILITIES. CALL "DIG-SAFE" 1-888-DIGSAFE (344-7233) AT LEAST 72 HOURS PRIOR TO EXCAVATING AT ANY LOCATION. A COPY OF THE DIGSAFE PROJECT REFERENCE NUMBER(S) SHALL BE GIVEN TO THE OWNER PRIOR TO EXCAVATION. STRUCTURES SHOWN MAY HAVE MULTIPLE SERVICES AND MAY HAVE OLD CONNECTIONS THAT MAY NOT HAVE BEEN PROPERLY ABANDONED. NO EXTRA PAYMENTS WILL BE MADE FOR EXPLORATION OF DEFUNCT UTILITIES LEFT IN THE GROUND.
5. WHERE AN EXISTING UNDERGROUND UTILITY IS FOUND TO CONFLICT WITH THE PROPOSED WORK, THE LOCATION, ELEVATION AND SIZE OF THE UTILITY SHALL BE ACCURATELY DETERMINED WITHOUT DELAY BY THE CONTRACTOR, AND THE INFORMATION FURNISHED TO THE ENGINEER FOR RESOLUTION OF THE CONFLICT.
6. THE CONTRACTOR SHALL MAKE ALL ARRANGEMENTS FOR THE ALTERATION AND ADJUSTMENT OF GAS, ELECTRIC, TELEPHONE AND ANY OTHER PRIVATE UTILITIES BY THE UTILITY COMPANIES.
7. AREAS OUTSIDE THE LIMITS OF PROPOSED WORK DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO THEIR ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
8. THE TERM "PROPOSED" (PROP.) MEANS WORK TO BE CONSTRUCTED USING NEW MATERIALS, OR, WHERE APPLICABLE, RE-USING EXISTING MATERIALS IDENTIFIED AS "REMOVE & RESET" (R & R).
9. ALL EXISTING SIGNS WITHIN THE PROJECT LIMITS SHALL BE RETAINED UNLESS NOTED OTHERWISE.
10. ALL SYMBOLS, WORDS, TRANSVERSE MARKINGS (STOP BARS AND CROSSWALK LINES), AS NOTED ON THE PAVEMENT MARKING LEGEND ON SHEET 2, SHALL BE THERMOPLASTIC.
11. THE CONTRACTOR SHALL CONTACT THE PORTSMOUTH DEPARTMENT OF PUBLIC WORKS AT 427-1530 ONE WEEK PRIOR TO PAVEMENT MARKING.
12. ALL EXISTING STATE, COUNTY, CITY AND TOWN LOCATION LINES AND PRIVATE PROPERTY LINES HAVE BEEN ESTABLISHED FROM AVAILABLE INFORMATION AND THEIR EXACT LOCATIONS ARE NOT GUARANTEED. ALL PROPERTY LINES BETWEEN ABUTTERS ARE APPROXIMATE ONLY.
13. ALL DISTURBED AREAS NOT DESIGNATED TO BE PAVED SHALL HAVE LOAM AND SEED PLACED WITH MULCH AND TACKIFIERS. THE LOAM SHALL HAVE A MINIMUM DEPTH OF 6 INCHES AND SHALL BE PLACED FLUSH WITH THE TOP OF THE ADJACENT CURB, EDGING, BERM OR PAVEMENT SURFACE.
14. THE LIMIT OF WORK AREA SHALL BE THE STREET RIGHT OF WAY UNLESS SHOWN OTHERWISE.
15. PRIOR TO THE START OF ANY NEW UTILITY WORK, ALL ELEVATIONS OF EXISTING UTILITIES IN THOSE AREAS ARE TO BE VERIFIED. THE ENGINEER IS TO BE NOTIFIED IMMEDIATELY SHOULD ANY DISCREPANCIES OCCUR.
16. ALL CASTINGS (BESIDES DRAINAGE MANHOLE COVERS AND DRAINAGE GRATES) SHALL BE SET FLUSH WITH FINISHED GRADE.
17. ALL GATE BOXES, SERVICE BOXES AND PUBLICLY OWNED MANHOLE FRAMES AND COVERS SHALL BE ADJUSTED TO GRADE BY THE CONTRACTOR. ADJUSTMENT OF THESE CASTINGS SHALL BE INCLUDED IN THE RESPECTIVE CONTRACT ITEMS.
18. THE CONTRACTOR SHALL PROVIDE SUBMITTALS FOR ALL MATERIALS TO BE USED ON THIS PROJECT. THE CONTRACTOR SHALL NOT PURCHASE ANY MATERIALS UNTIL THEY HAVE BEEN APPROVED FOR USE BY THE OWNER.
19. UTILITIES DAMAGED AS A RESULT OF THE CONTRACTORS OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
20. TEMPORARY AND OR PERMANENT PAVING REPAIRS SHALL MAINTAIN EXISTING LINE AND GRADE UNLESS INDICATED OTHERWISE (SEE PROFILES AND CROSS SECTIONS) OR OTHERWISE DIRECTED BY THE ENGINEER.
21. AS BUILT RED-LINE DRAWINGS NEED TO BE FURNISHED BY THE CONTRACTOR PRIOR TO FINAL PAYMENT. THE PREPARATION OF THESE AS BUILTS IS CONSIDERED SUBSIDIARY TO THE PROJECT.
22. OVERHEAD WIRES ARE SHOWN ON THE DRAWINGS BUT THE CITY MAKES NO WARRANTY TO THEIR COMPLETENESS OR THAT HEIGHT IS SUFFICIENT TO COMPLETE THE WORK. POLES THAT NEED TO BE HELD UP BY THE UTILITY COMPANY WILL BE PAID FOR BY THE CONTRACTOR WITH NO ADDITIONAL COST PASSED ON TO THE OWNER.
23. NO WORK SHALL BE PERFORMED ON PRIVATE PROPERTY UNTIL THE OWNER HAS SIGNED A MEMORANDUM OF UNDERSTANDING WITH THE CITY.
24. ALL EXISTING STRUCTURES ABANDONED IN PLACE SHALL BE REMOVED TO 3' BELOW GRADE AND FILLED WITH COMPACTED GRAVEL OR CRUSHED STONE. PIPES SHALL BE PLUGGED AT ALL OPEN AREAS AND ANY ABANDONED PIPE LARGER THAN 8" SHALL BE FILLED WITH PUMPED FLOWABLE FILL.
25. SAWCUTTING AND EXCAVATION OF THE EXISTING CONCRETE SLAB WITHIN THE LIMITS OF ROUTE ONE MAY BE REQUIRED TO COMPLETE THE WORK. DISTURBANCE TO THE EXISTING SLAB SHALL BE MINIMIZED TO THE EXTENT POSSIBLE. CONCRETE TO BE REMOVED SHALL BE SAWCUT AND REMOVED WITHOUT CAUSING EXCESSIVE DAMAGE TO THE SURROUNDING ROADWAY.

STORM DRAINAGE SYSTEM NOTES

1. THE CONTRACTOR SHALL PHASE THE CONSTRUCTION OF THE DRAIN TO MINIMIZE DISRUPTION TO THE EXISTING SYSTEM. THE SYSTEM SHALL NOT BE SURCHARGED AND ANY DAMAGE CAUSED BY A SURCHARGE WILL BE PAID FOR BY THE CONTRACTOR. MAINTENANCE OF THE DRAINAGE FLOWS IS SUBSIDIARY TO THE WORK.
2. DRAINAGE CONSTRUCTION WILL START AT THE LOW POINT OF THE SYSTEM UNLESS APPROVED BY THE ENGINEER.
3. DRAIN MANHOLE COVERS AND CATCH BASINS GRATES SHALL BE SET 1/4 OF AN INCH BELOW GRADE.
4. DRAIN MANHOLES AND CATCH BASINS FRAMES, COVERS AND GRATES SHALL BE IN CONFORMANCE WITH CITY OF PORTSMOUTH AND NHDOT STANDARDS.
5. ALL CATCH BASINS, DRAIN MANHOLES & DRAIN LINES SHALL BE CLEANED PRIOR TO ACCEPTANCE.
6. SAWCUTTING AND EXCAVATION OF THE EXISTING CONCRETE SLAB WITHIN THE LIMITS OF ROUTE ONE WILL BE PAID FOR UNDER ITEM 206.2 ROCK STRUCTURE EXCAVATION. REPLACEMENT OF THE SLAB WILL BE PAID FOR UNDER 520.2. THE EXISTING SLAB SHALL BE SAWCUT AND REMOVED WITHOUT CAUSING EXCESSIVE DAMAGE TO THE SURROUNDING ROADWAY.

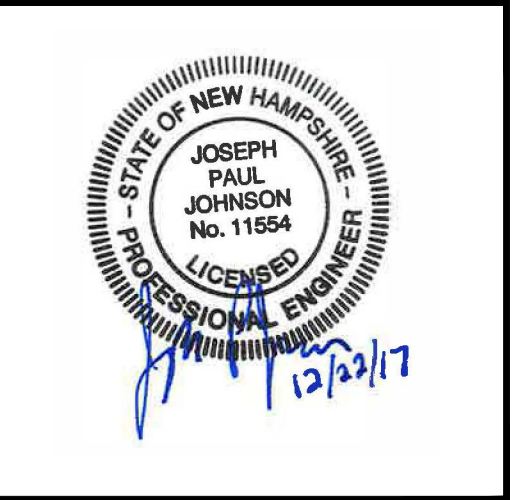
WATER CONSTRUCTION NOTES

1. TEST PITS TO LOCATE EXISTING UTILITIES ARE REQUIRED AS NOTED ON THE PLANS. ADDITIONAL TEST PITS MAY BE ORDERED BY THE ENGINEER. DRAINAGE MATERIALS SHALL NOT BE ORDERED UNTIL TEST PITS HAVE BEEN COMPLETED AND POTENTIAL CONFLICTS HAVE BEEN RESOLVED.
2. STONE WALLS, FENCES, MAIL BOXES, SIGNS, CURBS, LIGHT POLES, ETC. SHALL BE REMOVED AND REPLACED AS NECESSARY TO PERFORM THE WATER WORK. UNLESS OTHERWISE INDICATED, ALL SUCH WORK SHALL BE INCIDENTAL TO CONSTRUCTION OF THE PROJECT.
3. ALL PAVEMENT DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPLACED IN ACCORDANCE WITH THE SPECIFICATIONS AND AS SHOWN ON THE DRAWINGS.
4. ALL AREAS DISTURBED BY THE CONTRACTOR BEYOND PAYMENT LIMITS SHALL BE RESTORED AT NO ADDITIONAL COST TO THE OWNER.
5. THE CONTRACTOR SHALL NOT STORE ANY APPARATUS, MATERIALS, SUPPLIES, OR EQUIPMENT ON DRAINAGE STRUCTURES OR WITHIN 100 FEET OF WETLANDS.
6. EXISTING WATER MAINS OR SERVICES SHALL NOT BE ABANDONED WITHOUT THE APPROVAL OF THE OWNER. WATER SERVICE SHALL NOT BE INTERRUPTED WITHOUT 72 HOUR NOTICE AND PRIOR APPROVAL OF THE OWNER.
7. ALL HYDRANTS REMOVED SHALL BE SALVAGED AND DELIVERED TO A LOCATION TO BE DETERMINED BY THE OWNER. SURFACE RESTORATION SHALL BE IN KIND UNLESS OTHERWISE NOTED.
8. ANY HYDRANT WHICH IS NOT IN SERVICE SHALL BE COVERED WITH A SECURELY FASTENED AND APPROVED BAG.

EROSION & SEDIMENT CONTROL NOTES

1. ALL EROSION AND SEDIMENT CONTROLS SHALL MEET THE REQUIREMENTS ON THE NH STORMWATER MANUAL, VOLUME 3, EROSION AND SEDIMENT CONTROLS DURING CONSTRUCTION, 2008.
2. THIS PROJECT IS TO BE MANAGED IN A MANNER THAT MEETS THE REQUIREMENTS AND INTENT OF RSA 430:53 AND CHAPTER AGR 3800 RELATIVE TO INVASIVE SPECIES.
3. PRIOR TO CONSTRUCTION INSTALL SILT SACKS IN EXISTING CATCH BASINS TO PREVENT SEDIMENT FROM ENTERING THE DRAINAGE SYSTEM.
4. THE SMALLEST PRACTICABLE AREA SHALL BE DISTURBED DURING CONSTRUCTION, AND ALL DISTURBED AREAS SHALL BE STABILIZED WITHIN 72 HOURS OF ACHIEVING FINAL GRADE.
5. ALL EROSION CONTROLS SHALL BE INSPECTED WEEKLY AND AFTER EVERY HALF INCH OF RAINFALL.
6. THE ENTIRE PROJECT AREA SHALL BE SWEEPED UPON COMPLETION OF CONSTRUCTION AND PRIOR TO REMOVAL OF THE EROSION CONTROL DEVICES.
7. THE CONTRACTOR SHALL COMPLY WITH THE GENERAL EROSION AND SEDIMENTATION CONTROL NOTES AND DETAILS AS SHOWN ON THESE PLANS.
8. ALL AREAS DISTURBED BY THE CONTRACTORS OPERATIONS SHALL BE RESTORED TO A MINIMUM OF 6 INCHES OF TOPSOIL, GRASS SEED, FERTILIZER AND MULCH UNLESS OTHERWISE NOTED.
9. THE CONTRACTOR SHALL CLEAN OUT ALL DRAINAGE STRUCTURES IDENTIFIED FOR INSTALLATION OF SILT SACKS DURING CONSTRUCTION, UPON COMPLETION OF WORK.
10. AN AREA SHALL BE CONSIDERED STABLE ONCE BASE COURSE GRAVELS HAVE BEEN INSTALLED IN AREAS TO BE PAVED, A MINIMUM OF 85% VEGETATED GROWTH HAS BEEN ESTABLISHED, A MINIMUM OF 3 INCHES OF NON-EROSIVE MATERIAL, SUCH AS STONE OR RIP-RAP HAS BEEN INSTALLED OR EROSION CONTROL BLANKETS HAVE BEEN INSTALLED.
11. ALL AREAS SHALL BE STABILIZED WITHIN 45 DAYS OF INITIAL DISTURBANCE.
12. ALL PROPOSED VEGETATED AREAS THAT DO NOT EXHIBIT A MINIMUM OF 85 PERCENT VEGETATIVE GROWTH BY OCTOBER 15, OR WHICH ARE DISTURBED AFTER OCTOBER 15, SHALL BE STABILIZED BY SEEDING AND INSTALLING EROSION CONTROL BLANKETS ON SLOPES GREATER THAN 3:1, AND SEEDING AND PLACING 3 TO 4 TONS OF MULCH PER ACRE, SECURED WITH ANCHORED NETTING, ELSEWHERE. THE INSTALLATION OF EROSION CONTROL BLANKETS OR MULCH AND NETTING SHALL NOT OCCUR OVER ACCUMULATED SNOW OR ON FROZEN GROUND AND SHALL BE COMPLETED IN ADVANCE OF THAW OR SPRING MELT EVENTS.
13. ALL DITCHES OR SWALES WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY OCTOBER 15, OR WHICH ARE DISTURBED AFTER OCTOBER 15, SHALL BE STABILIZED TEMPORARILY WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW CONDITIONS.
14. ALL INCOMPLETE ROADWAY SURFACES, WHERE WORK HAS STOPPED FOR THE WINTER, SHALL BE PROTECTED WITH A MINIMUM OF 4 INCHES OF "WINTER" BINDER.
15. CONTRACTOR SHALL PROVIDE POSITIVE METHODS AND APPLY DUST CONTROL MATERIALS TO MINIMIZE RAISING DUST FROM CONSTRUCTION OPERATIONS. PROVIDE POSITIVE MEANS TO PREVENT AIR-BORNE DUST FROM DISPERSING INTO THE ATMOSPHERE. DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT BID ITEMS.
16. A NPDES PERMIT FOR CONSTRUCTION ACTIVITIES IS REQUIRED FOR THIS PROJECT. THE CONTRACTOR IS REQUIRED TO PREPARE A STORM WATER POLLUTION PREVENTION PLAN (SWPPP) AND TO SUBMIT A NOTICE OF INTENT (NOI) TO THE EPA TO FULFILL PROJECT REQUIREMENTS. THE SWPPP MUST BE PREPARED IN ACCORDANCE WITH THE EPA'S REQUIREMENTS. NO WORK IS TO PROCEED UNTIL THE SWPPP AND THE NOI IS SUBMITTED AND ACCEPTED BY THE OWNER.
17. EROSION & SEDIMENT CONTROL CONSTRUCTION SEQUENCE:
 - a. INSTALL INLET PROTECTION.
 - b. PERFORM PROJECT IMPROVEMENTS.
 - c. STABILIZE ALL DISTURBED AREAS.
 - d. ONCE THE SITE HAS BEEN STABILIZED, REMOVE ALL TEMPORARY EROSION CONTROLS.

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GENERAL NOTES

SCALE: N.T.S.

3 OF 33

CONSTRUCTION SEQUENCE (GENERAL)

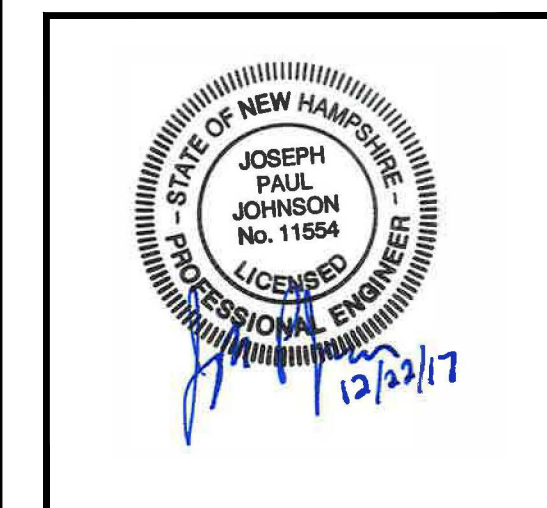
1. PROVIDE A SCHEDULE, SEQUENCE OF INSTALLATION, AND MATERIAL SUBMITTALS TO THE OWNER FOR REVIEW. MARK OUT AND CALL IN DIG SAFE. PREPARE FOR A PRECONSTRUCTION MEETING TO BE HELD WITH THE ABUTTERS. THE PERSON IN RESPONSIBLE CHARGE FOR THE PROJECT SHOULD PLAN ON ATTENDING THE MEETING.
2. INSTALL AND MAINTAIN TEMPORARY EROSION CONTROL DEVICES THROUGHOUT THE CONSTRUCTION PERIOD (INCLUDING WINTER SHUT DOWN PERIODS AS REQUIRED) AS SHOWN IN THE APPROVED SWPPP, ON THE DRAWINGS, OR AS APPROVED BY THE ENGINEER.
3. INSTITUTE EXPLORATORY EXCAVATION PROGRAM WITH ENGINEER TO IDENTIFY POTENTIAL CONFLICTS AT UTILITY CROSSINGS. ANY EXPLORATORY EXCAVATION COMPLETED OUTSIDE THE LIMITS SHOWN ON THE DRAWINGS WITHOUT PRIOR APPROVAL FROM THE ENGINEER WILL BE AT NO ADDITIONAL COST TO THE OWNER.
4. DISPOSE OF SURPLUS AND UNSUITABLE MATERIALS AS THE WORK PROGRESSES. STOCKPILES WILL NOT BE ALLOWED ON SITE UNLESS APPROVED BY THE ENGINEER AHEAD OF TIME. EXCAVATED MATERIALS WILL BE LOADED INTO TRUCKS AND TAKEN AWAY AS WORK PROGRESSES IN ORDER TO KEEP THE ROAD PASSABLE.
5. CONTRACTOR IS TO MAINTAIN DRY AND STABLE TRENCH CONDITIONS AT ALL TIMES. A DE-WATERING PLAN MUST BE PROVIDED AND APPROVED BY THE ENGINEER.
6. IMMEDIATELY STABILIZE DISTURBED AREAS AFTER PIPE INSTALLATION AND REESTABLISH TEMPORARY EROSION CONTROL DEVICES MOVED DURING CONSTRUCTION.
7. REPAIR TRENCHES EACH NIGHT WITH EITHER RECLAIMED BASE (IF CONSTRUCTION PHASING ALLOWS), SUITABLE EXCAVATED SUBBASE OR CRUSHED GRAVEL. THE TRENCH(ES) SHALL BE FLAT AND COMPACTED FIRM EACH NIGHT. CONSTRUCT TEMPORARY PAVEMENT REPAIRS AS SOON AS PRACTICAL FOLLOWING UTILITY INSTALLATION AND TESTING. TRENCH REPAIRS SHALL BE PAID FOR UNDER THE ASSOCIATED FINAL PAVING ITEMS AS DETAILED.
8. RESTORE ROAD DRAINAGE AT NIGHT PRIOR TO LEAVING THE SITE.
9. FINISH GRADING, LOAM AND SEED DISTURBED AREAS AND BACK UP PAVEMENT WITH GRAVEL IMMEDIATELY FOLLOWING PAVEMENT REPAIRS.
10. REMOVE ALL TEMPORARY EROSION CONTROL DEVICES AS SOON AS VEGETATION IS ESTABLISHED AND AREAS ARE STABILIZED.
11. TEMPORARY PAVEMENT (ITEM 403.99) SHALL BE USED FOR THE TOP COURSE OF TRENCH PATCHES PERFORMED PRIOR TO COLD PLANING. THIS ITEM SHALL ALSO BE USED AS NECESSARY AND AS DIRECTED BY THE ENGINEER TO MAINTAIN TEMPORARY DRIVEWAY AND PEDESTRIAN ACCESS.

ROAD WORK NOTES

1. EXISTING SIDEWALKS SHALL BE MAINTAINED PROVIDED THEY DO NOT CONFLICT WITH THE WORK ZONE. IF A SIDEWALK IS NOT PASSABLE, OR A PORTION IS WITHIN A WORK ZONE, THE SIDEWALK SHALL BE SIGNED AS 'CLOSED' IN ADVANCE SO THAT A PEDESTRIAN CAN CHOOSE AN ALTERNATIVE ROUTE.
2. AFTER THE INSTALLATION OF UTILITIES, EXISTING GRANITE CURBING SHALL BE REMOVED AND ANY PIECES 4' OR LONGER SHALL BE RETAINED FOR REUSE ON THE PROJECT.
3. IT IS THE INTENT OF THIS PROJECT TO RECLAIM THE EXISTING ASPHALT ALONG ANDREW JARVIS DRIVE PRIOR TO THE INSTALLATION OF ASSOCIATED UTILITY WORK. THESE TRENCHES SHALL BE REPAIRED AS PER THE CONTRACT DOCUMENTS TO PREVENT THE CONTAMINATION OF THE RECLAIMED MATERIAL.
4. RECLAIMED MATERIAL SHALL BE REMOVED AND REHANDLED IN ORDER TO PERFORM ADDITIONAL EXCAVATION TO ACCOUNT FOR THE DEPTH OF THE PROPOSED PAVEMENT LAYERS.
5. WHEN NECESSARY, STONE FOR BLENDING WILL BE APPLIED TO THE RECLAIMED SURFACE AND RE-BLENDED WITH THE PREVIOUSLY RECLAIMED MATERIAL TO MEET THE REQUIRED RECLAIMED STABILIZED BASE GRADATION.
6. WINDROWED OR GRADED RECLAIM PAVEMENT BORROW SHALL BE USED ON THE PROJECT UNDER AREAS OF WIDENING WALKS, DRIVES, FOR LEVELING. REMOVAL OF EXCESS RECLAIMED BASE MATERIAL IS SUBSIDIARY TO RECLAIMED STABILIZED BASE.
7. ALL EXISTING SUITABLE GRANITE CURBING REMOVED WITHIN THE PROJECT LIMITS WILL BE REUSED FIRST. ADDITIONAL GRANITE WILL BE PURCHASED TO MAKE UP THE DIFFERENCE. ANY EXCESS MATERIAL SHALL REMAIN PROPERTY OF THE CITY AND SHALL BE TRANSPORTED TO A DESIGNATED LOCATION.
8. ALL DRIVEWAYS SHALL RECEIVE TWO LAYERS OF ASPHALT AS CALLED FOR IN THE PAVEMENT NOTES. DO NOT REMOVE OR PAVE DRIVEWAYS BEYOND THE PROPERTY LINE UNLESS DEPICTED IN THE CONTRACT DOCUMENTS OR AS DIRECTED BY THE ENGINEER.
9. ALL SIDEWALKS WILL BE CONSTRUCTED TO ADA STANDARDS AND CROSS SLOPES SHALL NOT EXCEED 2%.
10. PAVEMENT WILL NOT BE PLACED AT SURFACE TEMPERATURES COLDER THAN 40 DEGREES F FOR BINDER COURSE OR 50 DEGREES F FOR WEARING COURSE. IF PAVING IS ALLOWED, IT IS THE CONTRACTORS RESPONSIBILITY TO COMPACT THE ASPHALT PROPERLY BEFORE IT SETS.
11. CONCRETE SIDEWALKS WILL NOT BE POURED IF FREEZING TEMPERATURES ARE EXPECTED WITHIN 24 HOURS. ALL CONCRETE SIDEWALKS WILL BE COVERED WITH PLASTIC FILM FOR 3 DAYS AFTER PLACEMENT. PLASTIC WILL BE PROPERLY SECURED AS NOT TO BLOW OFF.
12. PUBLIC SAFETY IS A PRIORITY. THE CONTRACTOR WILL SUPPLY THE PROPER HAZARD WARNINGS AS ADVISED. IT IS THE DUTY OF THE CONTRACTOR'S SAFETY COORDINATOR TO PREVENT PERSONAL INJURY AND/OR DAMAGES TO THE RESIDENTS.
13. IN THE EVENT THAT SUSPECTED CONTAMINATED SOIL, GROUNDWATER, OR OTHER MEDIA ARE ENCOUNTERED DURING EXCAVATION AND CONSTRUCTION ACTIVITIES, BASED ON VISUAL, OLFACTORY, OR OTHER EVIDENCE, THE CONTRACTOR SHALL STOP WORK IN THE VICINITY OF OF THE SUSPECT MATERIAL TO AVOID FURTHER CONTAMINATION OF ADJACENT MATERIAL, AND SHALL NOTIFY THE OWNER IMMEDIATELY, SO THAT THE APPROPRIATE TESTING AND SUBSEQUENT ACTION CAN BE TAKEN.
14. THE TERM TO "MEET EXISTING" SHALL MEAN TO MEET BOTH THE EXISTING ALIGNMENT AND ELEVATION.

TRAFFIC MANAGEMENT NOTES

1. ALL TRAFFIC MANAGEMENT AND WORK ZONE TRAFFIC CONTROL MEASURES SHALL CONFORM TO THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.), THE CURRENT NHDOT - "WORK ZONE TRAFFIC CONTROL STANDARD PLANS", THE STANDARD SPECIFICATIONS, AND THE FOLLOWING NOTES.
2. LANE RESTRICTIONS MAY NOT REMAIN OVERNIGHT OR DURING NON-WORKING HOURS. AFTER EACH WORKING DAY, TRAFFIC CONTROL DEVICES THAT ARE NOT REQUIRED SHALL BE MOVED OFF THE ROADWAY OR FULL DEPTH CONSTRUCTION AREA AND PLACED SO AS NOT TO IMPEDE PEDESTRIAN AREAS, ABUTTER ACCESS OR CAUSE CONFUSION TO MOTORISTS. IN CERTAIN CIRCUMSTANCES, AND ONLY WITH THE APPROVAL OF THE CITY AND THE ENGINEER, LANE RESTRICTIONS MAY REMAIN OVERNIGHT. THE STREETS IN THE PROJECT AREA WILL BE PASSABLE AND SAFE IN THE OPINION OF THE ENGINEER PRIOR TO WORK TERMINATING AT THE END OF THE DAY.
3. THE CONTRACTOR SHALL MAINTAIN ONE PASSABLE LANE AND SAFE PASSAGE FOR RESIDENTS TO AND FROM THEIR DWELLINGS IN THE NEIGHBORHOODS. WORK THAT REQUIRES THE COMPLETE SHUT DOWN OF THE STREET HAS TO BE APPROVED BY THE ENGINEER PRIOR TO THE WORK COMMENCING.
4. THE PROJECT SHALL BE SUBSTANTIALLY COMPLETE BY AUGUST 24, 2018.
5. TRAFFIC SIGNAL WORK MAY BE PERFORMED DURING THE SCHOOL YEAR WITHIN THE WINDOW OF TIME BETWEEN THE START OF THE SCHOOL DAY AND 30 MINUTES BEFORE SCHOOL DISMISSAL.
6. ALL OTHER WORK SHALL BE PERFORMED WHILE THE HIGH SCHOOL IS OUT OF SESSION FOR THE SUMMER, UNLESS APPROVED BY OWNER.



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**INTERSECTION IMPROVEMENT PROJECT
 U.S. ROUTE 1 AT ANDREW JARVIS DRIVE
 CITY OF PORTSMOUTH
 NEW HAMPSHIRE**

REVISIONS		
NO.	REVISION	DATE

**GENERAL
 NOTES**

SCALE: N.T.S.

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**INTERSECTION IMPROVEMENT PROJECT
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PAVEMENT NOTES

FULL DEPTH HOT BITUMINOUS PAVEMENT WIDENING (LAFAYETTE ROAD - U.S. ROUTE 1)

SURFACE: ITEM 403.11 HOT BITUMINOUS PAVEMENT, MACHINE METHOD, HIGH STRENGTH

1 1/2" WEARING COURSE, NHDOT 3/8" HIGH STRENGTH, OVER
 2 1/2" BINDER COURSE, NHDOT 1/2", OVER
 3 1/2" BASE COURSE, NHDOT 1", OVER

BASE: 8" RECLAIMED STABILIZED BASE, OR 8" CRUSHED GRAVEL, ITEM 304.301

SUBBASE: 12" GRAVEL (F), ITEM 304.2 (SUITABLE MATERIAL TO REMAIN)

HOT BITUMINOUS PAVEMENT OVERLAY (LAFAYETTE ROAD - U.S. ROUTE 1)

SURFACE: ITEM 403.11 HOT BITUMINOUS PAVEMENT, MACHINE METHOD, HIGH STRENGTH

1 1/2" COLD PLANING
 1 1/2" WEARING COURSE, NHDOT 3/8" HIGH STRENGTH

TACK COAT: EMULSIFIED ASPHALT FOR TACK COAT AT 0.05 GAL/SY OVER EXISTING PAVEMENT.

FULL DEPTH HOT BITUMINOUS PAVEMENT WITH RECLAIMED BASE (ANDREW JARVIS DRIVE)

SURFACE: ITEM 403.12 HOT BITUMINOUS PAVEMENT, MACHINE METHOD

1 1/2" WEARING COURSE, NHDOT 3/8", OVER
 3 1/2" BINDER COURSE, TO BE PLACED IN TWO LAYERS, NHDOT 3/4", OVER

BASE: 8" RECLAIMED STABILIZED BASE

FULL DEPTH HOT BITUMINOUS PAVEMENT WIDENING (ANDREW JARVIS DRIVE)

SURFACE: ITEM 403.12 HOT BITUMINOUS PAVEMENT, MACHINE METHOD

1 1/2" WEARING COURSE, NHDOT 3/8", OVER
 3 1/2" BINDER COURSE, TO BE PLACED IN TWO LAYERS, NHDOT 3/4", OVER

BASE: 8" RECLAIMED STABILIZED, OR 8" CRUSHED GRAVEL, ITEM 304.301

HOT BITUMINOUS PAVEMENT DRIVEWAY:

SURFACE: ITEM 403.12 HOT BITUMINOUS PAVEMENT, HAND METHOD

1 1/2" WEARING COURSE, NHDOT 3/8", OVER
 2" BINDER COURSE, NHDOT 1/2"

SUBBASE: 8" CRUSHED GRAVEL, ITEM 304.301 OR 8" RECLAIMED STABILIZED BASE

CONCRETE SIDEWALK

SURFACE: 4" FIBER REINFORCED CEMENT CONCRETE

SUBBASE: 8" CRUSHED GRAVEL (F), ITEM 304.301 OR 8" RECLAIMED STABILIZED BASE

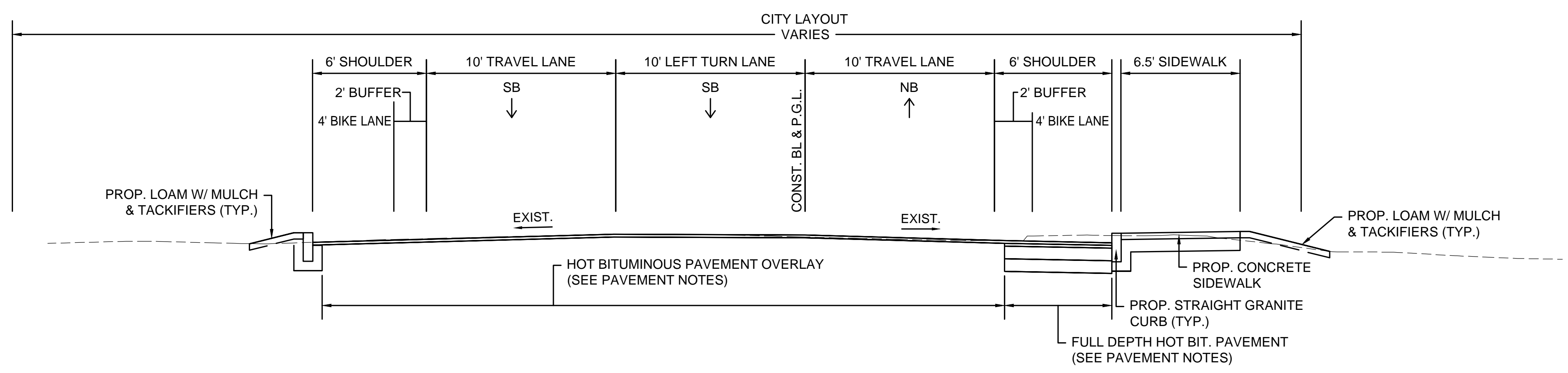
CONCRETE CURB RAMP

SURFACE: 6" FIBER REINFORCED CEMENT CONCRETE

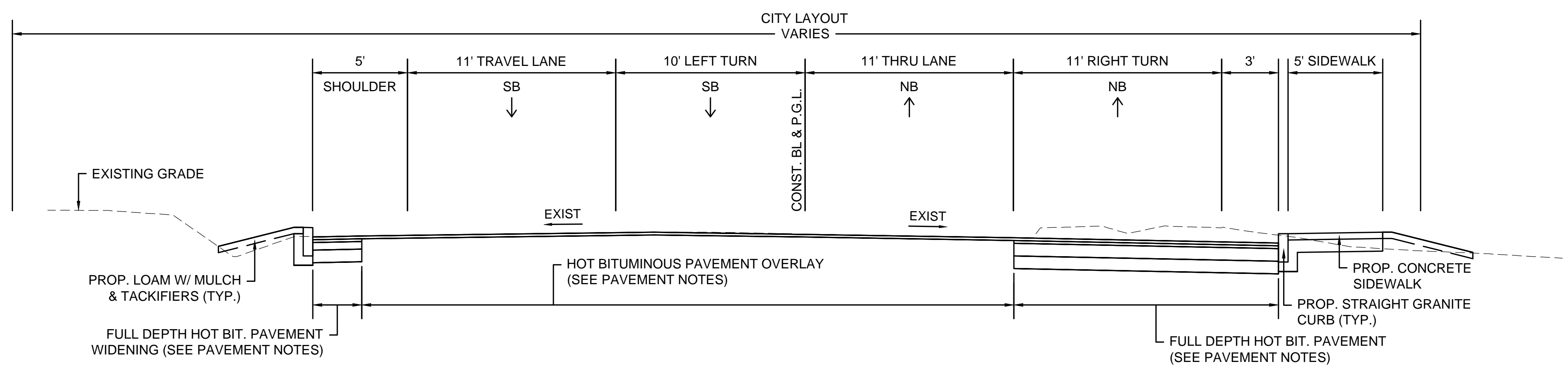
SUBBASE: 8" CRUSHED GRAVEL (F), ITEM 304.301

NOTES:

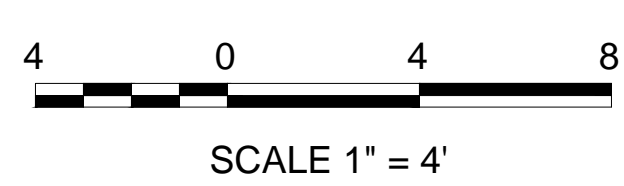
1. THE SECTIONS OF PROPOSED ROADWAY NOT COVERED IN THE RANGE OF STATIONS ASSOCIATED WITH THE TYPICAL SECTIONS ARE IN AREAS OF TRANSITION AND THEREFORE NOT COVERED.
2. ASPHALT EMULSION FOR TACK COAT SHALL BE APPLIED AT THE RATE OF 0.03 GALLONS PER SQUARE YARD OVER SMOOTH SURFACES AND 0.05 GALLONS PER SQUARE YARD OVER COLD PLANED SURFACES PER SUPPLEMENTAL SPECIFICATION FOR SECTION 410.
3. PROPOSED SIDEWALK WIDTHS MAY VARY SLIGHTLY FROM DIMENSIONS SHOWN ON THE TYPICAL SECTIONS AND CONSTRUCTION PLANS. WHERE CURBING, WALLS, ETC. ARE LOCATED ALONG THE FRONTAGE OF A PROPERTY, THE SIDEWALK WIDTH SHALL BE ADJUSTED TO MEET.
4. ALL SIDEWALKS SHALL PROVIDE MINIMUM 36 INCH CLEARANCE AROUND POINT OBSTRUCTIONS. THE CONTRACTOR WILL BE RESPONSIBLE FOR NOTIFYING THE ENGINEER WHEN THIS MINIMUM REQUIREMENT CANNOT BE MET FOR RESOLUTION.
5. ALL PROPOSED SIDEWALKS SHALL BE CONCRETE, UNLESS SPECIFICALLY NOTED ON THE ROADWAY AND PROFILE PLANS TO BE BITUMINOUS. ALL CURB RAMP SHALL BE CONCRETE.
6. ALL SIDE SLOPES TO BE ESTABLISHED WITH TURF SHALL BE 3:1 MAX. UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
7. RECLAIMED STABILIZED BASE SHALL COMPLY WITH NHDOT SECTION 306.
8. RECLAIMED MATERIAL TO BE TESTED TO DETERMINE IF ADDITIONAL STONE WILL BE REQUIRED FOR BLENDING.
9. AT WIDENED AREAS, CRUSHED GRAVEL (ITEM 304.301) SHALL BE USED IF REHANDLED RECLAIM QUANTITY IS NOT AVAILABLE.
10. PROVIDE 2' WIDE CRUSHED GRAVEL (ITEM 304.301) SHOULDER WHERE GRANITE CURB OR EDGING IS NOT PROPOSED.
11. EXCESS RECLAIMED BASE MATERIAL SHALL BE USED ON THE PROJECT UNDER WALKS, DRIVES, ISLANDS AND FOR LEVELING. REMOVAL OF EXCESS RECLAIMED BASE MATERIAL IS SUBSIDIARY TO RECLAIMED STABILIZED BASE.

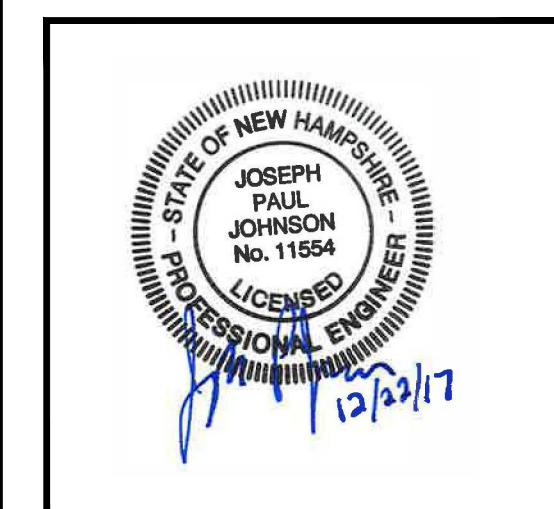


TYPICAL SECTION - U.S. ROUTE 1 (LAFAYETTE ROAD)
 SCALE: 1" = 4'
 STA. 5+02 TO STA. 6+83



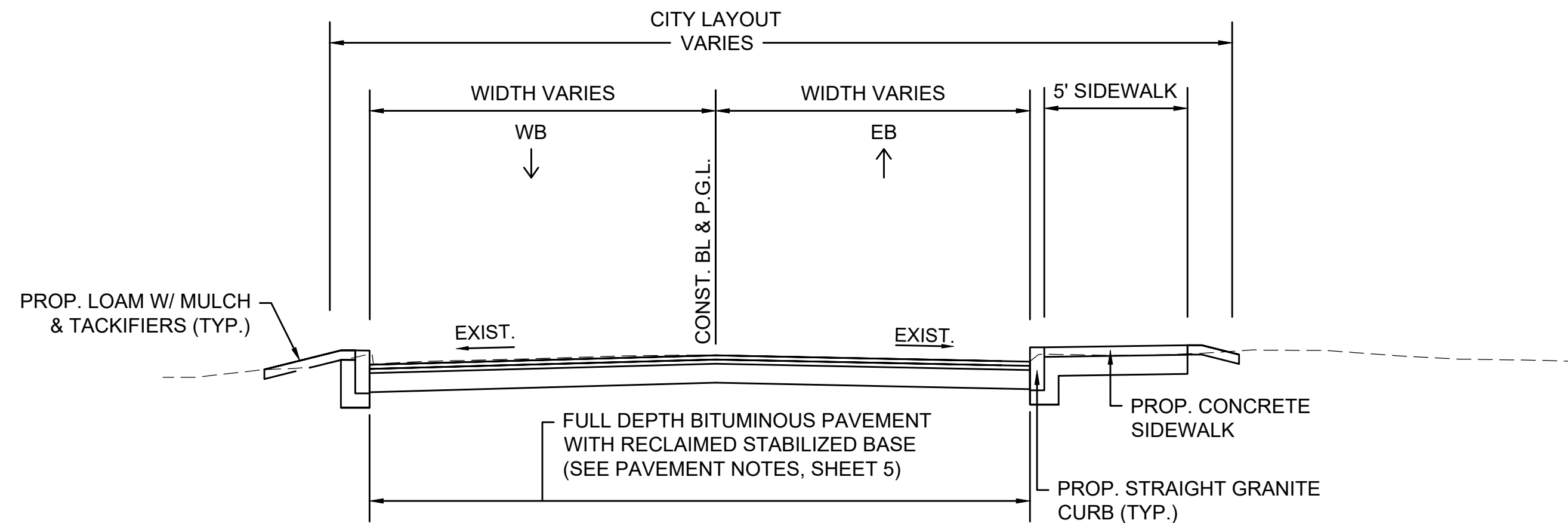
TYPICAL SECTION - U.S. ROUTE 1 (LAFAYETTE ROAD)
 SCALE: 1" = 4'
 STA. 3+24 TO STA. 4+10





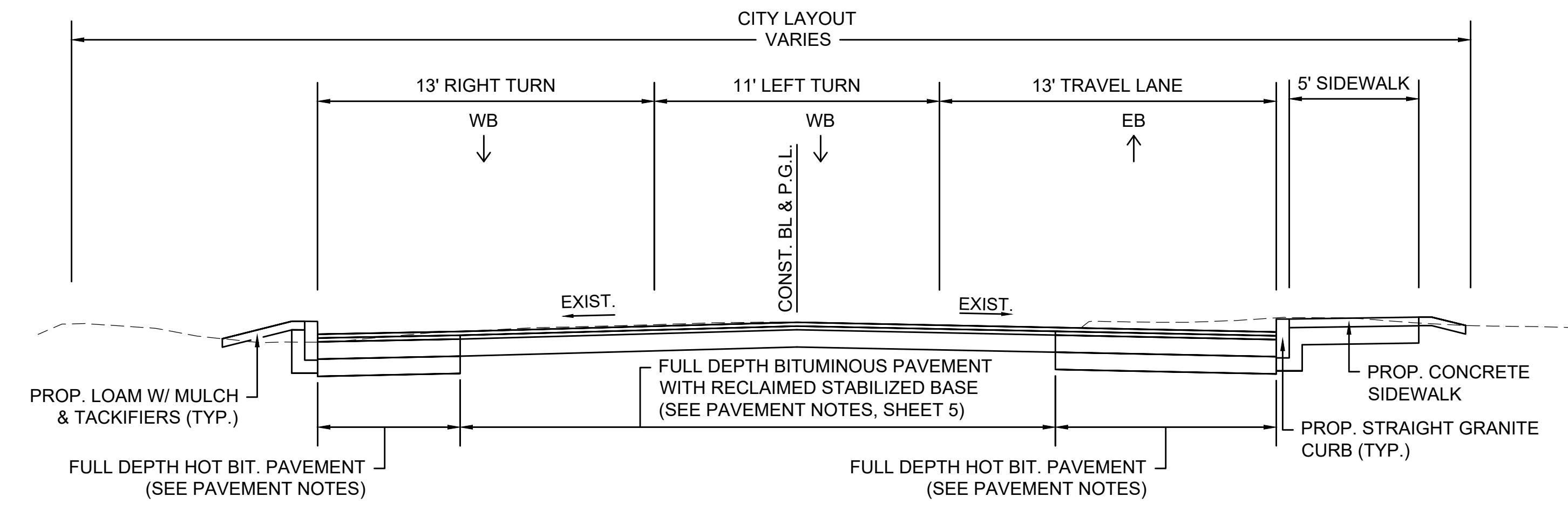
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**INTERSECTION IMPROVEMENT PROJECT
 U.S. ROUTE 1 AT ANDREW JARVIS DRIVE
 CITY OF PORTSMOUTH
 NEW HAMPSHIRE**



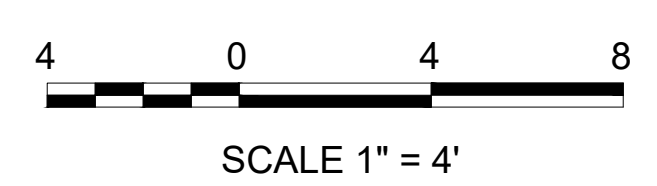
TYPICAL SECTION - ANDREW JARVIS DRIVE

SCALE: 1" = 4'
 STA. 23+41 TO STA. 28+00



TYPICAL SECTION - ANDREW JARVIS DRIVE

SCALE: 1" = 4'
 STA. 20+35 TO STA. 23+41



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NO.	REVISION	DATE

12/22/17
 DRAWN/DESIGN BY: DPE
 CHECKED BY: JRN

TYPICAL SECTIONS

SCALE: 1" = 4'

THE FOLLOWING STANDARD PLANS WILL BE USED ON THIS PROJECT
NHDOT STANDARD PLANS (MOST RECENT VERSION):

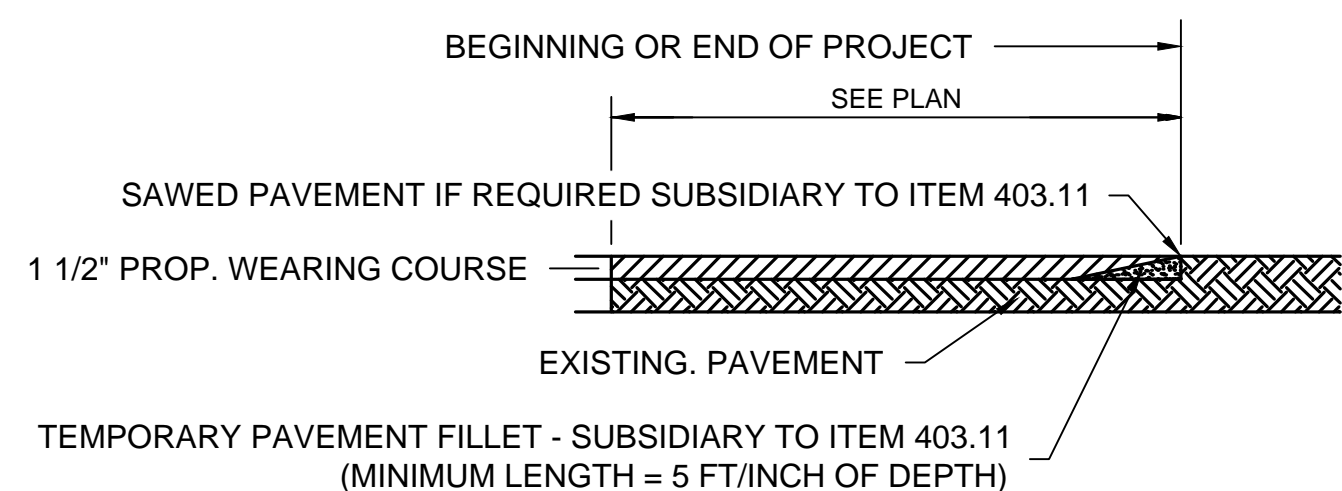
- CR-1 GRANITE CURB DETAILS
- CR-2 CURB DETAILS
- DR-1 GRATE AND FRAME DETAILS
- DR-2 D.I., MANHOLE COVER AND PAVEMENT DEPRESSION DETAILS
- DR-4 POLYETHYLENE LINER
- DR-5 PRECAST REINFORCED CONCRETE C.B., D.I., AND M.H.
- ES-1 END SECTION FOR PLASTIC & CORRUGATED STEEL PIPE
- SIDEWALK CURB RAMPS WITH DETECTABLE WARNINGS
- DRIVEWAY DETAILS

ALL PAVEMENT MARKING STANDARDS, TRAFFIC SIGNAL STANDARDS, PERMANENT SIGNING STANDARDS AND SIGNAL AND LIGHTING STANDARDS SHALL APPLY.

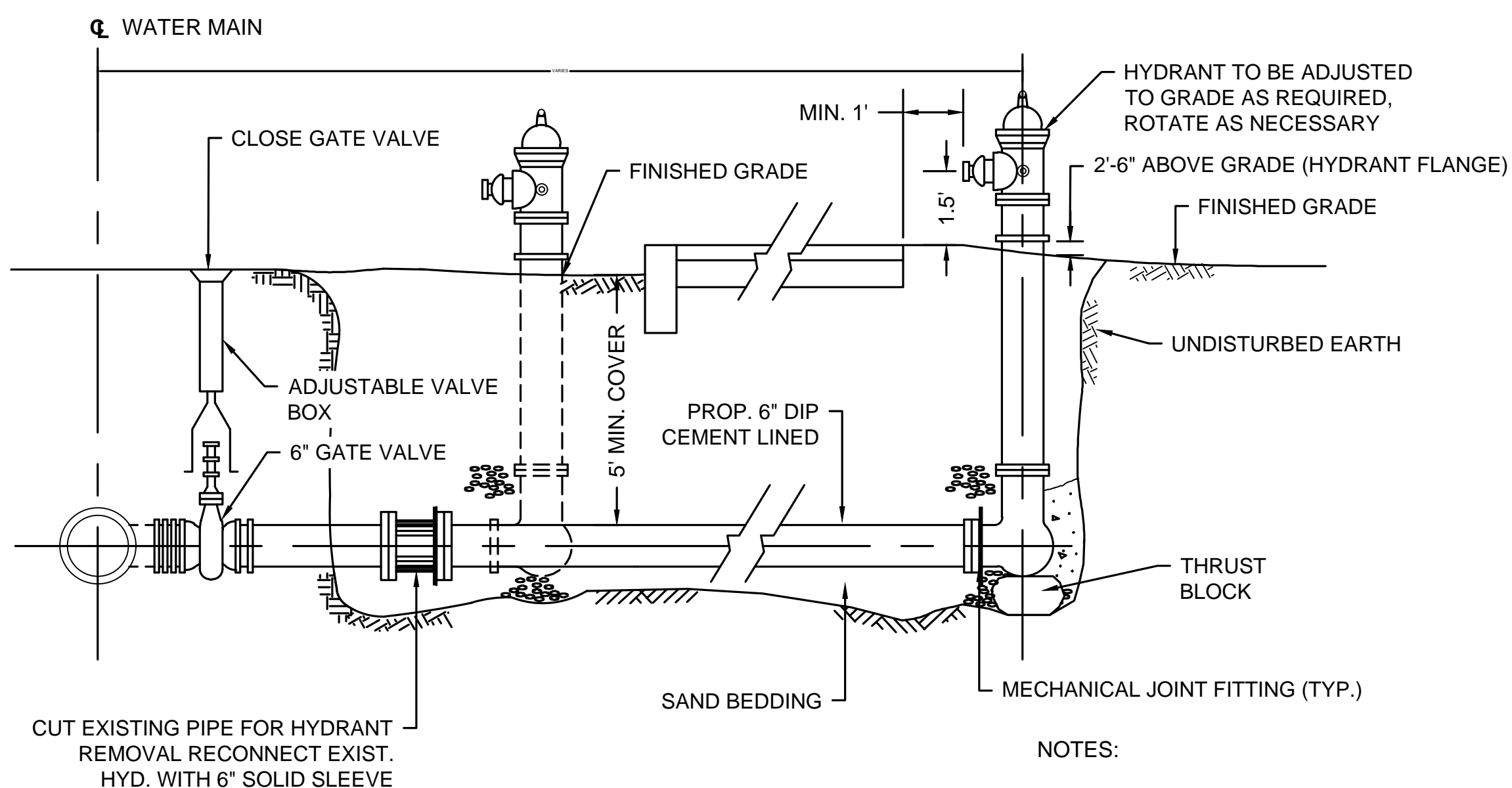
ALL WORK ZONE TRAFFIC CONTROL PLANS APPLICABLE TO THIS LOCATION.

SHEETS TC-1 THROUGH TC-8 AMEND PART VI OF THE 2009 EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) BY SUPERCEDING OR SUPPLEMENTING CERTAIN SECTIONS. THEY SHALL BE USED IN CONJUNCTION WITH THE MUTCD AND THE SPECIFICATIONS FOR WORK ZONE TRAFFIC CONTROL.

NOTE: CONTRACTOR IS RESPONSIBLE FOR DESIGNING, IMPLEMENTING, AND MAINTAINING AN EROSION CONTROL PLAN THAT IS ACCEPTABLE TO THE CITY OF PORTSMOUTH.

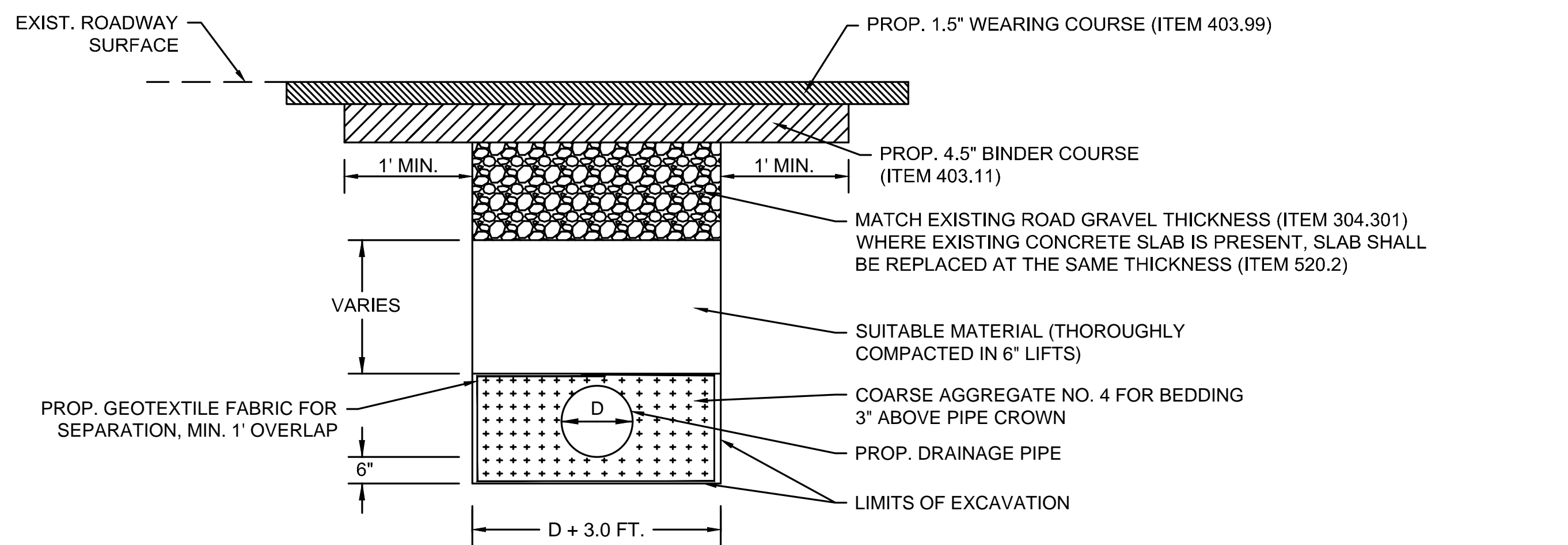


INLAY PAVEMENT MATCH TYPICAL
NOT TO SCALE



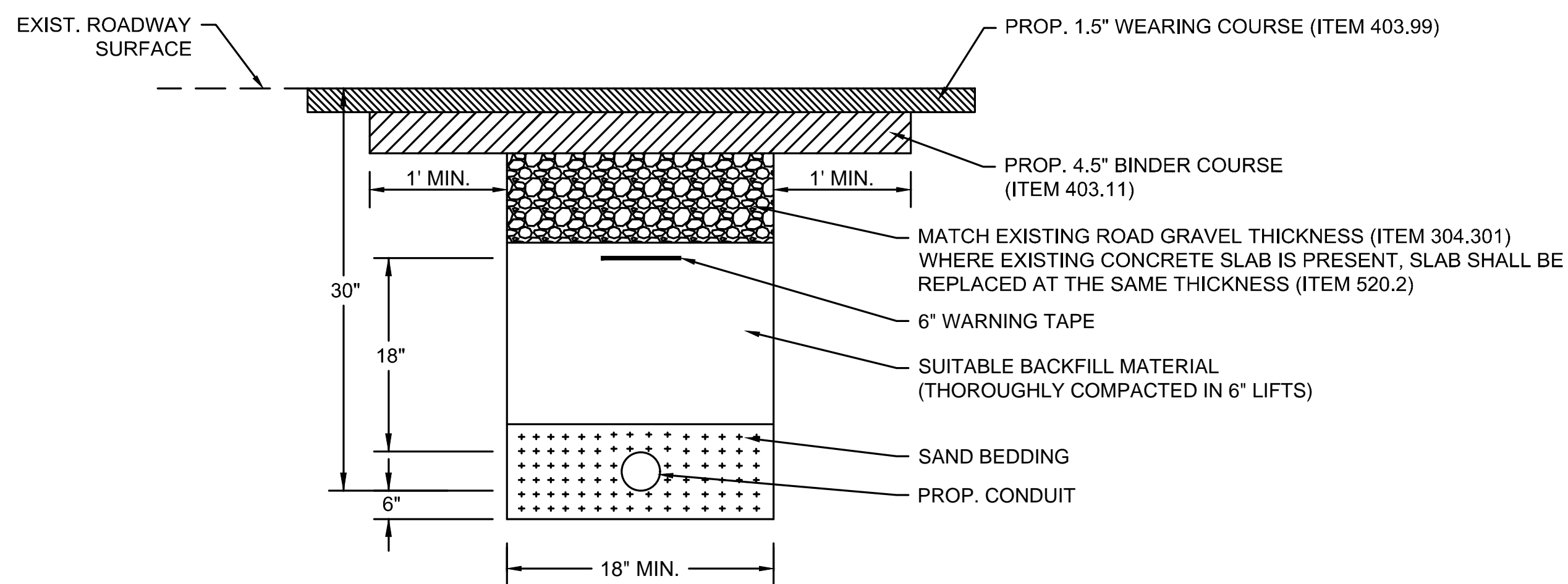
**HYDRANT INSTALLATION
HYDRANT REMOVE AND RESET**
NOT TO SCALE

- NOTES:
1. ALL WATER GATES & HYDRANTS TO OPEN RIGHT.
 2. ALL MECHANICAL RESTRAINT DEVICES, SOLID SLEEVES, STONE, SAND, GRAVEL, ETC. NECESSARY TO INSTALL OR REMOVE AND RESET HYDRANTS ARE INCIDENTAL TO ITEM 611.81100.

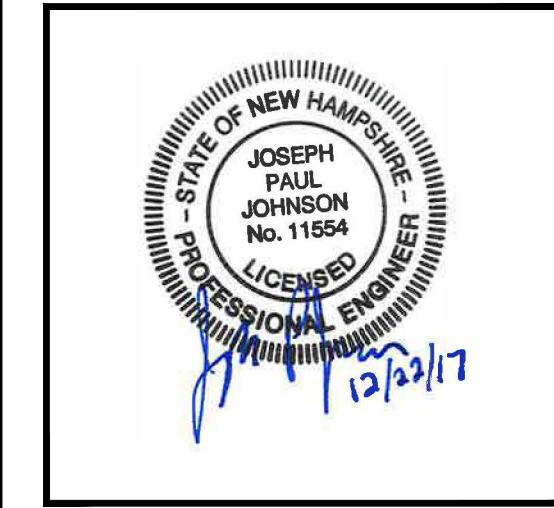


PERMANENT PATCH
(IN AREAS OF COLD PLANE & HOT BITUMINOUS OVERLAY)

1. PLACE TACK COAT BETWEEN ALL HMA COURSES AND SAWCUT SURFACES PRIOR TO PLACING HMA. SEE PAVEMENT NOTES-FULL DEPTH HMA CONSTRUCTION FOR DEPTH OF HMA.
2. IF UNSUITABLE MATERIAL IS ENCOUNTERED AS DETERMINED BY THE ENGINEER, ADDITIONAL BEDDING SHALL BE PAID FOR UNDER ITEM 304.4.



**TRAFFIC SIGNAL CONDUIT
TRENCH DETAIL**
(WITHIN ROADWAY LIMITS)



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CITY OF PORTSMOUTH
NEW HAMPSHIRE**

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12/22/17	
DRAWN/DESIGN BY CLS	CHECKED BY JPJ

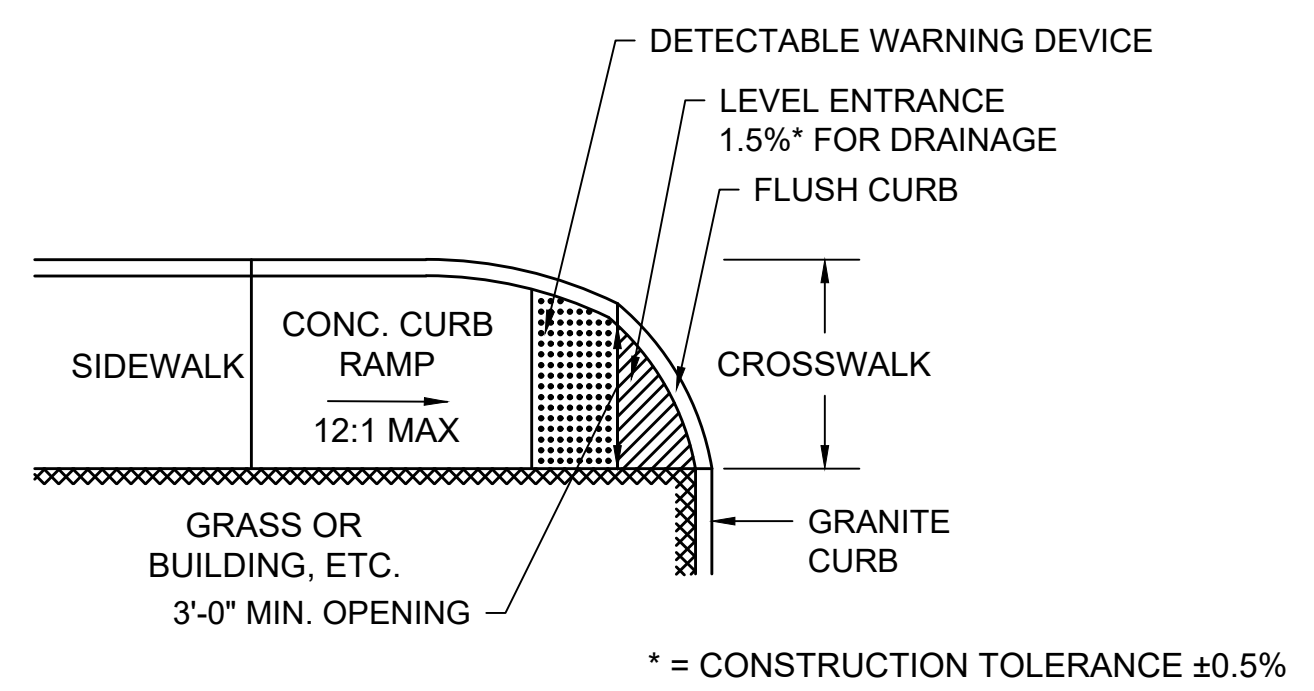
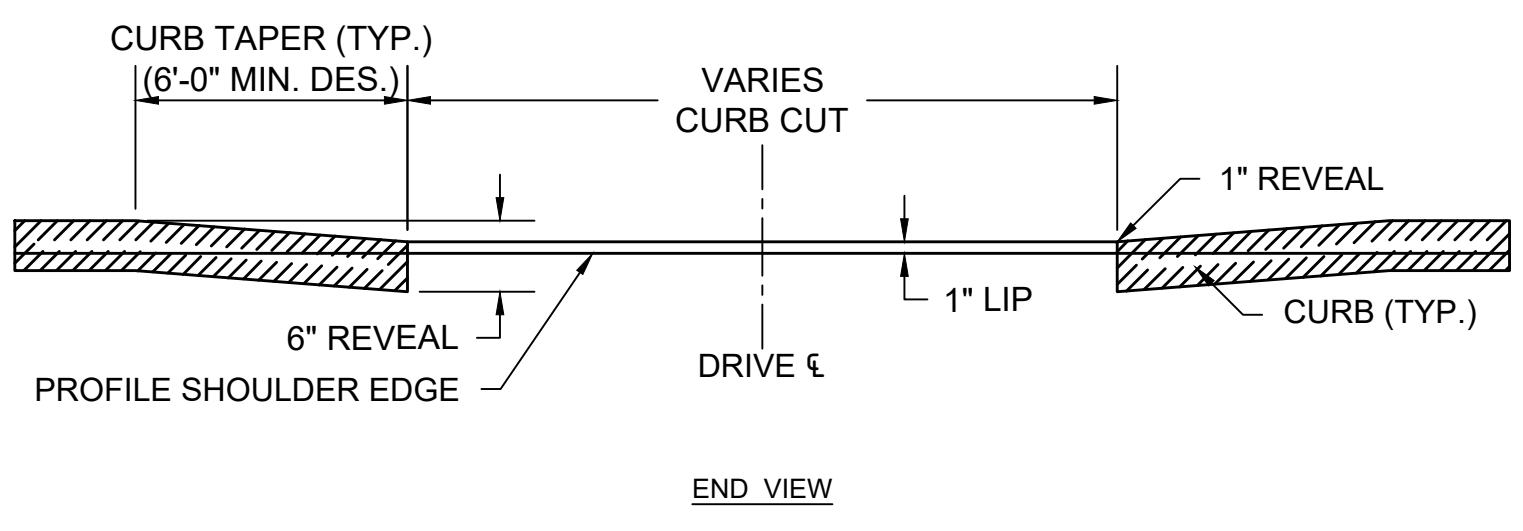
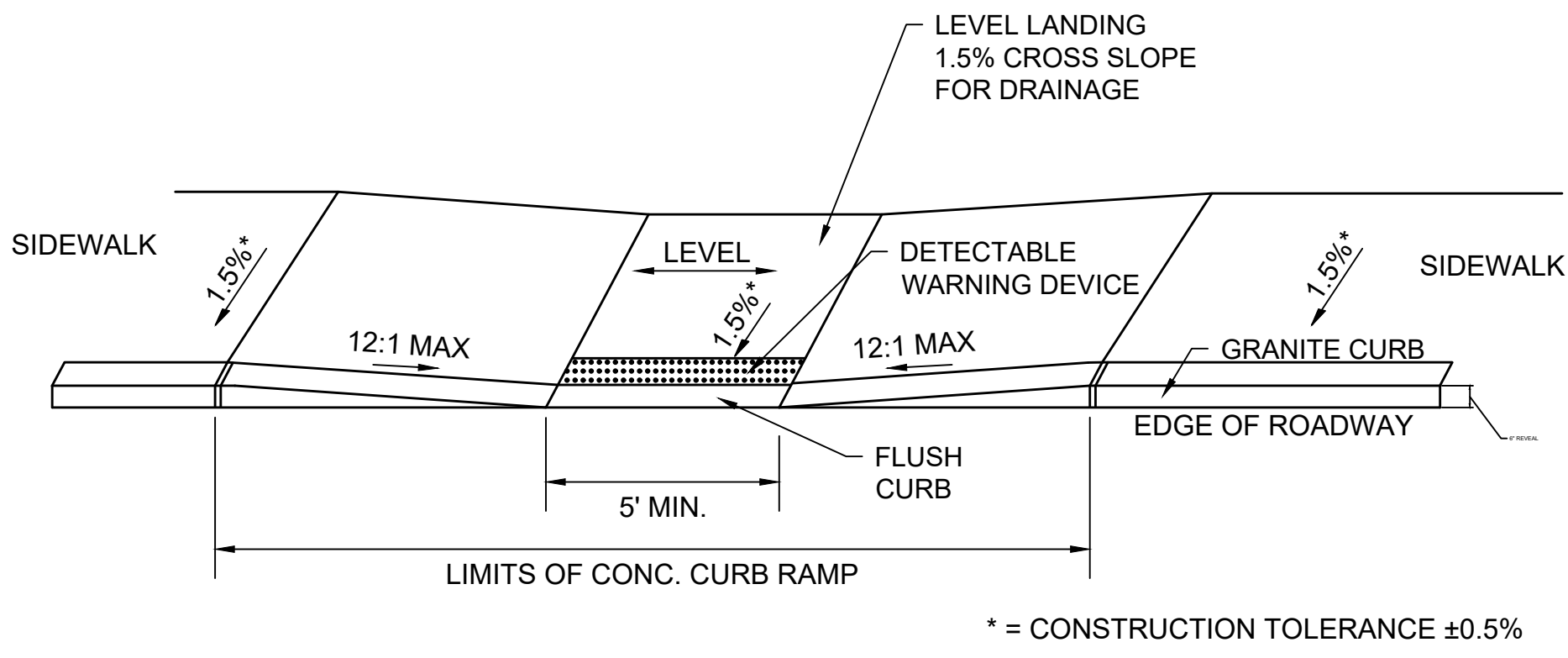
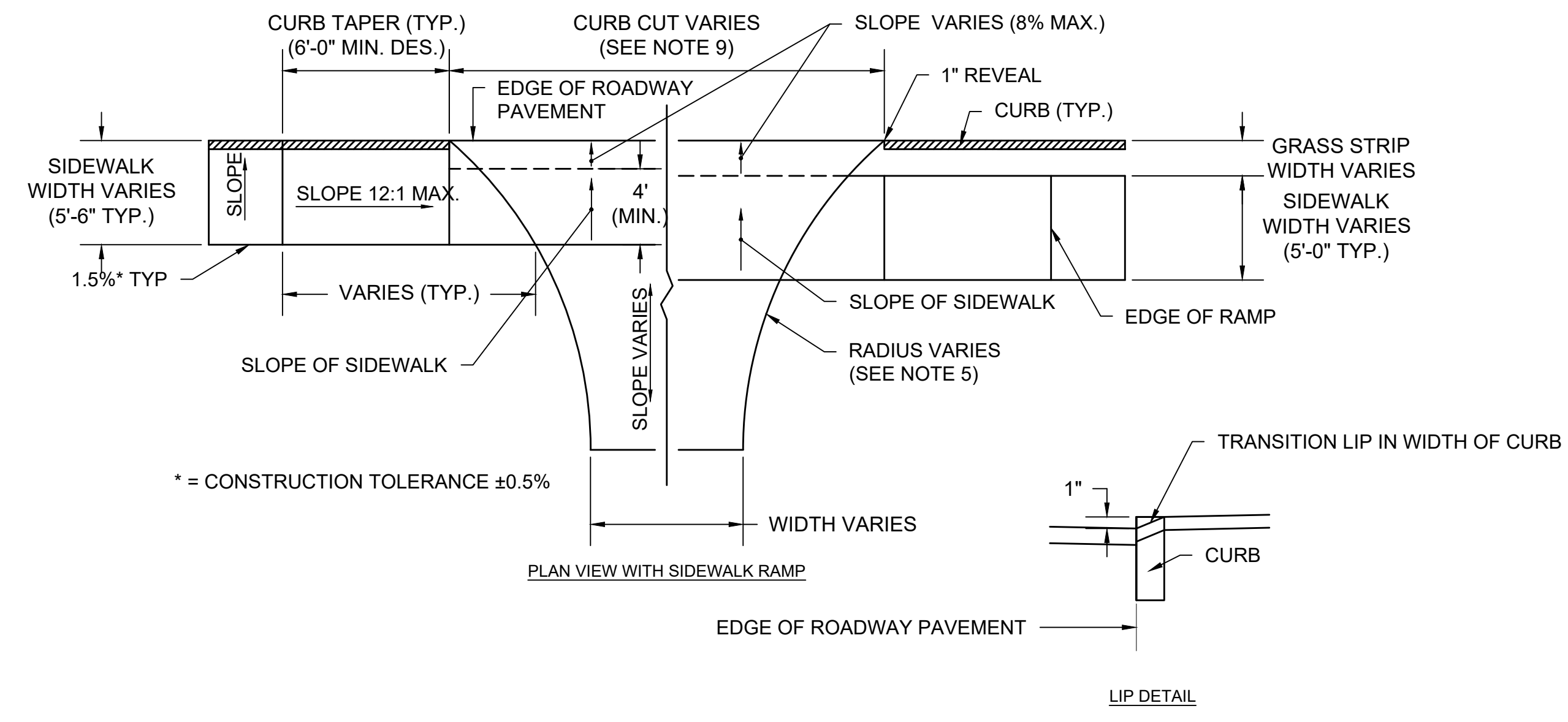
**CONSTRUCTION
DETAILS**

SCALE: N.T.S.

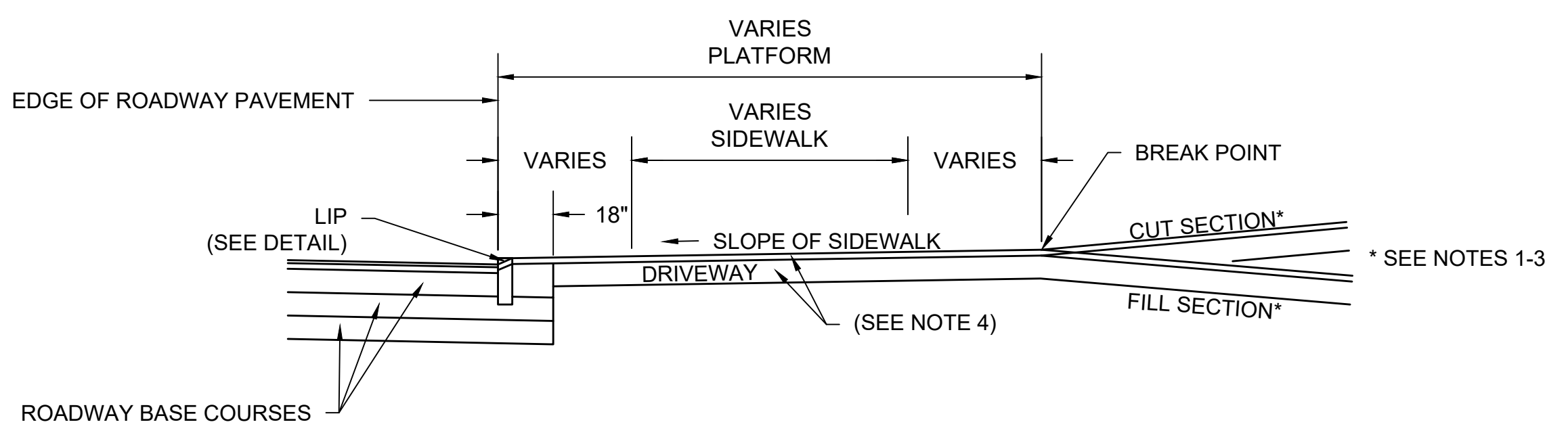


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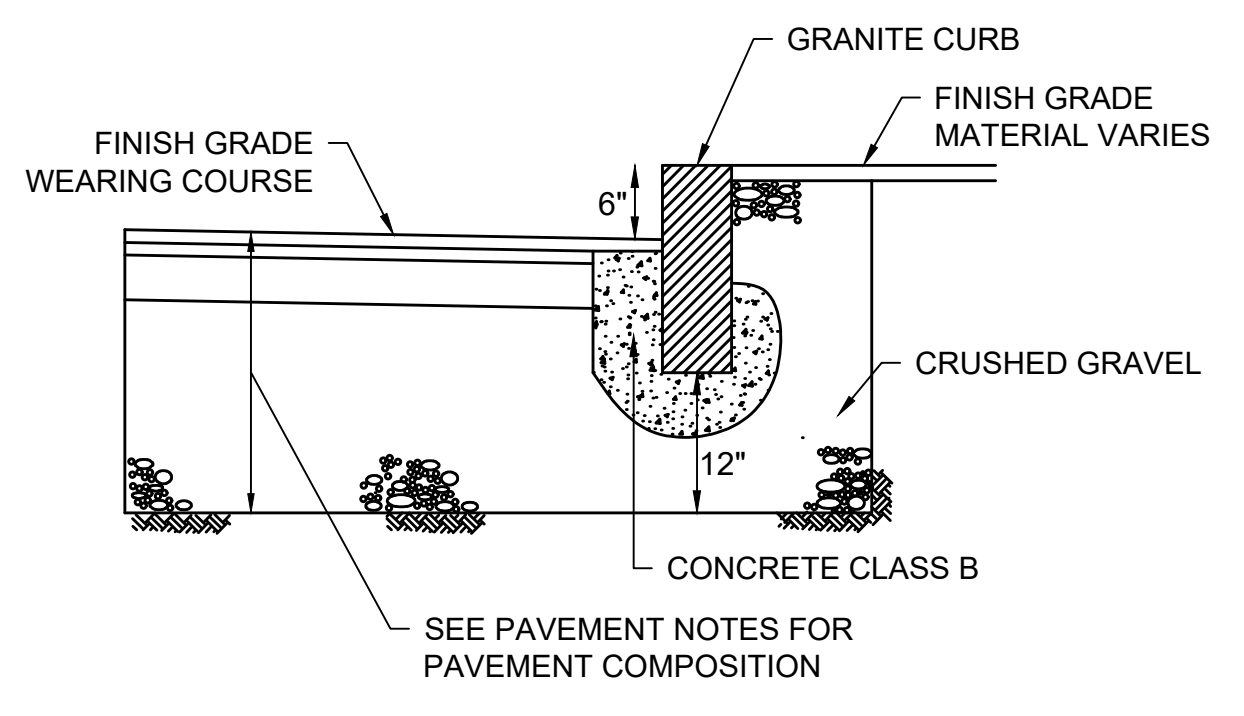


CURB RAMP DETAILS
 NOT TO SCALE

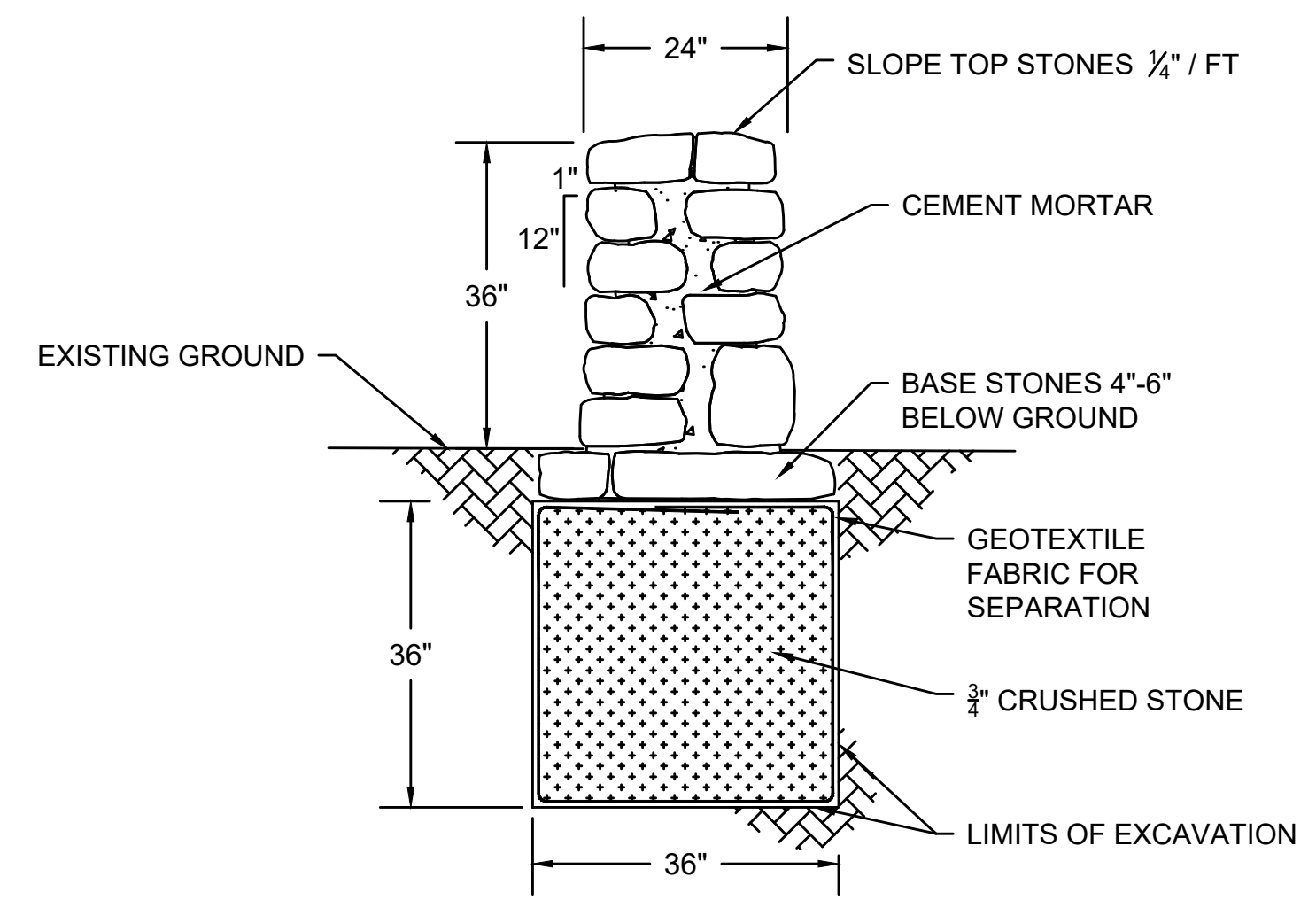


- NOTES:**
- GRADES OF MAJOR ENTRANCES BEYOND THE PLATFORM SHOULD NOT EXCEED 8%.
 - GRADES OF OTHER DRIVES BEYOND THE PLATFORM SHOULD NOT EXCEED 15%.
 - THE ALGEBRAIC DIFFERENCE BETWEEN TWO ADJACENT GRADES SHOULD NOT EXCEED 10%.
 - SEE SHEET 5 FOR PAVEMENT NOTES.
 - FOR DESIGN CRITERIA AND OTHER ADDITIONAL INFORMATION, REFER TO THE NHDOT DRIVEWAY MANUAL.
 - DITCHES ARE RECOMMENDED FOR UNCURBED DRIVEWAYS IN CUT SLOPES.
 - USE SLOPED END SECTIONS ON DRIVE PIPES FOR UNCURBED DRIVEWAYS.
 - CURBING CAN BE FLARED TO FIT DRIVE RADII IF APPROPRIATE OR ENDED AS DETAILED ABOVE.

TYPICAL URBAN CURVED DRIVE IN CUT/FILL SECTION
 NOT TO SCALE



GRANITE CURB
 NOT TO SCALE



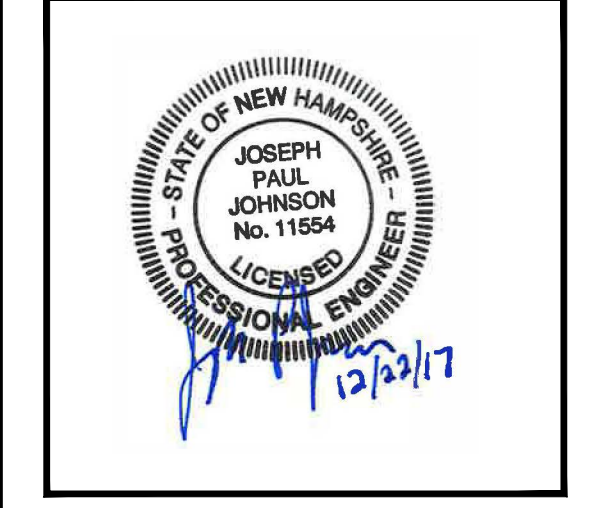
STONE MASONRY WALL DETAIL
 NOT TO SCALE

REVISIONS		
NO.	REVISION	DATE

CONSTRUCTION
 DETAILS

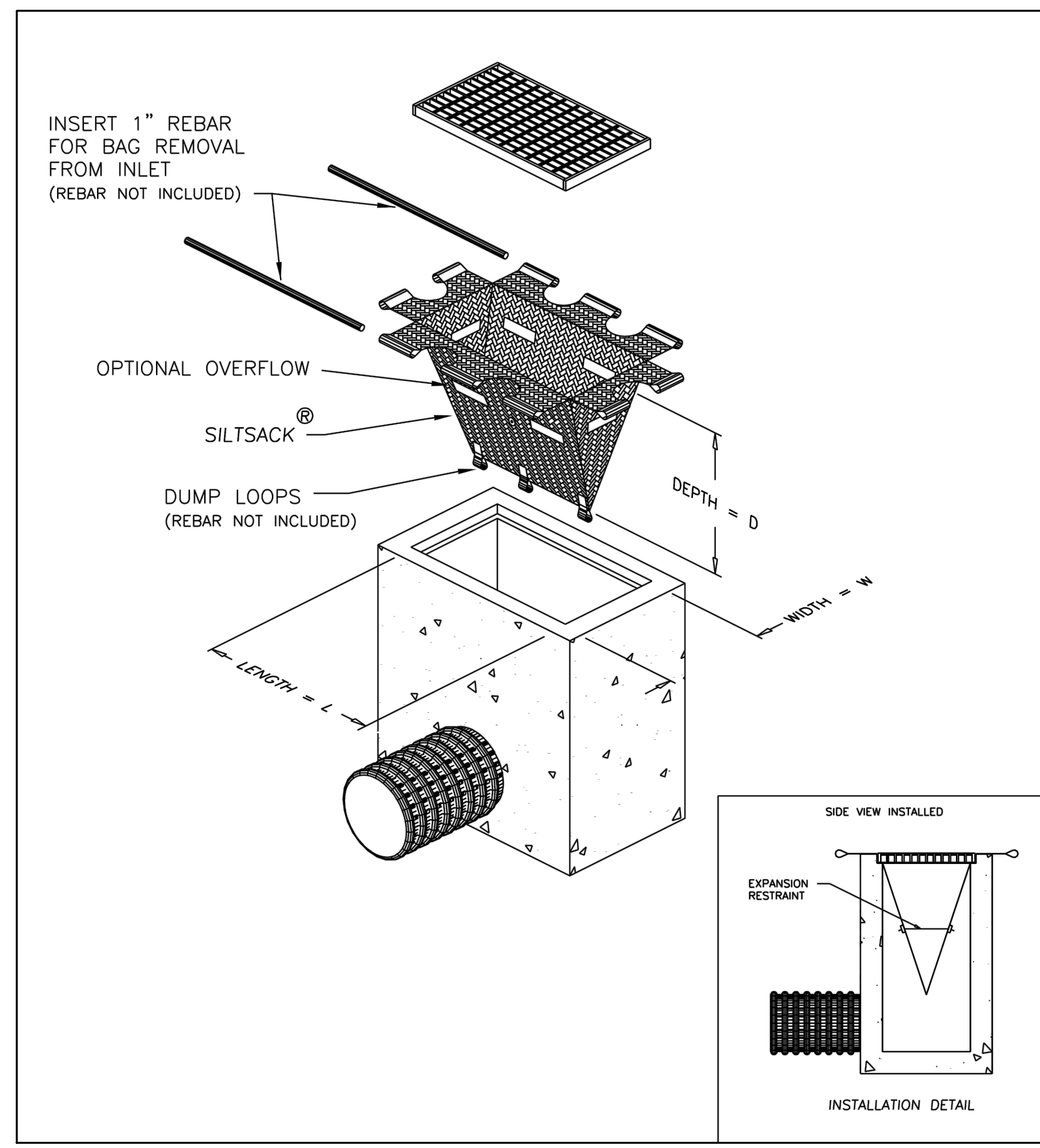
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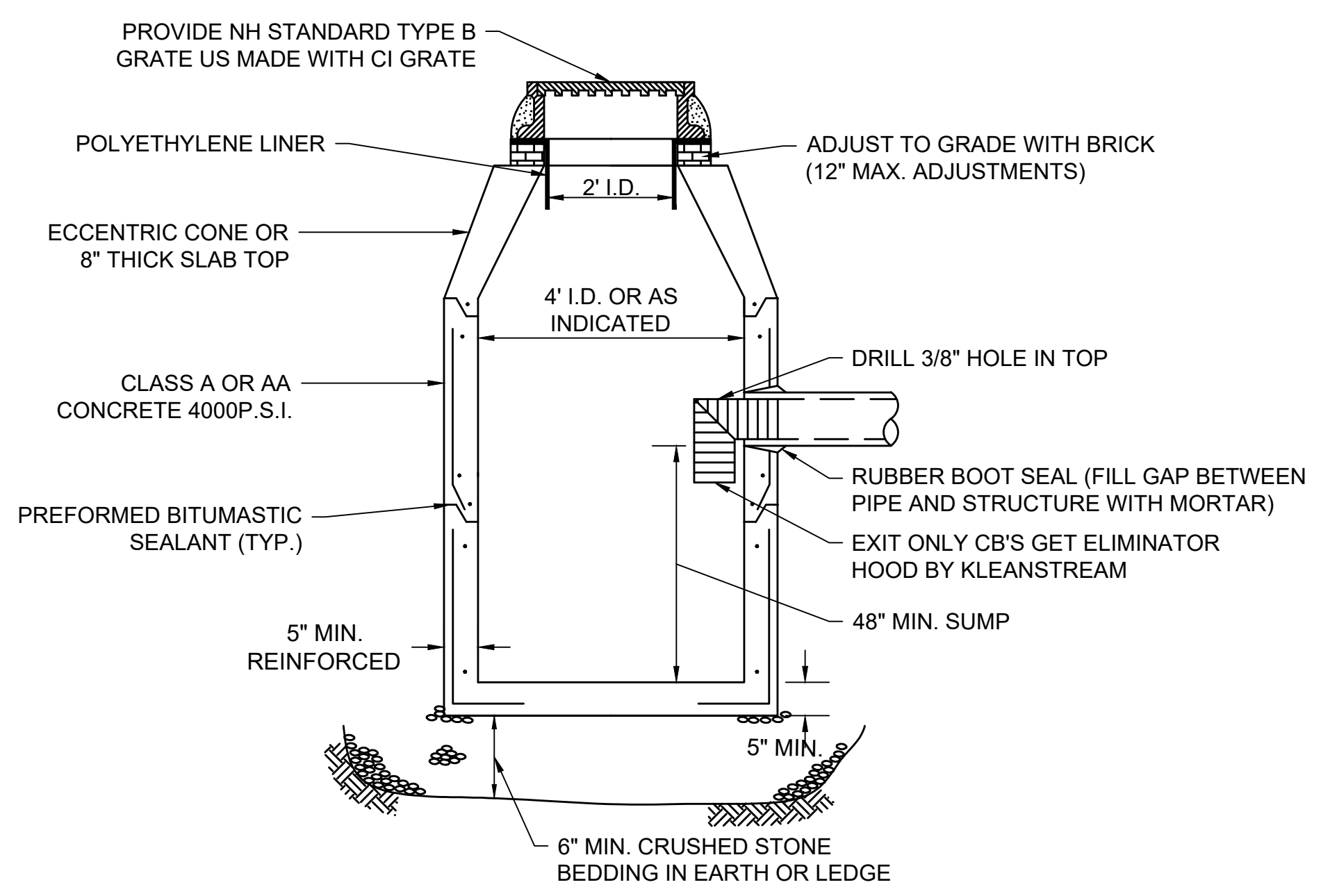


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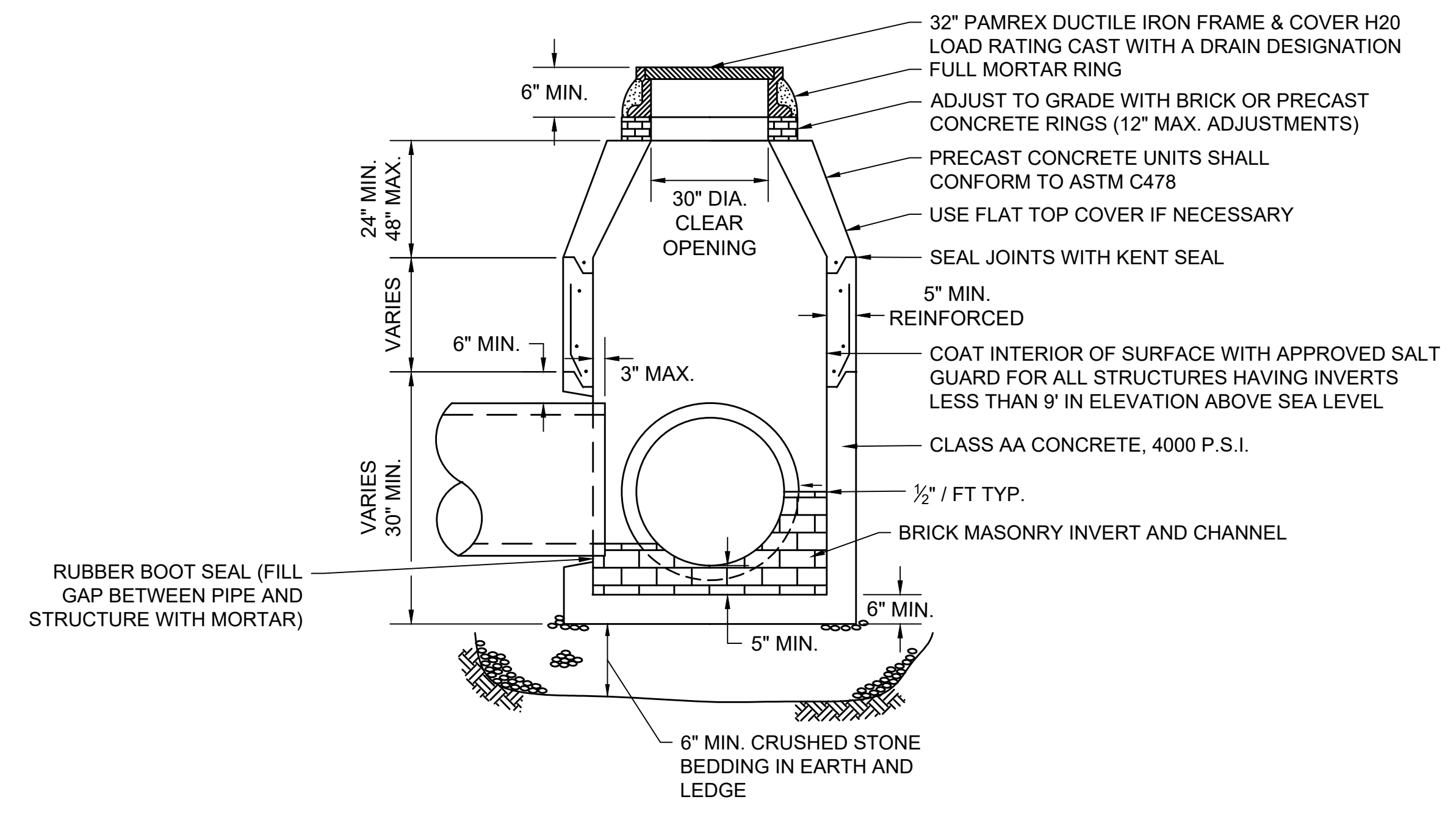


SILT SACK
 NOT TO SCALE



- NOTES:**
- COAT INTERIOR OF SURFACE WITH APPROVED SALT GUARD FOR ALL STRUCTURES HAVING INVERTS LESS THAN ELEVATION 9' ABOVE SEA LEVEL
 - ENTIRE STRUCTURE SHALL BE CAPABLE OF WITHSTANDING AN H-20 LOAD. DETAILS OF REINFORCEMENT TO BE FURNISHED BY MANUFACTURER

CONCRETE CATCH BASIN
 NOT TO SCALE



TYPICAL DRAINAGE MANHOLE
 NOT TO SCALE

- NOTES:**
- BARRELS AND TOP SECTIONS SHALL BE PRECAST REINFORCED CONCRETE.
 - PRECAST CONCRETE BARREL SECTIONS, CONES, AND BASES SHALL CONFORM TO ASTM C47B.
 - INVERTS AND SHELVES: MANHOLES SHALL HAVE A BRICK PAVED SHELF AND INVERT, CONSTRUCTED TO CONFORM TO THE SIZE OF PIPE AND FLOW. CARE SHALL BE TAKEN TO INSURE THAT THE BRICK INVERT IS A SMOOTH CONTINUATION OF THE INVERT. INVERT BRICKS SHALL BE LAID ON EDGE. AT CHANGES IN DIRECTION, THE INVERTS SHALL BE LAID OUT IN CURVES OF THE LONGEST POSSIBLE TANGENT TO THE CENTERLINE OF THE PIPES. SHELVES SHALL BE CONSTRUCTED TO AN ELEVATION OF 1/2 THE PIPE DIA. AND SLOPE TO DRAIN TOWARD THE FLOWING THROUGH CHANNEL.
 - FRAMES AND COVERS: MANHOLE FRAMES AND COVERS SHALL BE OF HEAVY DUTY DESIGN AND PROVIDE A 30-INCH CLEAR OPENING. LETTER "D" OR WORD "DRAIN" IN 3-INCH LETTERS SHALL BE PLAINLY CAST INTO THE CENTER OF EACH COVER.
 - BEDDING: SCREENED GRAVEL AND/OR CRUSHED STONE FREE FROM CLAY, LOAM, ORGANIC MATTER AND MEETING ASTM C33. STONE SIZE NO. 67.
 - 100% PASSING 1 INCH SCREEN
 - 0-10% PASSING #4 SIEVE
 - 90-100% PASSING 3/4 INCH SCREEN
 - 0-5% PASSING #8 SIEVE
 - 20-55% PASSING 3/8 INCH SCREEN
 WHERE ORDERED BY THE ENGINEER TO STABILIZE THE BASE, SCREENED GRAVEL OR CRUSHED STONE 1-1/2 INCH TO 1/2 INCH SHALL BE USED.
 - SHALLOW MANHOLE: IN LIEU OF A CONE SECTION, WHEN MANHOLE IS LESS THAN 5 FEET, A REINFORCED CONCRETE SLAB COVER HAVING AN ECCENTRIC ENTRANCE AND CAPABLE OF SUPPORTING H-20 LOADS MAY BE USED.

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NO.	REVISION	DATE

12/22/17

DRAWN/DESIGN BY CLS	CHECKED BY JPJ
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CONSTRUCTION DETAILS

SCALE: N.T.S.

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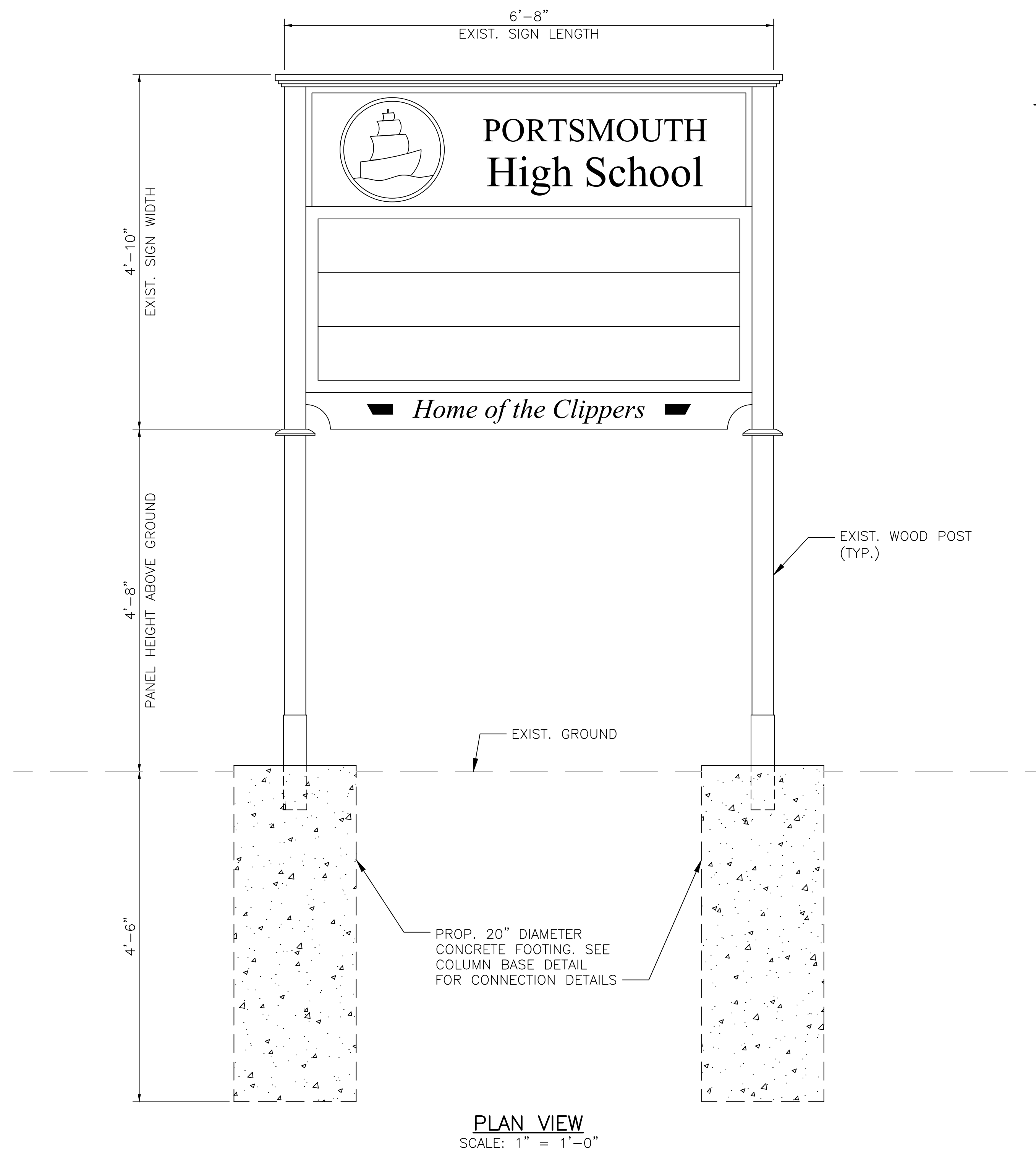
PREPARED FOR
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 DEPT. OF PUBLIC WORKS
 680 PEVERLY HILL ROAD
 PORTSMOUTH, NH 03801

**INTERSECTION IMPROVEMENT PROJECT
 U.S. ROUTE 1 AT ANDREW JARVIS DRIVE
 CITY OF PORTSMOUTH
 NEW HAMPSHIRE**

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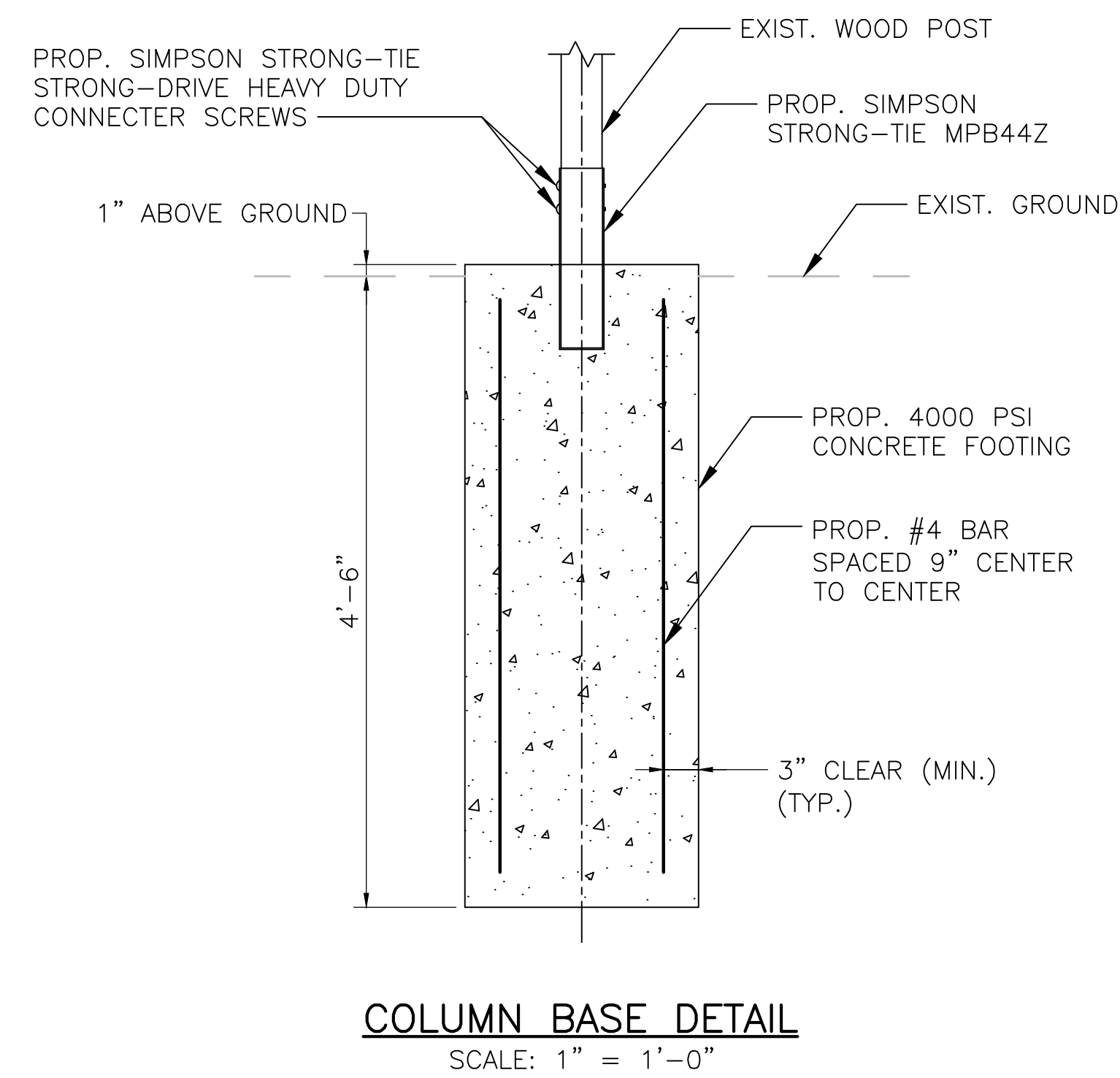
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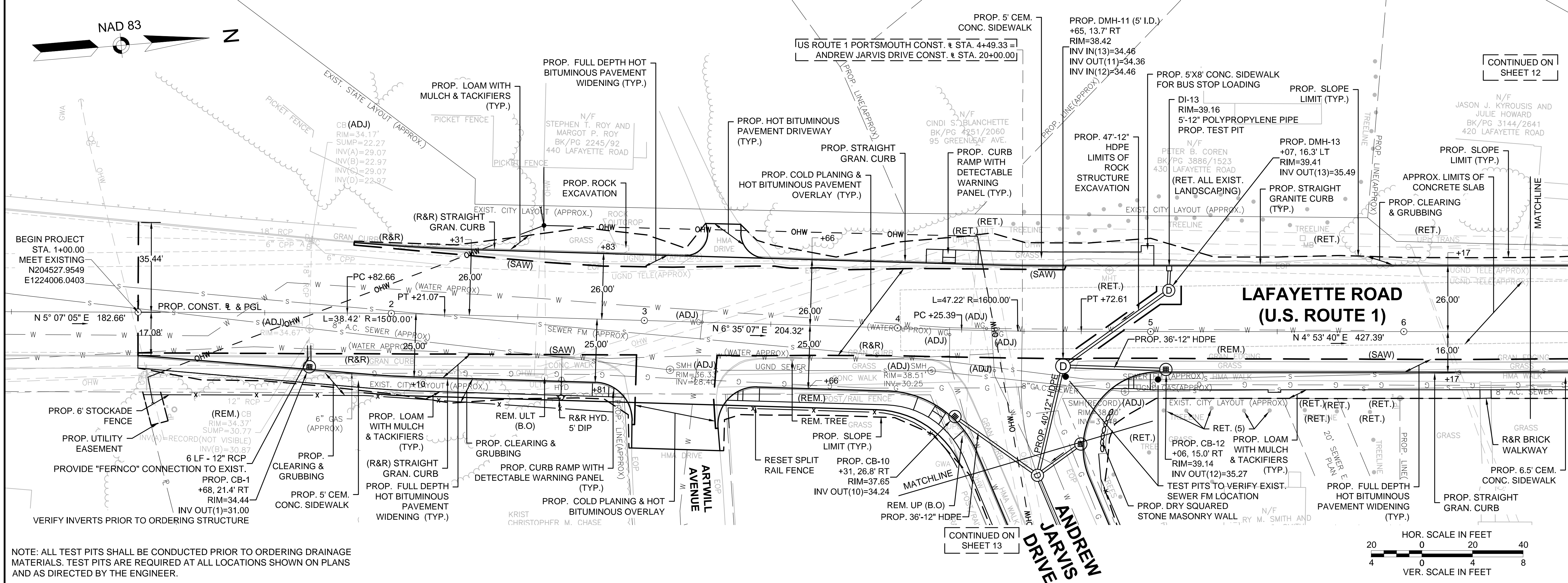
CONSTRUCTION DETAILS
 SCALE: 1" = 1'-0"
 10 OF 33



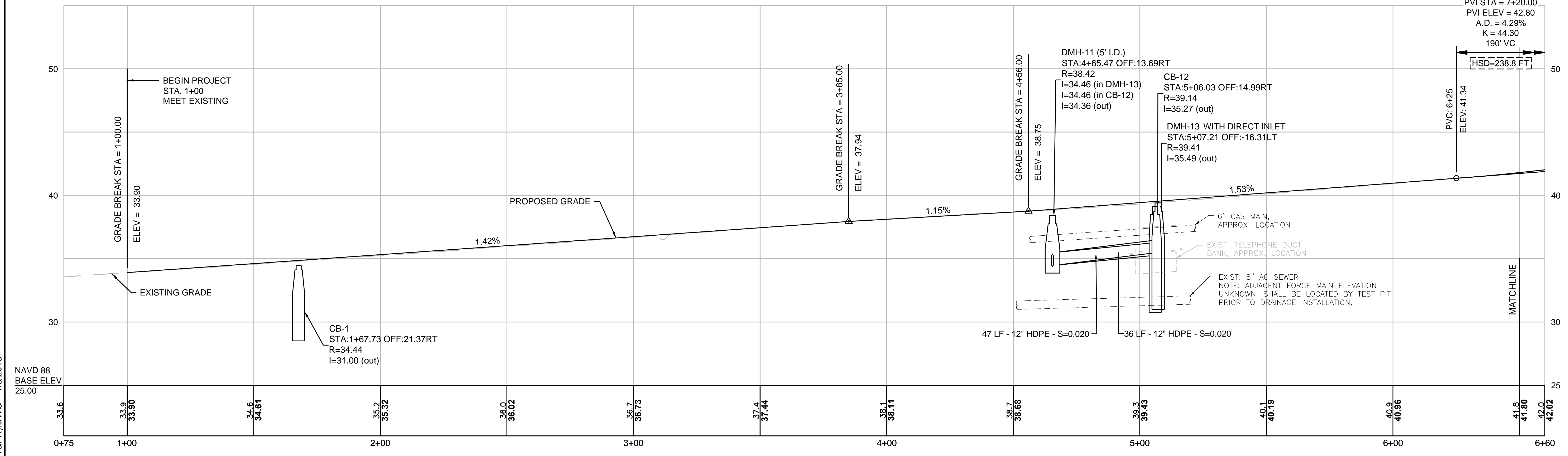
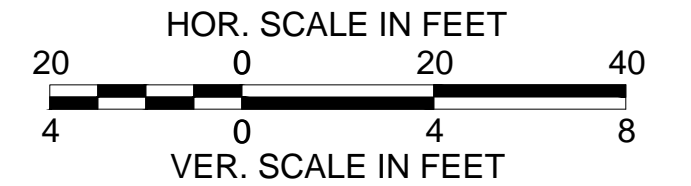
FOUNDATION NOTES:

1. IN ACCORDANCE WITH AMERICAN CONCRETE INSTITUTE BUILDING CODE REQUIREMENTS FOR STRUCTURAL CONCRETE (ACI 318), LATEST EDITION. 2013 STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS.
2. CONTRACTOR REQUIRED TO VERIFY ALL DIMENSIONS SHOWN PRIOR TO CONSTRUCTION.
3. MOMENT POST BASE (MPB44Z) SHALL BE INSTALLED USING MANUFACTURER GUIDELINES PER SIMPSON STRONG-TIE WOOD CONSTRUCTION CONNECTORS.
4. REINFORCEMENT STEEL SHALL BE EPOXY COATED AND SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M 31 GRADE 60.
5. CAST-IN-PLACE CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH AT 28 DAYS OF 4,000 PSI.
6. FINAL LOCATION TO BE APPROVED BY OWNER.

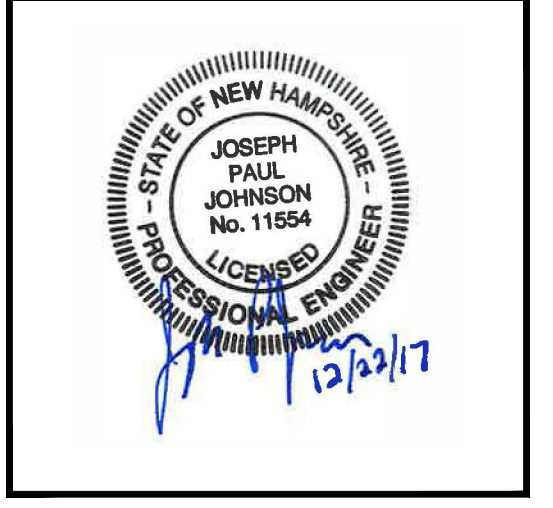




NOTE: ALL TEST PITS SHALL BE CONDUCTED PRIOR TO ORDERING DRAINAGE MATERIALS. TEST PITS ARE REQUIRED AT ALL LOCATIONS SHOWN ON PLANS AND AS DIRECTED BY THE ENGINEER.



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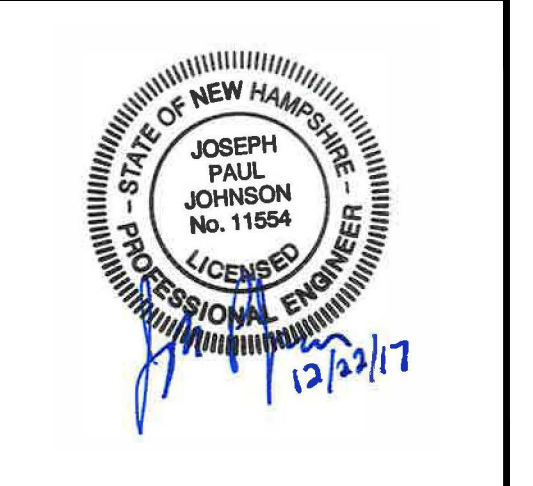
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 DRAWN/DESIGN BY: CLS
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**ROADWAY
 PLANS AND
 PROFILES**

SCALE: 1"=20'

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14051_HD (RDWY&PR).DWG 1/5/2018



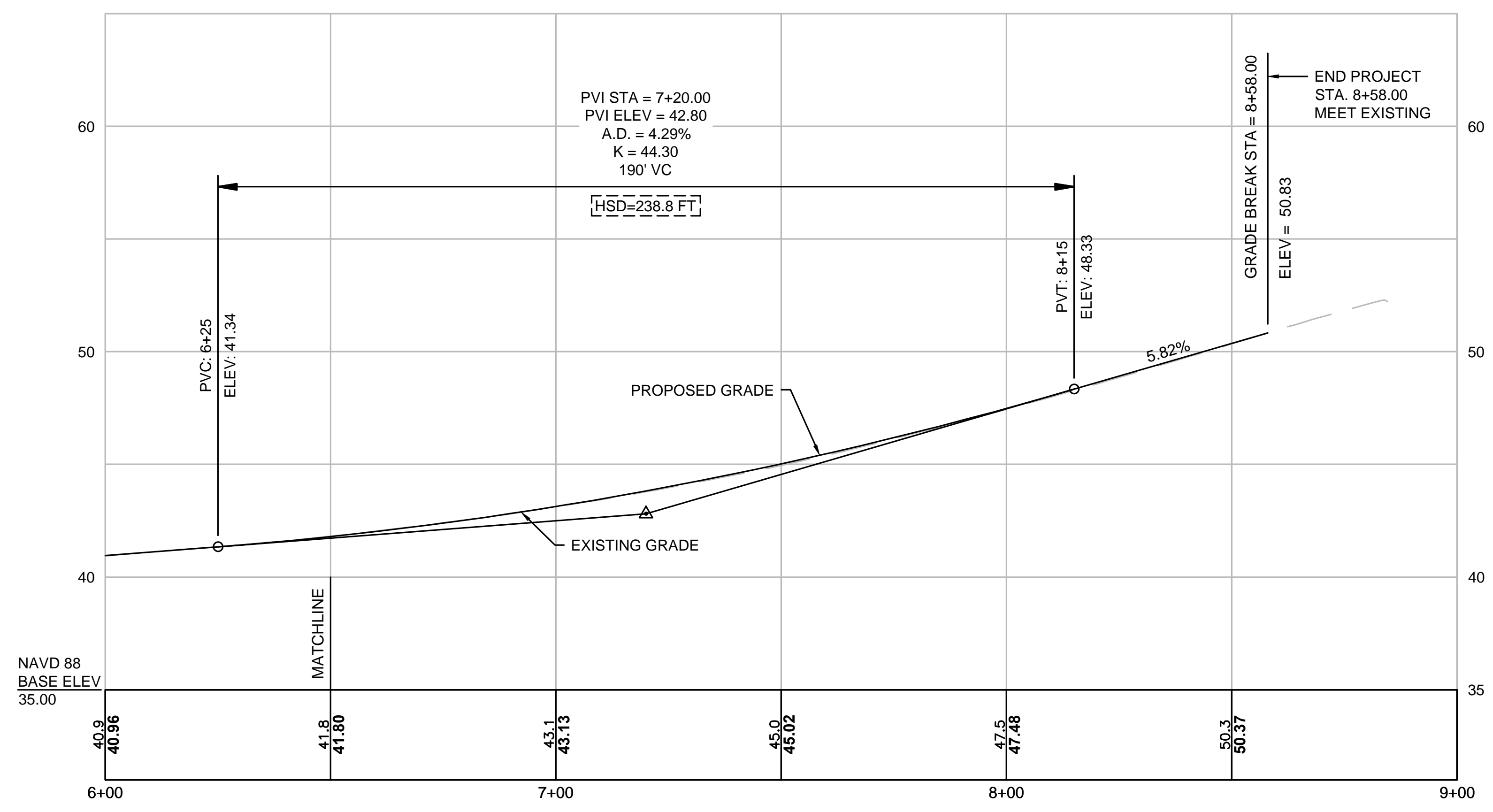
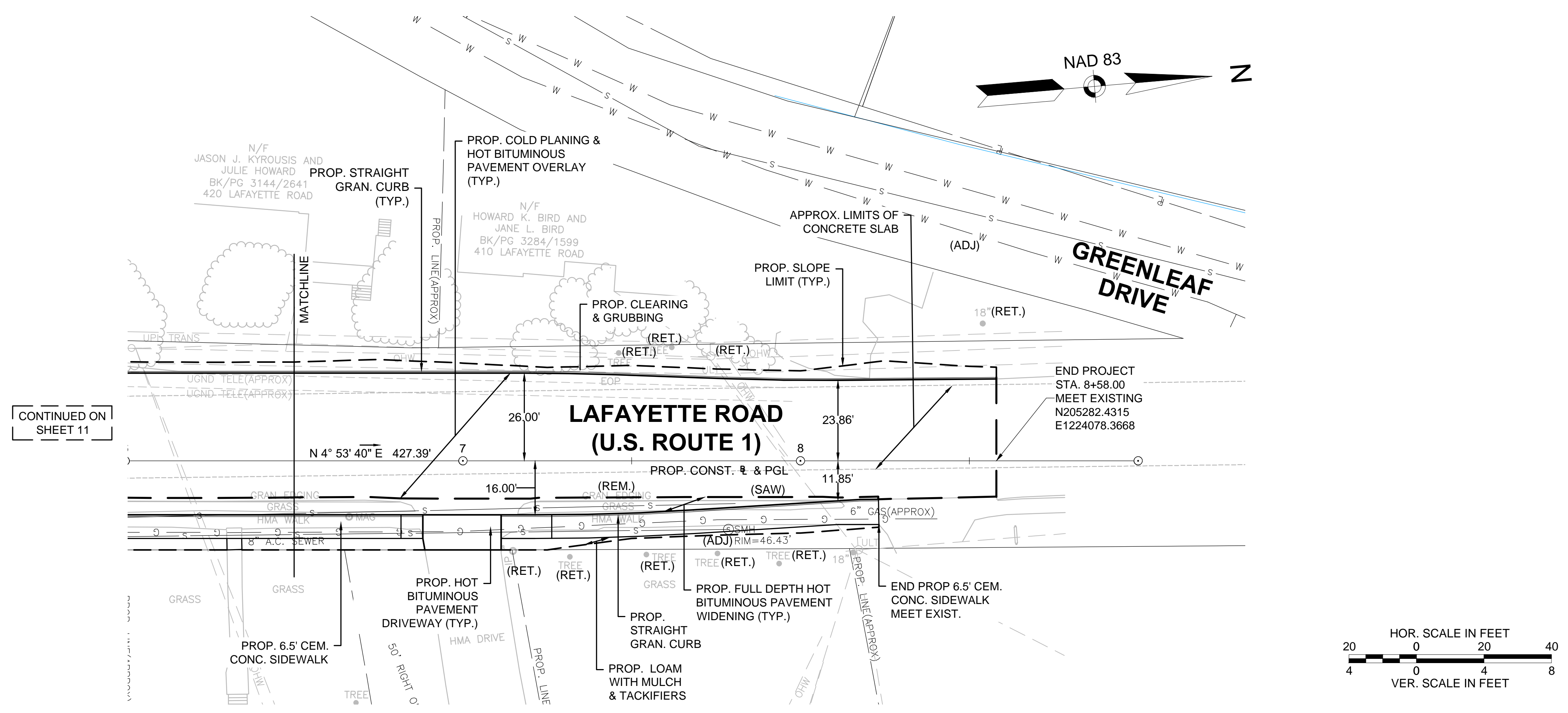
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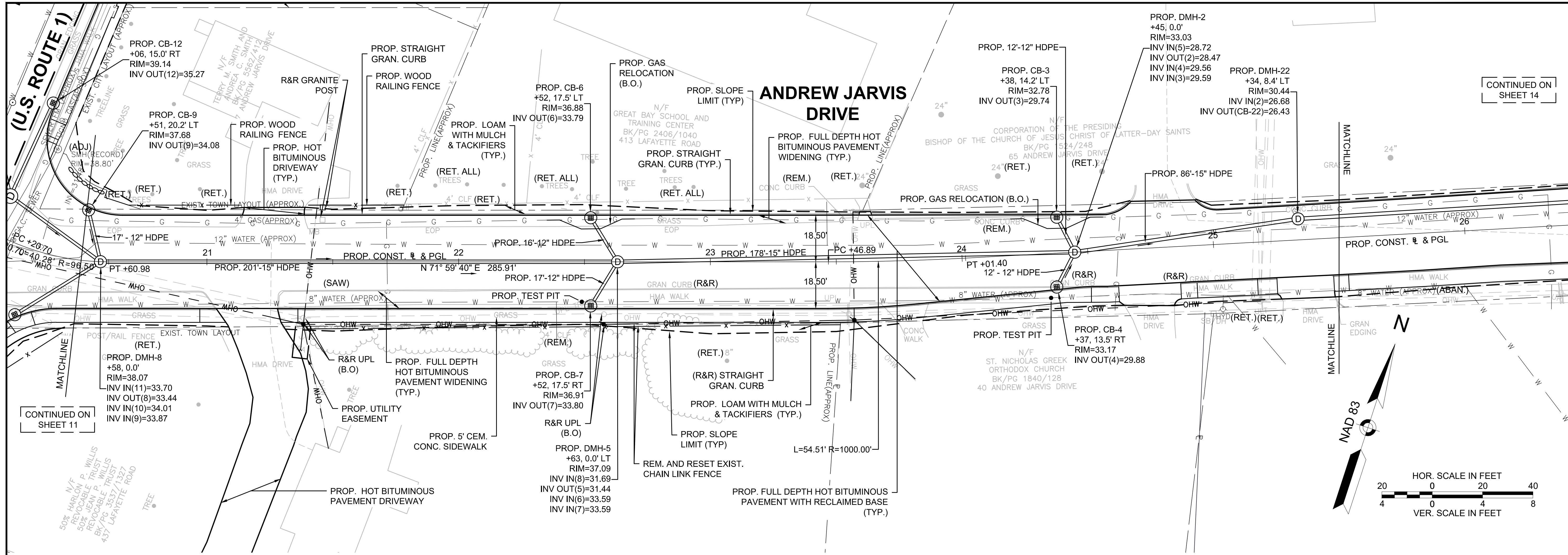
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**ROADWAY
 PLANS AND
 PROFILES**

SCALE: 1"=20'





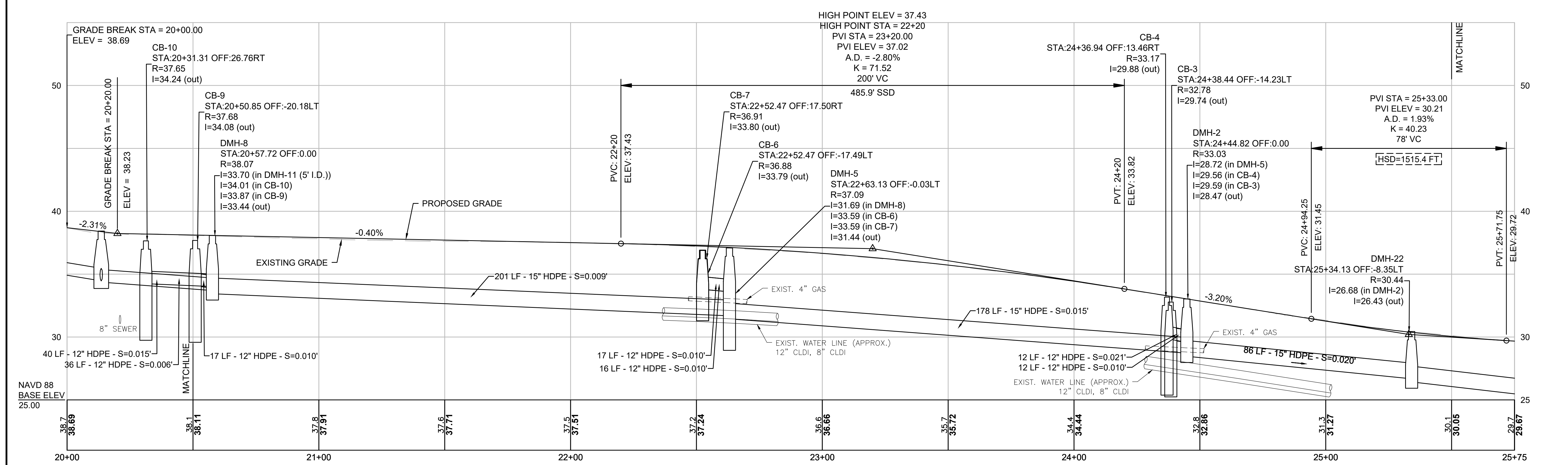
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STATE OF NEW HAMPSHIRE
 JOSEPH PAUL JOHNSON
 No. 11954
 LICENSED PROFESSIONAL ENGINEER
 12/23/17

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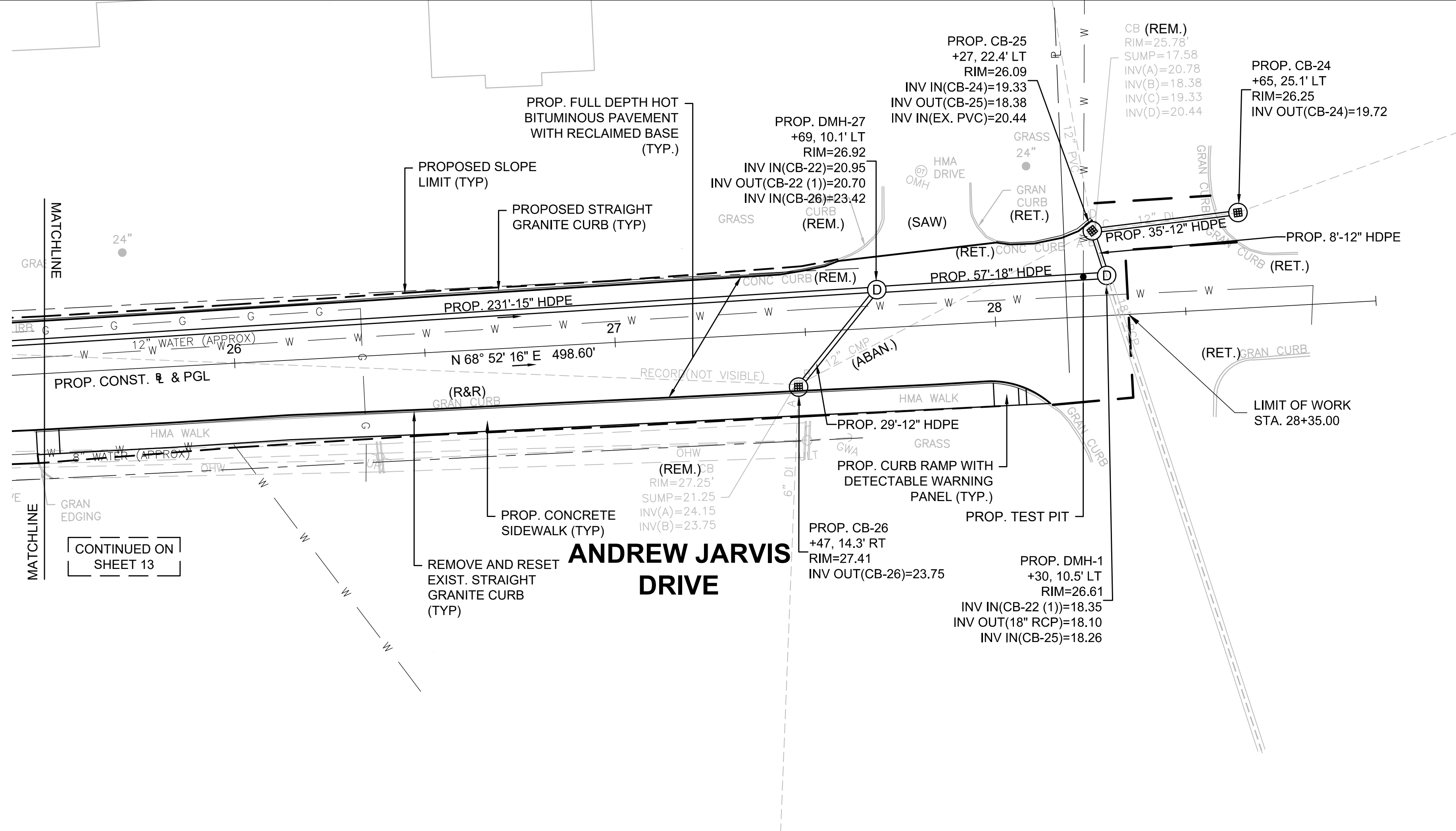
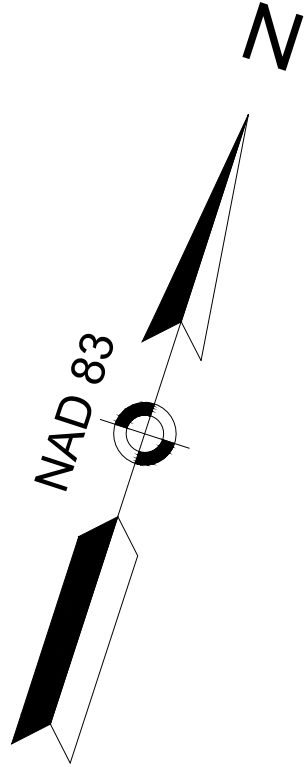
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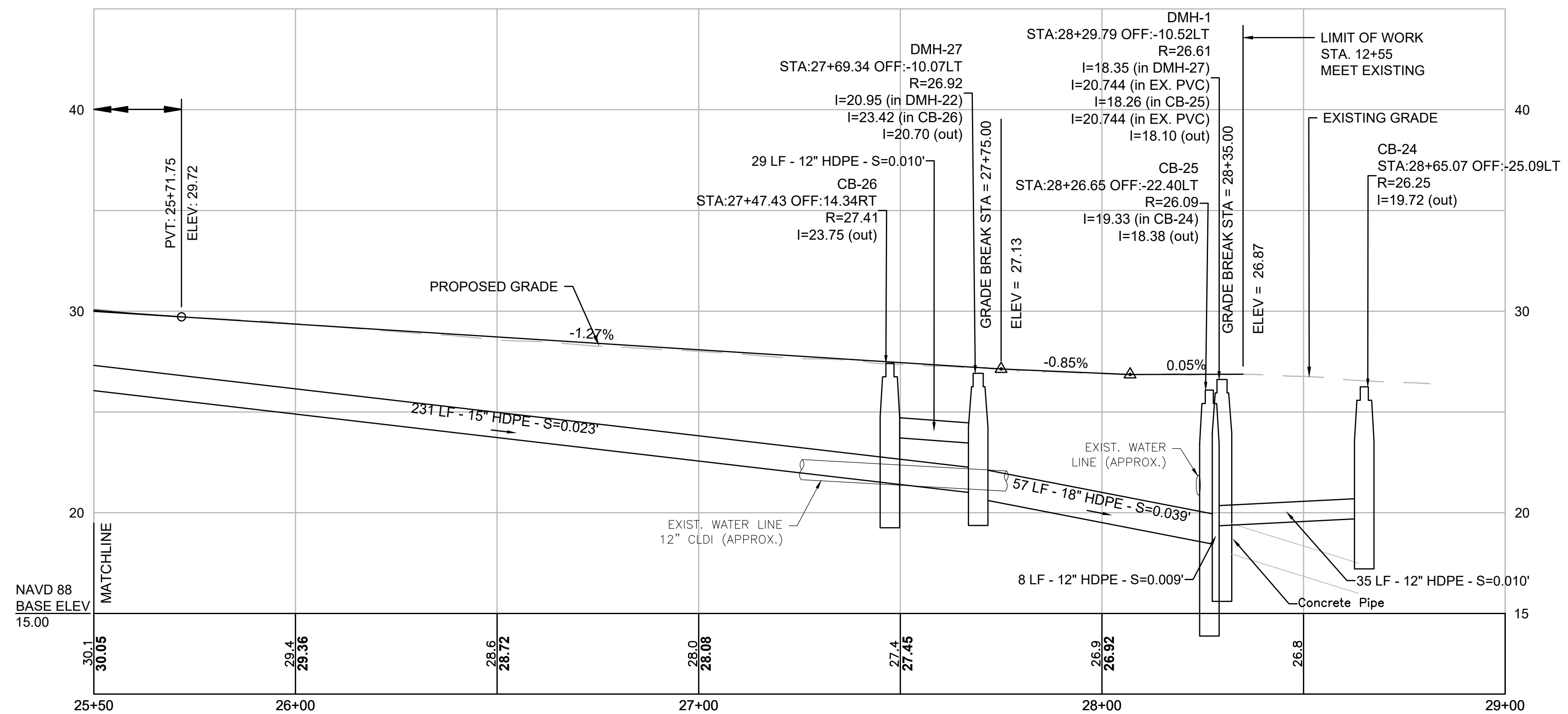
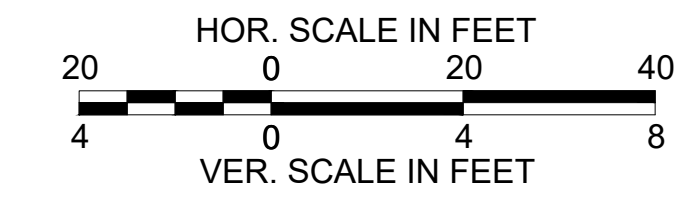
**ROADWAY
 PLANS AND
 PROFILES**

SCALE: 1"=20'

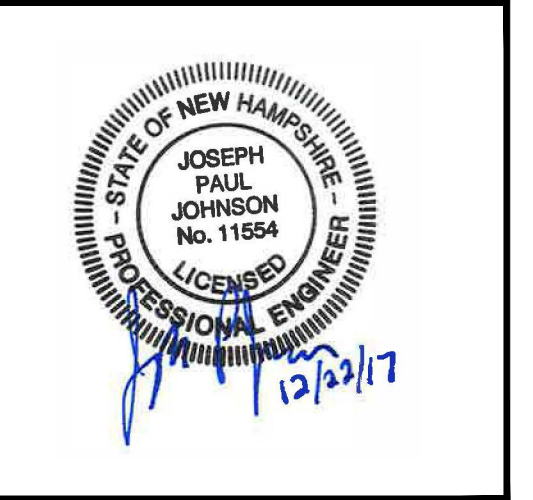
13 OF 33



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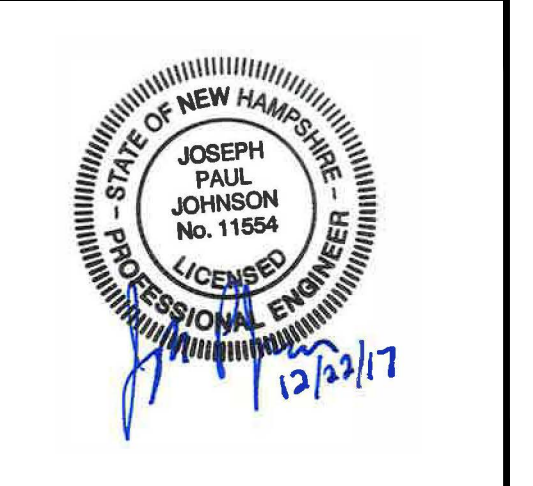
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**ROADWAY
 PLANS AND
 PROFILES**

SCALE: 1"=20'



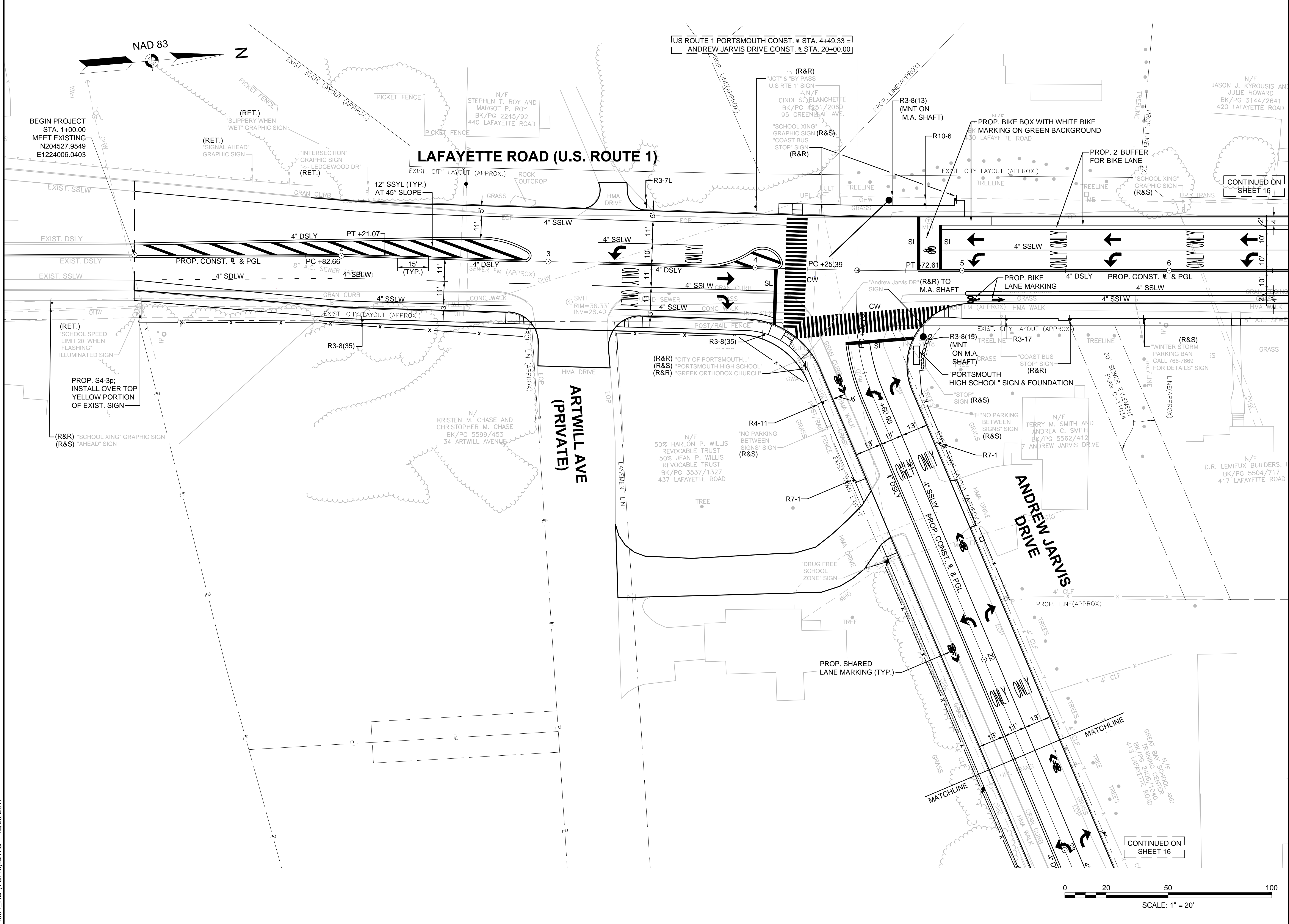
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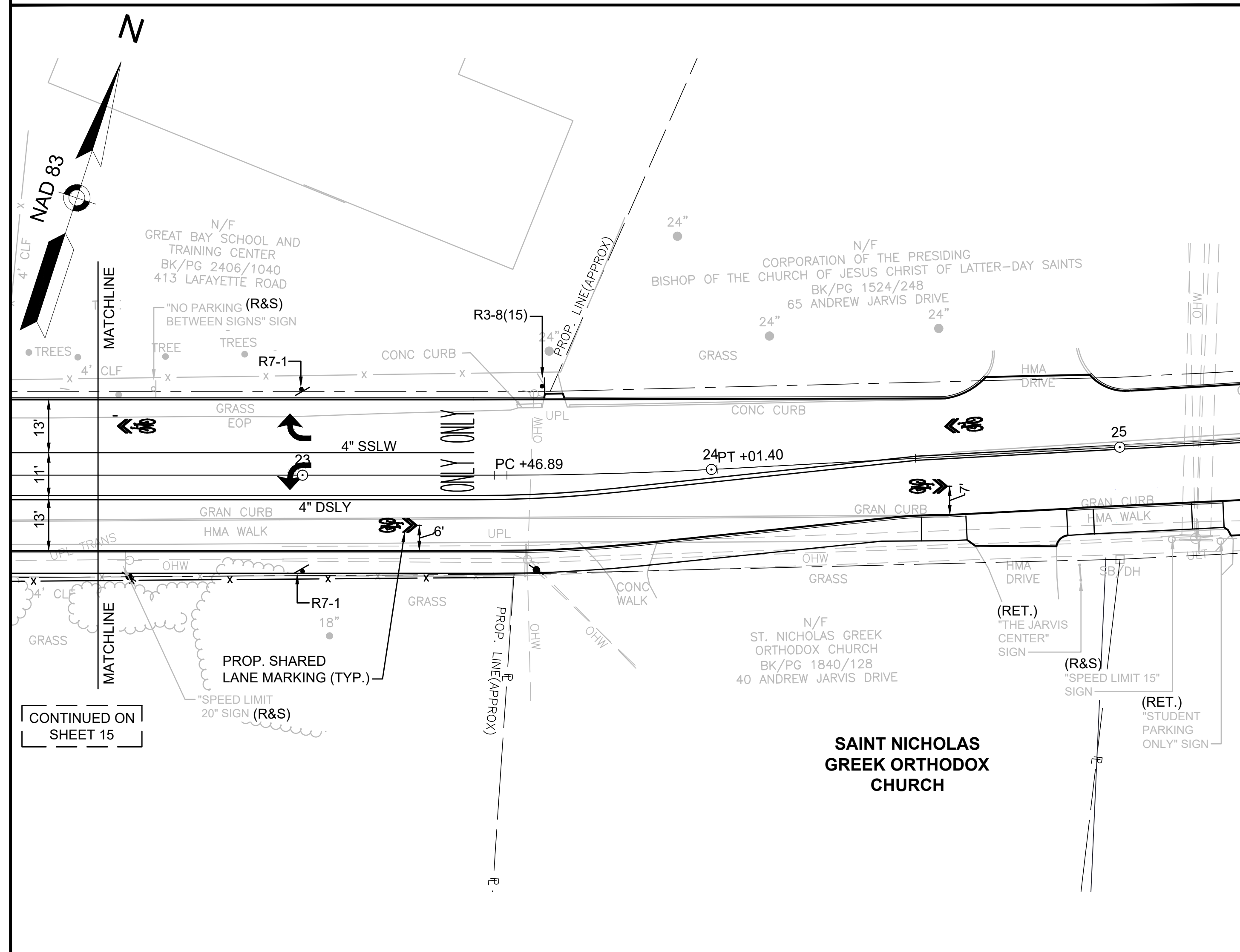
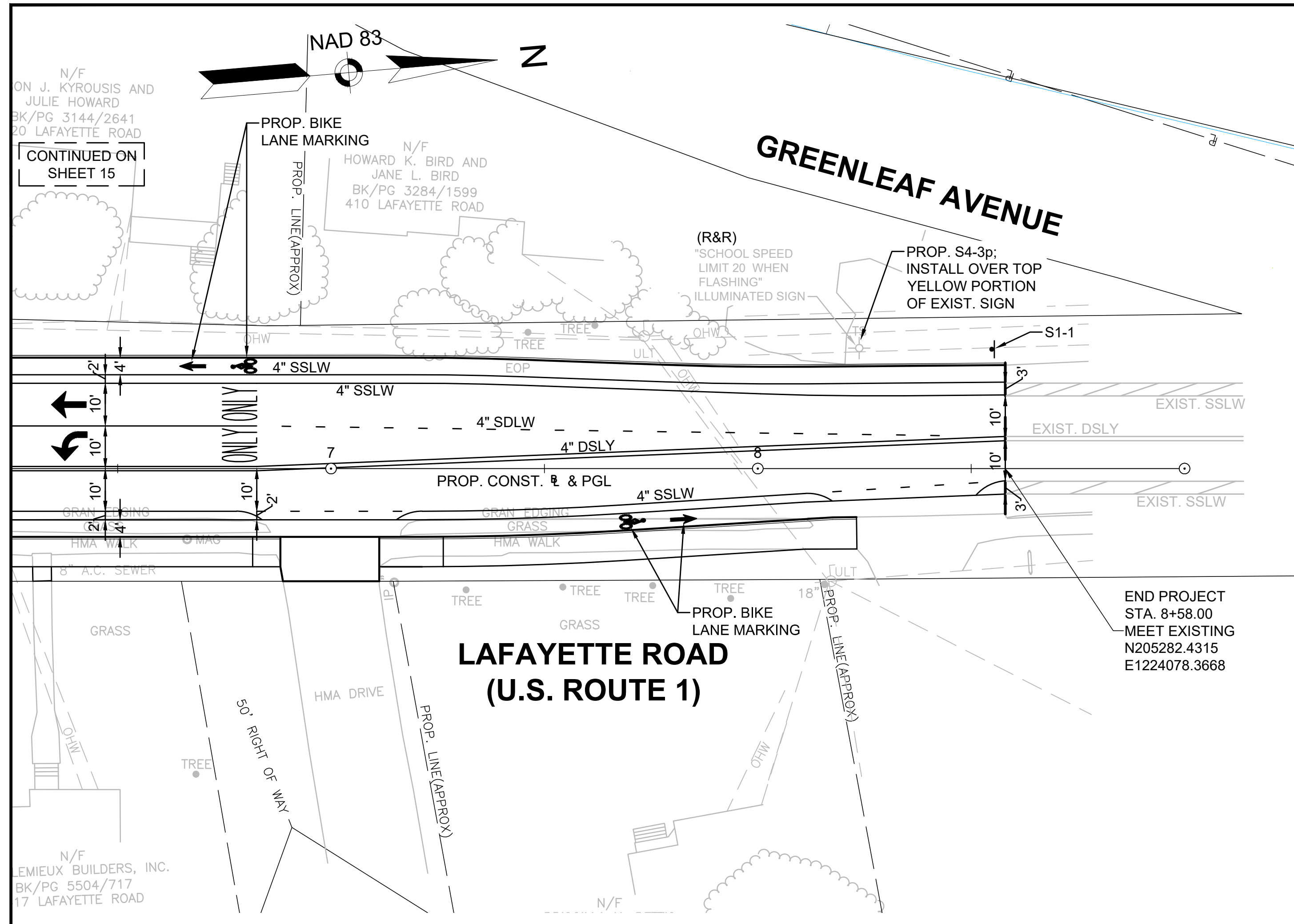
REVISIONS		
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SIGN & PAVEMENT MARKING PLANS

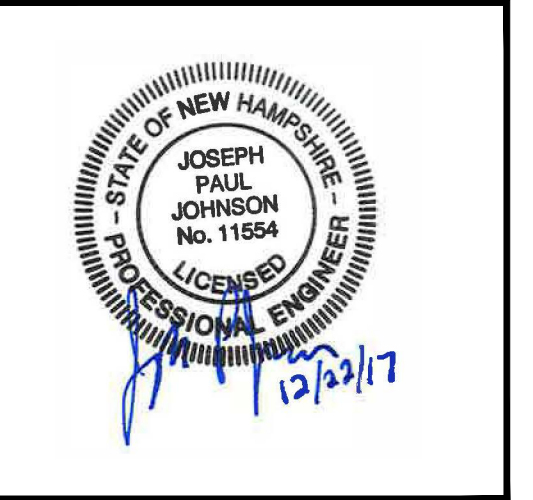
SCALE: 1"=20'



14051_HD (TSPM).DWG 12/20/2017



IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)		NUMBER OF SIGNS REQUIRED	COLOR	LEGEND	BORDER	POST SIZE AND NUMBER REQUIRED	UNIT AREA IN SQUARE FEET	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING							
R3-7L	30"	30"	LEFT LANE MUST TURN LEFT	MUTCD STANDARD		1	WHITE	BLACK	BLACK	U-CHANNEL (1 REQ'D)	6.25	6.25
R3-8(13)	30"	30"	ONLY	NHDOT STANDARD		2	WHITE	BLACK	BLACK	1 ON U-CHANNEL; 1 ON M.A. SHAFT	6.25	12.50
R3-8(15)	30"	30"	ONLY ONLY	NHDOT STANDARD		2	WHITE	BLACK	BLACK	1 ON U-CHANNEL; 1 ON M.A. SHAFT	6.25	12.50
R3-8(35)	30"	30"	ONLY	NHDOT STANDARD		2	WHITE	BLACK	BLACK	U-CHANNEL (2 REQ'D)	6.25	12.50
R3-17	30"	24"	BIKE LANE	MUTCD STANDARD		1	WHITE	BLACK	-	U-CHANNEL (1 REQ'D)	5.00	5.00
R4-11	30"	30"	BIKE LANE MAY USE FULL LANE	MUTCD STANDARD		3	WHITE	BLACK	BLACK	U-CHANNEL (2 REQ'D)	6.25	12.50
R7-1	12"	18"	NO PARKING ANY TIME	MUTCD STANDARD		5	WHITE	RED	RED	U-CHANNEL (4 REQ'D)	1.50	6.00
R10-6	24"	36"	STOP HERE ON RED	MUTCD STANDARD		1	WHITE	BLACK	BLACK	MNT ON MAST ARM	6.00	6.00
S1-1	36"	36"	PEDESTRIAN CROSSING	MUTCD STANDARD		1	FLUORESCENT YELLOW GREEN	BLACK	BLACK	U-CHANNEL (1 REQ'D)	9.00	9.00
S4-3p	24"	8"	SCHOOL	MUTCD STANDARD		2	FLUORESCENT YELLOW GREEN	BLACK	BLACK	MNT ON EXIST. SCHOOL SPEED LIMIT SIGNS	1.33	2.67



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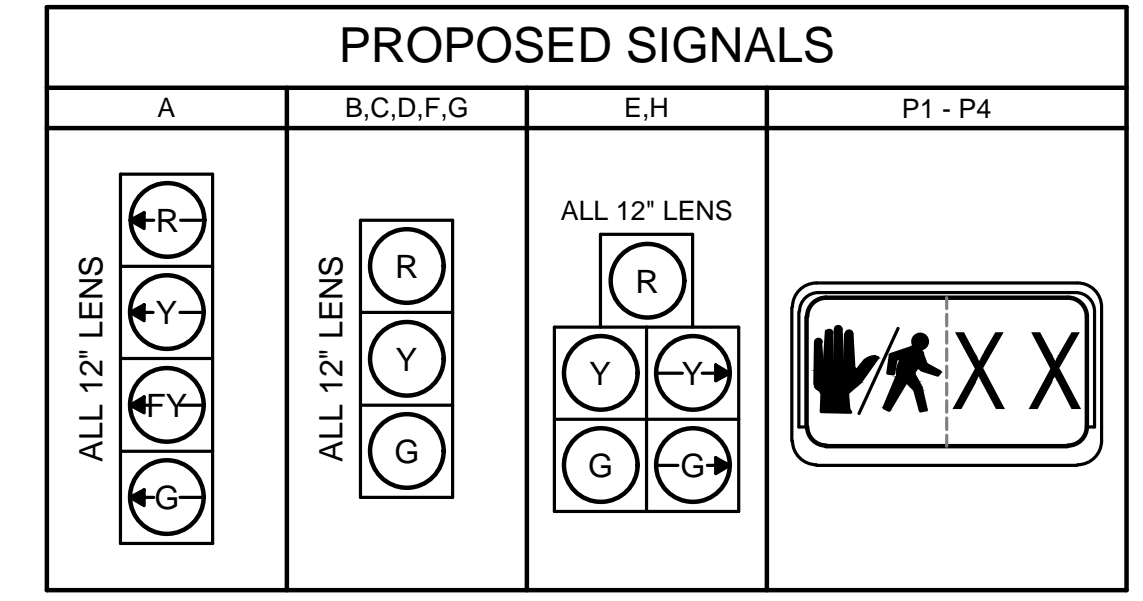
**INTERSECTION IMPROVEMENT PROJECT
 U.S. ROUTE 1 AT ANDREW JARVIS DRIVE
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12/22/17
 DRAWN/DESIGN BY: TQN
 CHECKED BY: JPJ
TRAFFIC SIGNAL PLAN
 SCALE: 1"=20'
 17 OF 33

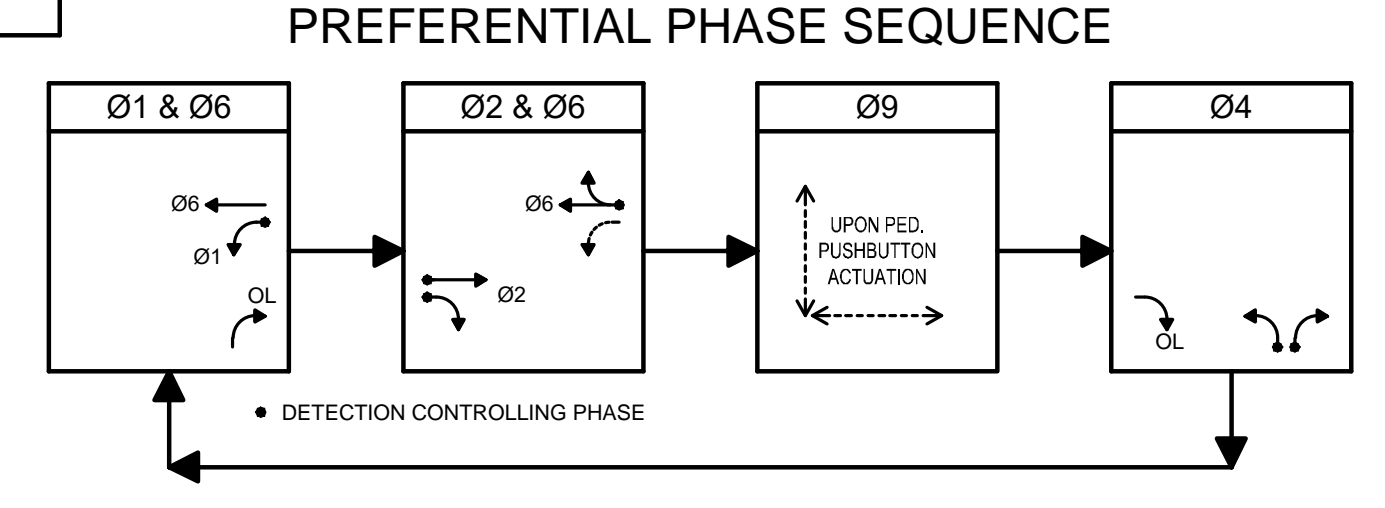
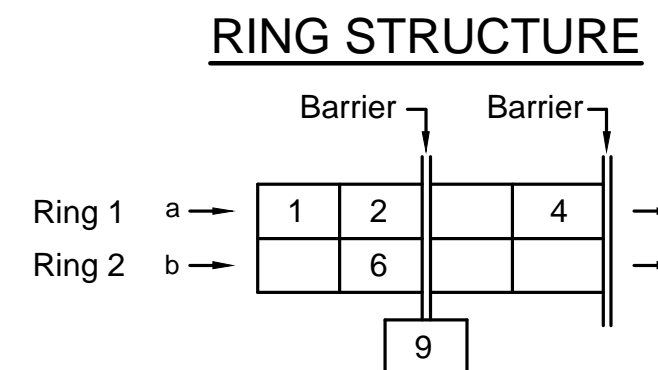
MAJOR ITEMS REQUIRED	
QUANTITY	DESCRIPTION
1	TRAFFIC CONTROLLER & CABINET NEMA TS2-TYPE 1 w/FDN. & CONC. PAD
1	SERVICE CONNECTION (OVERHEAD FROM UTILITY POLE)
1	40' MAST ARM ASSEMBLY, BASE & FDN.
1	30' MAST ARM ASSEMBLY, BASE & FDN.
2	8' SIGNAL POST, PEDESTAL BASE & FDN.
1	10' SIGNAL POST, PEDESTAL BASE & FDN.
5	SIGNAL HEAD, 3 SECTION
1	SIGNAL HEAD, 4 SECTION
2	SIGNAL HEAD, 5 SECTION (DOGHOUSE)
4	PEDESTRIAN HOUSING GRAPHIC LED WITH COUNTDOWN TIMER
3	AUDIBLE & VIBRO-TACTILE PEDESTRIAN PUSH BUTTON (APS TYPE) INTEGRATED R10-3e SIGN & SADDLE w/LED CONFIRMATION LIGHT
1	360° VIDEO DETECTION SYSTEM
3	UNIDIRECTIONAL SINGLE CHANNEL OPTICAL DETECTOR (RECEIVER)
2	PREEMPTION 2-CHANNEL PHASE SELECTOR
1	PREEMPTION CONFIRMATION STROBE (CLEAR)
3	14" CONCRETE PULL BOX TYPE A (TRAFFIC SIGNAL)

PLUS ALL MISCELLANEOUS EQUIPMENT AND MATERIAL NECESSARY TO PROVIDE A COMPLETE OPERATING TRAFFIC CONTROL SIGNAL.



- NOTES:
- ALL INDICATIONS SHALL BE "LED" TYPE LENS IN ACCORDANCE WITH THE SPECIAL PROVISIONS.
 - ALL OVERHEAD SIGNALS SHALL BE RIGIDLY MOUNTED. ALL SIGNAL HEADS SHALL BE EQUIPPED WITH CUT TUNNEL VISORS AND 5" LOUVERED BACKPLATES.
 - ALL VEHICLE SIGNAL HEAD BACKPLATES SHALL INCLUDE 2" RETRO-REFLECTIVE BORDER IN ACCORDANCE WITH THE SPECIAL PROVISIONS.
 - ALL PEDESTRIAN DISPLAY SHALL BE "FULL" DISPLAYS. NO OUTLINE SYMBOL SHALL BE PERMITTED.

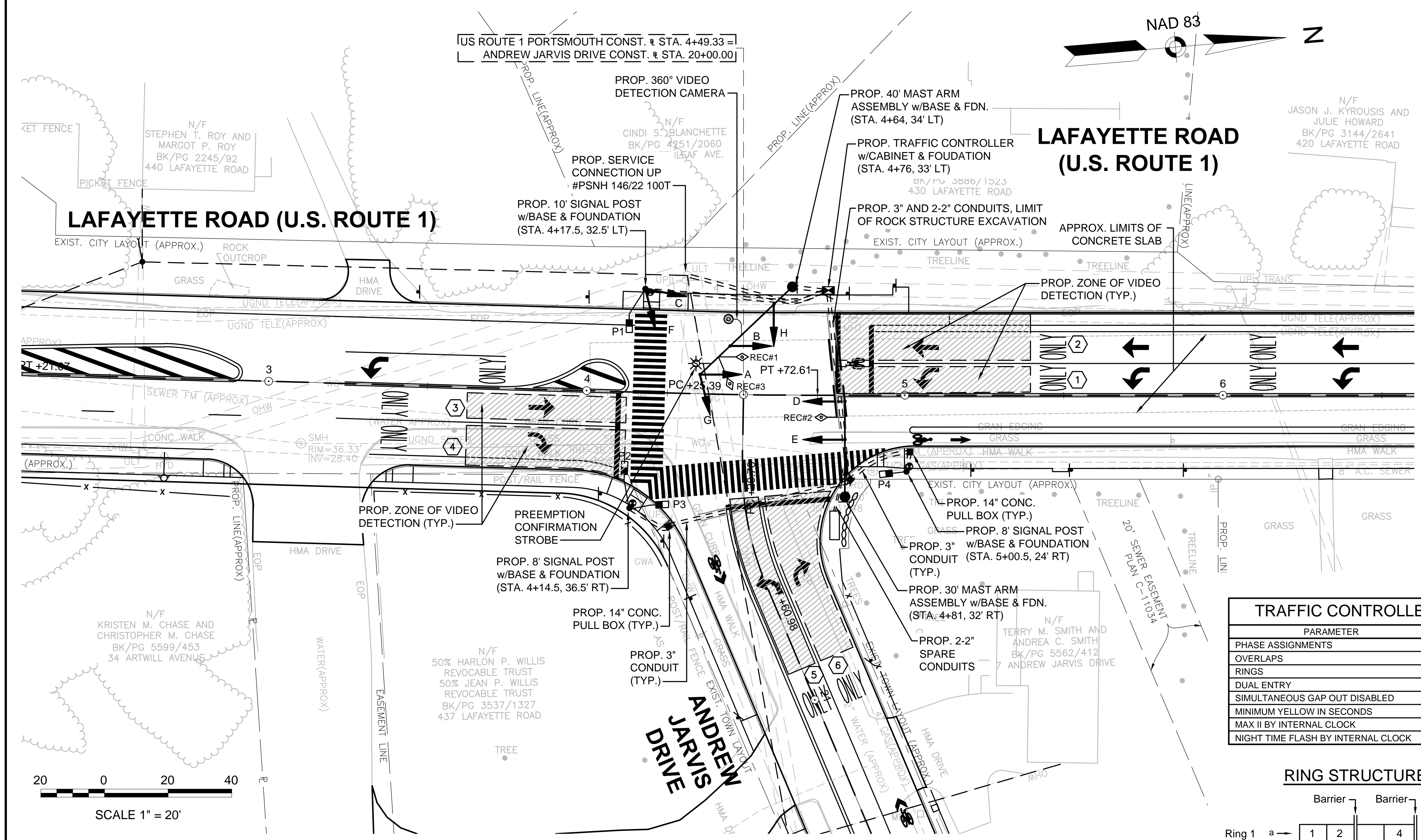
TRAFFIC CONTROLLER DATA	
PARAMETER	SELECTION
PHASE ASSIGNMENTS	STD. NEMA
OVERLAPS	STD. NEMA
RINGS	DUAL
DUAL ENTRY	ON (Ø2 & Ø6)
SIMULTANEOUS GAP OUT DISABLED	YES (Ø2 & Ø6)
MINIMUM YELLOW IN SECONDS	3
MAX II BY INTERNAL CLOCK	YES
NIGHT TIME FLASH BY INTERNAL CLOCK	NOT USED



FULLY-ACTUATED	<input checked="" type="checkbox"/>	ISOLATED	<input checked="" type="checkbox"/>
SEMI-ACTUATED	<input type="checkbox"/>	COORDINATED	<input type="checkbox"/>
PRE-TIMED	<input type="checkbox"/>	WIRE	<input type="checkbox"/>
		TBCU	<input type="checkbox"/>

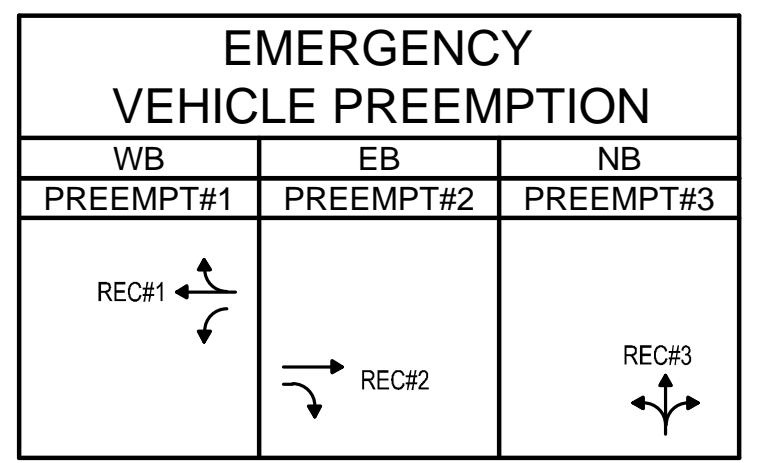
STREET		DIRECTION	HOUSINGS	1	2	3	4	5	6	7	8	9	13	14	15	16	17	18	FLASH OPER.
LAFAYETTE ROAD (U.S. ROUTE 1)	SB	A	GL	YL	RL	RL	RL	RL	RL	RL	RL	RL	FYL	YL	RL	RL	RL	RL	FYL
LAFAYETTE ROAD (U.S. ROUTE 1)	SB	B,C	R	R	R	R	R	R	R	R	R	R	G	Y	R	R	R	R	FY
LAFAYETTE ROAD (U.S. ROUTE 1)	NB	D	R	R	R	G	Y	R	R	R	R	R	R	R	R	R	R	R	FY
LAFAYETTE ROAD (U.S. ROUTE 1)	NB	E	R	R	R	G	Y	R	R/GR	R/YR	R	R	R	R	R	R	R	R	FY
ANDREW JARVIS DRIVE	WB	F,G	R	R	R	R	R	R	G	Y	R	R	R	R	R	R	R	R	FR
ANDREW JARVIS DRIVE	WB	H	R/GR	R/YR	R	R	R	R	G	Y	R	R	R	R	R	R	R	R	FR
PEDESTRIAN	ALL	P1-P4	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	W	FDW	DW	OUT

SEQUENCE AND TIMING		TIMING IN SECONDS										EMERGENCY							
MINIMUM INITIAL PASSAGE		6	2	10	2	6	10	2	10	2	10								
MAXIMUM 1 (MON-FRI: 6:45am-8:45am)		25	25	30	30	15	30	30	30	30	30								
MAXIMUM 2 (MON-FRI: 2:30pm - 4:00pm)		10	10	30	30	40	40	40	40	40	40								
MAXIMUM 3 ALL OTHER TIMES		10	10	30	30	10	10	40	40	40	40								
CHANGE		4	4	2	2	4	4	2	2	4	4	2	2	4	4	2	2	2	2
PEDESTRIAN																7	20	3	
RECALL		OFF	SOFT	OFF	SOFT	OFF	SOFT	OFF	SOFT	OFF	SOFT	OFF	SOFT	OFF	SOFT	OFF	SOFT	OFF	SOFT
MEMORY		NON-LOCKING	NON-LOCKING	NON-LOCKING	NON-LOCKING	NON-LOCKING	NON-LOCKING	NON-LOCKING	NON-LOCKING	NON-LOCKING	NON-LOCKING	NON-LOCKING	NON-LOCKING	NON-LOCKING	NON-LOCKING	LOCKING	LOCKING	LOCKING	LOCKING



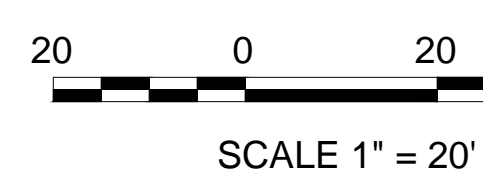
- GENERAL NOTES:
- REMOVE & STACK EXIST. FLASHING BEACONS AND EQUIPMENT COMPLETE.
 - OPTICAL RECEIVERS MAY BE RELOCATED BY THE SIGNAL CONTRACTOR FOR OPTIMUM LINE OF SIGHT DETECTION. CHANGES SHALL BE VERIFIED BY THE ENGINEER.
 - SEE "SIGN AND PAVEMENT MARKING PLANS" FOR ALL PROPOSED SIGNS.

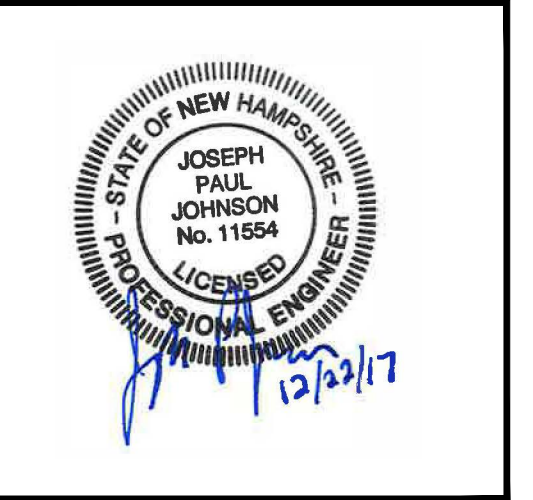
VIDEO DETECTION DATA						
DETECTION ZONE NO.	LOCATION	Ø CALLED	Ø EXT.	MODE A=PULSE B=PRES.	DELAY TIME	EXT. TIME
1	LAFAYETTE ROAD (U.S. ROUTE 1) SB LEFT	1	1	B	-	-
2	LAFAYETTE ROAD (U.S. ROUTE 1) SB THRU	6	6	B	-	-
3	LAFAYETTE ROAD (U.S. ROUTE 1) NB THRU	2	2	B	-	-
4	LAFAYETTE ROAD (U.S. ROUTE 1) NB RIGHT	2	2	B	-	-
5	ANDREW JARVIS DRIVE WB LEFT	4	4	B	-	-
6	ANDREW JARVIS DRIVE WB RIGHT	4	4	B	-	-



EMERGENCY VEHICLE PREEMPTION OPERATION

- EMERGENCY VEHICLE PREEMPTION SIGNAL SHALL BE OPTICALLY TRANSMITTED BY OPTICAL EMITTERS MOUNTED IN EMERGENCY VEHICLES AND RECEIVED BY OPTICAL DETECTORS (RECEIVERS) LOCATED AT THE INTERSECTION.
- PREEMPTION SIGNALS SHALL BE SERVICED ON A FIRST COME FIRST SERVED BASIS. IN CASE WHERE TWO OR MORE CALLS ARE COMING IN AT THE SAME TIME, PREEMPTION SHALL BE PRIORITIZED WITH PREEMPT #1 (U.S. ROUTE 1 SOUTHBOUND) FIRST, THEN FOLLOW BY PREEMPT #2 (U.S. ROUTE 1 NORTHBOUND), THEN PREEMPT #3 (ANDREW JARVIS DRIVE).
- IN RESPONSE TO A PREEMPTION SIGNAL RECEIVED BY OPTICAL DETECTOR REC#1 (OR REC#2, REC#3), THE CONTROLLER SHALL HOLD OR ADVANCE TO AND HOLD IN EMERGENCY VEHICLE PREEMPTION SIGNAL #1 (OR #2, #3) GREEN FOR A MINIMUM OF TEN (10) SECONDS OR UNTIL PREEMPTION SIGNAL CEASES. THE CONTROLLER SHALL THEN TIME THE RELEVANT CLEARANCE INTERVALS AND SERVICE THE SUBSEQUENT EMERGENCY VEHICLE PREEMPTION SIGNAL IF NECESSARY. NORMAL SIGNAL OPERATION SHALL RESUME PROVIDED THAT ALL PREEMPTION SIGNALS HAVE CEASED.
- NORMAL VEHICLE CLEARANCES SHALL BE PROVIDED ON PHASES THAT ARE TO BE TERMINATED BY PREEMPTION DEMAND.
- PREEMPTION STROBE SHALL BE ILLUMINATED ONLY WHEN THE PREEMPTION PHASE IS ON GREEN.





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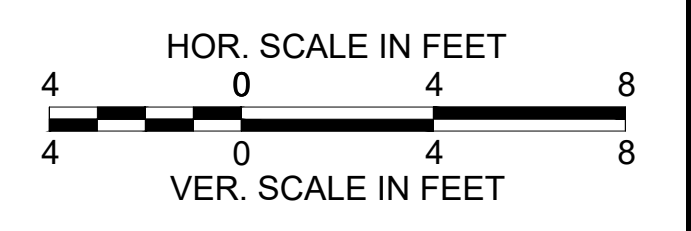
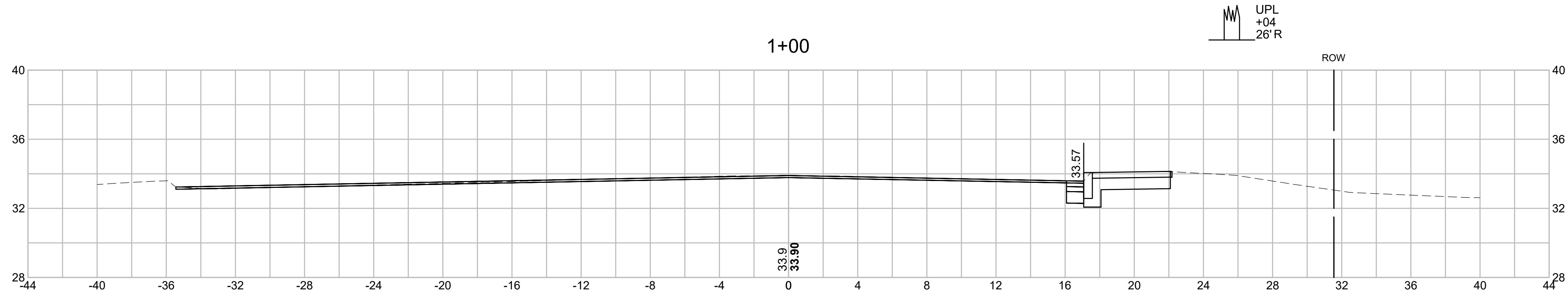
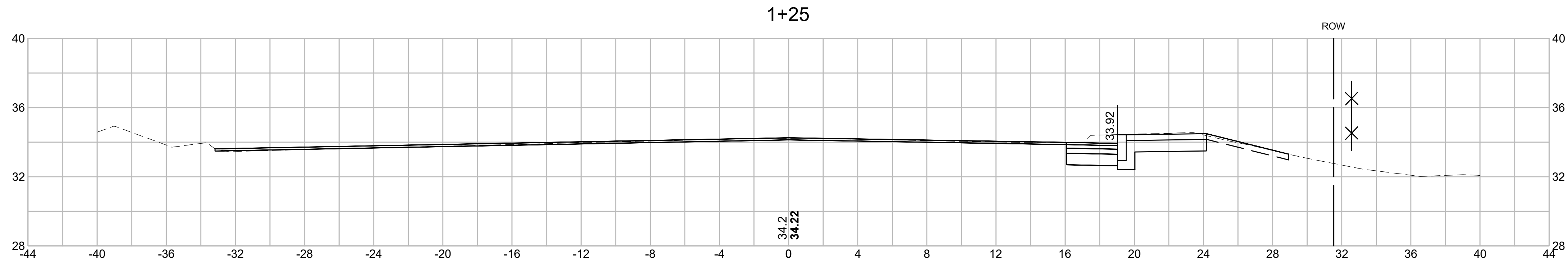
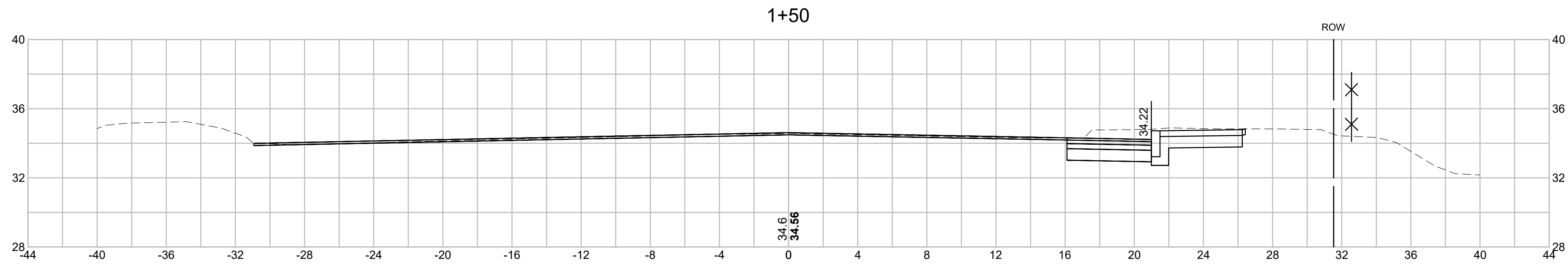
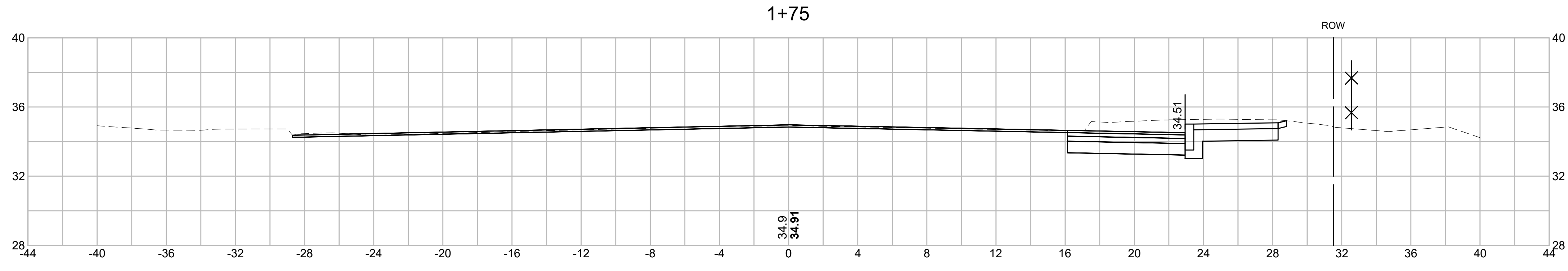
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REVISIONS		
NO.	REVISION	DATE

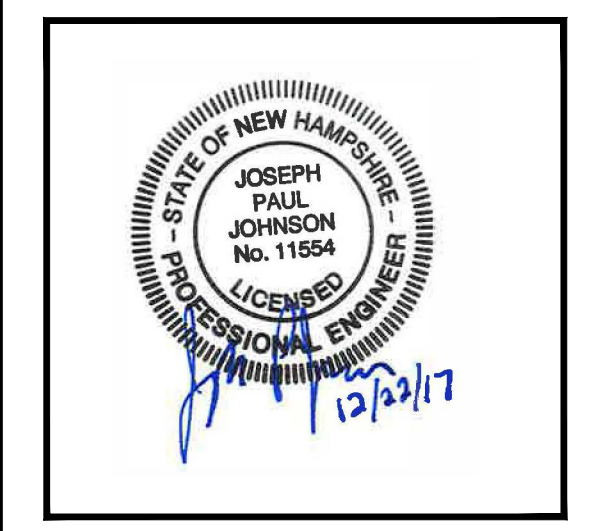
12/22/17
 DRAWN/DESIGN BY: CLS CHECKED BY: JPJ

CROSS SECTIONS

SCALE: 1"=4'



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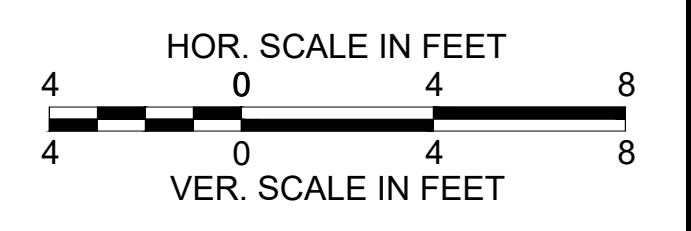
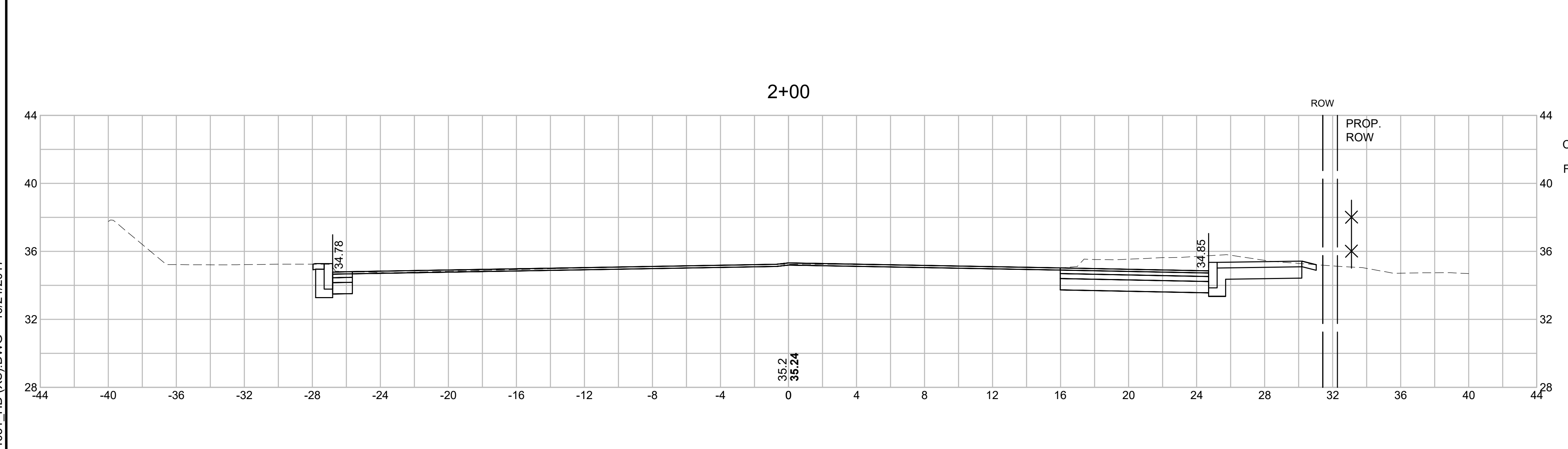
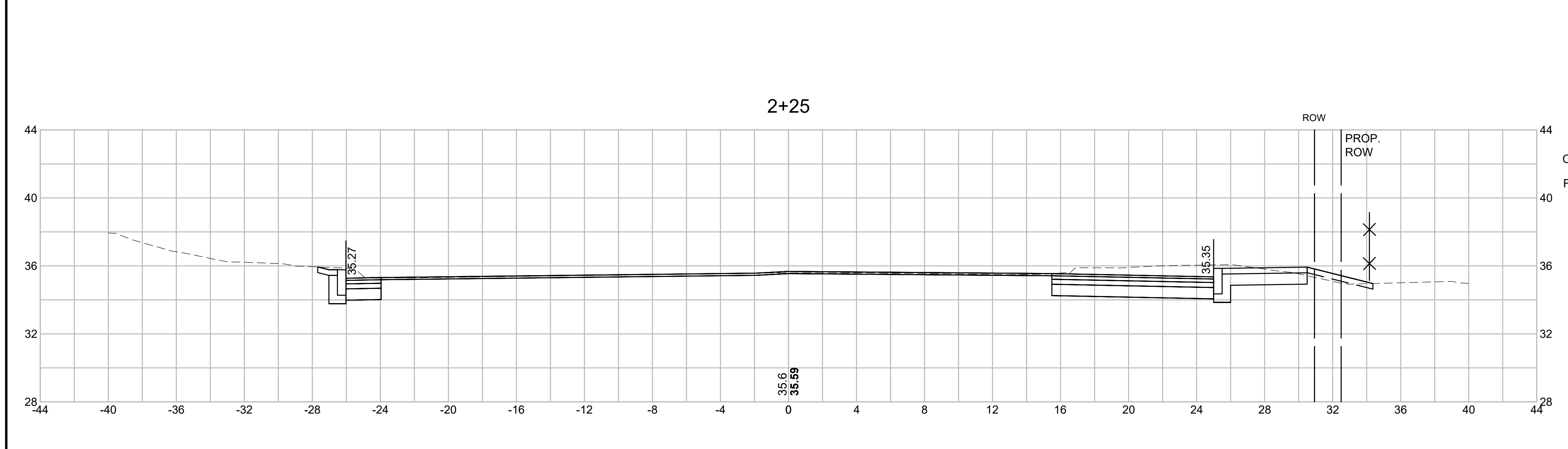
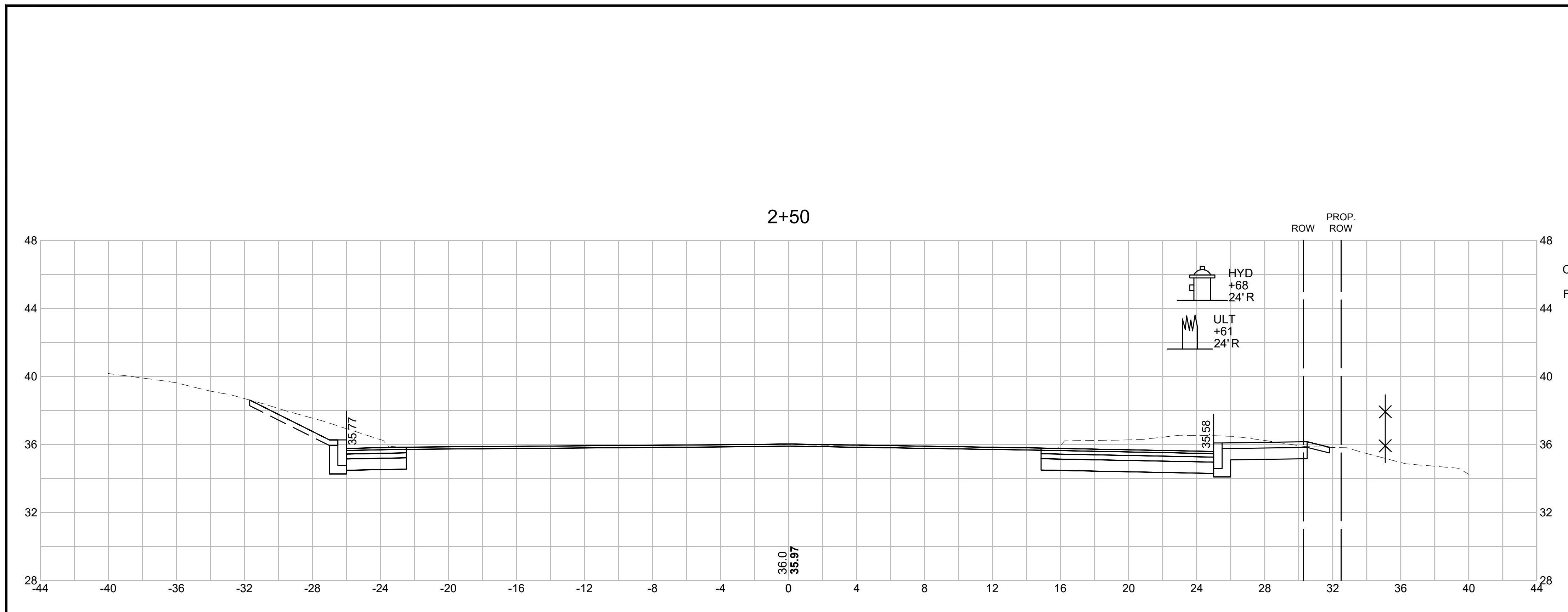
PREPARED FOR
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 680 PEVERLY HILL ROAD
 PORTSMOUTH, NH 03801

**INTERSECTION IMPROVEMENT PROJECT
 U.S. ROUTE 1 AT ANDREW JARVIS DRIVE
 CITY OF PORTSMOUTH
 NEW HAMPSHIRE**

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**INTERSECTION IMPROVEMENT PROJECT
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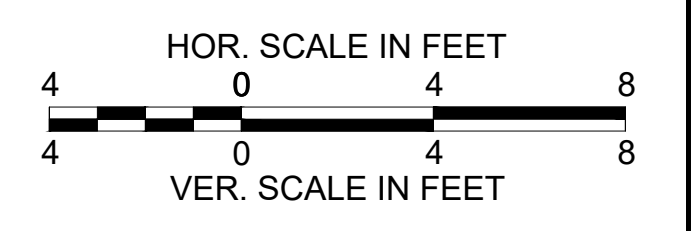
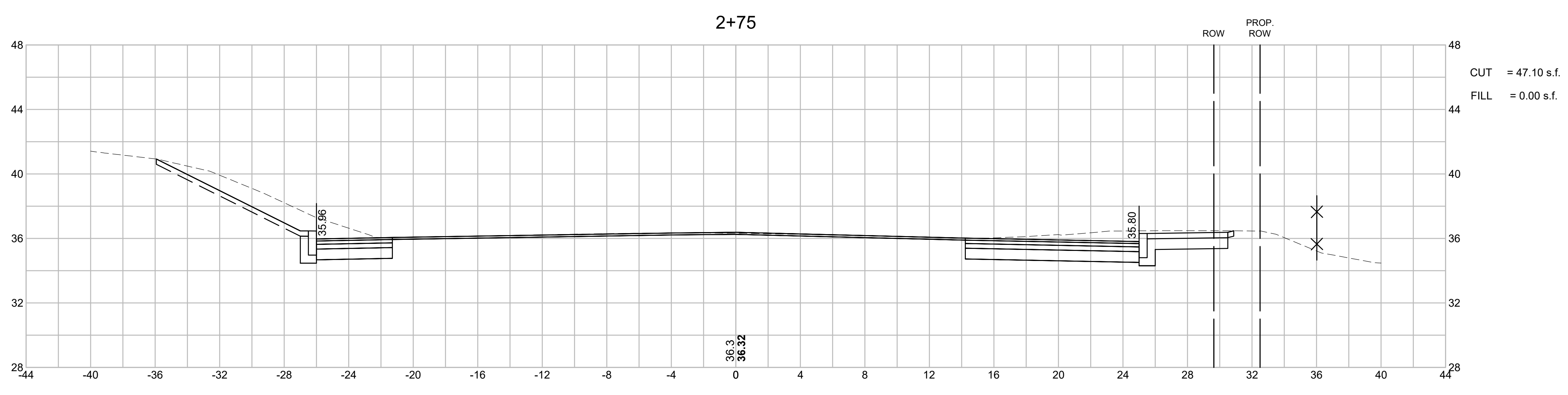
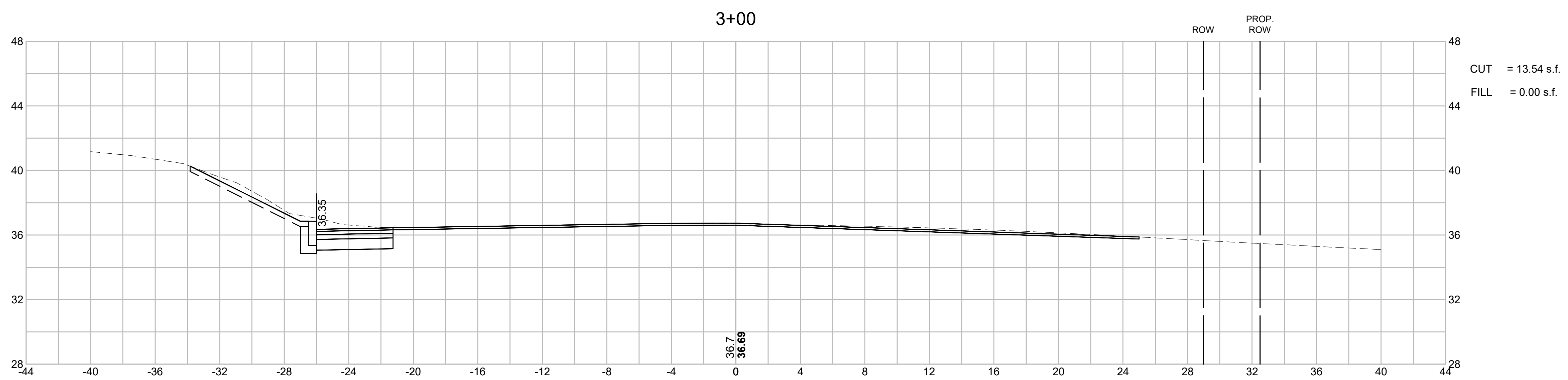
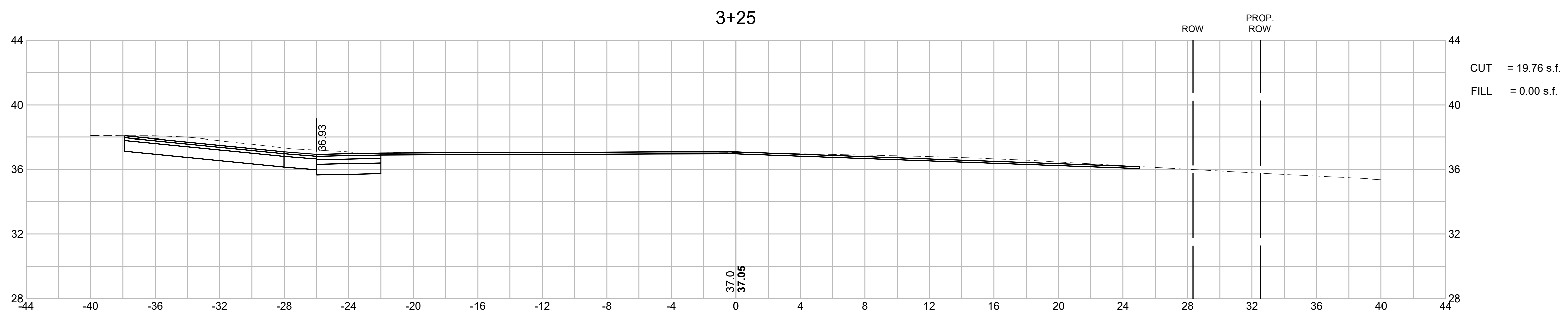
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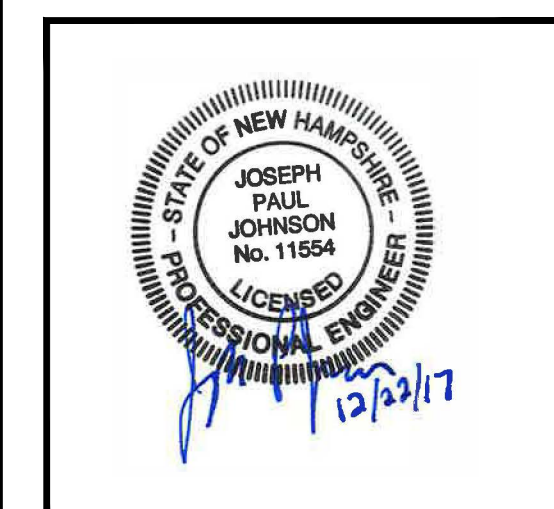
NO.	REVISION	DATE

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CROSS SECTIONS

SCALE: 1"=4'





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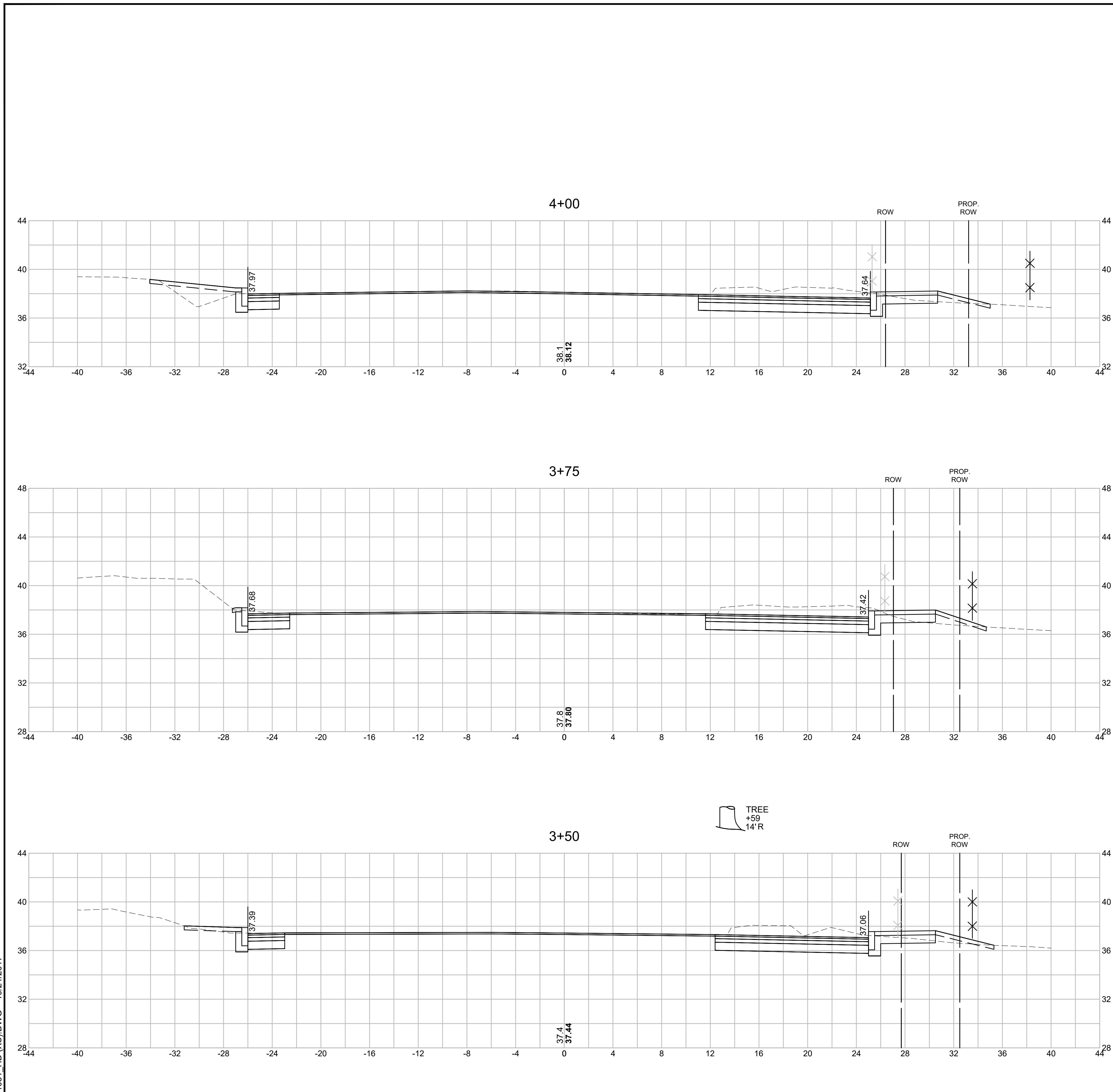
**INTERSECTION IMPROVEMENT PROJECT
 U.S. ROUTE 1 AT ANDREW JARVIS DRIVE
 CITY OF PORTSMOUTH
 NEW HAMPSHIRE**

REVISIONS		
NO.	REVISION	DATE

CROSS SECTIONS

SCALE: 1"=4'

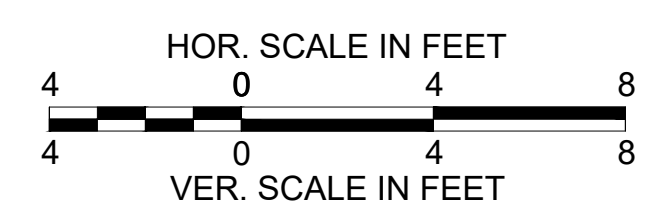
21 OF 33



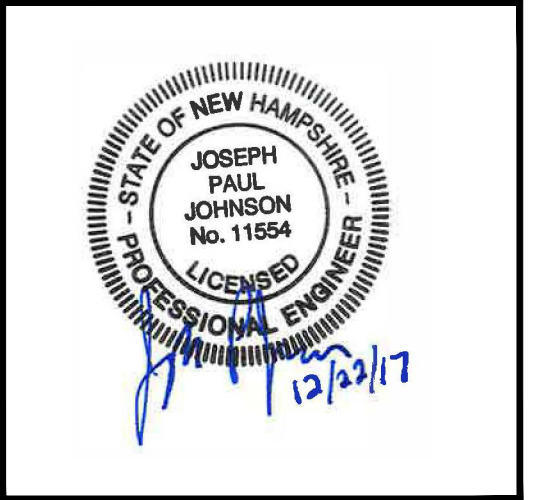
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CUT = 32.61 s.f.
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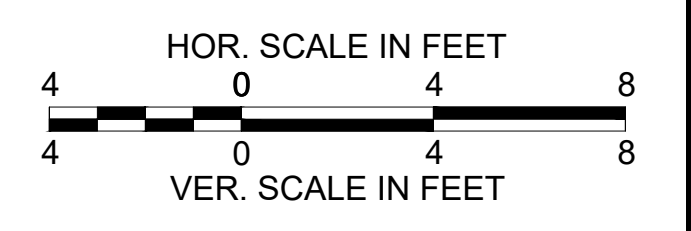
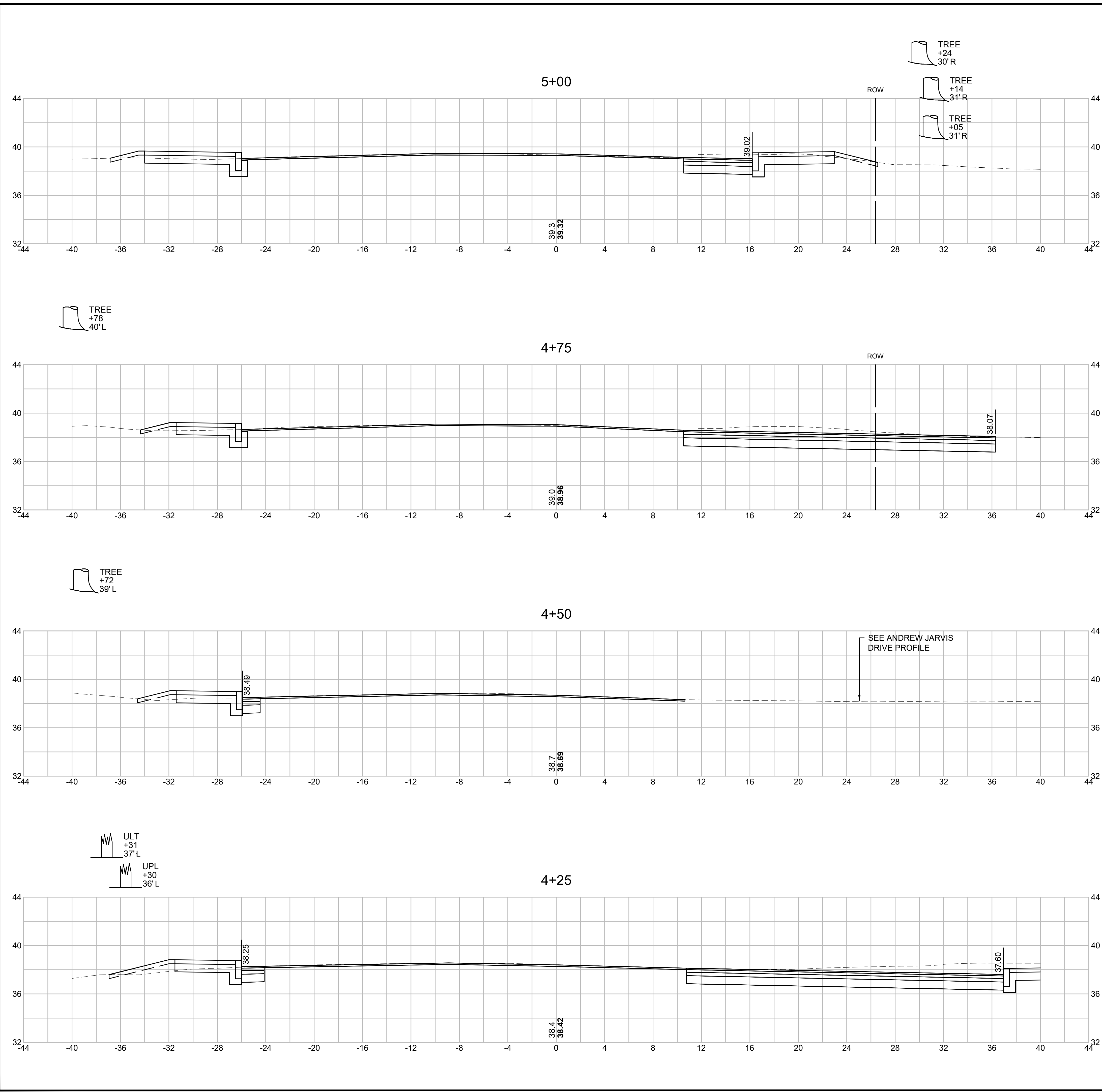
**INTERSECTION IMPROVEMENT PROJECT
 U.S. ROUTE 1 AT ANDREW JARVIS DRIVE
 CITY OF PORTSMOUTH
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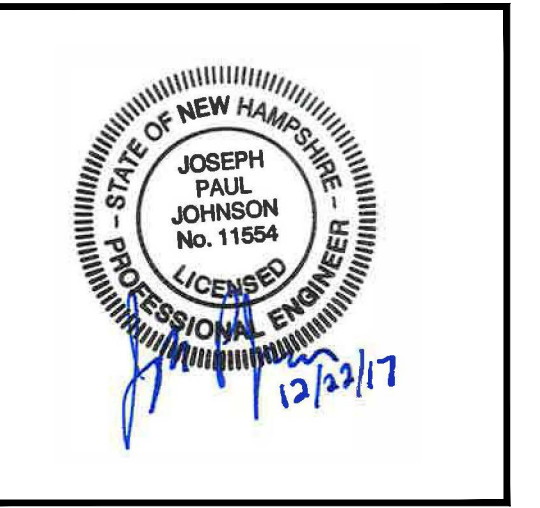
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**CROSS
 SECTIONS**

SCALE: 1"=4'





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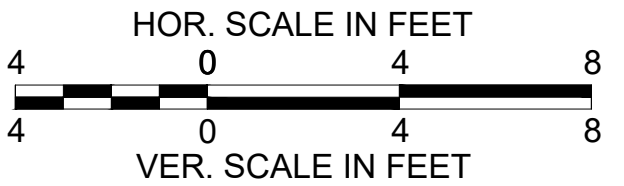
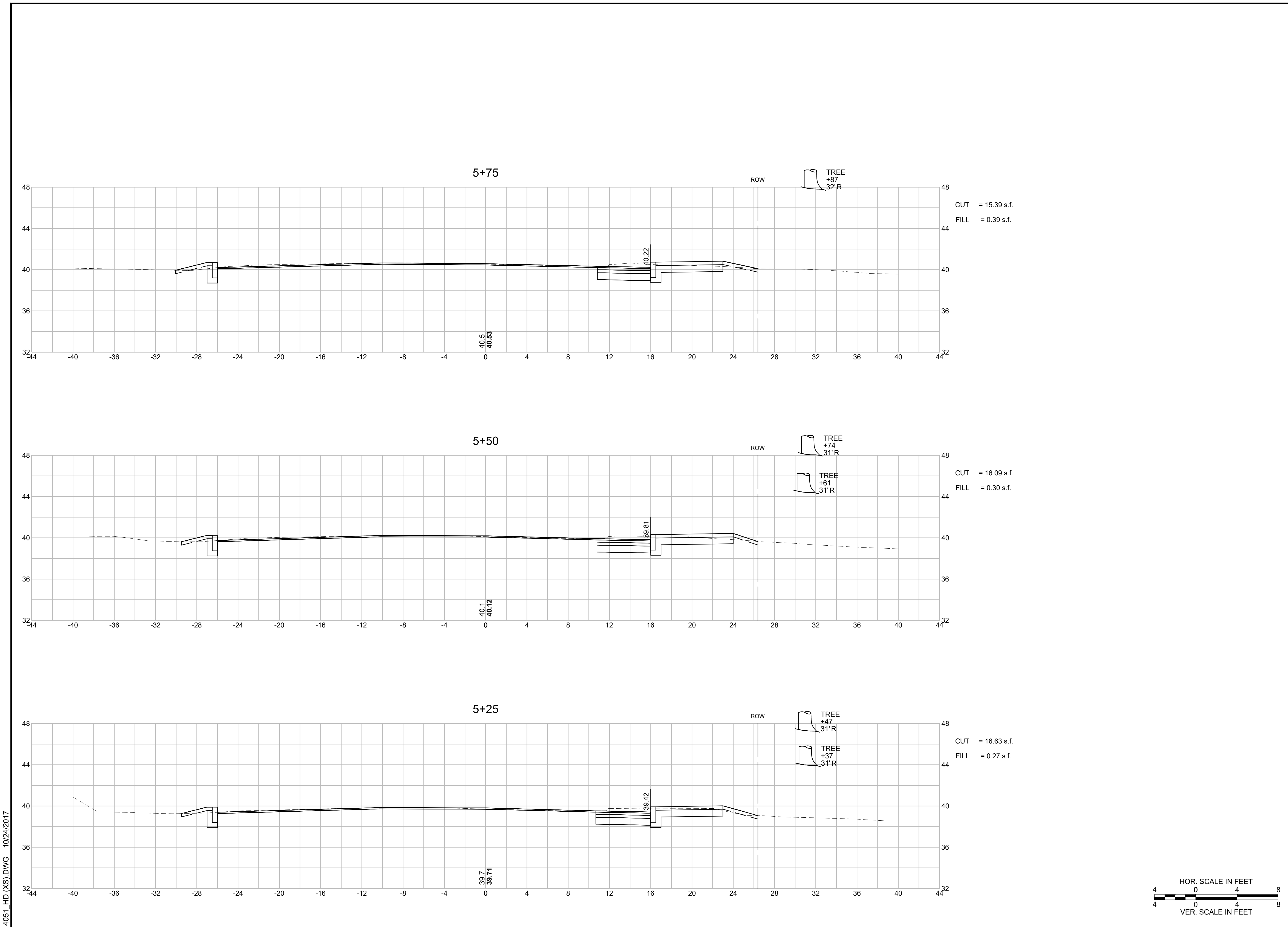
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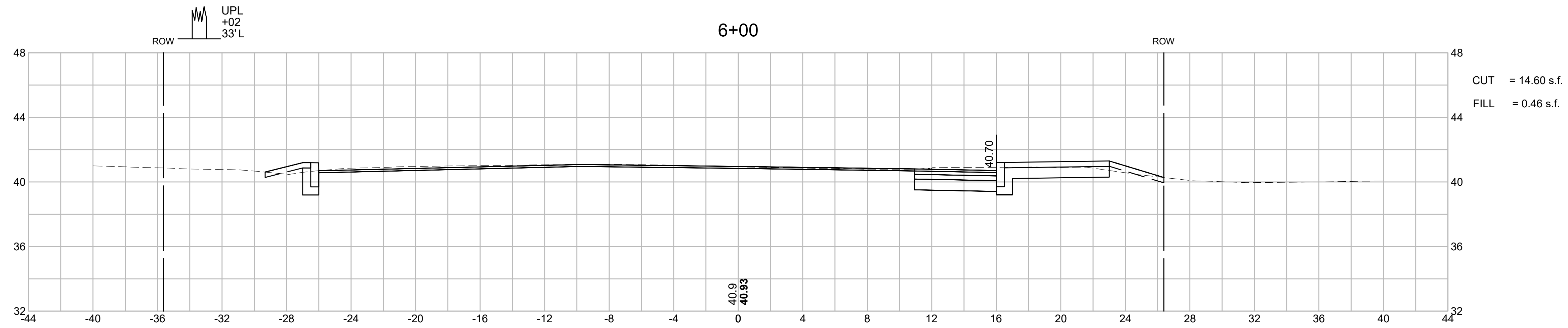
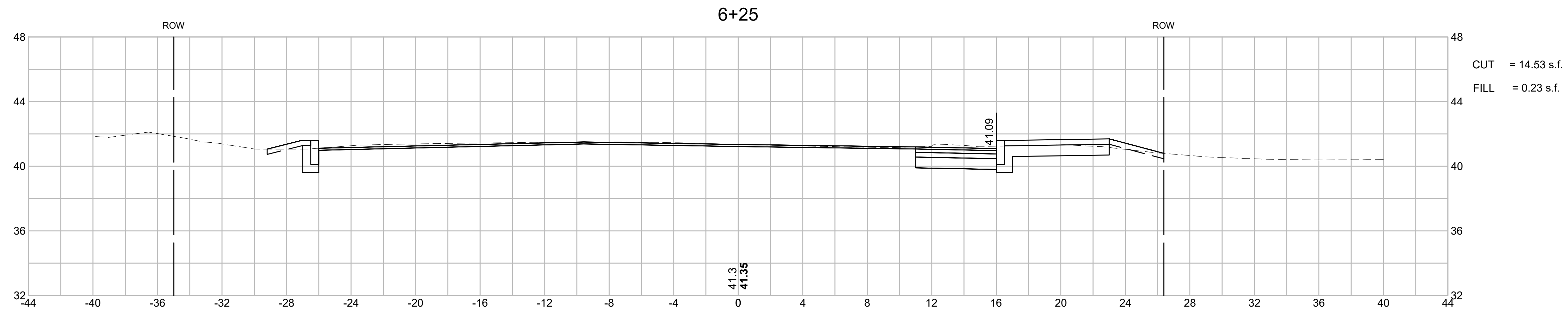
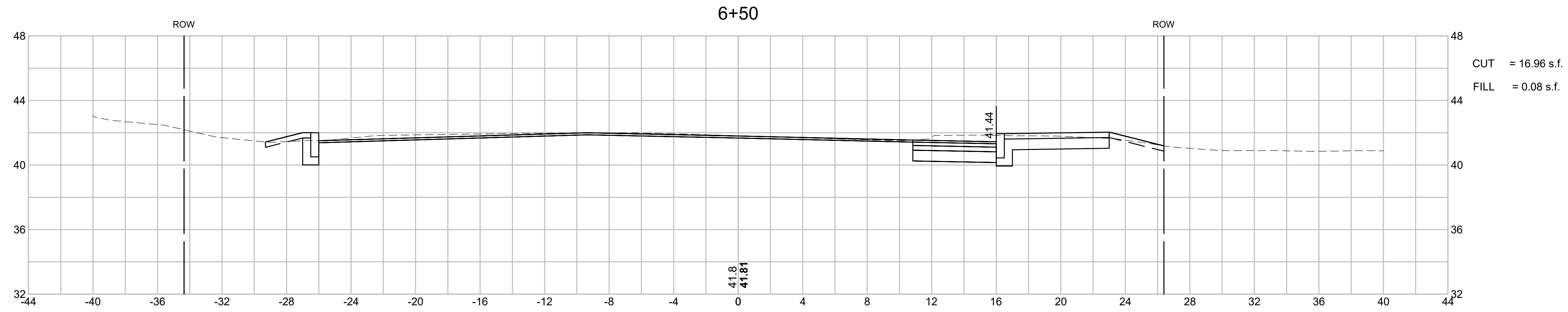
SCALE: 1"=4'
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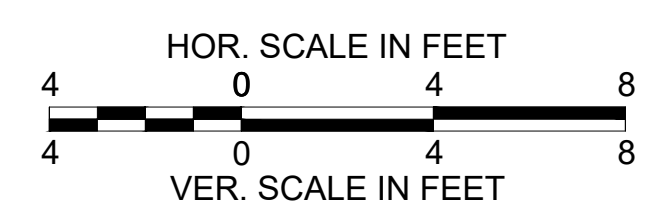


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CROSS SECTIONS

SCALE: 1"=4'

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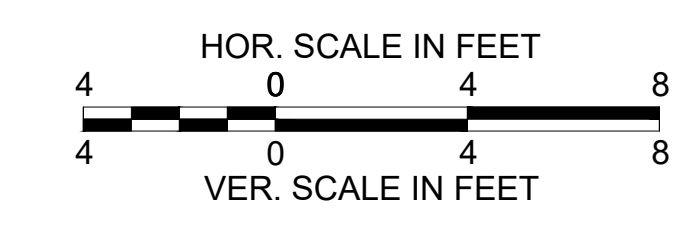
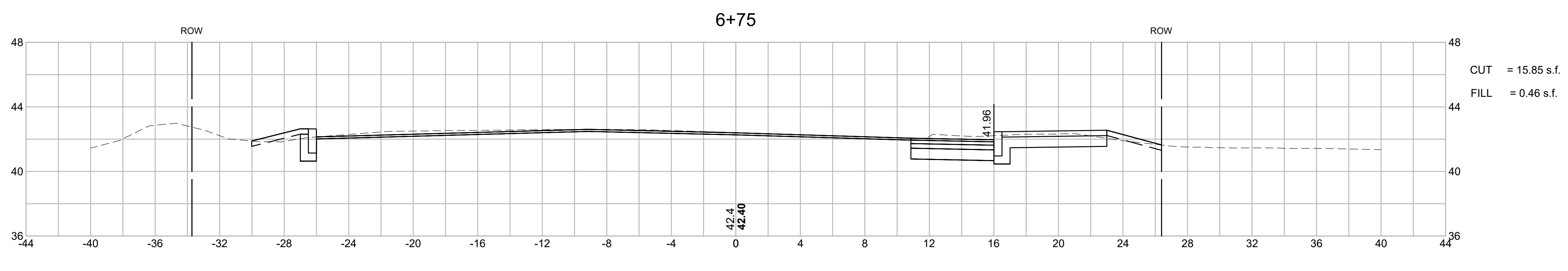
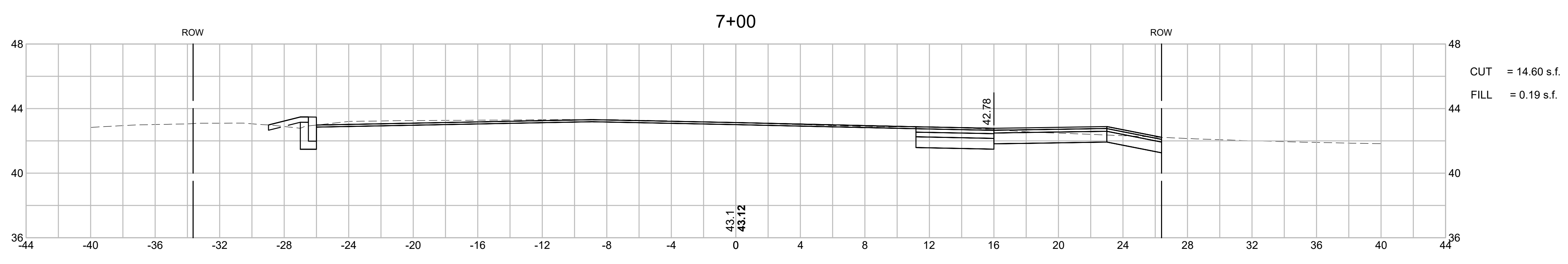
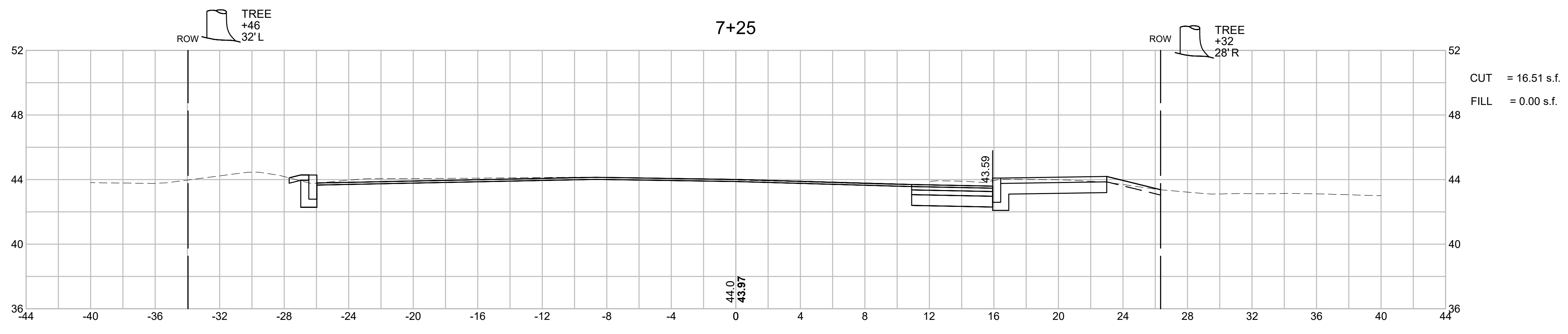


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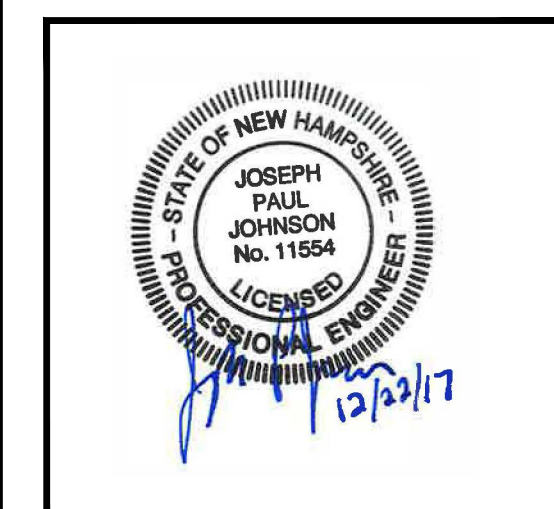
**INTERSECTION IMPROVEMENT PROJECT
 U.S. ROUTE 1 AT ANDREW JARVIS DRIVE
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 NEW HAMPSHIRE**

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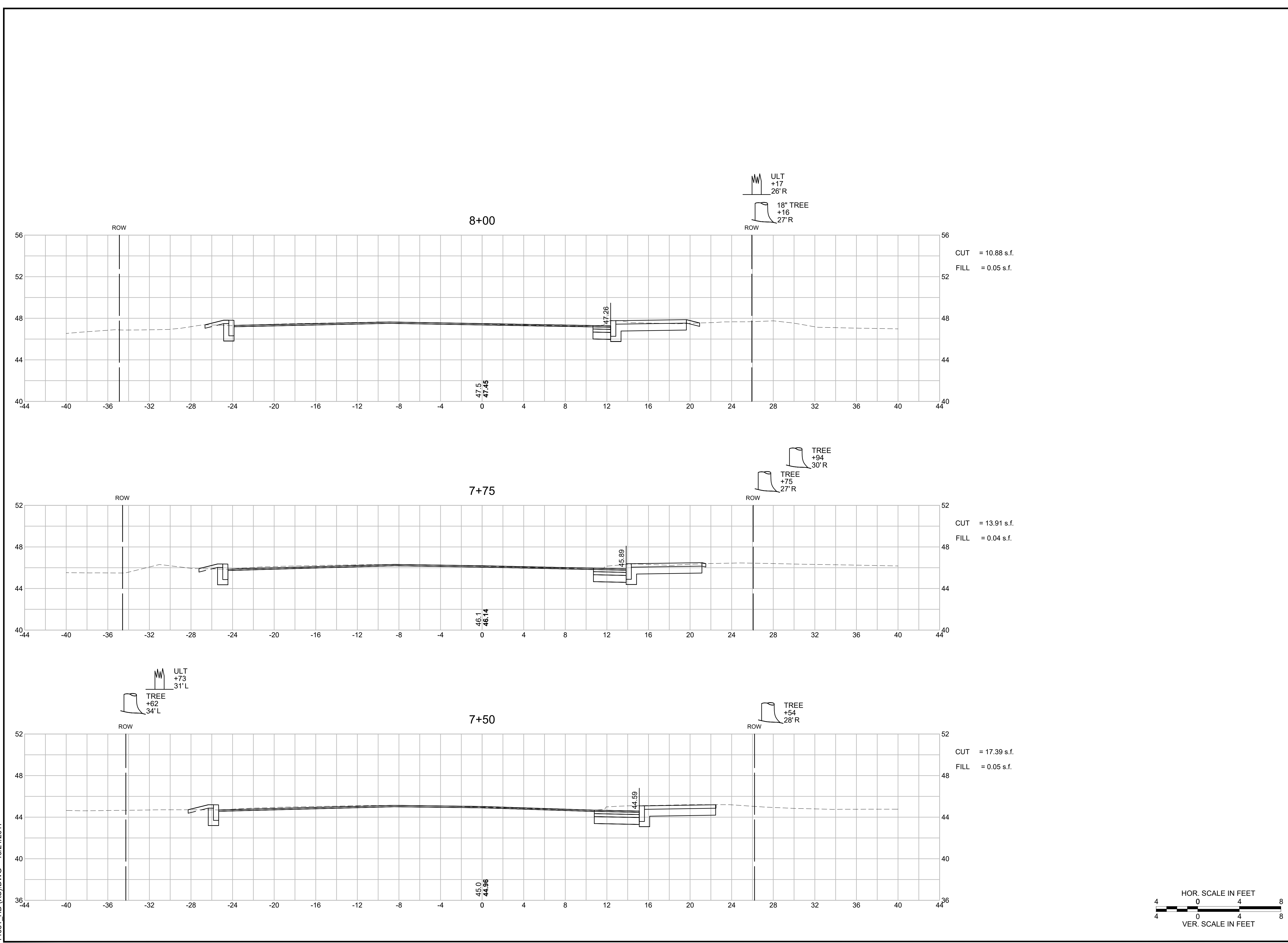
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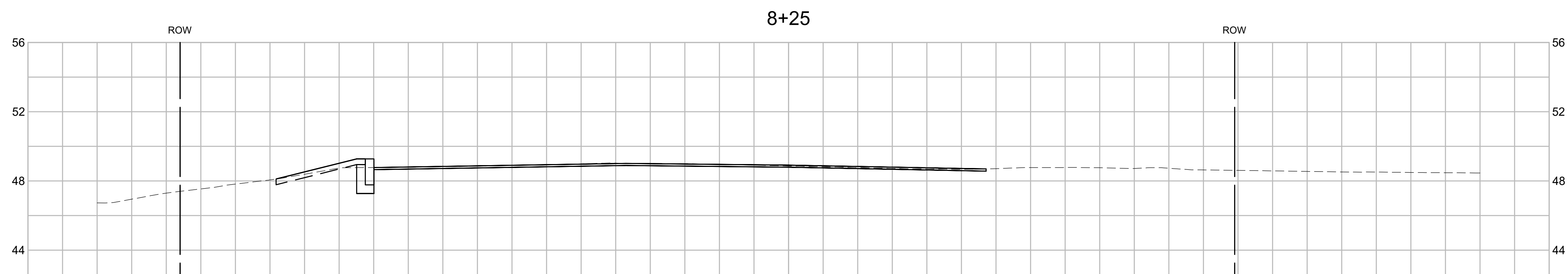
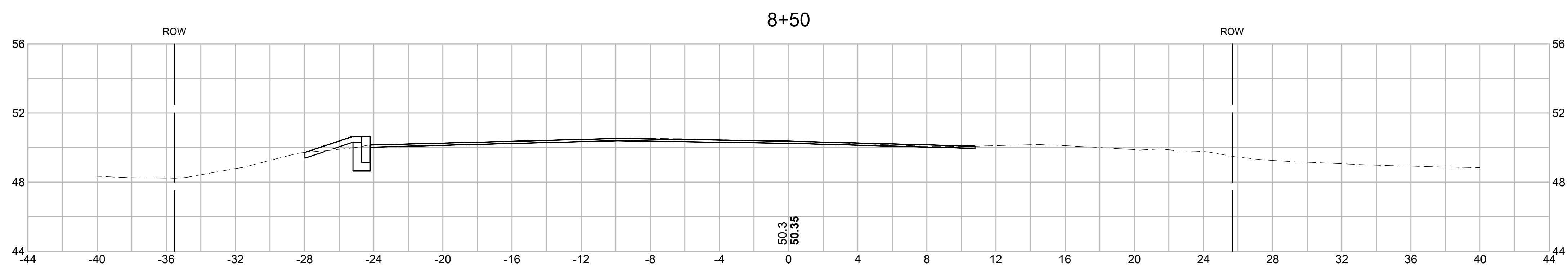
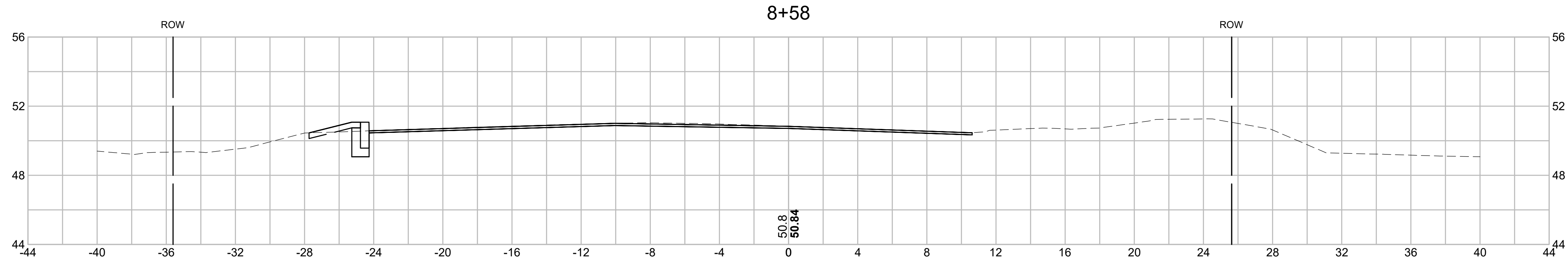
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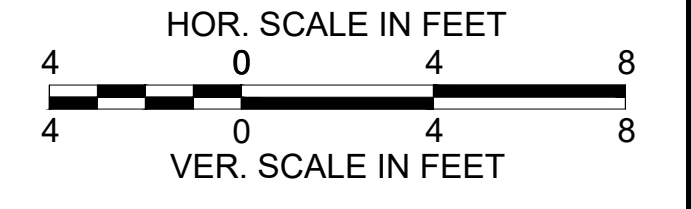
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12/22/17

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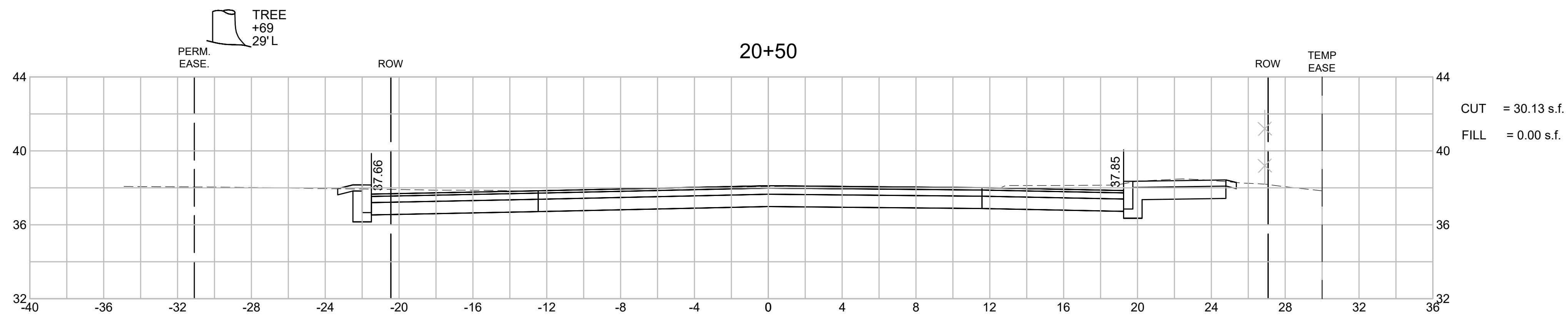
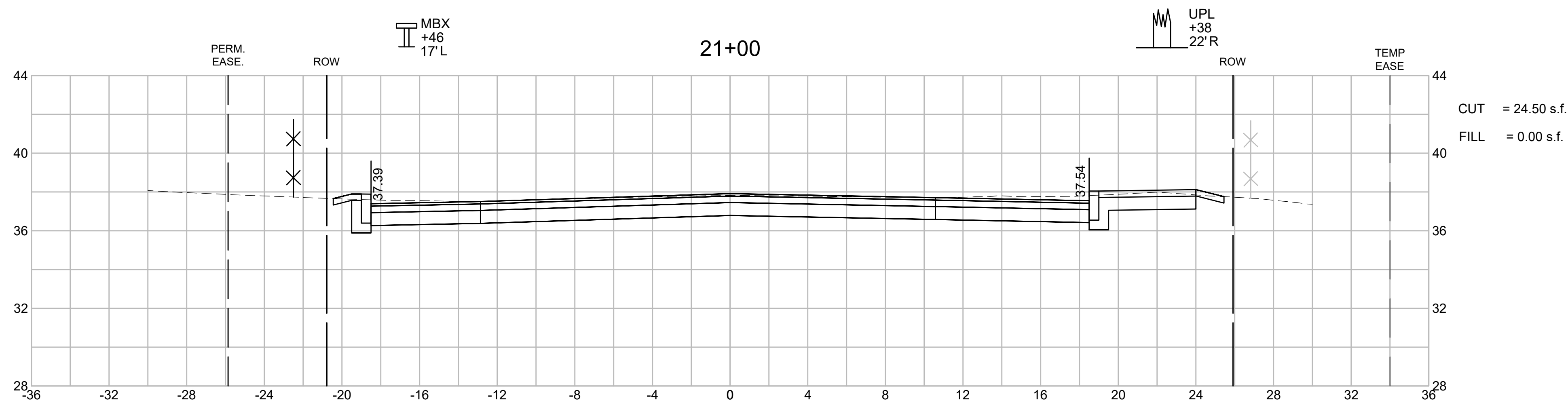
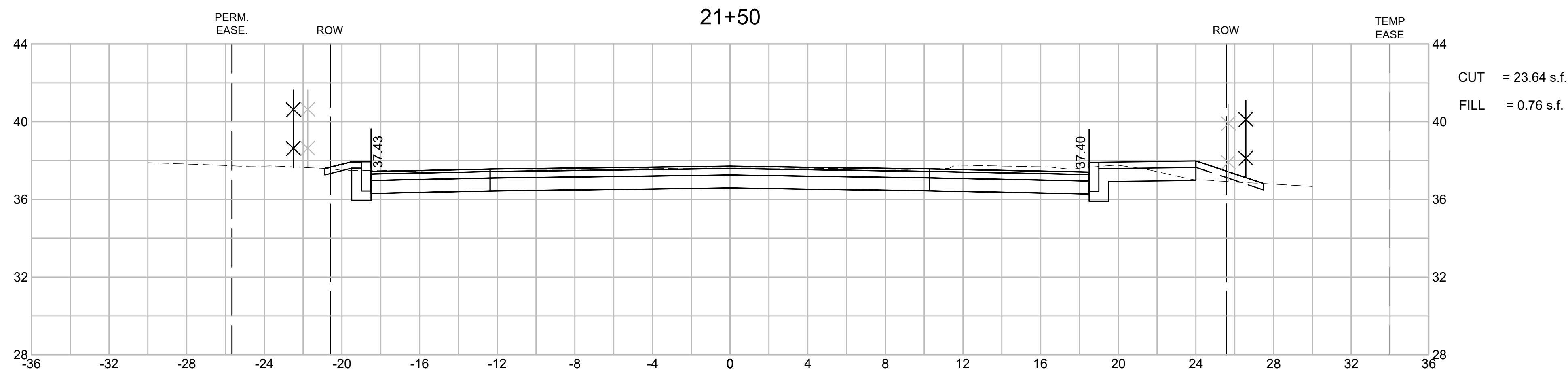
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**INTERSECTION IMPROVEMENT PROJECT
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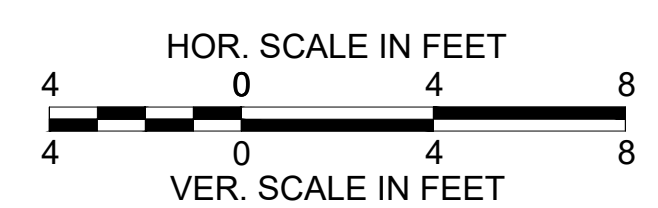


REVISIONS		
NO.	REVISION	DATE

CROSS SECTIONS

SCALE: 1"=4'

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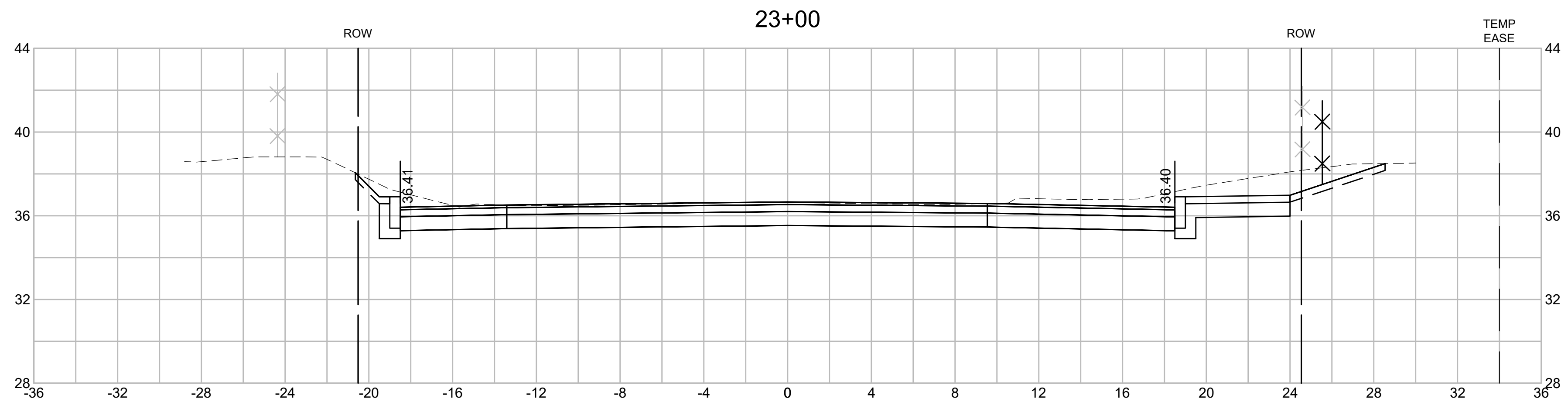
**INTERSECTION IMPROVEMENT PROJECT
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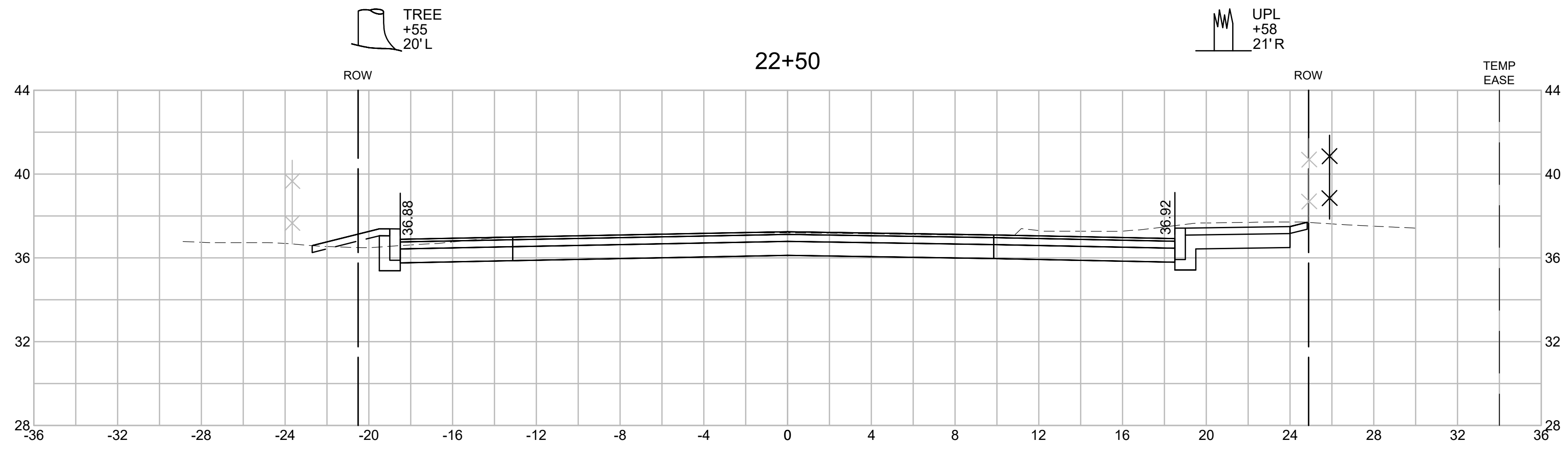
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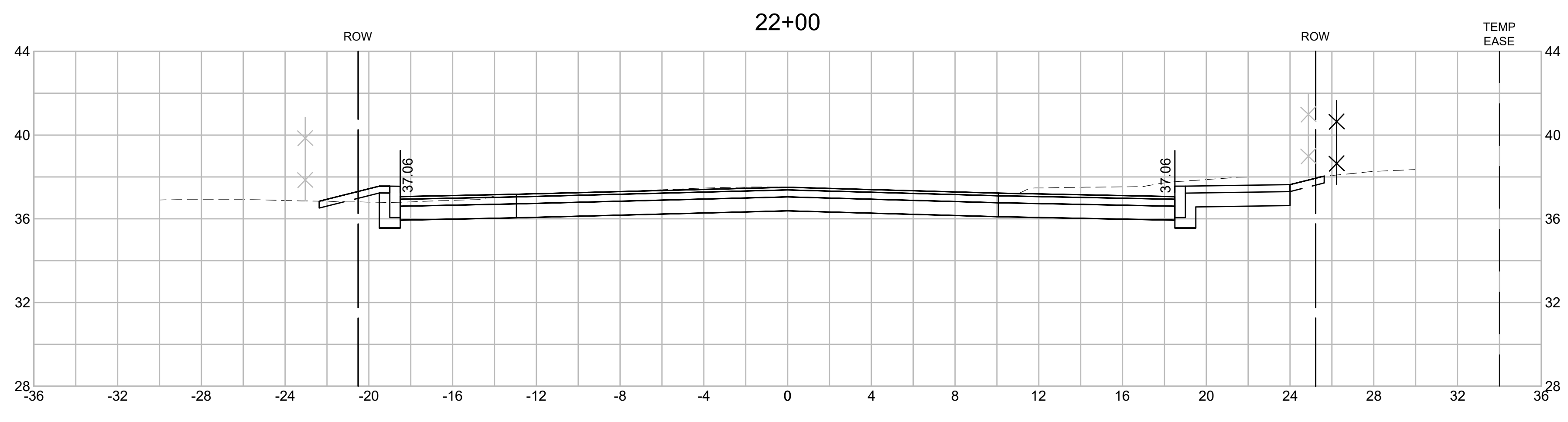
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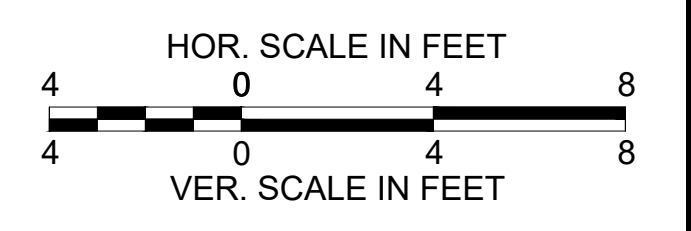
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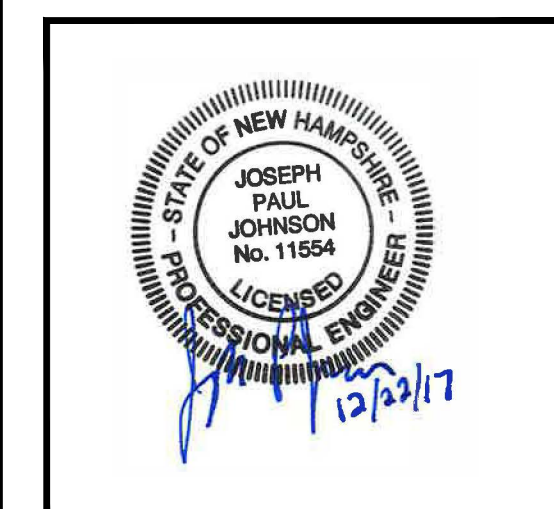
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14051_HD(XS).DWG 10/24/2017



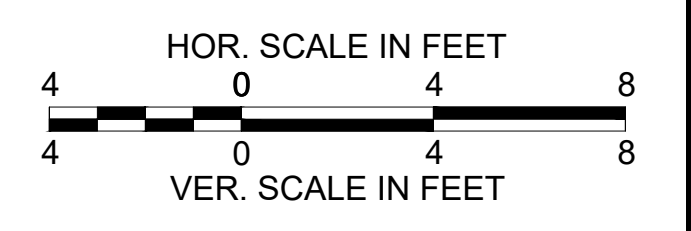
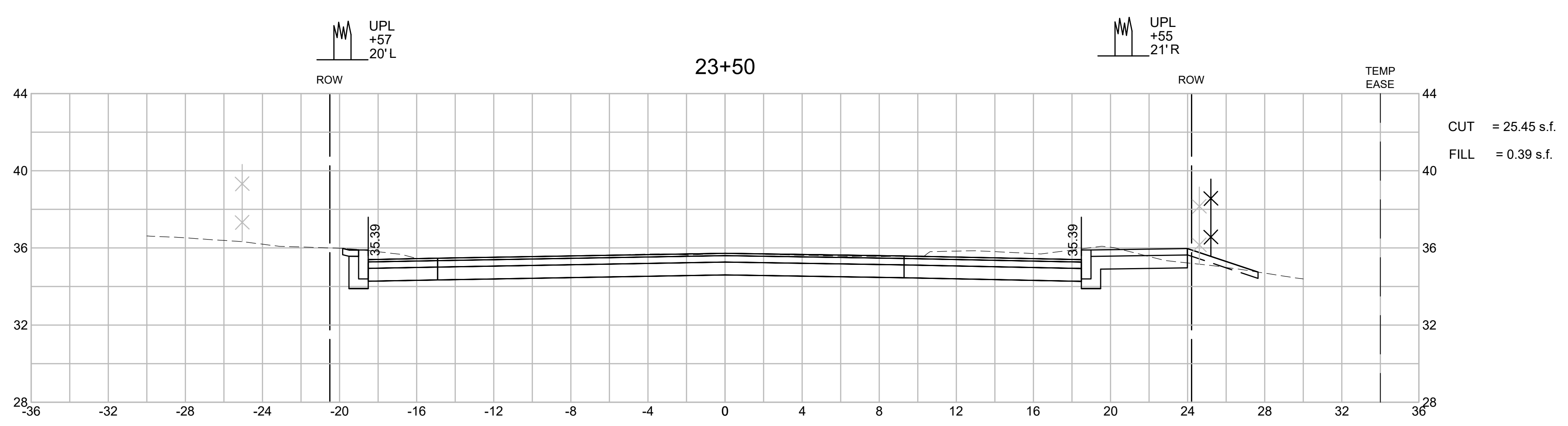
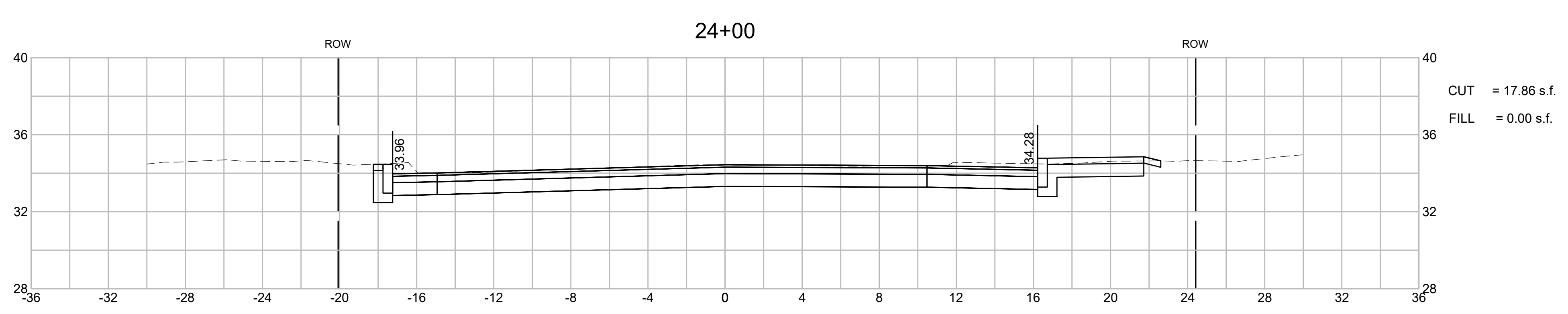
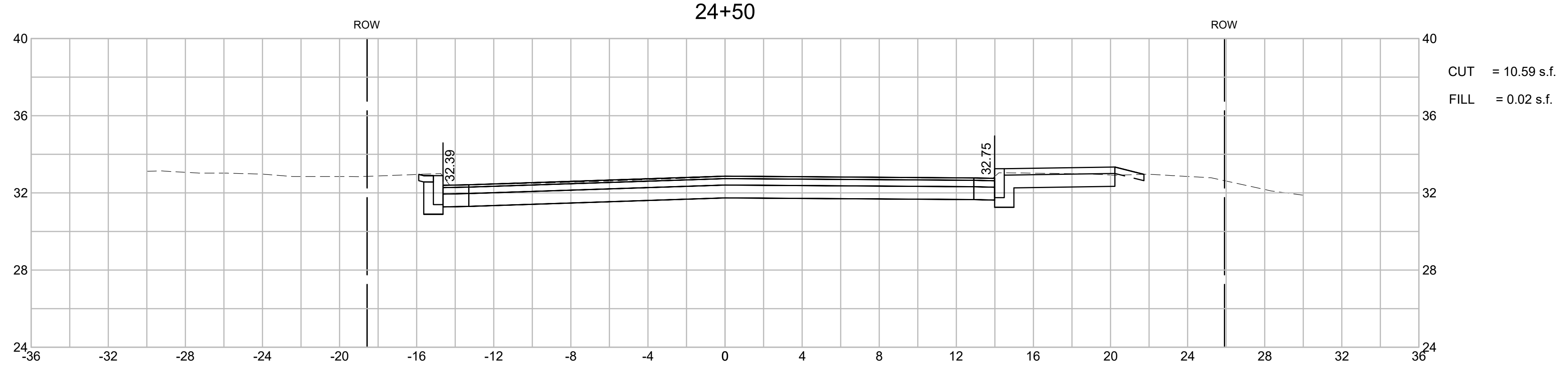
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**INTERSECTION IMPROVEMENT PROJECT
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 CITY OF PORTSMOUTH
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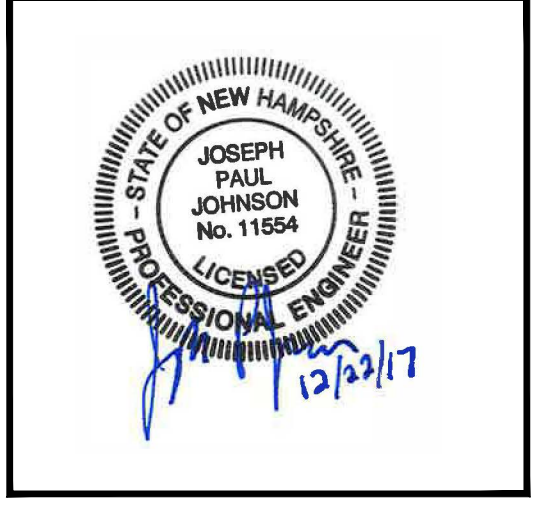
REVISIONS		
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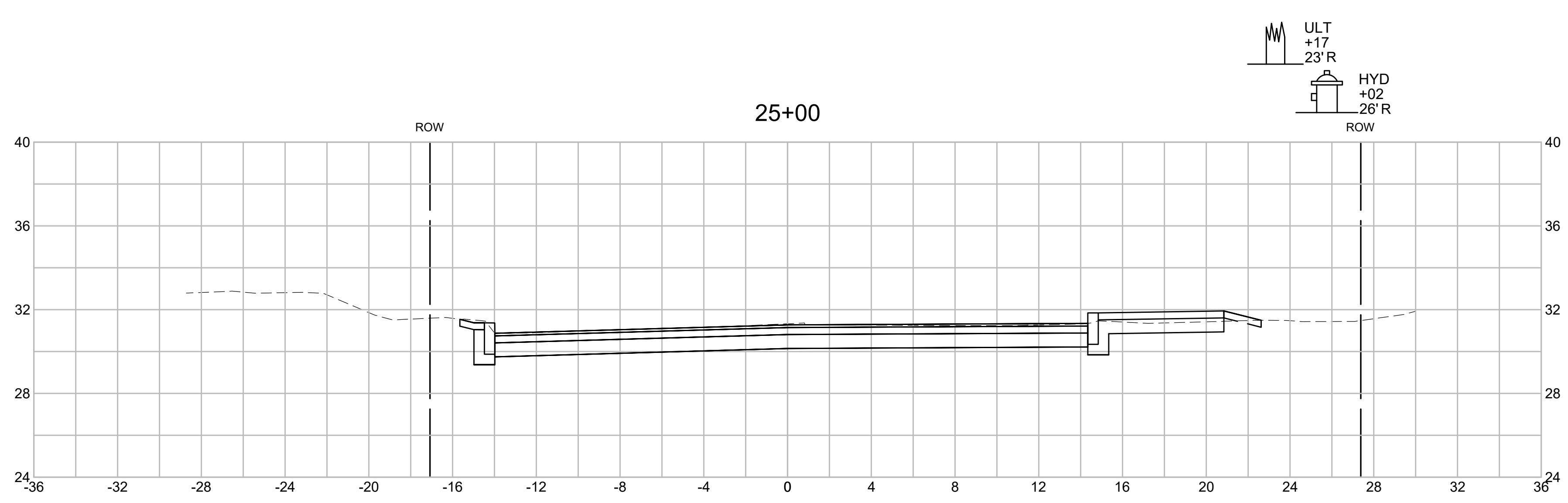
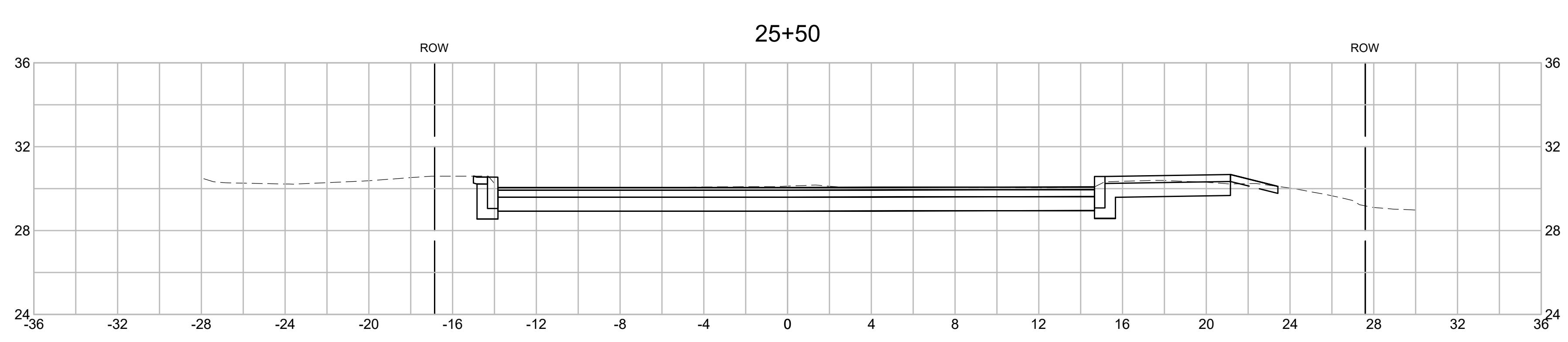
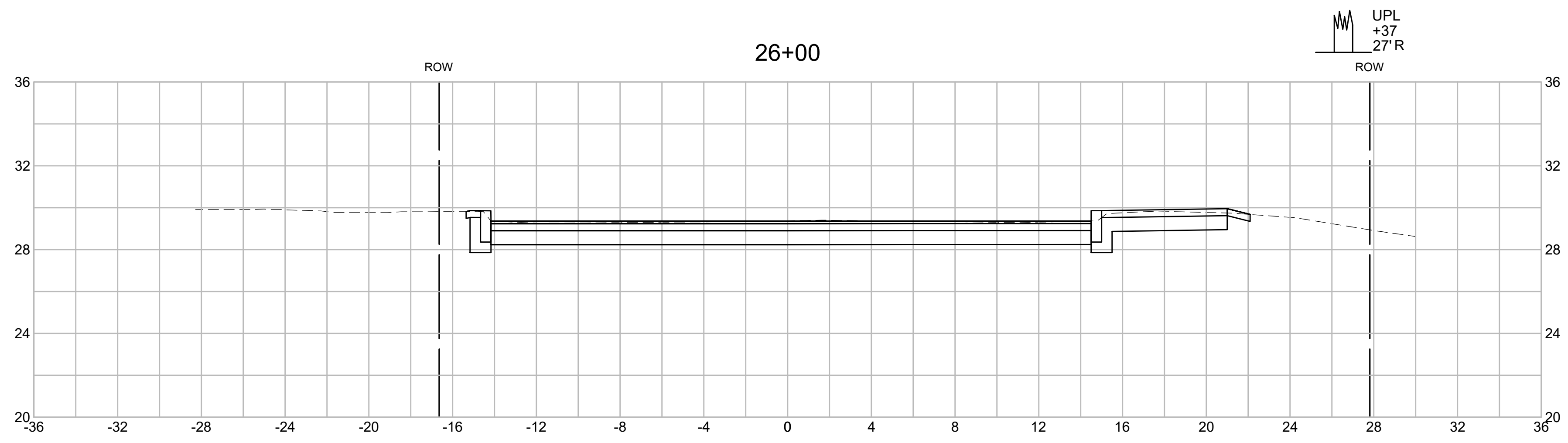


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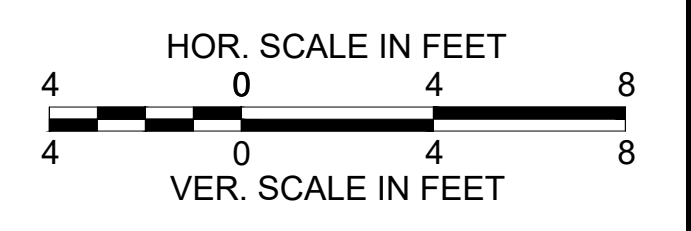


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CROSS SECTIONS
 SCALE: 1"=4'

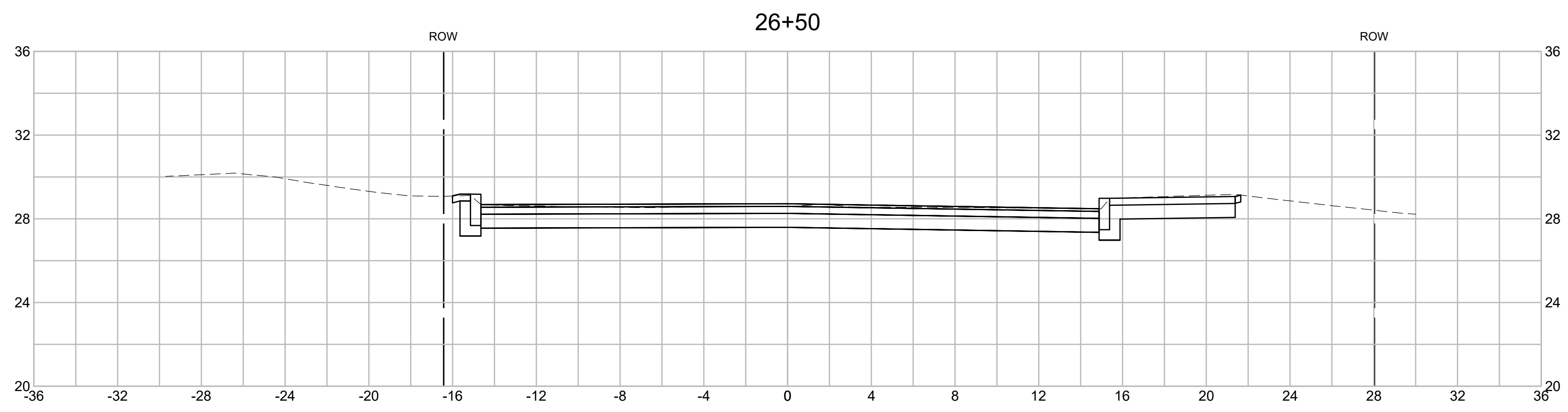
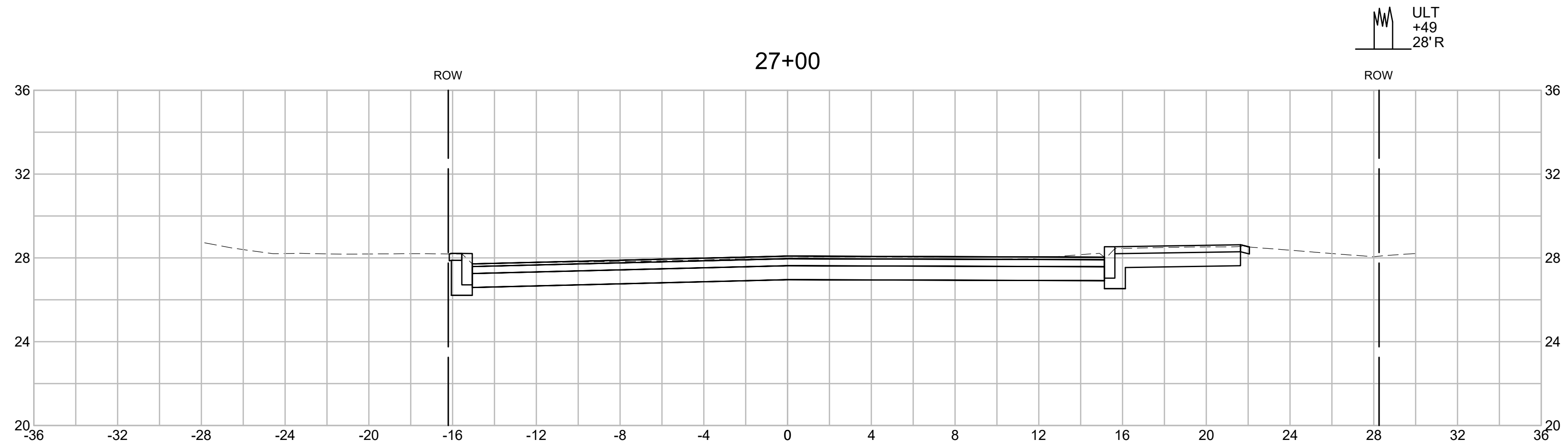
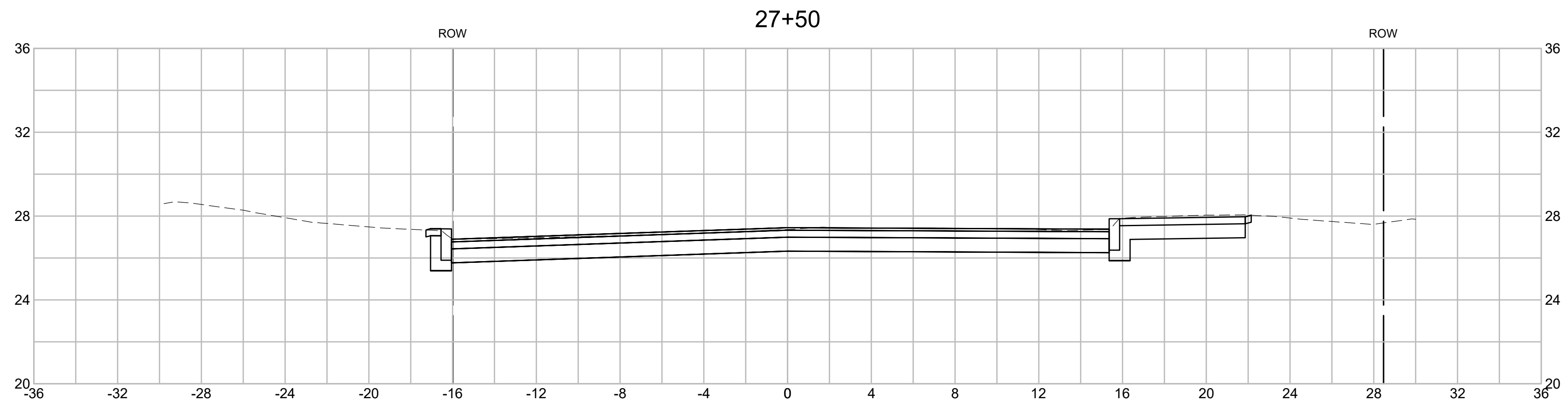


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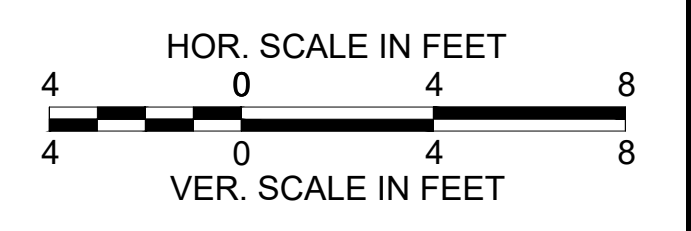
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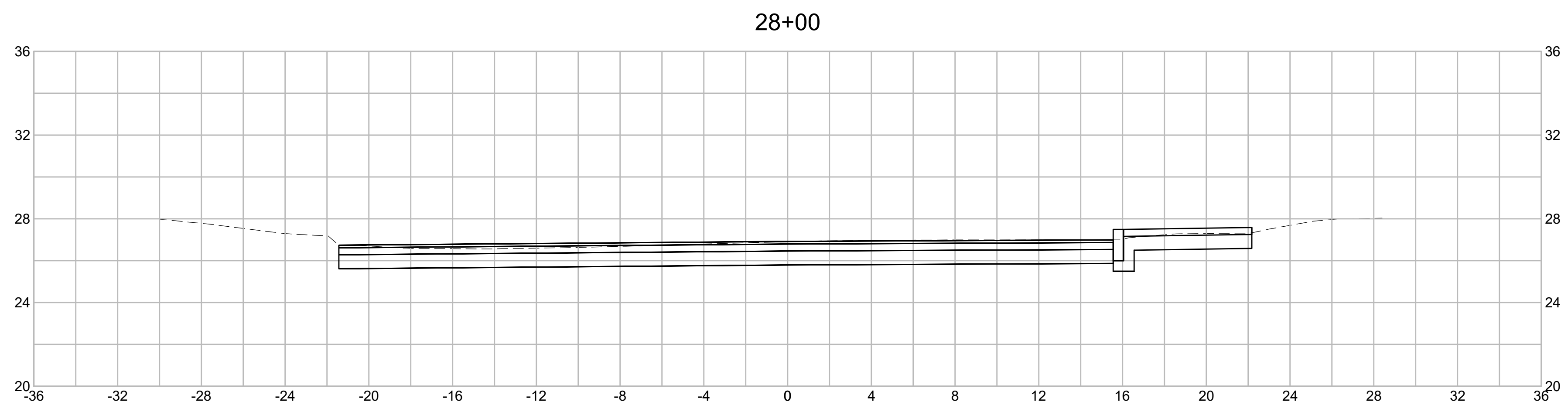
REVISIONS		
NO.	REVISION	DATE

CROSS SECTIONS

SCALE: 1"=4'

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CUT = 5.60 s.f.
 FILL = 0.00 s.f.

