# HANOVER STREET GARAGE RESTORATION



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# 34 HANOVER STREET. PORTSMOUTH, NH 03801

PROJECT NO: 16-003129.00 DRAWING INDEX SHEET NAME R-001 GENERAL NOTES, LEGENDS, & WORK ITEM SCHEDULE PHASING NOTES AND LEGENDS 2023 PHASING PLANS 2023 PHASING PLANS R-103A THIRD LEVEL FLOOR PLAN R-103B THIRD LEVEL UNDERSIDE PLAN R-104A FOURTH LEVEL FLOOR PLAN REPAIR DETAILS REPAIR DETAILS R-506 REPAIR DETAILS RG-101 GROUND LEVEL STRIPING PLAN RG-102 SECOND LEVEL STRIPING PLAN RG-103 THIRD LEVEL STRIPING PLAN RG-104 FOURTH LEVEL STRIPING PLAN

RG-105 FIFTH LEVEL STRIPING PLAN



SITE LOCATION MAP



- 1. CONSTRUCTION SHALL BE IN ACCORDANCE WITH ALL APPLICABLE FEDERAL. STATE OF NEW HAMPSHIRE AND CITY OF PORTSMOUTH CODES AND ORDINANCES (IBC 2015) INCLUDING FIRE CODES.
- 2. FIELD VERIFY ALL EXISTING DIMENSIONS AND CONDITIONS SHOWN ON DRAWINGS. REPORT ALL DISCREPANCIES IN THE FIELD TO ENGINEER IMMEDIATELY.
- 3. GENERAL CONSTRUCTION MEANS, METHODS, JOB SAFETY AND CONSTRUCTION PROCEDURES ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- 4. INCASE OF CONFLICT BETWEEN THE NOTES, DETAILS AND SPECIFICATIONS, THE MOST RIGID REQUIREMENTS SHALL GOVERN. CONTRACTOR SHALL MAKE NO DEVIATIONS FROM THE CONTRACT DRAWINGS WITHOUT AUTHORIZATION FROM THE ENGINEER.
- 5. THE CONTRACTOR SHALL REPAIR DAMAGE DONE TO EXISTING SURROUNDING STRUCTURE DONE DURING CONSTRUCTION AT NO COST TO THE OWNER.
- REPORT ALL DISCREPANCIES IN THE FIELD TO ENGINEER IMMEDIATELY.
- DO NOT SCALE DRAWINGS.

### DETAILS AND SYMBOLS

- 1. ALL REPAIR DETAILS ARE SHOWN ON DRAWINGS SERIES R-500.
- 2. DETAILS LABELED "FOR ALL CLARIFICATION ONLY" DO NOT REPRESENT ADDITIONAL INFORMATION IN SOME CASES THESE DETAILS SHOW VARIATION OF THE TYPICAL CONDITION.
- 3. WHEN THE WORK ITEM BUBBLE IS NOTED (TYP.), IT MEANS THE WORK ITEM OCCURS AT ALL LOCATIONS WITH THE APPLICABLE DETERIORATION OR DESIGNATION SYMBOLS OCCURS ON A PLAN.
- 4. WHERE (T.A.R) IS NOTED IT MEANS THERE MAY BE AREAS OF THIS WORK IN ADDITION TO THE PARTICULAR
- DESIGNATED AREAS.
- 5. WHERE TWO OR MORE WORK ITEM BUBBLES ARE SHOWN GROUPED TOGETHER IT MEANS ANY OR ALL OF THE DESIGNATED WORK ON AS MAY APPLICABLE.
- 6. COORDINATION OF WORK ITEMS IS THE CONTRACTOR'S RESPONSIBILITY.
- 7. WHEN WORK ITEM DETAILS ARE LISTED AS INCIDENTAL, THIS WORK IS INCLUDED IN THE PAYMENT OF THE OTHER WORK ITEM AND DOES NOT HAVE A SEPARATE PRICE.
- 8. WHEN THE DETAIL IS LABELED FOR REFERENCE ONLY IR PROVIDES IN FORMATION ABOUT INCIDENTAL WORK AND DOES NOT HAVE A PAY UNIT.
- 9. CONTRACTOR IS RESPONSIBLE FOR DETERMINING ACTUAL EXTENT AND LOCATIONS OF REPAIR AREAS IN ACCORDANCE WITH THE SPECIFICATIONS: WORK ITEMS ARE SHOWN ONLY TO REPRESENT THE TYPES OF
- 10. SEE WORK ITEM SPECIFICATION FOR INFORMATION REGARDING DETAILS.

### CONCRETE REPAIR MATERIAL REQUIREMENTS SEE SECTIONS 020010 FOR SPECIFIC USES

- 1. 1. CAST-ON-PLACE CONVENTIONAL CONCRETE
  - COMPRESSIVE STRENGTH 5000 PSI @ 28 DAYS (2500 @ 3DAYS) WATER-CEMENT RATIO 1/2" INCH (SELECT LARGER AGG FOR SECTION GREATER THAN MAX SIZE AGGREGATE
- SLUMP (MAXIMUM)

a. CONTINUOUSLY WATER BURLAP

PROPER CURING OF THE CONCRETE.

6" W/ SUPERPLASTICIZER (AFTER WATER REDUCER ADDITION) AIR CONTENT 7% ± 1-1/2% CEMENT CONTENT 658 LB./C.Y. MIN. 2. NO WATER SHALL BE ADDED TO THE CONCRETE AT THE JOBSITE/. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE CONCRETE SUPPLIER TO ENSURE A PUMPABLE AND

RETARDANTS AND OTHER ADDITIVES SHALL BE AT THE OPTION OF THE CONTRACTOR SUBJECT TO THE

APPROVAL OF THE STRUCTURAL ENGINEER. FOLLOW THE RECOMMENDATIONS OF THE MANUFACTURER

FOR THE PROPER USE OF THE ADDITIVES. THE USE OF CALCIUM CHLORIDE OR OTHER CHLORIDE BEARING SALTS SHALL NOT BE PERMITTED. 3. CURING OF SLABS SHALL COMPLY WITH THE RECOMMENDATIONS OF ACI 308. PROVIDE 7 DAY CURING OF

WORKABLE MIX WITHOUT THE ADDITION OF WATER AT THE JOBSITE. THE USE OF PLASTICIZERS,

- SLABS IMMEDIATELY AFTER FINISHING USING ONE OF THE FOLLOWING METHODS
- 4. ALL CONCRETE WORK AND REINFORCING BAR DETAILS SHALL CONFORM TO THE LATEST AC STANDARDS, ACI 318, 301 AND ACI SP-66 (DETAILING MANUAL).
- 5. PROTECT THE CONCRETE SURFACE BETWEEN FINISHING OPERATIONS ON HOT, DRY DAYS OR ANY OTHER TIME THAT PLASTIC SHRINKAGE CRACKS COULD DEVELOP BY USING WET BURLAP. PLASTIC MEMBRANE OR FOGGING. PROTECT CONCRETE SLAB AT ALL TIMES FROM RAIN, HAIL OR OTHER INJURIOUS EFFECTS.
- 6. HOT WEATHER CONCRETE INSTALLATIONS AS DEFINED IN ACI 305 SHALL BE PERFORMED IN ACCORDANCE WITH ACI 305.
- 7. COLD WEATHER CONCRETE INSTALLATION AS DEFINED BY ACI 306 SHALL BE PERFORMED IN ACCORDANCE WITH ACI 306.
- 8. PROVIDE MINIMUM 5-DAY FROST PROTECTION FOR CONCRETE PLACED AND/OR CURED IN COLD WEATHER CONDITIONS.
- 9. WHEN CONCRETE PLACEMENT IS TO OCCUR DURING HOT OR COLD WEATHER CONDITIONS AS DEFINED BY ACI 305 OR 306, THE CONTRACTOR SHALL SUBMIT PROPOSED CURING METHODS TO THE STRUCTURA ENGINEER FOR REVIEW AT LEAST 10 DAYS IN ADVANCE OF THE PROPOSED CONCRETE PLACEMENT DATE. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT CURING METHODS ARE WORKING EFFECTIVELY DURING THE SPECIFIED DURATION FOR CURING. THE CONTRACTOR IS RESPONSIBLE FOR
- 10. EXTERIOR SLAB SHALL RECEIVED A BROOM FINISH. FINISHING OF SLAB SURFACES SHALL COMPLY WITH THE RECOMMENDATIONS OF ACI 302.1 AND 304.
- 11. FORMWORK SHALL BE LEFT IN PLACED FOR A MINIMUM 7 DAYS. REFER TO REPAIR SEQUENCE NOTES FOR ADDITIONAL INFORMATION.
- 12. PROVIDE AND INSTALL NECESSARY TIE BARS, SPACER BARS, CHAIR BARS AND BOLSTERS AS REQUIRED TO MAINTAIN STEEL IN A RIGID POSITION PRIOR TO PLACING CONCRETE. ALL SUPPORTS AND ACCESSORIES SHALL CONFORM TO REQUIREMENTS OF CRSI "RECOMMENDED PRACTICE FOR PLACING
- 13. WELDED WIRE REINFORCEMENT SUPPORTS SHALL BE CONTINUOUS AND SPACED AT 2 FEET O.C.,
- 14. EPOXY-COATED WELDED WIRE REINFORCEMENT SHALL CONFORM TO ASTM A884, CLASS A, PLAIN STEEL. a. WELDED WIRE REINFORCEMENT SHALL BE PROVIDED IN MATS ONLY. ROLL STOCK IS PROHIBITED.
- 15. FABRICATE STEEL REINFORCEMENT ACCORDING TO CRSI'S "MANUAL OF STANDARD PRACTICE."
- 16. REINFORCEMENT BARS SHALL CONFORM TO ASTM A 615, GRADE 60 U.N.O.
- 17. FORM TIES: FACTORY FABRICATED, ADJUSTABLE-LENGTH, REMOVABLE OR SNAP-OFF METAL FORM TIES, DESIGNED TO PREVENT UNITS THAT WILL LEAVE NO METAL CLOSER THAT 1.5 IN. TO EXPOSED SURFACE.
- **18.** PROVIDE COMMERCIAL FORMULATION FORM-COATING COMPOUNDS WITH A MAXIMUM VOC OF 350 GRAM/LITER THAT WILL NOT BOND WITH, STRAIN OR ADVERSELY AFFECT CONCRETE SURFACES AND WILL NOT IMPAIR SUBSEQUENT TREATMENTS OF CONCRETE SURFACES, INCLUDING BUT NOT LIMITED TO WATER-CURING COMPOUND, STAINS OR PAINTS.

### **CONCRETE PATCHING AND REPAIRS**

- 1. PERFORM CONCRETE PATCHING AND REPAIRS IN ACCORDANCE WITH THE REQUIREMENTS OF THE CONTRACT SPECIFICATIONS, IN THE PARTICULAR SECTIONS 03300-CAST-IN-PLACE CONCRETE, 03740-STRUCTURAL CRACK REPAIR BY EPOXY INJECTION, AND 03930-CONCRETE REHABILITATION.
- 2. CONTRACTORS SHALL QUANTIFY AND MARK-OUT REPAIR LOCATIONS AND CONFIRM QUANTITIES WITH ENGINEER PRIOR TO THE START OF DEMOLITION.
- 3. CONTRACTOR SHALL USE A PATCHOMETER OR OTHER APPROVED DEVICE TO LOCATE AND MARK-OUT EXISTING STEEL REINFORCING AND OTHER EMBEDDED ITEMS, SUCH AS ELECTRICAL CONDUITS OR PIPING
- 4. INSTALLATION OF REINFORCEMENT AND PREPARATION OF CONCRETE REPAIR CAVITIES SHALL BE SUBJECT TO OBSERVATION BY THE ENGINEER PRIOR TO PLACING CONCRETE AND/OR PATCHING

### STRUCTURAL STEEL

- 1. DESIGN FABRICATION AND ERECTION OF STRUCTURAL STEEL AND MISCELLANEOUS METALS SHALL CONFORM TO THE "STEEL CONSTRUCTION MANUAL." AISC CURRENT ENDITION
- 2. STRUCTURAL STEEL SHALL BE NEW STEEL CONFORMING TO THE FOLLOWING: a. WF SHAPES, WT SHAPES, PLATES, BARS, ANGLES ASTM A992

PRIOR TO SAWCUTTING CONCRETE REPAIRS OR DEMO OF CONCRETE.

- b. HIGH STRENGTH BOLTS ASTM A325, UNO c. WELDING ELECTRODE
- 3. FIELD CUTTING OF STEEL OR ANY FIELD MODIFICATIONS OF STEEL SHALL NOT BE MADE WITHOUT PROPER
- WRITTEN APPROVAL BY THE ENGINEER FOR EACH SPECIFIC CASE. 4. ALL SHOP AND FIELD WELDS SHALL BE MADE BY CERTIFIED WELDERS, AND SHALL CONFORM TO
- "STRUCTURAL WELDING CODE STEEL" (AWS D1.1). 5. BOLTED FIELD CONNECTIONS SHALL BE MADE 3/4" DIAMETER 1325 BOLTS, MINIMUM UNLESS NOTED
- 6. STRUCTURAL STEEL FRAMING SHALL BE WITHIN TOLERANCE BEFORE CONNECTIONS ARE FINALLY BOLTED OR WELDED IN ACCORDANCE WITH AISC 360-10.

### MASONRY BRICK

- 1. ALL MASONRY BRICK WORK SHALL CONFORM TO THE "BUILDING CODE REQUIREMENTS OF MASONRY STRUCTURES" (ACI 530-11/ASCE 5-11) AND (ACI 530.1-11/ASCE 6-11).
  - VENEER BRICK ASTM C216, GRADE SW MORTAR (ABOVE GRADE) ASTM C270, TYPE N OR S
- 2. MORTAR COLOR TO MATCH EXISTING COLOR AS CLOSE AS ACHIEVABLE.
- MATCH EXISTING SHAPE AND SIZE.
- 4. ALL GROUT SHALL BE FINE GROUT CONTAINING SAND, PORTLAND CEMENT, AND LIME (OPTIONAL) FOR GROUT SPACES LESS THAN 2 INCHES IN ANY HORIZONTAL DIRECTION, UNLESS SPECIFIED OTHERWISE

3. BRICK THAT NEEDS TO BE REPLACED SHALL BE GRADE "N" IN ACCORDANCE WITH ASTM C55. COLOR TO

- 5. GROUT SHALL ATTAIN A MINIMUM OF 28 DAYS COMPRESSIVE STRENGTH OF 2500 PSI TESTED ACCORDING TO
- 6. THE FOLLOWING STEPS ARE TO BE FOLLOWED WHEN LAYING MASONRY IN THE TEMPERATURES STATED BELOW:
- a. 40°-30° F, MEAN DAILY AIR TEMPERATURE HEAT MIXING WATER OR AGGREGATE TO 70° F. PROTECT MASONRY FROM RAIN OR SNOW FOR 24 HOURS.
- b. 2°-20° F, MEAN DAILY AIR TEMPERATURE HEAT MIXING WATER AND AGGREGATE TO 70° F. PROVIDE WIND BREAKS FOR WIND VELOCITY IN EXCESS OF 15 M.P.H. COVER MASONRY WITH INSULATING BLANKETS FOR 24 HOURS AND PROVIDE HEAT SOURCES ON BOTH SIDES OF MASONRY
- c. BELOW 20° F, MEAN DAILY AIR TEMPERATURE HEAT MIXING WATER AND AGGREGATE TO 70° F PROVIDE ENCLOSURES AND HEAT TO MAINTAIN 40° F MINIMUM WHEN LAID. MAINTAIN MASONRY ABOVE 40° FOR 24 HOURS BY ENCLOSURES AND SUPPLEMENTAL HEAT.

- THE SCOPE OF THIS PROJECT INVOLVES SELECTIVE DEMOLITION AND REPAIR OF STRUCTURAL ELEMENTS THAT WILL REQUIRE TEMPORARY SHORING OF EXISTING AND NEW CONSTRUCTION TO REMAIN. CONTRACTOR SHALL GENERATE A CONSTRUCTION/SHORING PROGRAM AND SUBMIT TO ENGINEER FOR RECORD TWO (2) WEEKS PRIOR TO THE COMMENCEMENT OF WORK.
- CONTRACTOR SHALL PROVIDE ALL SHORING, BRACING, SHEETING, ETC. REQUIRED FOR SAFETY AND PROPER EXECUTION OF THE WORK.
- 3. CONTRACTOR IS SOLELY RESPONSIBLE TO PREPARE SHOP DRAWINGS FOR BRACING ENSURING MEMBERS DESIGNED AND STAMPED/SEALED BY A REGISTERED PROFESSIONAL ENGINEER (REGISTERED IN THE STATE OF NEW HAMPSHIRE) AND SUBMIT THEM TO THE ENGINEER FOR RECORD.

### PHASING INSTRUCTION AND NOTES

- a. CONSTRUCTION PHASING, SEQUENCING AND TRAFFIC MAINTENANCE WORK SEQUENCE SHALL BE COORDINATED WITH THE OWNERS REPRESENTATIVE AND IDENTIFIED PROJECT REPRESENTATIVES. OWNERS WILL CONTINUE TO USE STRUCTURE DURING RESTORATION. CONTRACTOR MUST PHASE AND ARRANGE WORK SO AS TO MAINTAIN ACCESS AT ALL ITEMS TO AREAS THAT ARE NOT UNDER CONSTRUCTION FOR
- b. PROTECTION ZONE: THIS IS AN AREA THAT IS TAKEN OUT OF SERVICE AND ISOLATED FROM THE PLAZA'S NORMAL PEDESTRIAN AND VEHICULAR CIRCULATION TO PROTECT THE PATRONS FROM HAZARDS RESULTING FROM WORK BEING PERFORMED. WORK THAT REQUIRES PROTECTION ZONES SHALL INCLUDE CONCRETE

BOTH VEHICLES AND PEDESTRIANS. SEE PHASING SHEET FOR ADDITIONAL INFORMATION.

- c. WORK ZONE: THIS IS AN AREA THAT IS CAPTURED FROM THE PURPOSE OF PERFORMING PROJECT RELATED ITEMS. THE AREA SHALL BE BARRICADED TO PREVENT PATRONS/VEHICLES FROM ENTERING/PARKING WHILE CONTRACTORS IN CONTROL OF THE AREA.
- d. WORK PHASE AREA: THIS INCLUDES ALL AREAS AFFECTED BY A PARTICULAR PHASE INCLUDING THE WORK ZONE, PROTECTION ZONE, AND THEIR USE FOR TEMPORARY PEDESTRIAN AND VEHICULAR CIRCULATION.
- PROTECTION PARTITION/BARRICADES: THIS DESCRIBES THE BARRICADES AND PROTECTION ENCLOSURES THAT WILL BE INSTALLED AROUND THE WORK AND PROTECTION ZONES TO KEEP GARAGE PATRONS AND THEIR VEHICLES OUT OF THOSE RESPECTIVE AREAS. IN ADDITION THESE ELEMENTS MUST KEEP DUES AND OTHER CONSTRUCTION RELATED DEBRIS FROM MIGRATING IN THE OCCUPIED AREAS OF THE GARAGE. PHASING INSTRUCTIONS AND NOTES.
- a. PHASING SHALL BE SCHEDULED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER/OWNER.
- b. THE CONTRACTOR SHALL PROVIDE UNOBSTRUCTED PEDESTRIAN ACCESS (PROTECTED WHEN NECESSARY) TO ALL EMERGENCY EGRESS STAIRWAYS AT ALL ITEMS. THE CONTRACTOR SHALL INSTALL AND MAINTAIN SIGNAGE THAT CLEARLY DIRECTS PATRONS TO AND FROM THESE DESTINATIONS. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY LIGHTING FOR THESE PATHS IN THE EVENT THE EXISTING LIGHTING IS BLOCKED OR REDUCED BY PROJECT RELATED WORK.
- c. TEMPORARY SIGNAGE SHALL BE INSTALLED AND ADJUSTED BY THE CONTRACTOR DURING THE WORK/PROTECTION ZONE CAPTURE EFFORT. NOTES THAT NEW AREAS CANNOT BE CAPTURED PRIOR TO WORK ZONES OR PORTIONS THEREOF BEING RETURNED TO SERVICE. ALLOW 24 HOURS BETWEEN RETURNING ZONES TO SERVICE.
- d. THE SCOPE OF THIS PROJECT INVOLVES A SELECTIVE DEMOLITION AND REPAIR OF STRUCTURAL BUILDING ELEMENTS THAT WILL REQUIRE TEMPORARY SHORING OF EXISTING AND NEW CONSTRUCTION TO REMAIN. CONTRACTOR SHALL GENERATE A CONSTRUCTION/SHORING PROGRAM AND SUBMIT TO ENGINEER FOR RECORD TWO (2) WEEKS PRIOR TO THE COMMENCEMENT OF WORK.
- e. CONTRACTOR SHALL PROVIDE ALL SHORING, BRACING, SHEETING, ETC. REQUIRED FOR SAFETY AND PROPER EXECUTION OF THE WORK.
- f. CONTRACTOR IS SOLELY RESPONSIBLE FOR THE DESIGN AND INSTALLATION OF SHORING. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING CALCULATIONS AND SHORING SHOP DRAWINGS STAMPED AND SEALED BUY A REGISTERED PROFESSIONAL ENGINEER IN THE STATE OF NEW HAMPSHIRE FOR RECORD BEFORE COMMENCING AND DEMOLITION WORK.
- A MAXIMUM OF 30% OF THE PARKING STALLS MAY BE OUT OF SERVICE AT ONE TIME WHILE THE GARAGE IS OPERATING FOR CONCRETE REPAIRS AND TRAFFIC TOPPING INSTALLATION. THE NUMBER OF SPACES WILL BE COORDINATED WITH THE OWNER/GARAGE OPERATOR.
- h. THE CONTRACTOR SHALL PROVIDE UNOBSTRUCTED PEDESTRIAN ACCESS (PROTECTED WHEN NECESSARY) TO ALL EMERGENCY EGRESS STAIRS AS WELL AS THE MAIN STAIR ELEVATOR TOWER AT ALL TIMES. AT NO TIME SHALL MULTIPLE STAIRS BE TAKEN OUT OF SERVICE AT A FACILITY TO PERFORM REPAIRS. THE CONTRACTOR SHALL INSTALL AND MAINTAIN SIGNAGE THAT CLEARLY DIRECTS PATRONS TO AND FROM THESE DESTINATIONS. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY LIGHTING FOR THESE PATHS IN THE EVENT THE GARAGE LIGHTING IS BLOCKED OR REDUCED BY PROJECT RELATED WORK.
- i. IT SHALL BE THE OWNER OR HIS AGENT'S RESPONSIBILITY TO CLEAR WORK PHASE AREA OF VEHICLES. THE CONTRACTOR SHALL WORK IN UNISON WITH THE CLEARING OPERATION TO CAPTURE THE AREAS BY INCREMENTALLY INSTALLING BARRICADES TO PREVENT VEHICLES FROM REENTERING THE AREA AS IT IS CLEARED. IN ADDITION, TEMPORARY SIGNAGE SHALL BE INSTALLED AND ADJUSTED BY THE CONTRACTOR DURING THE WORK/PROTECTION ZONE CAPTURE EFFORT. NOTE THAT NEW AREAS CANNOT BE CAPTURED PRIOR TO WORK ZONES OR PORTIONS THEREOF BEING RETURNED TO SERVICE SO THAT THE MAXIMUM NUMBER OF SPACES OUT OF SERVICE SHALL NOT BE EXCEEDED. ALLOW 24 HOURS BETWEEN THE TURNING SPACES TO SERVICE AN OCCUPYING EQUAL NUMBER OF SPACES FOR THE NEXT PHASE OF WORK.
- BARRICADES SHALL BE OF SUFFICIENT CONSTRUCTION TO PREVENT INADVERTENT ACCESS BY PATRON VEHICLES AND PEDESTRIANS. THE BARRICADES CAN BE INCORPORATED INTO THE WORK ZONE PROTECTION THAT ARE INTENDED TO PREVENT THE ESCAPE OF DUST AND OTHER DEBRIS FROM THE WORK ZONE.
- k. THE FOLLOWING PARAMETERS SHALL BE CONSIDERED WHEN DEVELOPING PHASING PLANS. PEDESTRIAN PATH WIDTH (MINIMUM): 6'-0"
  - PEDESTRIAN PATH HEADROOM (MINIMUM): 7'-0" TEMPORARY VEHICLE ONE-WAY TRAVEL LANE (MINIMUM): 12'-0" \*

TEMPORARY VEHICLE LANE HEADROOM HEIGHT: AS POSTED AT GARAGE ENTRANCE

I. CONTRACTOR SHALL BE RESPONSIBLE TO LOCKOUT STAIR ACCESS INTO WORK, WORK PHASE, AND AREAS UNDER CONSTRUCTION TO PREVENT UNAUTHORIZED PATRON ACCESS. DOOR SHALL BE SEALED OFF TO PREVENT AIRBORNE DUST FROM ENTERING INTO STAIRWAYS AND ELEVATOR SHAFTS.

### **ABBREVIATIONS**

1.	APPROX	=	APPROXIMATELY
2.	AGG	=	AGGREGATE
3.	BM	=	BEAM
4.	BOT	=	BOTTOM
5.	CIP	=	CAST IN PLACE
6.	CJ	=	CONSTRUCTION JOINT/CONTROL JOINT
7.	CLR	=	CLEARANCE
8.	COL	=	COLUMN
9.	CONC	=	CONCRETE
10.	DET	=	DETAIL
11.	EA	=	EACH
12.	E.E.	=	EACH END
13.	E.S.	=	EACH SIDE
14.	EMBED	=	EMBEDMENT LENGTH
15.	EJ	=	EXPANSION JOINT
16.	EXIST	=	EXISTING
17.	FIN	=	FINISHED
18.	FL	=	FLOOR
19.	IN	=	INCHES

= LINEAR FOOT = LUMP SUM 22. LS = MAXIMUM 23. MAX = MINIMUM 24. MIN = NOT APPLICABLE 26. OC = ON CENTER

= OPPOSITE HAND = PRECAST = POST-TENSIONING 29 P-T = REINFORCEMENT 31. REQ'D = REQUIRED= SQUARE FOOT 32. SF = SIMILAR

41. WI = WORK ITEM

42. WWR = WELDED WIRE REINF

= SLAB ON GROUND 34. SOG 35. SPEC = SPECIFICATION 36. SUPT = SUPPORTED 37 T = TOP38. TAR = TYPICAL AS REQUIRED 39. TYP = TYPICAL 40. UN OR UNO= UNLESS NOTED OTHERWISE

### CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

- 1. ACCESS TO THE GARAGE MUST BE MAINTAINED UNDER CONSTRUCTION PHASING THAT COULD OCCUR OVER, SINGLE OR MULTIPLE CONSTRUCTION YEARS DEPENDENT UPON TIMING OF CONSTRUCTION INITIATION AND PHASING REQUIREMENTS. FURTHER, THE CONSTRUCTION PROCESSES MUST MEET COMPLIANCE WITH SAFETY
- POLICES AS PROVIDED FOR THIS PROJECT. REFER TO OWNER'S BID INSTRUCTIONS FOR MORE INFORMATION. 2. THE GENERAL CONTRACTORS SHALL PROVIDE A CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP) FOR THE PROJECT THAT WILL ADDRESS THE FOLLOWING KEY ISSUES AND PROCESSES THAT ARE AN INTEGRAL
  - PART OF THE CONSTRUCTION PHASING PROVIDED FOR THIS PROJECT. THE CTMP SHALL BE SUBMITTED FOR OWNER AND ENGINEER APPROVAL AND ADDRESS THE FOLLOWING: SAFETY AND AMENITY OF ROAD USERS AND THE PUBLIC;
- CONSTRUCTION SITE SECURITY; PROJECT INFORMATIONAL AND DIRECTIONAL SIGNAGE; TRAFFIC CONTROL SIGNAGE AND DEVICES;
- CONSTRUCTION PHASE SEQUENCE CHANGE OVERS AND
- MAINTENANCE OF ROADWAYS AND WALKWAYS DURING CONSTRUCTION:
- TRAFFIC AND SAFETY MANAGEMENT RESPONSIBILITIES; IMPACT OF CONSTRUCTION ON GARAGE ENTRY/EXIT, CIRCULATION,
- AND TRAFFIC FLOW DURING CONSTRUCTION PHASING: EMERGENCY AND INCIDENT RESPONSE PLAN DURING CONSTRUCTION PROJECT.

### STRATEGIES FOR THE PROJECT

1. THE PARKING AREA AND SERVICE LANES FOR ACCESS TO PATRONS AND PEDESTRIANS WILL BE AFFECTED BY CONSTRUCTION OF THE PROJECT AND THEY WILL VARY GREATLY DURING CONSTRUCTION PHASING. HOWEVER, THE REQUIREMENT REMAINS THE SAME AND IMPACTS IN BOTH CASES MUST BE KEPT TO A

MINIMUM. THEREFORE, ANY MODIFICATIONS OR CHANGES TO THE PROPOSED PHASING MUST INCLUDE:

- A SCHEMATIC DESIGN THAT WORKS TO ENSURE THE SERVICE LANE EXISTS AT ALL TIMES AND MINIMIZES CLOSURES IN BOTH NUMBER AND DURATION.
- WORK SEQUENCES THAT COMPLIES WITH ALL APPLICABLE POLICIES BY LOCAL AUTHORITIES. **EXCLUSIONS FOR WORK DURING HOLIDAYS AND SUNDAYS:**
- SCHEDULING THE WORK TO MINIMIZE TEMPORARY TRANSITIONS AND TIE-INS TO EXISTING WORK. ENSURE TEMPORARY/EMERGENCY PROVISIONS ARE AVAILABLE AND PROVIDED WHEN NEEDED TO FACILITATE ALTERNATE FOOT PATHS

AND INGRESS AND EGRESS PATHS ARE MAINTAINED.

### TRAFFIC MANAGEMENT OBJECTIVES

- 1. THE PROJECT OBJECTIVES HAVE BEEN DEVELOPED TO ALIGN WITH THE SCOPE OF WORK PLANNED FOR EACH PROJECT PHASE. IN SUMMARY, KEY REQUIREMENTS TO BE ADOPTED BY THE CONTRACTOR ARE: KEEP TRAFFIC DELAYS TO A MINIMUM;
  - MINIMIZE DISRUPTION TO THE PATRONS: COMPLY WITH POLICIES (REFER TO SPECIFICATIONS) AND STATUTORY AUTHOR REQUIREMENTS; MAINTAIN SATISFACTORY BUILDING ACCESS;
  - PROVIDE INTERIM MEASURES DURING CONSTRUCTION TO PREVENT MOISTURE PENETRATION FROM **ENTERING AREAS BELOW;** MINIMIZE DISTURBANCE TO THE ENVIRONMENT (AIRBORNE CONTAMINANTS, ODORS, NOISE, ETC.); ENSURE TRAFFIC IMPACTS ON ROAD USERS ARE KEPT TO A MINIMUM AND MEET PLANNING APPROVAL; ENSURE SAFETY FOR GENERAL PUBLIC PEDESTRIANS, DELIVERIES, AND TRAFFIC WITH THE PROJECT
- DESIGN CONSTRUCTION SCHEDULE AND WORK PHASING TO ACCOMPLISH THE RESTORATION PROJECT WHILE THE GARAGE SERVICE LANE REMAINS IN LIMITED BUT CONTINUOUS SERVICE. PROJECT / CONSTRUCTION / ENGINEERING MANAGERS THE PROJECT MANAGERS WILL BE RESPONSIBLE FOR ENSURING PEDESTRIAN ACCESS AND TRAFFIC MANAGEMENT:
- IS PROPERLY PLANNED, ORGANIZED, DIRECTED, AND CONTROLLED IS PROPERLY RESOURCE WITH PEOPLE, EQUIPMENT, FACILITIES AND SYSTEMS; COMPLIES WITH FIRE DEPARTMENT RESPONSE PROTOCOLS FOR ACCESS IN THE PROJECT. CONDUCT REGULAR INSPECTIONS WHERE NECESSARY, INSTRUCT THE RECTIFICATION OF DEFICIENCIES IS ACHIEVING ALL PROJECT OBJECTIVES.
- 3. PROJECT SUPERINTENDENT
  - COORDINATE THE FIELD RESOURCES DURING CONSTRUCTION SEQUENCE PHASING AND TRANSITIONAL PHASING DURING THE COURSE OF THE PROJECTS; ASSISTS WITH THE IMPLEMENTATION OF THE CTMP; PROVIDE DIRECTIONS AND SUPPORT TO ENABLE EFFECTIVE PLANNING OF TEMPORARY TRAFFIC MANAGEMENT ARRANGEMENTS FOR ENTRY AND EXIT FROM THE PARKING AREA: ENSURES THE FIELD TEAM MEMBERS RECEIVE APPROPRIATE TRAINING: ENSURES THAT CONTROL MEASURES ARE MAINTAINED AND THE WORK-IN-PROGRESS IS INSPECTED;
- ENSURES SUBCONTRACTORS/SUPPLIERS HAVE SUITABLE QUALIFICATIONS AND EXPERIENCE. a. THE SUPERINTENDENT (OR DELEGATED INDIVIDUAL IN THEIR ABSENCE) WILL BE CONTRACTIBLE AT ALL TIMES (7 DAYS A WEEK) DURING THE CONSTRUCTION PHASE OF THE WORK. SITE EMERGENCY CONTACT LIST SHALL BE PROVIDED ON THE CONTRACTORS EMERGENCY RESPONSE PLAN AND MUST BE DISPLAYED ON SITE AT THE CONSTRUCTION OFFICE OR JOBSITE TRAILER.
- b. THE SUPERINTENDENT WILL HAVE THE AUTHORITY TO STOP WORK ON ANY ACTIVITY IF IS CONSIDERED TO BE NECESSARY TO PREVENT INCIDENTS, OR THE COMPLY WITH LOCAL AUTHORITIES.

### PUBLIC SAFETY PROVISIONS FOR ROADWAY AND FOOT TRAFFIC

- 1. THE CONTRACTOR MUST ACCOMMODATE AND MAINTAIN TRAFFIC FLOW OF PATRONS, VISITORS AND FLOOR TRAFFIC AT AN ACCEPTABLE LEVEL OF SERVICE AND CONVENIENCE. FOR EACH PHASE OF THE PROJECT, SAFETY WILL BE CONSIDERED BASED UPON THE SPECIFIES OF THE WORK. AS PART OF THE CONSTRUCTION PHASING SEQUENCING PLAN THE CONTRACTOR SHALL MAKE PROVISIONS FOR SEPARATION BETWEEN THE PUBLIC AND THE WORK. SEPARATION SHALL CONSIST OF A MINIMUM; MOVABLE LANE DELINEATORS OR TRAFFIC BARRIERS WITH CONSTRICTION PARTITION FENCING WITH SCREENING, TRAFFIC CONTROLS, STAGING WORK, AND APPROVED DIRECTIONAL AND INFORMATIONAL SIGNAGE AS NECESSARY TO PROVIDE SUFFICIENT
- SEPARATION AND ROUTING TO PROTECT THE USERS ENTERING THE GARAGE. 2. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING A "RED" BOOK AT THE SITE OFFICE. THE FOLLOWING REGULATIONS ARE PERMITS SHALL BE FOLLOWED AND MAINTAINED BY CONTRACTORS DOCUMENTATION CONTROL STATION ON SITE DURING CONSTRUCTION:
- a. NFPA 241 b. PERMITS AND DAILY CHECKLIST c. FIRE WATCH DOCUMENTATION

HAZARDOUS CONDITIONS ARE PRESENT

### MISCELLANEOUS NOTES

- 1. OWNER AND THE ENGINEER RESERVE THE RIGHT TO ISSUE A STOP WORK ORDER FOR SPECIFIC AREAS / WORK ITEMS IF IN THE OPINION OF EITHER ONGOING WORK DOES NOT MEET THE PHASING PARAMETERS OF THE PROJECTS OR PRESENTS HAZARD TO THE PATRONS OF THE GARAGE. THE CONTRACTOR SHALL NOT BE ENTITLED TO ADDITIONAL COMPENSATION. WORK ZONES SHALL NOT BE RETURNED TO SERVICE WHEN
- 2. THE PHASING CONCEPTS CONTAINED IN THESE DOCUMENTS ARE INTENDED TO INDICATE REASONABLE SCENARIOS FOR PHASING THE WORK. NOT ALL INFORMATION REQUIRED FOR CONTRACTOR PHASING SUBMITTALS ARE INCLUDED IN THESE CONCEPTS AND THEY DO NOT INCLUDE ALL CHALLENGES THAT WILL BE PRESENT IN PERFORMING THIS PROJECT IN A PHASED MANNER.
- 3. WORK ZONES SHALL NOT BE RETURNED TO SERVICE IF HAZARDOUS CONDITIONS ARE PRESENT,

<b></b>			WORK IIE	M SCHEDULE		
WORK ITEM	M	DESCRIPTION	UNITS	2021 WORK QUANTITY (THIRD LEVEL)	2022 WORK QUANTITIY (GROUND AND SECOND LEVELS)	2023 WORK QUANTITY (FOURTH AND FIFTH LEVELS) (ST#4)
1.0		GENERAL REQUIREMENTS				, , ,
	1.1	Project Mobilization	L.S.	1	1	1
		Concrete Formwork			ork Item Series 3.0, 4.0, 5.0, 6.0 8	
ļ		Concrete Shores and Reshores Concrete Reinforcement			ork Item Series 3.0, 4.0, 5.0, 6.0 & ork Item Series 3.0, 4.0, 5.0, 6.0 &	
		Overhead Protection/Temporary Signage/Traffic Control			ntal to Work Item Series 1.0	<i>t 1</i> .0
3.0	1.0	CONCRETE FLOOR REPAIR			That to Tronk item concerns	
		Floor Repair - Partial Depth / Shallow at Construction Joint	S.F.	800	5000	400
	3.3	Floor Repair - Full Depth at Construction Joint	S.F.	600	2000	100
		Floor Repair - Curbs / Walks	S.F.	0	450	25
<u> </u>		Floor Repair - Partial Depth Topping Strip Removal/Replacement Floor Repair - Concrete Wash	S.F. S.F.	13,000 175	9,300 175	0 200
<b></b>		Floor Repair - Scaled Concrete Surface	S.F.	50	50	350
<b>—</b>		Floor Repair - Stair Nosing	EA.	5	5	13
	3.11	Floor Repair - Stair Landing Partial Depth	S.F.	20	20	140
	3.12	Floor Repair - Remove / Replace ADA Curb Cut	EA.	1	3	0
4.0		CONCRETE CEILING REPAIR		700		150
<b></b>		Ceiling Repair - Partial Depth/Shallow	S.F. S.F.	700	900 700	150 75
6.0	4.2	Ceiling Repair - Partial Depth/Deep COLUMN REPAIR	S.F.	440	700	/5
0.0	6.6	Column Repair - Pier Base	EA.	8	0	0
		Column Repair - Pilaster	S.F.	250	0	0
7.0		CONCRETE WALL REPAIR			-	
	7.1	Wall Repair - Partial Depth / Shallow	S.F.	0	30	0
9.0		EXPANSION JOINT PREPARATION				
100	9.2	Expansion Joint Preparation - Blockout Repair	L.F.	25	25	25
10.0	10.2	EXPANSION JOINT REPAIR AND REPLACEMENT  Expansion Joint - Elastomeric Concrete Edged	L.F.	230	125	170
<del> </del>		Expansion Joint - Elastoment Concrete Edged  Expansion Joint - Adhered	L.F.	90	60	220
11.0	10.0	CRACK AND JOINT REPAIR	L.I .	30	00	220
	11.1	Remove and Replace Control Joint Sealant 1/2" Wide	L.F.	6500	3800	10800
		Replace / Install Crack/Joint Sealant	L.F.	7500	7500	7500
		Tool and Seal Patch Perimeters and Control Joints			cidental to WI Series 3.0	
<u> </u>		Remove and Replace Control Joint Sealant 1 1/2" Wide	L.F.	900	150 idental to WI Series 16.0	5200
15.0	11.7	Cove Sealant PROTECTIVE SEALER		Inc	idental to WI Series 16.0	
13.0	15 1	Concrete Sealer - Floors	S.F.	18,500	23,000	42,000
16.0	10.1	TRAFFIC TOPPING	0.1 .	10,000	20,000	12,000
		Traffic Topping - Vehicular	S.F.	54,000	50,000	49,000
	16.3	Traffic Topping - Pedestrian Areas (Stairtowers 1, 2, 3)	S.F.	590	460	500
25.0	16.4	Traffic Topping - Recoat Stairtower Landings Stair #4  MECHANICAL - DRAINAGE	S.F.	0	0	650
25.0	25.1	Mechanical - Drainage  Mechanical - Allowance	ALL.	1	1	1
		Mechanical - Allowance Mechanical - Supplemental / Replacement Floor Drain	EA.	11	10	9
		Mechanical - Pipe & Hangers 4" Line (New Supplimentary drains)	L.F.	85	90	5
	25.4	Mechanical - Pipe & Hangers 6" Line	L.F.	95	125	100
	25.5	Mechanical - Pipe & Hangers 8" Line	L.F.	0	62	0
26.0	26.2	MECHANICAL - FIRE PROTECTION	1 -	0	245	0
30.0	20.2	Mechanical - Fire Standpipe Replacement  ELECTRICAL-LIGHTING	L.F.	0	345	0
30.0	30 1	Electrical Allowance	ALL.	1	1	1
37.0		DOORS, FRAMES AND HARDWARE	, ,	·		· ·
	37.1	Door and Frame Replacement	EA.	1	1	2
40.0		CONNECTIONS / BEARINGS				
14.0		Re-weld Shear Connector	EA.	10	10	10
41.0		STEEL STAIRS Replace Stair Riser/Pan/Fill	EA.	20	10	1
<u> </u>		Stair Landing Replacement	S.F.	130	65	60
43.0	r 1.2	MISCELLANEOUS METALS	J.1 .	1.00	55	
	43.1	Install Steel Plate at Building Perimeter Gap	L.F.	450	450	450
		Install Chain Link Fence at Vehicle Barrier	L.F.	460	410	460
45.0		PAINTING				
<b></b>		Paint Traffic Markings	L.S.	1	1	1
<u> </u>		Paint Steel Framing in Stairtower Clean and Paint Structural Steel	EA. L.S.	1	1 1	0
		Touch Uo Paint Concrete / Masonry Surfaces (Stairtower #1)	L.S.	0	1	0
	45.5	Paint Standpipe	L.S.	0	1	0
	45.6	Clean and Paint Galvanized Steel Framing - Expansion Area	L.S.	1	1	1
46.0		STRUCTURAL STEEL		,		
<u> </u>		Structural Allowance - Inspections	ALL.	1	0	0
<u> </u>		Flange Repair Bottom Flange Repair	EA. EA.	2	0	0
<del>                                     </del>		Beam Repair	EA.	8	0	0
		Stiffener and Gusset Plate Replacement	EA.	1	0	0
<b>—</b>	46.1E	Angle Support	EA.	4	0	0
		Column Flange Repair	EA.	1	0	0
		Caluman Danain	EA.	1	0	0
	46.1G	Column Repair		4		_
	46.1G 46.1H	Column Flange Repair	EA.	1	0	0
	46.1G 46.1H	Column Repair Column Flange Repair Supplemental Steel Beam Installation @ Damaged Filigree Plank		1 7	0 0	0

**WORK ITEM SCHEDULE** 

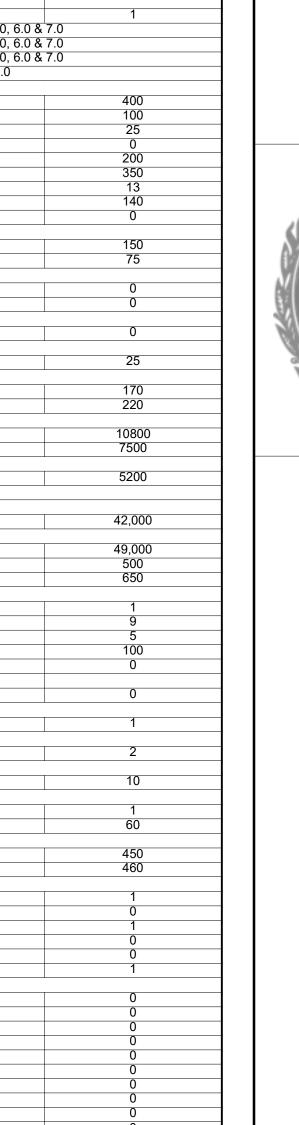
46.2	Supplemental Steel Beam Installation @ Damaged Filigree Plank	EA.	7	0
WORK ITEM	DESCRIPTION	UNITS	2021 WORK QUANTITY	QUANTITY PER ELEVATION
74.0	JOINT AND SEALANT REPAIR			N S E W INT.
74.1	Isolation Joint Sealant Repair	L.F.	925	500 (N) 125 (E) 250 (W) 25 (INT)
	Capstone Joint Repair	L.F.	245	60 (N) 125 (S) 25 (E) 25 (W)
76.0	CRACK REPAIR AND TUCKPOINTING			
	Rout and Seal Façade Cracks	L.F.	40	30 (N) 5 (W)
	Masonry Tuckpointing	S.F.	950	450 (N) 150 (S) 300 (W) 150
	Grout Joint Repair - Cornice	L.S.	3	1 (S) 1 (E) 1 (W)
30.0	BRICK/CONCRETE MASONRY UNIT FAÇADE			
	Remove and Replace Face Brick	EA.	50	10 (N) 5(S) 10 (W) 25 (INT)
	Remove and Replace Rowlock Cap	L.F.	160	20 (N) 100 (S) 40 (W)
	Remove and Replace Concrete Masonry Unit	EA.	50	30 (S) 20 (INT)
	Remove and Replace Capstone/Flashing	L.F.	105	20 (N) 50 (S) 5(W) 30 (INT)
	Remove and Replace Parapet Wall	L.F.	62	62 (S)
	Remove and Replace Parapet Wall w/ Top Rail	L.F.	40	40 (S)
	Reset Precast Unit	EA.	7	6 (S) 1 (E)
91.0	FAÇADE COATING/SEALING			
91.3	Penetration Sealer	L.S.	2	1 (N) 1 (W)
95.0	Doors & Windows			
95.1	Replace Window Frame/Façade Perimeter Sealant	L.F.	350	110(N) 230 (W)

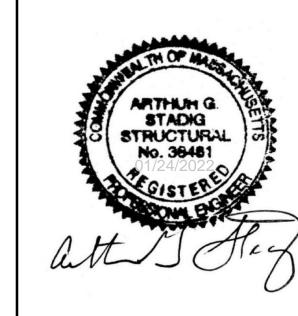
WORK ITEM NUMBER

(SEE SPECIFICATIONS

CLARIFICATION DETAIL NO

WORK ITEM IDENTIFICATION





20 Park Plaza, Suite 1202

Boston MA 02116

617.350.5040 Ph

www.walkerconsultants.com

34 HANOVER STREET. PORTSMOUTH, NH 03801

DESCRIPTION REVISIONS BIDDING AND CONSTRUCTION ISSUE DATE: 1/25/2022 PROJECT NO: 16-003129.00

IN ANY FORM OR BY ANY MEANS WITHOUT PERMISSION FROM WALKER CONSULTANTS. GENERAL NOTES, LEGENDS, & WORK ITEM SCHEDULE

NCH

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DRAWN BY:

CHECKED BY: MAZ

A. WORK ZONE: THIS IS AN AREA THAT IS CAPTURED PER THE PHASING PLAN FOR THE PURPOSE OF PERFORMING PROJECT RELATED WORK ITEMS. THE AREA SHALL BE BARRICADED TO PREVENT GARAGE PATRONS/VEHICLES FROM ENTERING/PARKING WHILE CONTRACTOR IS IN CONTROL OF THE AREA.

B. PROTECTION ZONE: THIS IS AN AREA THAT IS TAKEN OUT OF SERVICE AND ISOLATED FROM THE GARAGES NORMAL PEDESTRIAN AND VEHICLE CIRCULATION TO PROTECT THE PATRONS FROM HAZARDS RESULTING FROM WORK BEING PERFORMED ABOVE.

. THE MAXIMUM NUMBER OF PARKING STALLS OUT OF SERVICE AT ONE SHALL BE LIMITED TO THE AMOUNT PER PHASE AS DICTATED IN THE SHEET NOTES ON R-001. GARAGE SHALL REMAIN OPERATIONAL AT ALL TIMES. . THE CONTRACTOR SHALL PROVIDE UNOBSTRUCTED PEDESTRIAN ACCESS (PROTECTED WHEN NECESSARY) TO ALL EMERGENCY EGRESS STAIRS AS WELL AS THE MAIN STAIR ELEVATOR TOWER AT ALL TIMES. THE CONTRACTOR SHALL INSTALL AND MAINTAIN SIGNAGE THAT CLÉARLY DIRECTS PATRONS TO AND FROM THESE DESTINATIONS. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY LIGHTING FOR THESE PATHS IN THE EVENT THE GARAGE LIGHTING IS BLOCKED OR REDUCED BY PROJECT RELATED WORK.

THE CONTRACTOR SHALL SUBMIT DETAILED PHASING PLANS TO THE OWNER AND ENGINEER FOR EACH PHASE OF WORK. THE PHASING PLANS SHALL BE KEYED INTO A PROJECT SCHEDULE. THE PHASING PLANS SHALL INCLUDE BUT NOT BE LIMITED TO THE FOLLOWING:

a. LIMITS OF WORK ZONE INCLUDING DIMENSIONS TO BARRICADES. b. LIMITS OF PROTECTION ZONE INCLUDING DIMENSIONS TO BARRICADES.

c. TEMPORARY PEDESTRIAN PATHS TO EMERGENCY EGRESS STAIRS AND ELEVATOR. d. TEMPORARY VEHICLE CIRCULATION FOR ALL AREAS AFFECTED BY THE PHASE. e. SIGNAGE LAYOUT FOR TEMPORARY PEDESTRIAN AND VEHICLE CIRCULATION.

f. INCLUDE KEY FOR TEXT TO FACILITATE OWNER REVIEW. g. FLAGMEN POSITIONS (WHEN REQUIRED).

PHASING PLANS SHALL BE SUBMITTED TO THE ENGINEER/OWNER A MINIMUM OF THREE WEEKS BEFORE THE START OF WORK IN A PHASE AREA TO ALLOW SUFFICIENT TIME FOR REVIEW AND CAPTURE OF AREA.

WORK ZONE AND PROTECTION ZONE BARRICADES SHALL BE OF SUFFICIENT CONSTRUCTION TO PREVENT INADVERTENT ACCESS BY PATRON VEHICLES AND

PEDESTRIANS. THE BARRICADES CAN BE INCORPORATED INTO THE WORK ZONE PROTECTION PARTITIONS THAT ARE INTENDED TO PREVENT THE ESCAPE OF DUST AND

OTHER DEBRIS FROM THE WORK ZONE AND PROTECTION ZONE. THE FOLLOWING PARAMETERS SHALL BE CONSIDERED WHEN DEVELOPING PHASING PLANS.

 PEDESTRIAN PATH WIDTH (MINIMUM): 8'-0" PEDESTRIAN PATH HEADROOM (MINÍMUM): 7'-6"

 TEMPORARY VEHICLE ONE - WAY TRAVEL LANE (MINIMUM): 12'-0" \* TEMPORARY VEHICLE TWO - WAY TRAVEL LANE (MINIMUM): 24'-0" \*

TEMPORARY VEHICLE LANE HEADROOM HEIGHT: AS POSTED AT GARAGE ENTRANCE

\*ADD 2'-0" TO THE LANE DIMENSION AT TURNING AREAS.

J. MISCELLANEOUS NOTES

a. OWNER AND THE ENGINEER RESERVE THE RIGHT TO ISSUE A STOP WORK ORDER FOR SPECIFIC AREAS / WORK ITEMS IF IN THE OPINION OF EITHER ONGOING WORK DOES NOT MEET THE PHASING PARAMETERS OF THE PROJECTS OR PRESENTS HAZARD TO THE PATRONS OF THE GARAGE. THE CONTRACTOR SHALL NOT BE

ENTITLED TO ADDITIONAL COMPENSATION.

b. WORK ZONES SHALL NOT BE RETURNED TO SERVICE WHEN HAZARDOUS CONDITIONS ARE PRESENT. c. DO NOT RETURN AREAS TO SERVICE UNLESS EXISTING STRIPING AFFECTED BY WORK HAS BEEN RESTORED OR NEW STRIPING HAS BEEN APPLIED.

d. CONTRACTOR SHALL PROVIDE FULL HEIGHT PARTITIONING WITHIN WORK AND SAFE ZONES PRIOR TO START OF WORK. ALL PARTITIONING, FALSEWORK, SHORING, BARRICADES, SIGNAGE, ENCLOSURES, AND METHODS OF CONTROLLING VEHICLE TRAVEL AND PARKING AROUND THE WORK AND SAFETY ZONE MUST BE SUBMITTED WITH DETAILS TO THE ENGINEER FOR APPROVAL PRIOR TO THE START OF INSTALLATION WORK.

e. ALL PARTITIONS SHALL HAVE SOUND DEADENING MATERIAL TO REDUCE DEMOLITION NOISE OUTPUT OUTSIDE THE STRUCTURE. f. THE ENCLOSED AREAS SHALL BE PROVIDED WITH VENTILATION AND FILTRATION DEVICES TO PREVENT AIRBORNE DEBRIS FROM EXITING THE ENCLOSED AREA(S). THE ENCLOSED AREAS SHALL BE PROVIDED WITH ACCESS DOORWAYS FOR ENTRY AND SECURITY OF THE AREAS UNDER CONTROL BY THE CONTRACTOR.

TRAFFIC CONTROL SIGNAGE SHALL BE PROVIDED AND INSTALLED IN A PLAIN LINE OF SIGHT TO INSTRUCT PATRONS THROUGH AND AROUND THE GARAGE CONSTRUCTION AREAS. i. SUSPENDED CONSTRUCTION FALSE WORK SHALL BE PROVIDED OVER ALL VEHICLE AND PEDESTRIAN ENTRY AND EXIT PORTALS TO ALLOW ACCESS TO THESE

AREA(S) UNTIL CONSTRUCTION WORK IS UNDERTAKEN IN THESE AREAS MUST BE CLOSED. j. CONTRÁCTOR SHALL BE RESPONSIBLE TO PROVIDE TEMPORARY LIGHTING AS NEEDED TO PROVIDE PROPER LIGHTING AROUND WORK AND SAFETY ZONES AND

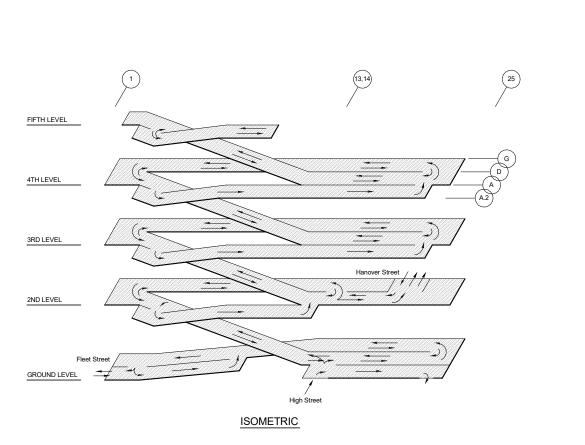
WHERE FALSE WORK IS INSTALLED OVER PATRON ACCESS AREAS NOTED IN NOTE I ABOVE. k. CONTRACTOR SHALL SUPPLY AND USE INDUSTRIAL MISTING FANS WITHIN THE WORK ZONES TO MINIMIZE AIRBORNE DEBRIS DURING CONCRETE DEMOLITION. CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE AIR VENTILATION AND FILTRATION FROM WORK ZONE ENCLOSED AREAS TO CONTROL AIRBORNE DEBRIS FROM

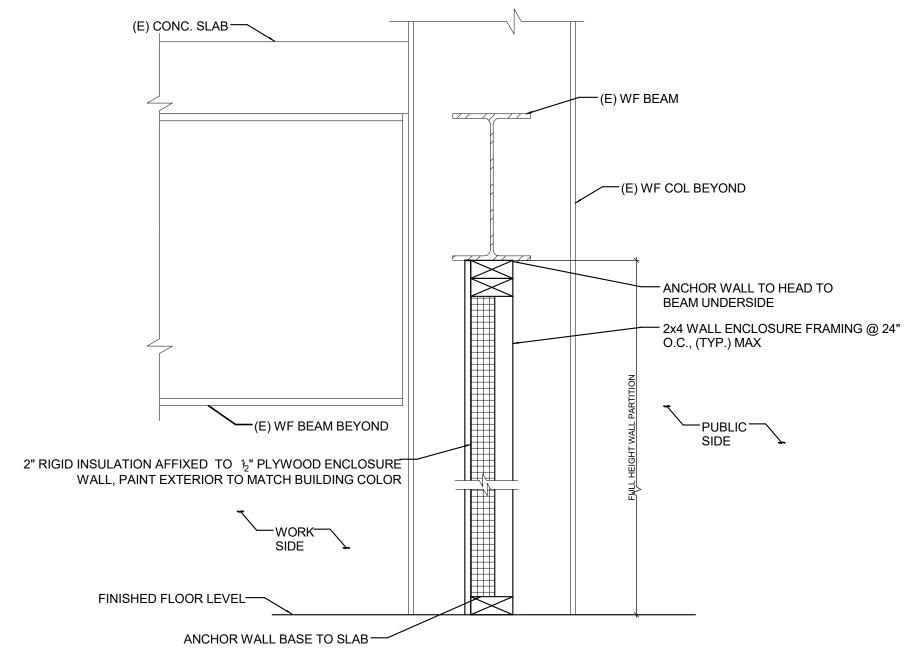
EXHAUSTING OUT OF THE WORK ZONE DURING DEMOLITION. I. CONTRACTOR SHALL TEMPORARILY RELOCATE ALL ADA PARKING SPACES CAPTURED IN WORK OR PROTECTION ZONES SUCH THAT THE TOTAL NUMBER OF ADA

SPACES WITHIN THE GARAGE IS MAINTAINED AT ALL TIMES. m. CONTRACTOR SHALL PROVIDE A CONSTRUCTION SIGNAGE SCHEDULE AND CONSTRUCTION TRAFFIC MANAGEMENT PLAN SHOWING THE LOCATIONS OF TEMPORARY SIGNAGE THAT IS TO BE INSTALLED IN THE GARAGE DURING CONSTRUCTION.

n. CONTRACTOR'S PHASING PLAN FOR WATERPROOFING COATING APPLICATION MAY DIFFER FROM THE CONCEPTUAL PHASING SCHEMES SHOWN AND WOULD NOT REQUIRE THE ENCLOSED PERIMETER SOUND DEADONING WALLS. HOWEVER, CONTRACTOR MUST PROVIDE POSITIVE BARRICADES AND SIGNAGE TO PREVENT PATRONS FROM ENTERING THESE WORK ZONES DURING WATERPROOFING APPLICATION.

		TE	MPORARY CO	NSTRU(	CTION SIG	GN SCHEDULE
SI	ZE	TEXT/MESSAGE	BACKGROUND	LETTERS/SYMBOLS		REMARKS
W	L	TEXT/MESSAGE	COLOR	н	COLOR	REWARRS
2'-0"	3'-0"	CAUTION UNDER CONSTRUCTION WATCH FOR SIGNS	ORANGE	4"	BLACK	
2'-0"	3'-0"	NO PARKING THIS AREA	ORANGE	4"	BLACK	
2'-0"	3'-0"	PARKING	ORANGE	6"	BLACK	MANUFACTURE SIGN WITH A ROTATABLE ARROW THAT CAN BE LOCKED INTO POSITION.
2'-0"	3'-0"	EXIT	ORANGE	6"	BLACK	MANUFACTURE SIGN WITH A ROTATABLE ARROW THAT CAN BE LOCKED INTO POSITION.
2'-0"	3'-0"	STAIR CLOSED USE OTHER STAIR	WHITE	4"	BLACK	
3'-0"	3'-0"	DO NOT ENTER	RED	4"	WHITE	
2'-0"	3'-0"	CAUTION TWO-WAY TRAFFIC AHEAD	ORANGE	6"	BLACK	
2'-0"	3'-0"	LEFT TURN ONLY	ORANGE	6"	BLACK	
2'-0"	3'-0"	RIGHT TURN ONLY	ORANGE	6"	BLACK	
2'-0"	3'-0"	ONE WAY	ORANGE	6"	BLACK	
2'-0"	3'-0"	CONSTRUCTION PERSONNEL	ORANGE	6"	BLACK	





NOTE:

1. WALL SHALL FORM ENTIRE ENCLOSED PERIMETER OF WORK AND PROTECTION ZONES WITH SOUND DEADONING MATERIALS

### TYPICAL CONSTRUCTION BARRIER DETAIL

### PHASING LEGEND



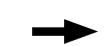
INDICATES AREA CAPTURED FOR WORK OR PROTECTION ZONE



INDICATES SECONDARY PHASE WITHIN WORK OR PROTECTION ZONE



INDICATES OVERHEAD FALSE WORK AREA



INDICATES TEMPORARY CHANGE IN TRAFFIC CIRCULATION AROUND WORK OR PROTECTION ZONE



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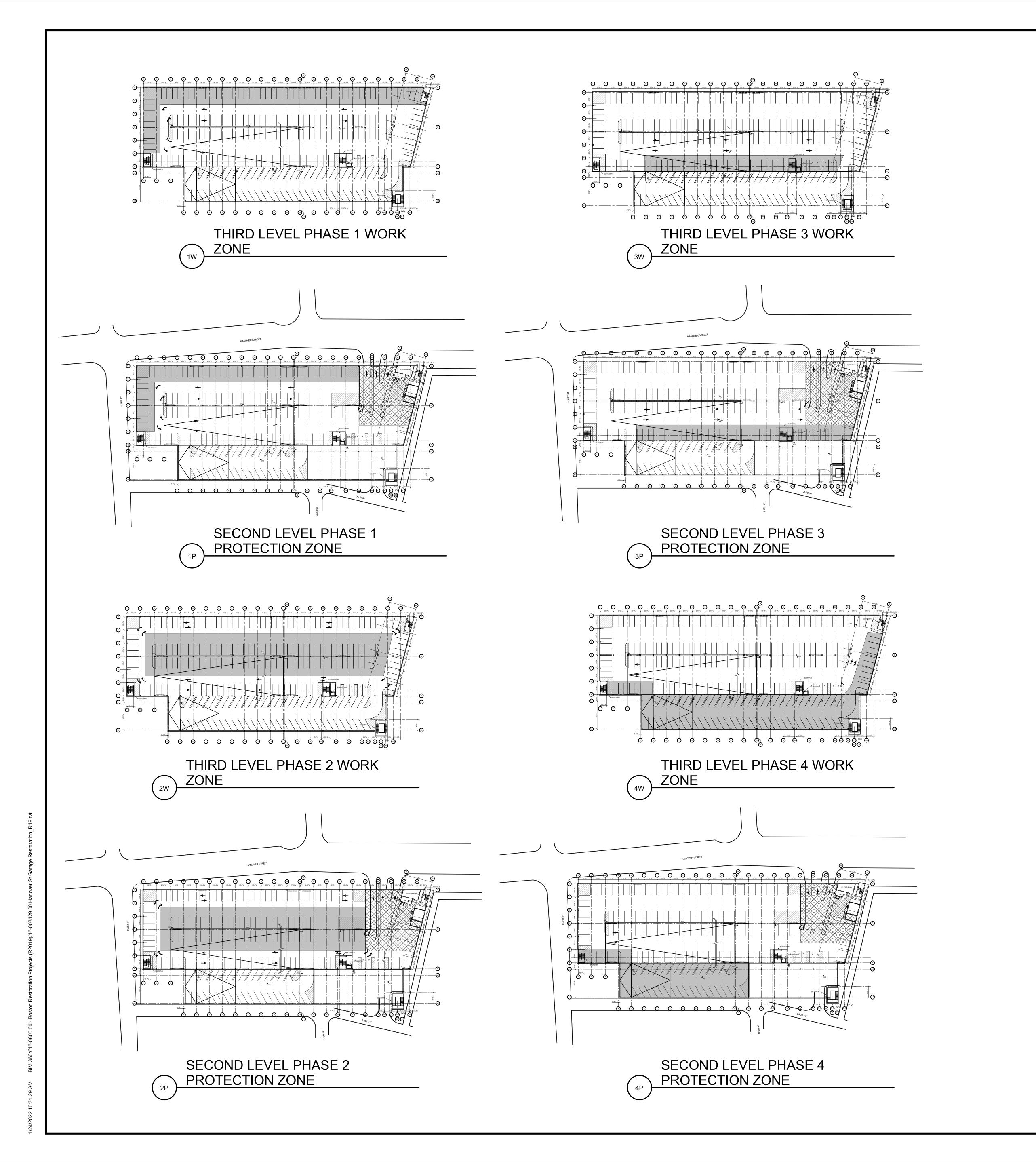
HANOVER STREET **GARAGE RESTORATION** 

> 34 HANOVER STREET. PORTSMOUTH, NH 03801

- 14 - 14		
MARK	DATE	DESCRIPTION
		REVISIONS
ISSUE:		BIDDING AND CONSTRUCTION
ISSUE DATE:		1/25/2022
PROJECT NO:		16-003129.00
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PHASING NOTES AND LEGENDS



NOTE:

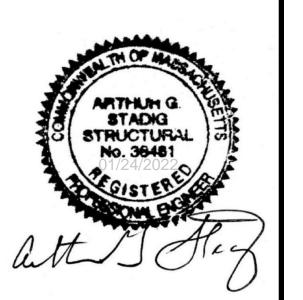
SEE R-002 FOR PHASING NOTES, CONSTRUCTION SIGNAGE SCHEDULE, AND TYPICAL CONSTRUCTION BARRIER DETAIL.



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HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET. PORTSMOUTH, NH 03801

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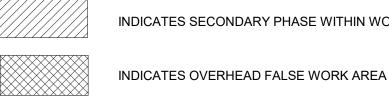
2022 PHASING PLANS

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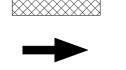
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PHASING LEGEND

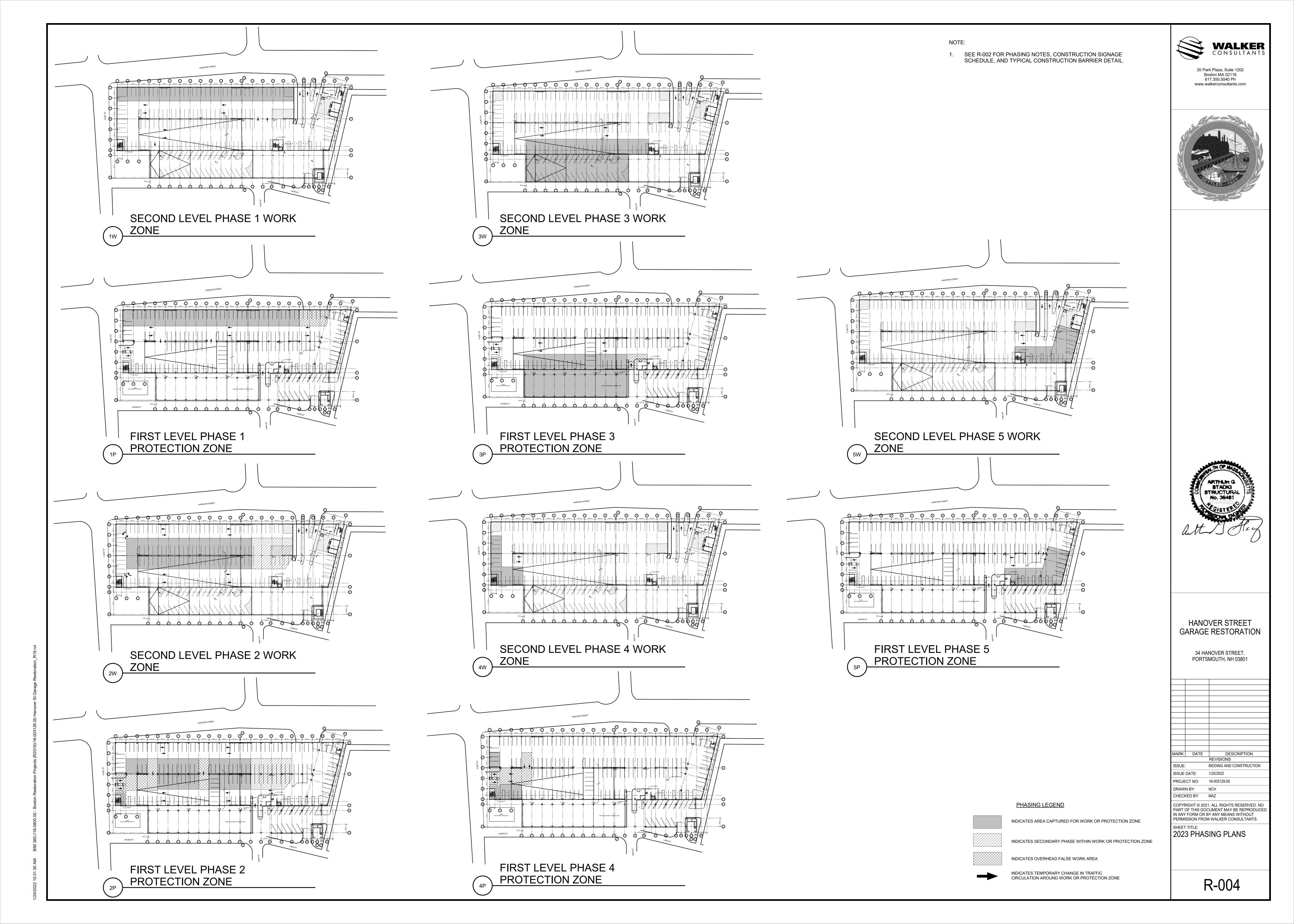
INDICATES AREA CAPTURED FOR WORK OR PROTECTION ZONE

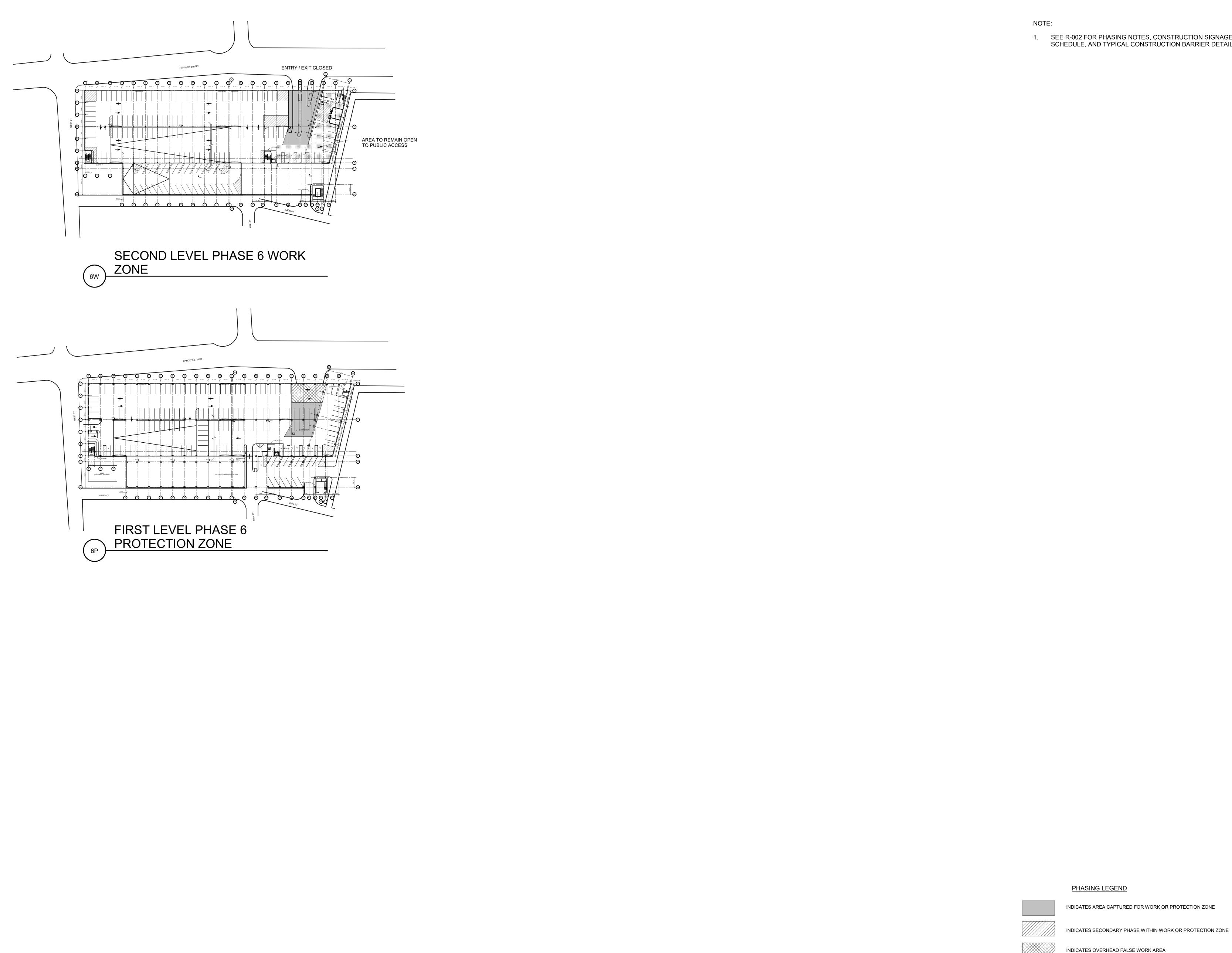


INDICATES SECONDARY PHASE WITHIN WORK OR PROTECTION ZONE



INDICATES TEMPORARY CHANGE IN TRAFFIC CIRCULATION AROUND WORK OR PROTECTION ZONE





1. SEE R-002 FOR PHASING NOTES, CONSTRUCTION SIGNAGE SCHEDULE, AND TYPICAL CONSTRUCTION BARRIER DETAIL.



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HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET. PORTSMOUTH, NH 03801

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2023 PHASING PLANS

INDICATES TEMPORARY CHANGE IN TRAFFIC CIRCULATION AROUND WORK OR PROTECTION ZONE

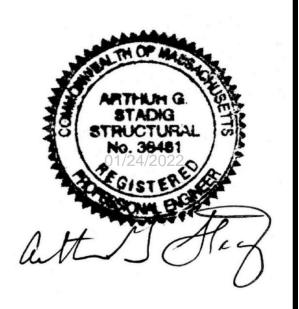
SEE R-002 FOR PHASING NOTES, CONSTRUCTION SIGNAGE SCHEDULE, AND TYPICAL CONSTRUCTION BARRIER DETAIL



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HANOVER STREET **GARAGE RESTORATION** 

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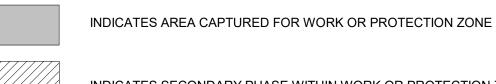
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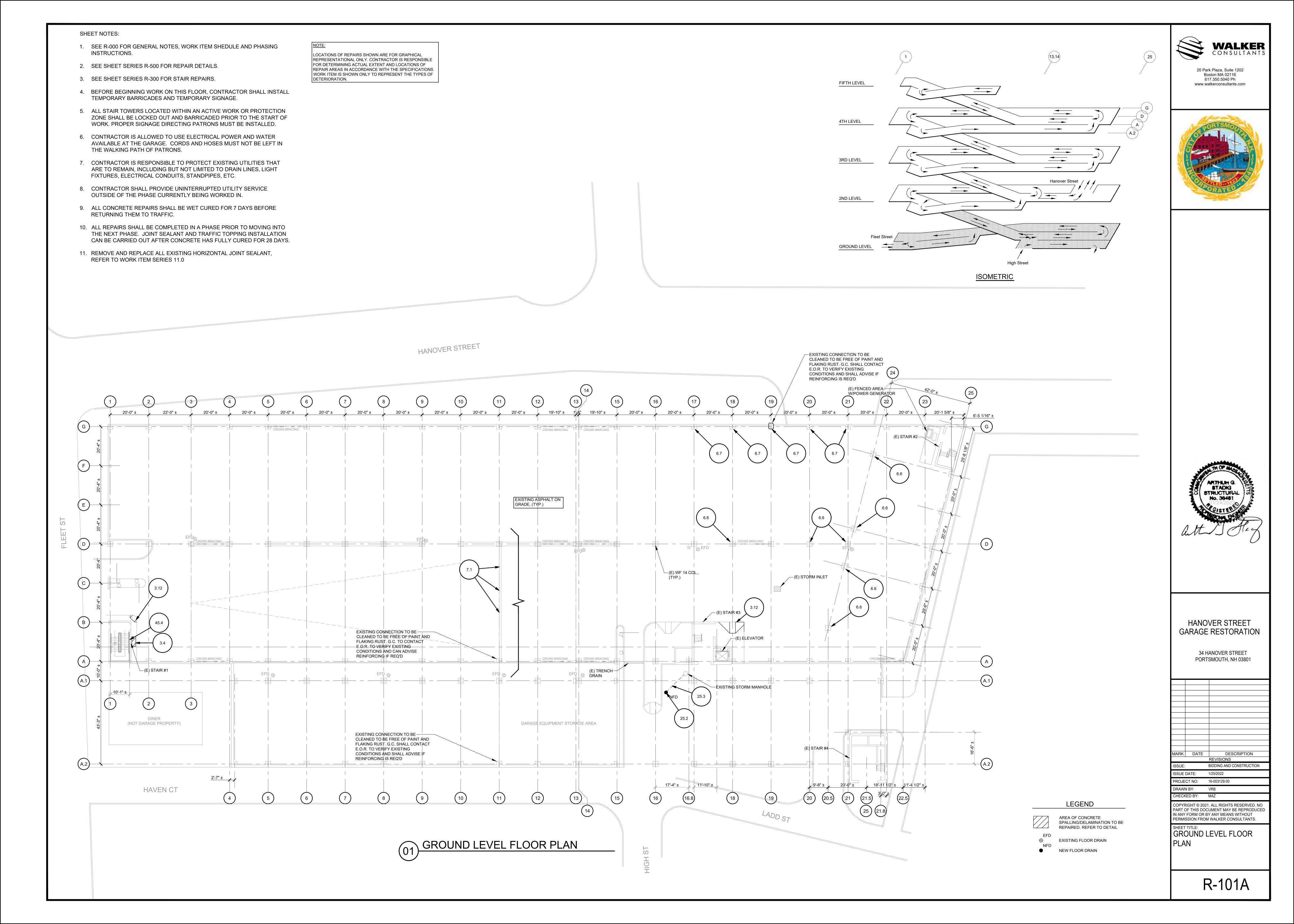
2024 PHASING PLANS

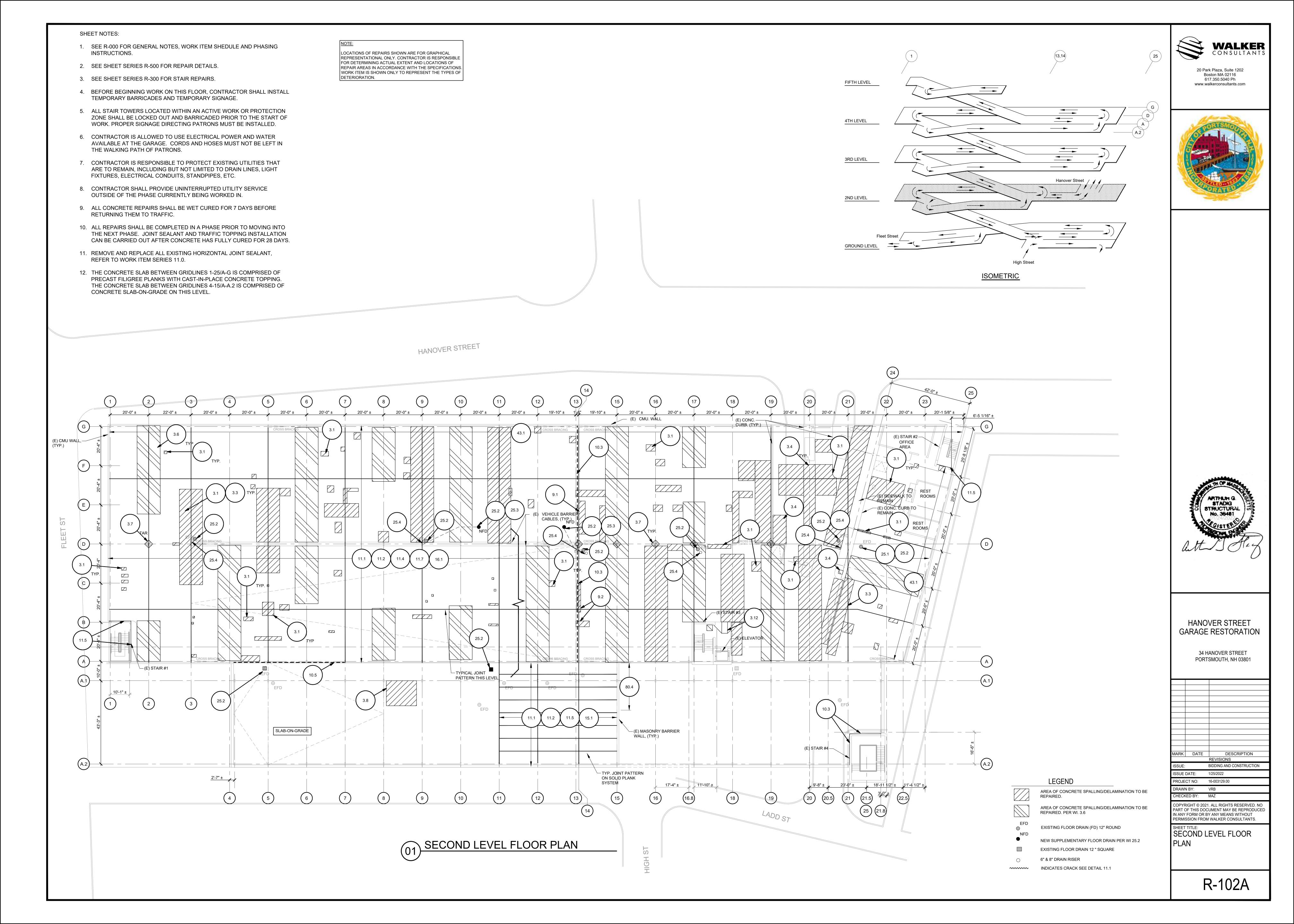
PHASING LEGEND



INDICATES SECONDARY PHASE WITHIN WORK OR PROTECTION ZONE INDICATES OVERHEAD FALSE WORK AREA

INDICATES TEMPORARY CHANGE IN TRAFFIC CIRCULATION AROUND WORK OR PROTECTION ZONE





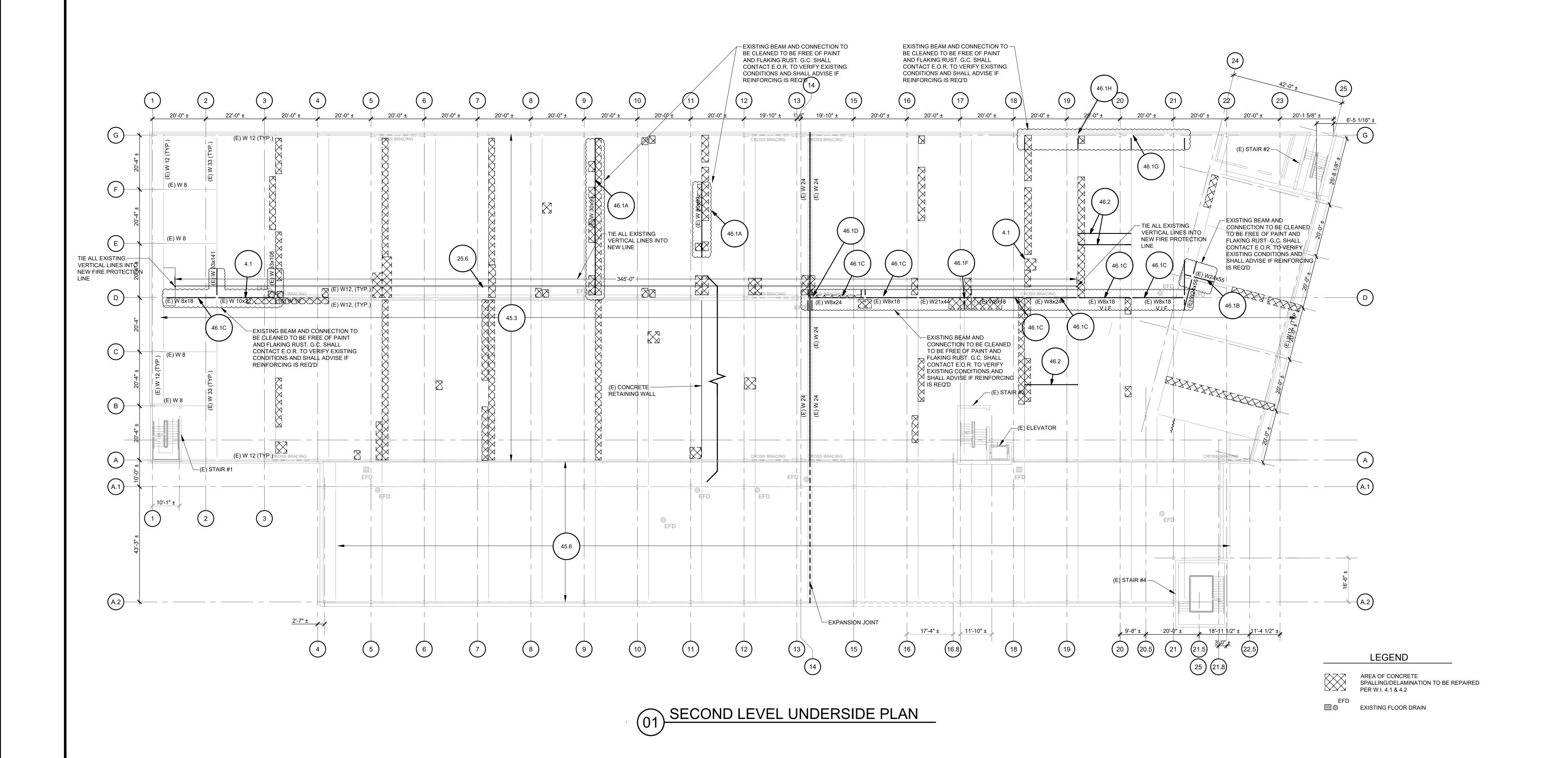
- 1. SEE R-000 FOR GENERAL NOTES, WORK ITEM SHEDULE AND PHASING INSTRUCTIONS.
- 2. SEE SHEET SERIES R-500 FOR REPAIR DETAILS.
- 3. SEE SHEET SERIES R-300 FOR STAIR REPAIRS.
- 4. BEFORE BEGINNING WORK ON THIS FLOOR, CONTRACTOR SHALL INSTALL TEMPORARY BARRICADES AND TEMPORARY SIGNAGE.
- 5. ALL STAIR TOWERS LOCATED WITHIN AN ACTIVE WORK OR PROTECTION ZONE SHALL BE LOCKED OUT AND BARRICADED PRIOR TO THE START OF WORK. PROPER SIGNAGE DIRECTING PATRONS MUST BE INSTALLED.
- 6. CONTRACTOR IS ALLOWED TO USE ELECTRICAL POWER AND WATER AVAILABLE AT THE GARAGE. CORDS AND HOSES MUST NOT BE LEFT IN THE WALKING PATH OF PATRONS.
- 7. CONTRACTOR IS RESPONSIBLE TO PROTECT EXISTING UTILITIES THAT ARE TO REMAIN, INCLUDING BUT NOT LIMITED TO DRAIN LINES, LIGHT FIXTURES, ELECTRICAL CONDUITS, STANDPIPES, ETC.
- 8. CONTRACTOR SHALL PROVIDE UNINTERRUPTED UTILITY SERVICE OUTSIDE OF THE PHASE CURRENTLY BEING WORKED IN.
- 9. ALL CONCRETE REPAIRS SHALL BE WET CURED FOR 7 DAYS BEFORE RETURNING THEM TO TRAFFIC.
- 10. ALL REPAIRS SHALL BE COMPLETED IN A PHASE PRIOR TO MOVING INTO THE NEXT PHASE. JOINT SEALANT AND TRAFFIC TOPPING INSTALLATION CAN BE CARRIED OUT AFTER CONCRETE HAS FULLY CURED FOR 28 DAYS.
- 11. REMOVE AND REPLACE ALL EXISTING HORIZONTAL JOINT SEALANT, REFER TO WORK ITEM SERIES 11.0.
- 12. THE CONCRETE SLAB BETWEEN GRIDLINES 1-25/A-G IS COMPRISED OF PRECAST FILIGREE PLANKS WITH CAST-IN-PLACE CONCRETE TOPPING. THE CONCRETE SLAB BETWEEN GRIDLINES 4-15/A-A.2 IS COMPRISED OF CONCRETE SLAB-ON-GRADE ON THIS LEVEL.

LOCATIONS OF REPAIRS SHOWN ARE FOR GRAPHICAL REPRESENTATIONAL ONLY. CONTRACTOR IS RESPONSIBLE FOR DETERMINING ACTUAL EXTENT AND LOCATIONS OF REPAIR AREAS IN ACCORDANCE WITH THE SPECIFICATIONS. WORK ITEM IS SHOWN ONLY TO REPRESENT THE TYPES OF DETERIORATION.

**ISOMETRIC** 









HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET PORTSMOUTH, NH 03801

MARK	DATE	DESCRIPTION
		REVISIONS
ISSUE:		BIDDING AND CONSTRUCTION
ISSUE DATE:		1/25/2022
PROJECT NO:		16-003129.00
DRAWI	N BY:	VRB
CHECK	(ED BY:	MAZ

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SECOND LEVEL UNDERSIDE PLAN

R-102B

- 1. SEE R-000 FOR GENERAL NOTES, WORK ITEM SHEDULE AND PHASING INSTRUCTIONS.
- 2. SEE SHEET SERIES R-500 FOR REPAIR DETAILS.
- 3. SEE SHEET SERIES R-300 FOR STAIR REPAIRS.
- 4. BEFORE BEGINNING WORK ON THIS FLOOR, CONTRACTOR SHALL INSTALL TEMPORARY BARRICADES AND TEMPORARY SIGNAGE.
- 5. ALL STAIR TOWERS LOCATED WITHIN AN ACTIVE WORK OR PROTECTION ZONE SHALL BE LOCKED OUT AND BARRICADED PRIOR TO THE START OF WORK. PROPER SIGNAGE DIRECTING PATRONS MUST BE INSTALLED.
- 6. CONTRACTOR IS ALLOWED TO USE ELECTRICAL POWER AND WATER AVAILABLE AT THE GARAGE. CORDS AND HOSES MUST NOT BE LEFT IN THE WALKING PATH OF PATRONS.
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2'-7" ±

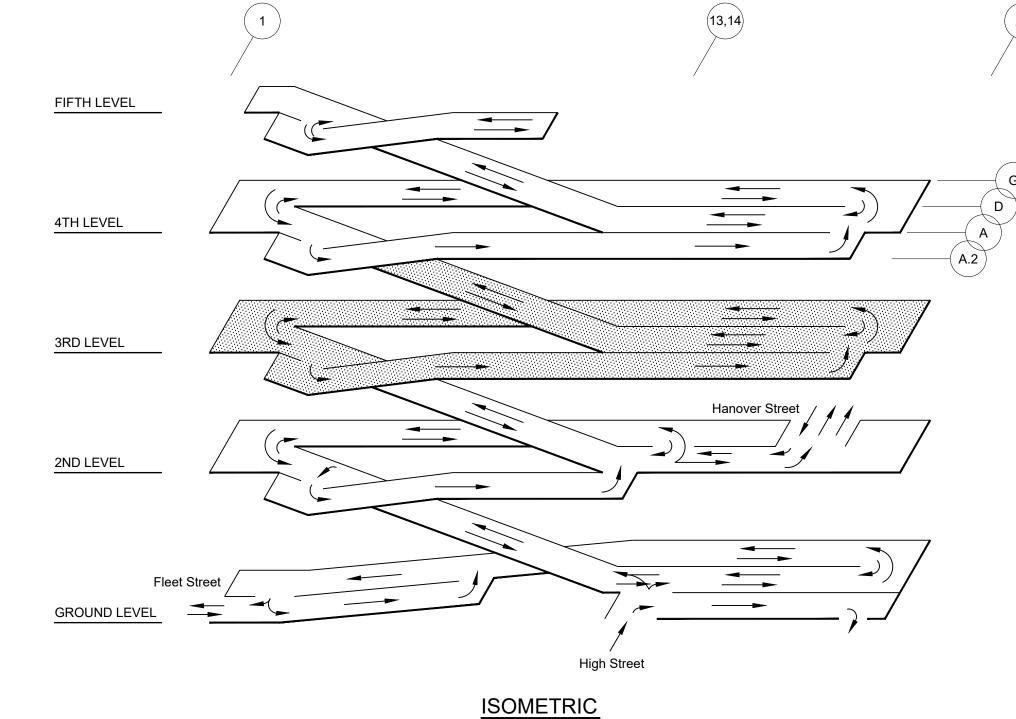
LOCATIONS OF REPAIRS SHOWN ARE FOR GRAPHICAL REPRESENTATIONAL ONLY. CONTRACTOR IS RESPONSIBLE FOR DETERMINING ACTUAL EXTENT AND LOCATIONS OF REPAIR AREAS IN ACCORDANCE WITH THE SPECIFICATIONS. WORK ITEM IS SHOWN ONLY TO REPRESENT THE TYPES OF DETERIORATION.

(E) VEHICLE BARRIER —

THIRD LEVEL FLOOR PLAN

CABLES, (TYP.)

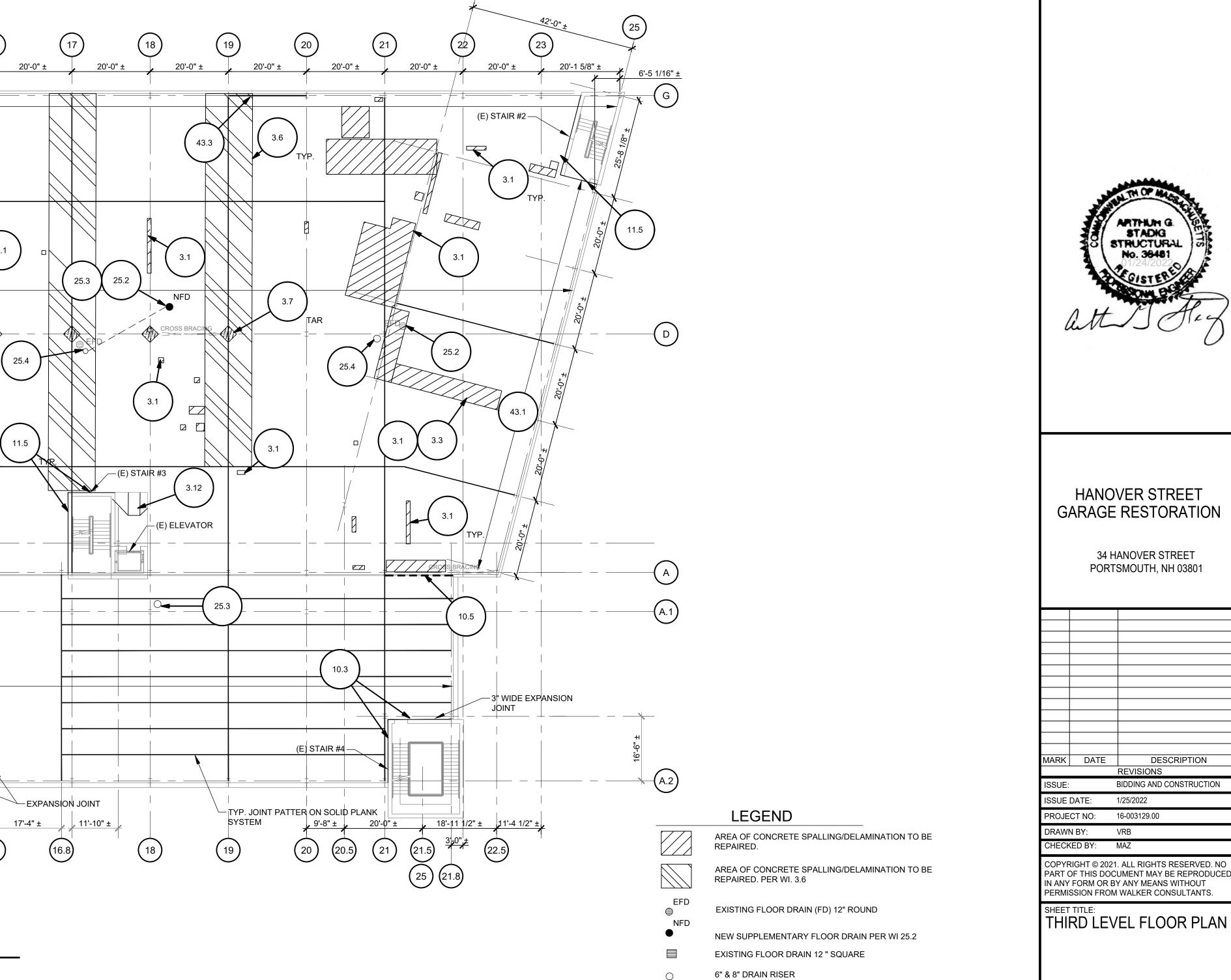
TYPICAL JOINT PATTERN THIS LEVE







R-103A



INDICATES CRACK SEE DETAIL 11.1

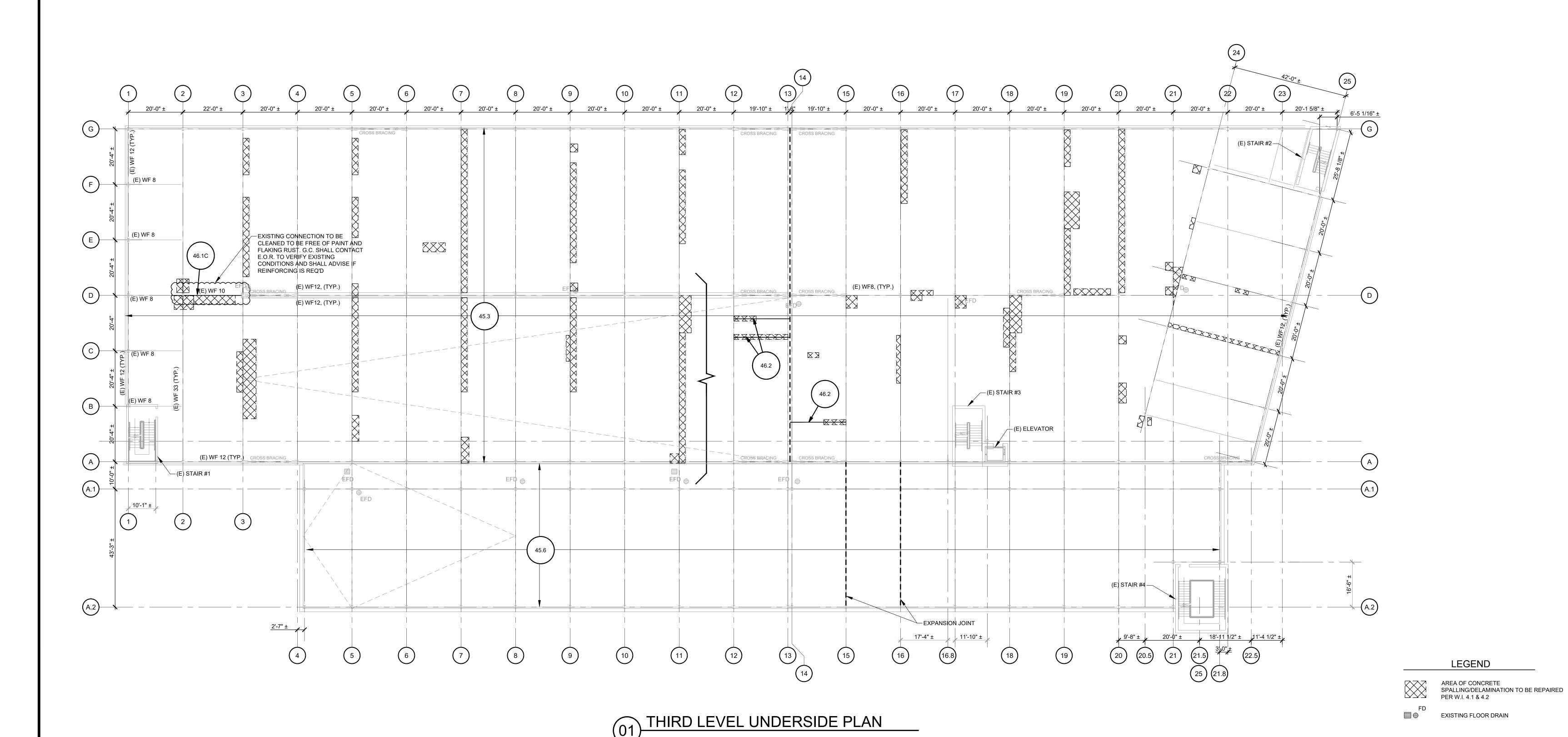
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- 3. SEE SHEET SERIES R-300 FOR STAIR REPAIRS.
- 4. BEFORE BEGINNING WORK ON THIS FLOOR, CONTRACTOR SHALL INSTALL TEMPORARY BARRICADES AND TEMPORARY SIGNAGE.
- 5. ALL STAIR TOWERS LOCATED WITHIN AN ACTIVE WORK OR PROTECTION ZONE SHALL BE LOCKED OUT AND BARRICADED PRIOR TO THE START OF WORK. PROPER SIGNAGE DIRECTING PATRONS MUST BE INSTALLED.
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- 11. REMOVE AND REPLACE ALL EXISTING HORIZONTAL JOINT SEALANT, REFER TO WORK ITEM SERIES 11.0

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**ISOMETRIC** 







HANOVER STREET **GARAGE RESTORATION** 

> 34 HANOVER STREET PORTSMOUTH, NH 03801

MARK	DATE	DESCRIPTION
		REVISIONS
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CHECK	(ED BY:	MAZ
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EXISTING FLOOR DRAIN

LEGEND

THIRD LEVEL UNDERSIDE

R-103B

### SHEET NOTES: 1. SEE R-000 FOR GENERAL NOTES, WORK ITEM SHEDULE AND PHASING INSTRUCTIONS. 2. SEE SHEET SERIES R-500 FOR REPAIR DETAILS. 3. SEE SHEET SERIES R-300 FOR STAIR REPAIRS. TEMPORARY BARRICADES AND TEMPORARY SIGNAGE. 5. ALL STAIR TOWERS LOCATED WITHIN AN ACTIVE WORK OR PROTECTION

- 4. BEFORE BEGINNING WORK ON THIS FLOOR, CONTRACTOR SHALL INSTALL
- ZONE SHALL BE LOCKED OUT AND BARRICADED PRIOR TO THE START OF WORK. PROPER SIGNAGE DIRECTING PATRONS MUST BE INSTALLED.
- AVAILABLE AT THE GARAGE. CORDS AND HOSES MUST NOT BE LEFT IN THE WALKING PATH OF PATRONS. 7. CONTRACTOR IS RESPONSIBLE TO PROTECT EXISTING UTILITIES THAT

6. CONTRACTOR IS ALLOWED TO USE ELECTRICAL POWER AND WATER

- ARE TO REMAIN, INCLUDING BUT NOT LIMITED TO DRAIN LINES, LIGHT FIXTURES, ELECTRICAL CONDUITS, STANDPIPES, ETC.
- 9. ALL CONCRETE REPAIRS SHALL BE WET CURED FOR 7 DAYS BEFORE RETURNING THEM TO TRAFFIC.

8. CONTRACTOR SHALL PROVIDE UNINTERRUPTED UTILITY SERVICE

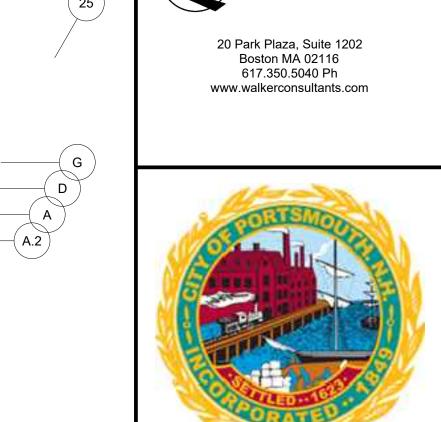
OUTSIDE OF THE PHASE CURRENTLY BEING WORKED IN.

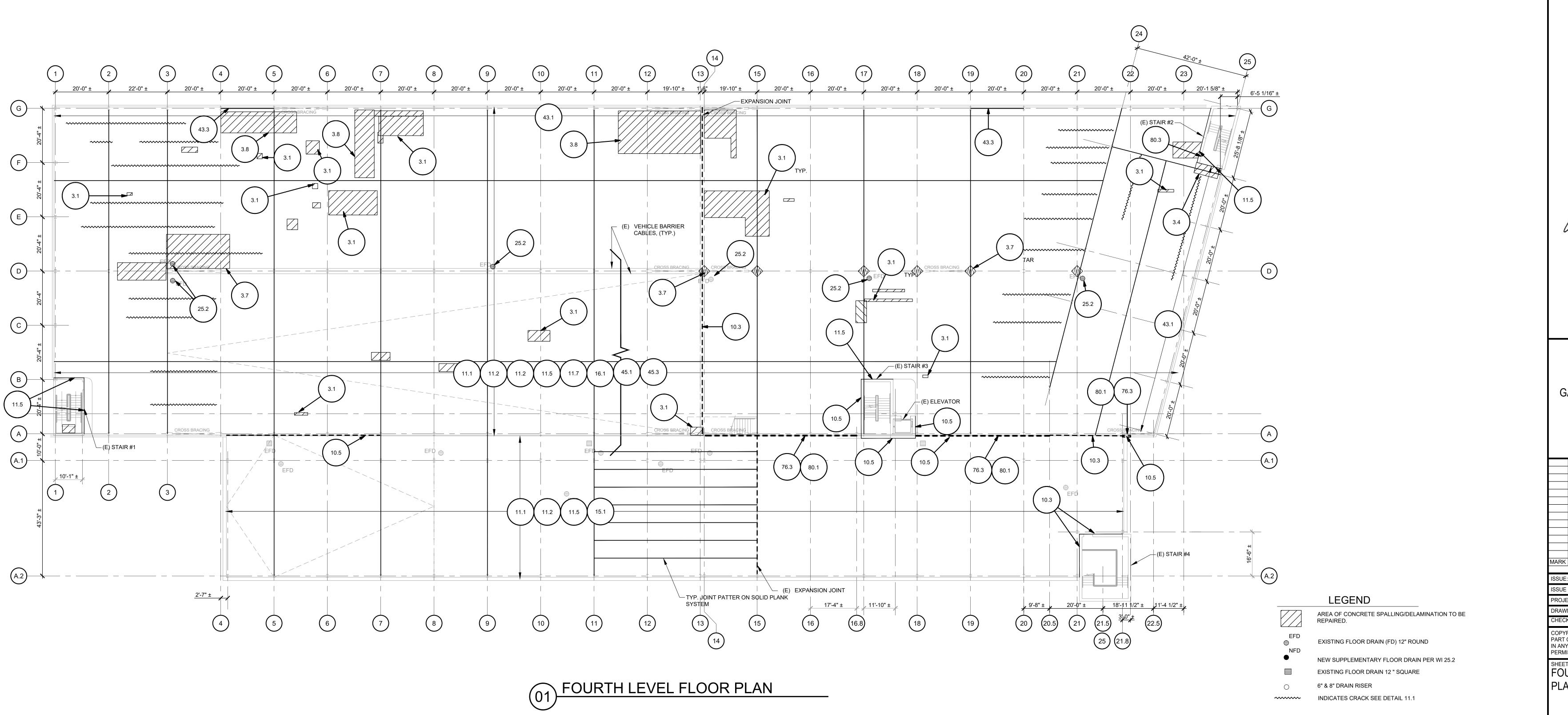
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FIFTH LEVEL -**4TH LEVEL** \_\_\_ 3RD LEVEL 2ND LEVEL \_\_\_ \_ **GROUND LEVEL** 

**ISOMETRIC** 







**GARAGE RESTORATION** 

34 HANOVER STREET PORTSMOUTH, NH 03801

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		REVISIONS		
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FOURTH LEVEL FLOOR

R-104A

- 1. SEE R-000 FOR GENERAL NOTES, WORK ITEM SHEDULE AND PHASING INSTRUCTIONS.
- 2. SEE SHEET SERIES R-500 FOR REPAIR DETAILS.
- 3. SEE SHEET SERIES R-300 FOR STAIR REPAIRS.
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DETERIORATION.

FIFTH LEVEL

ATH LEVEL

ATH LEVEL

AND LEVEL

Fleet Street

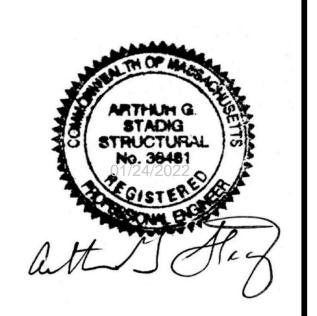
GROUND LEVEL

High Street

<u>ISOMETRIC</u>

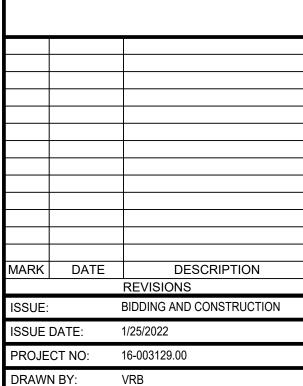






HANOVER STREET GARAGE RESTORATION

> 34 HANOVER STREET PORTSMOUTH, NH 03801

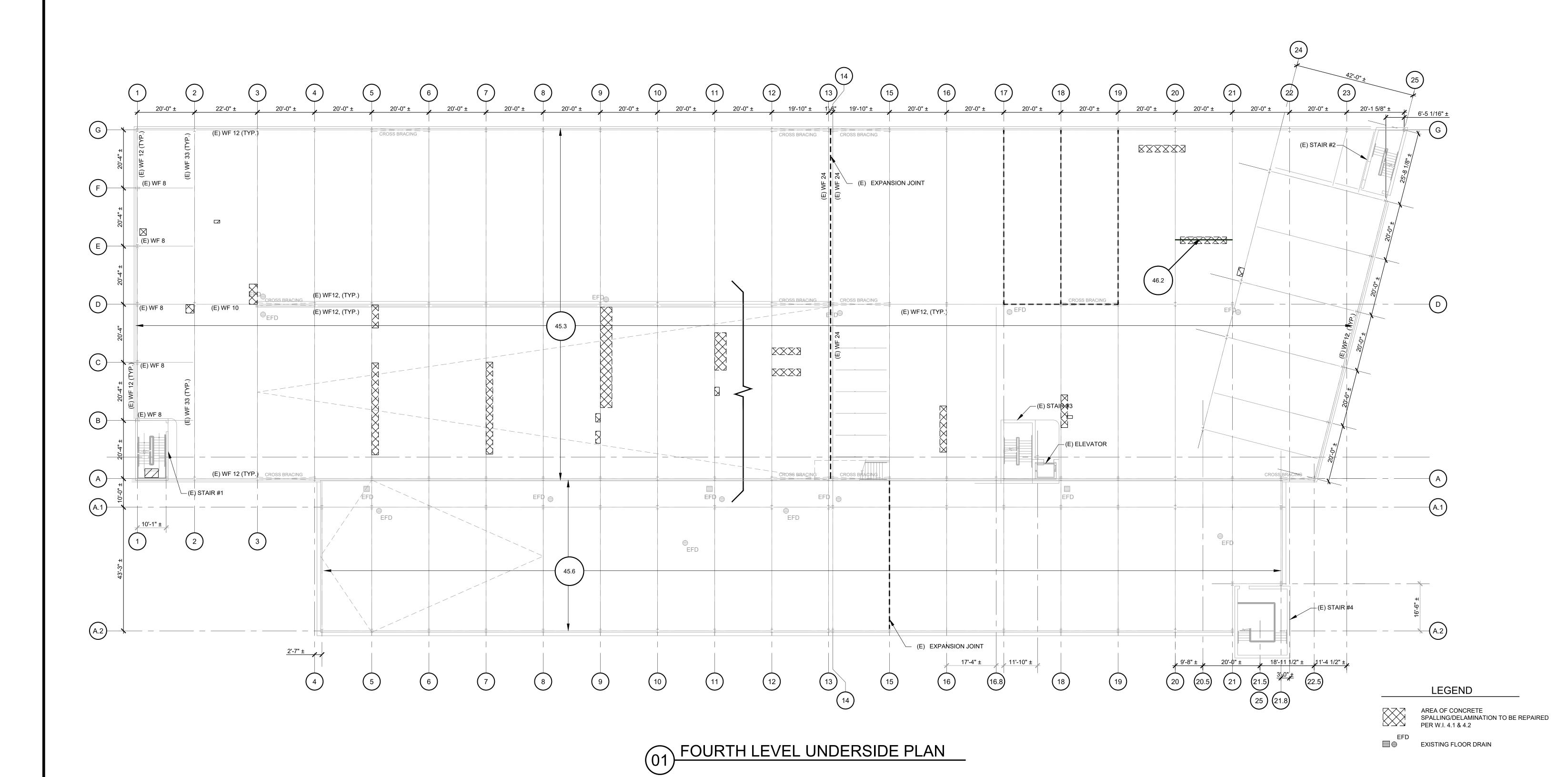


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SHEET TITLE:
FOURTH LEVEL UNDERSIDE

R-104B



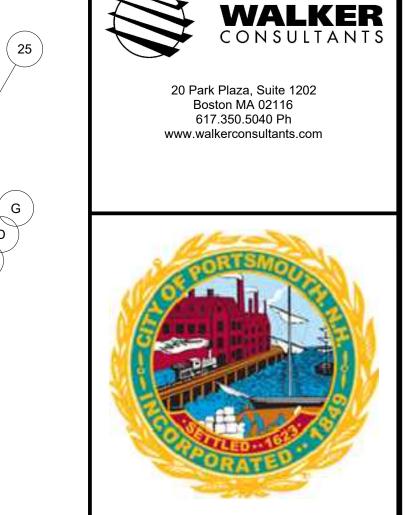
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- 3. SEE SHEET SERIES R-300 FOR STAIR REPAIRS.
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- 11. REMOVE AND REPLACE ALL EXISTING HORIZONTAL JOINT SEALANT, REFER TO WORK ITEM SERIES 11.0.
- 12. THE CONCRETE SLAB IS COMPRISED OF SOLID PRECAST PLANKS ON THIS LEVEL.

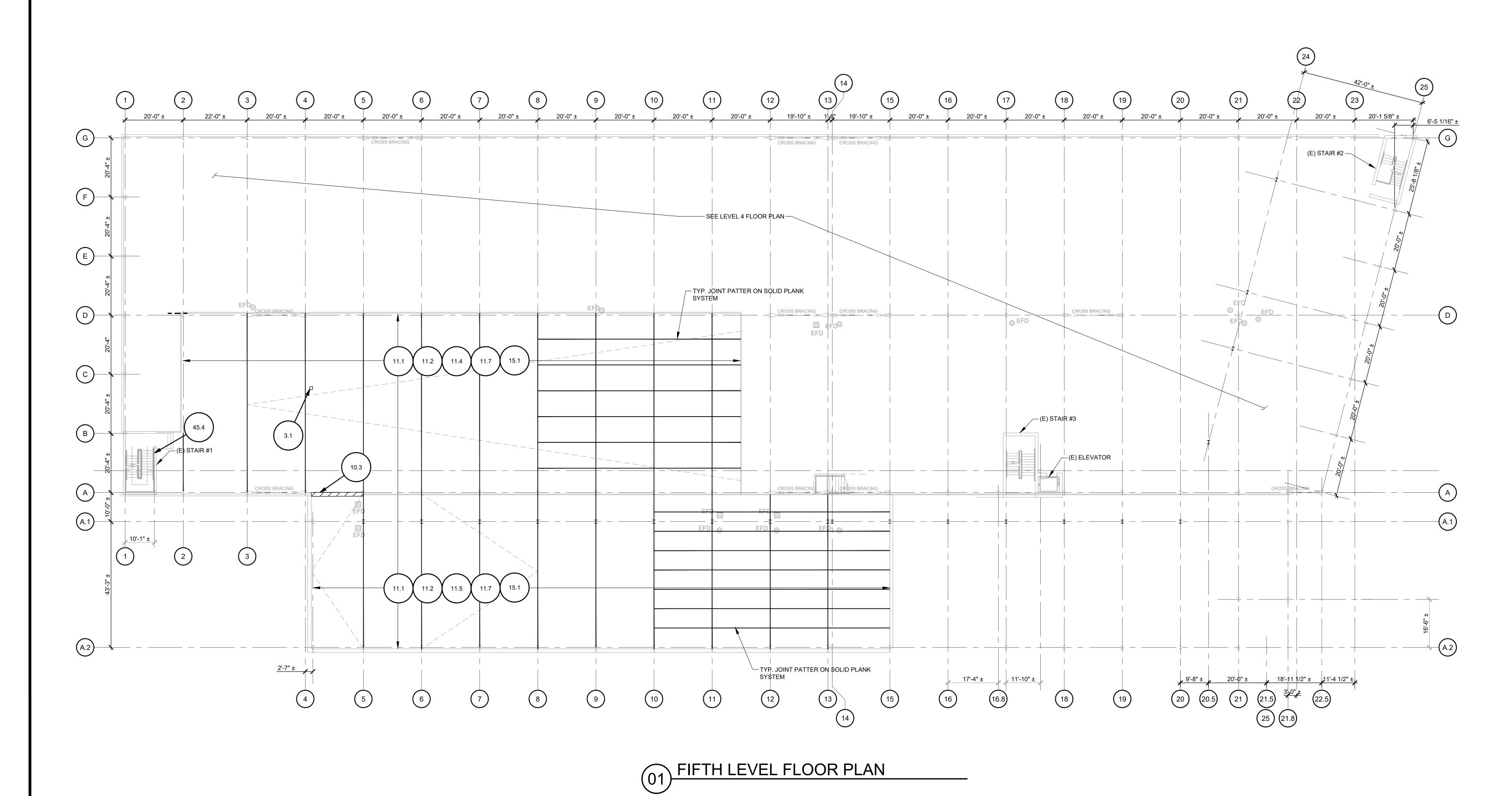
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# FIFTH LEVEL ATH LEVEL ATH LEVEL AND LEVEL Fleet Street GROUND LEVEL

<u>ISOMETRIC</u>

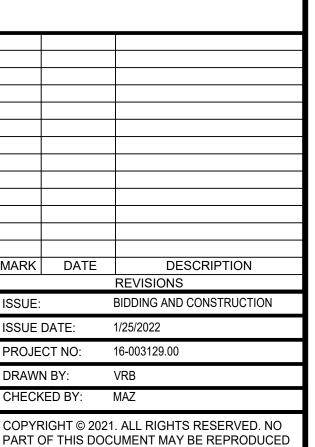






HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET PORTSMOUTH, NH 03801



AREA OF CONCRETE SPALLING/DELAMINATION TO BE REPAIRED.

EFD

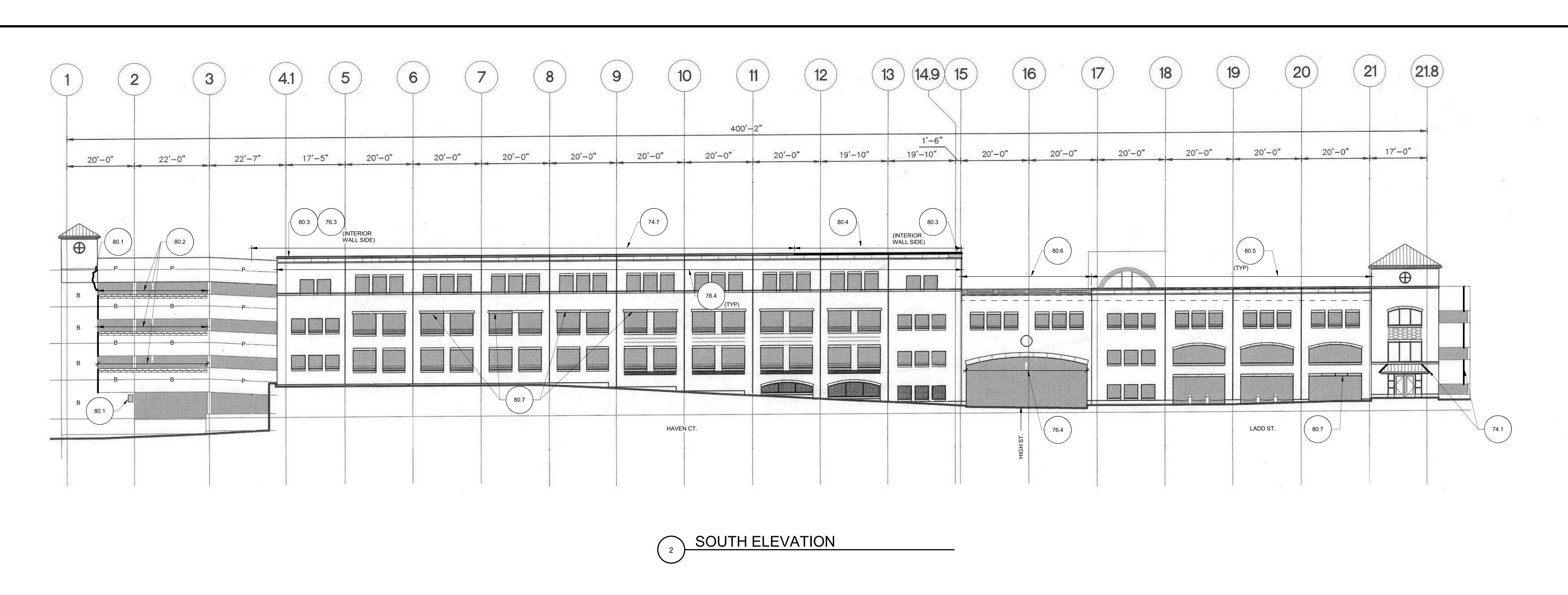
EXISTING FLOOR DRAIN

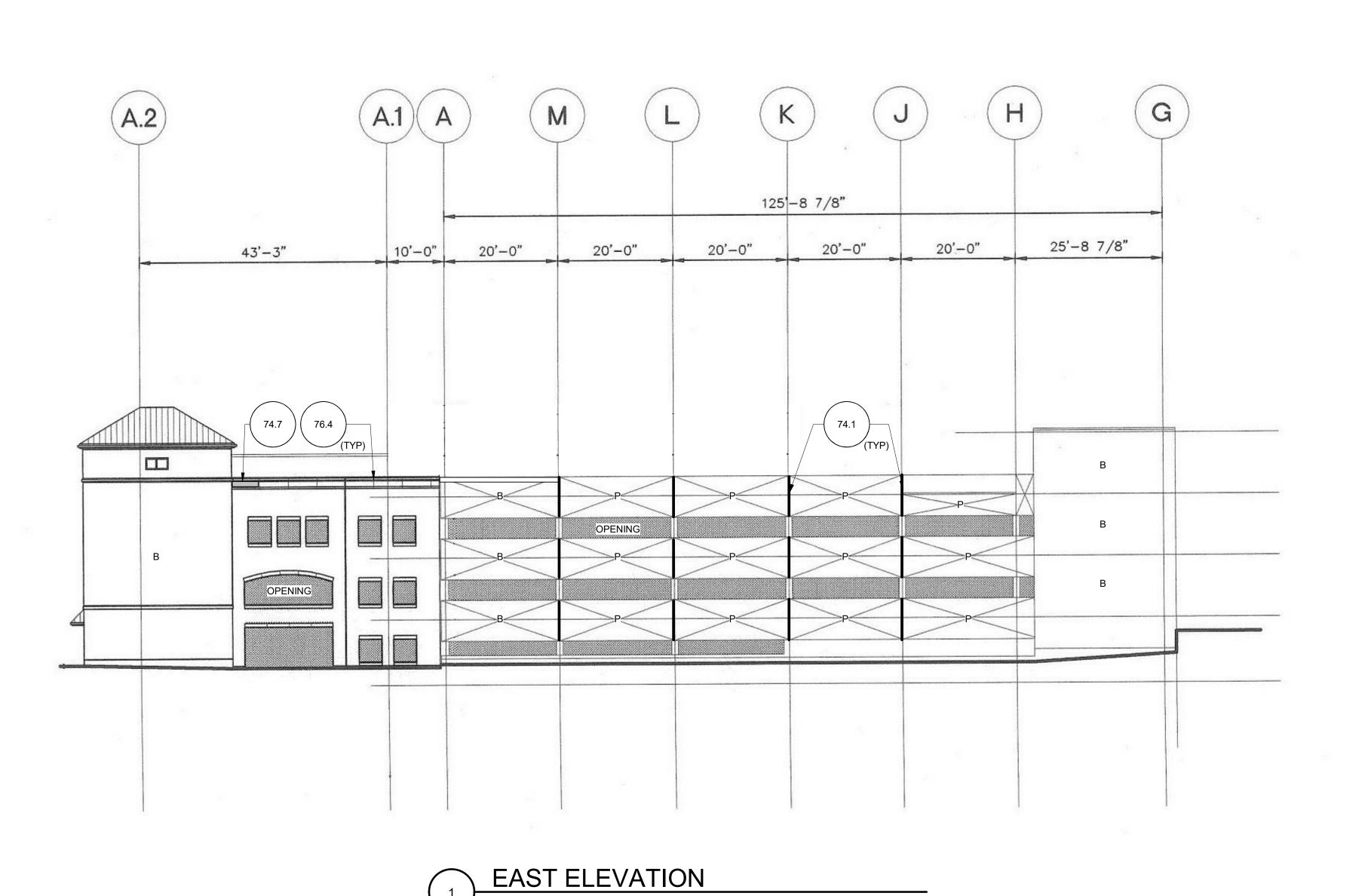
PERMISSION FROM WALKER CONSULTANTS.

SHEET TITLE:
FIFTH LEVEL FLOOR PLAN

R-105A

IN ANY FORM OR BY ANY MEANS WITHOUT





LEGEND

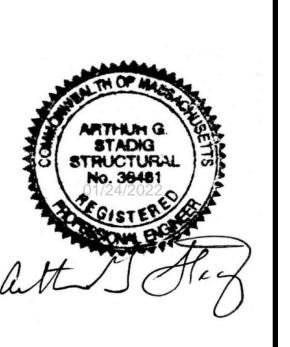
- PRECAST WALL PANEL B OR BRICK - BRICK VENEER

- WALL OPENING - INDICATES TYPICAL AS SHOWN

WALKER CONSULTANTS 20 Park Plaza, Suite 1202 Boston MA 02116 617.350.5040 Ph



www.walkerconsultants.com

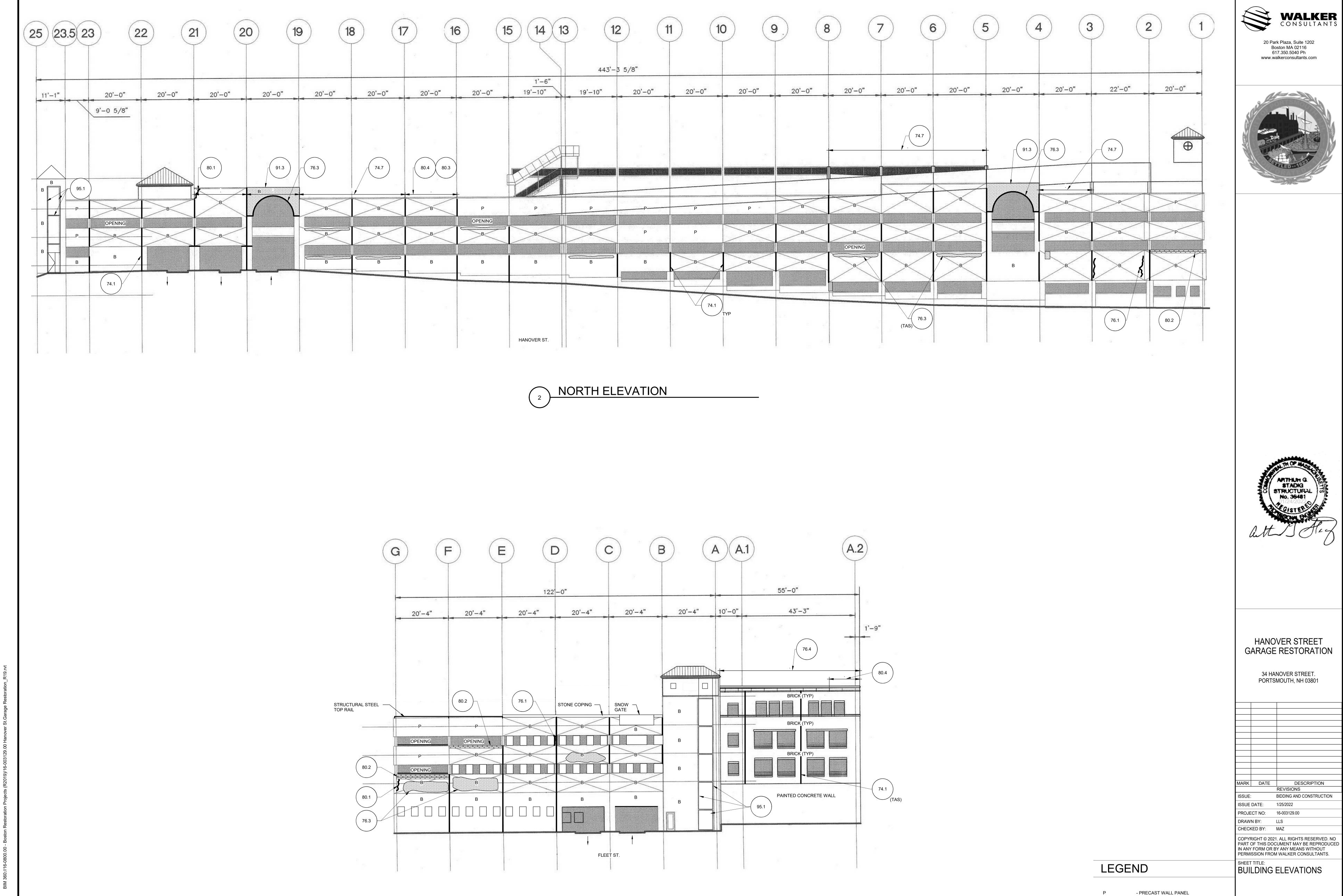


HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET. PORTSMOUTH, NH 03801

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SHEET TITLE:
BUILDING ELEVATIONS



WEST ELEVATION

B OR BRICK - BRICK VENEER

- WALL OPENING

- INDICATES TYPICAL AS SHOWN

- 1. REFER TO SHEET R-001 FOR GENERAL NOTES AND WORK ITEM SCHEDULE.
- 2. THIS DRAWING REPRESENTS THE EXISTING STAIR ELEVATION. THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFYING ALL DIMENSIONS AND EXISTING CONDITIONS PRIOR TO CONSTRUCTION NAND DEMOLITION. DO NOT SCALE DRAWINGS.
- 3. CONTRACTOR IS TO DEMOLISH EXISTING CONCRETE LANDING SHOWING SIGNS OF RUSTING AND CRACKING CONNECTING TO STAIR STRINGERS. CONTRACTOR IS TO FIELD VERIFY LIMITS OF CONCRETE LANDING NEEDING REPLACEMENT PRIOR TO DEMOLITION OF EXISTING CONCRETE.
  - 4. CONTRACTOR IS SOLELY RESPONSIBLE FOR THE DESIGN AND INSTALLATION OF TEMPORARY SHORING NECESSARY TO COMPLETE THE WORK.
- 5. ALL REBAR THAT IS EXPOSED SHALL BE CLEANED AND COATED WITH A CORROSION INHIBITOR.
- 6. DAMAGE THAT OCCURS OUTSIDE OF THE WORK AREA SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR ANY COST THAT IS INCURRED AT NO COST TO THE OWNER.
- 7. CONTRACTORS PROCEDURES, BRACING, MEANS, METHODS AND SAFETY PRECAUTIONS ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR DOING
- 8. DIMENSIONS SHOWN ON THE PLAN ARE AT THE TIME OF CONSTRUCTION AND SHOULD BE FIELD VERIFIED BEFORE FABRICATION AND INSTALLATION OF NEW STAIR LANDINGS AND STAIR TREADS.

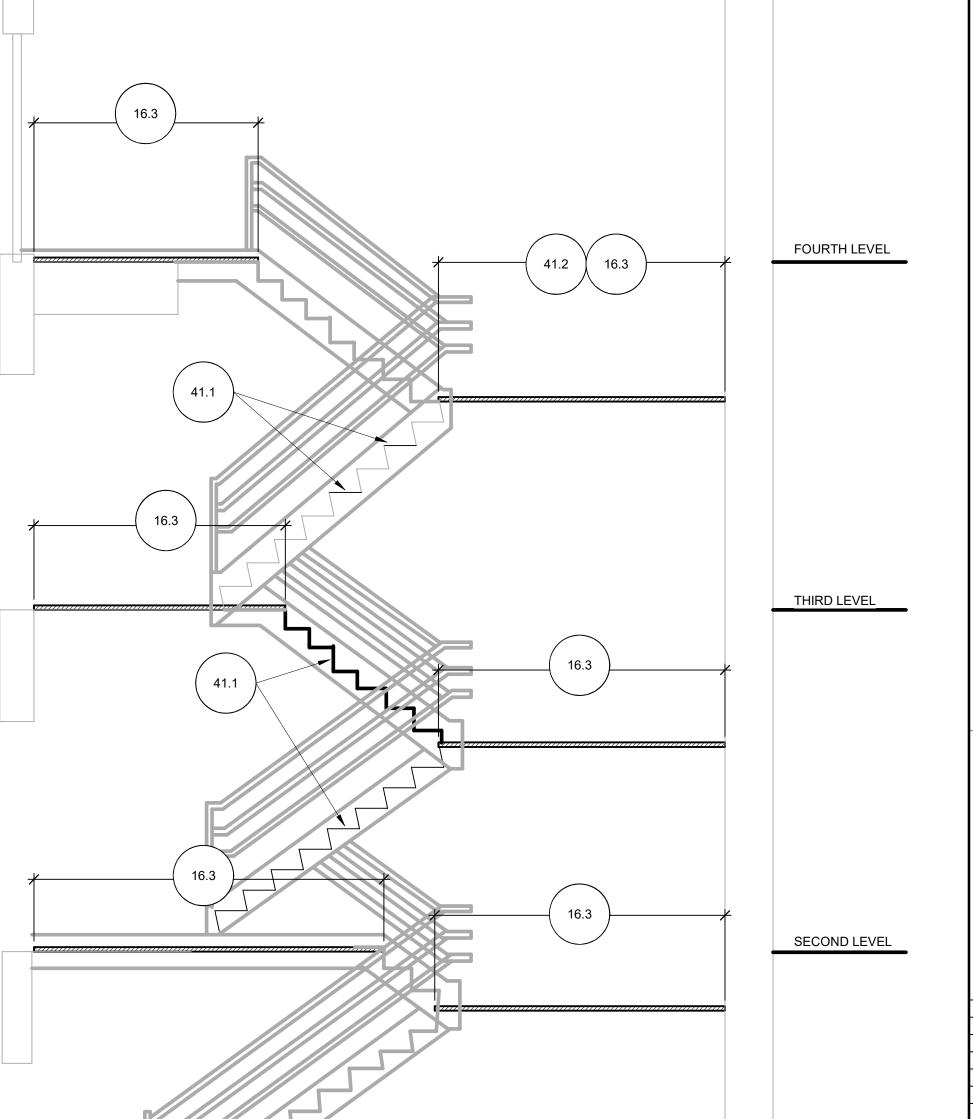


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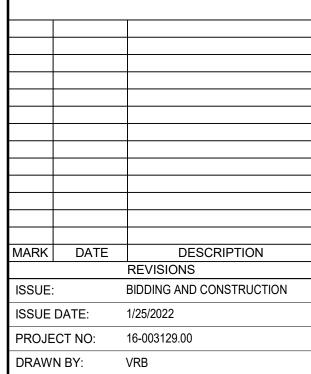






HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET. PORTSMOUTH, NH 03801



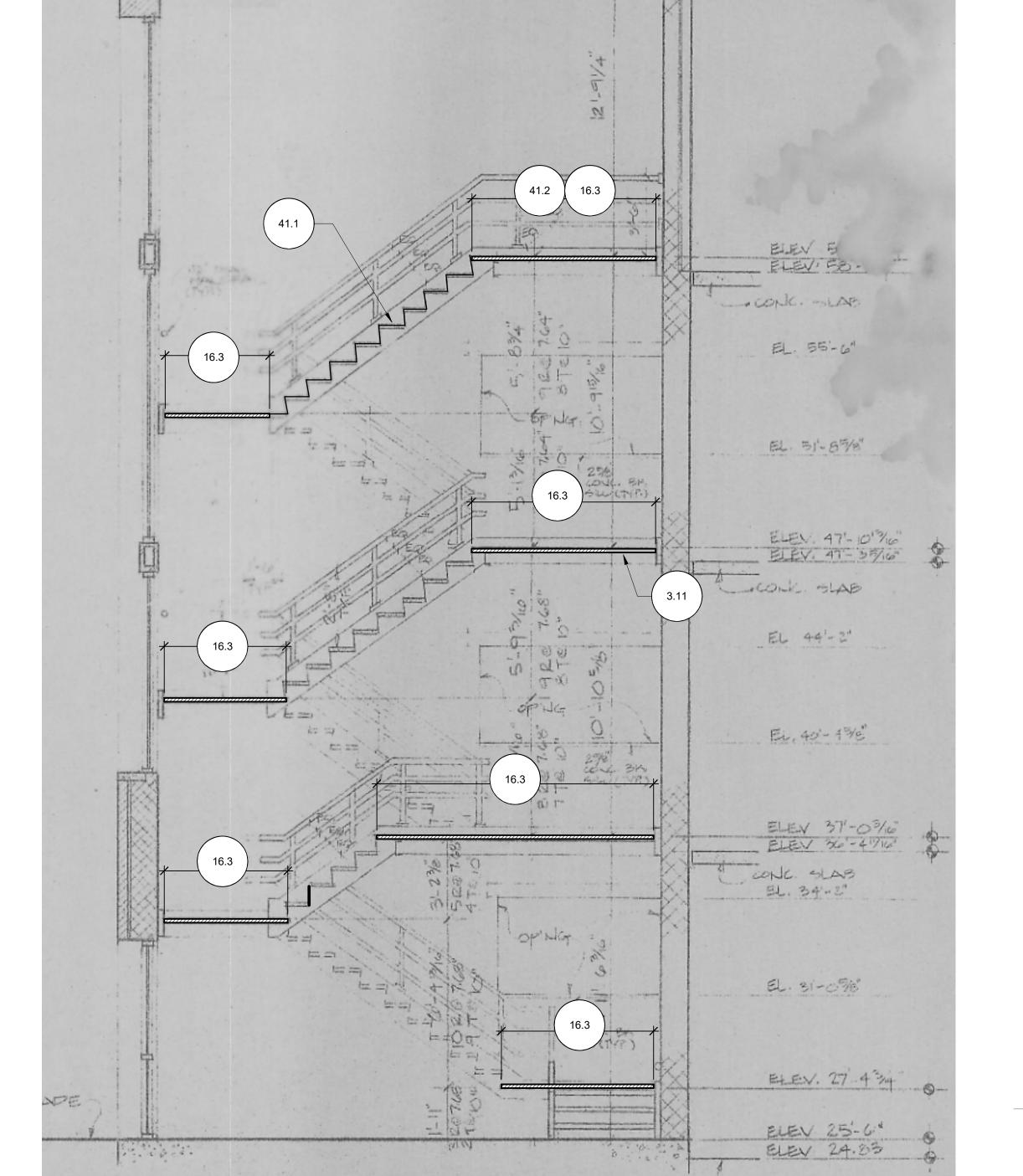
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FIRST LEVEL

STAIR REPAIR @ STAIR 1, 2

R-301





4" S" PFIE-CAYST

conti cap.

4 00NC . BLOCK



REPAIRS AT STAIR 3

REPAIRS AT STAIR 1

BLA CAP CONC.

cold slab-

conse sias.

WE TEDIATE

400 PRECAST

◆ EL...69'-10"

千万年1000

B'CONG.

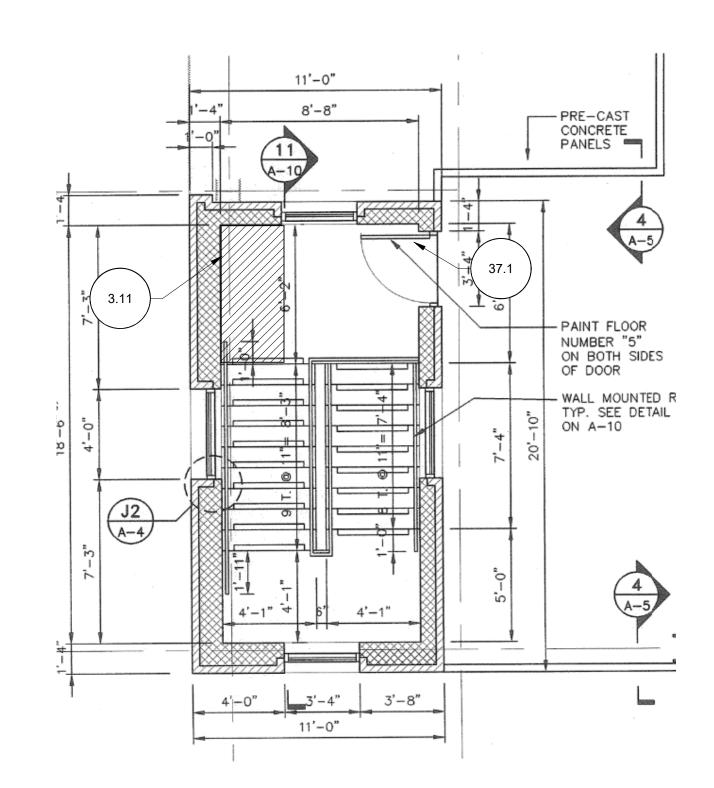
-4" conc slocks

THE EXISTING DRAWING (SCREENED) ARE FROM PARKING FACILITY HIGH AND HANOVER STREETS FOR THE CITY OF PORTSMOUTH DATED DECEMBER 21, 1984 AND ARE FOR INFORMATION ONLY. THE ELEVATIONS SHOWS ARE INTENDED TO SHOW REPAIR AND MAINTENANCE WORK. (DO NOTE SCALE DRAWINGS)

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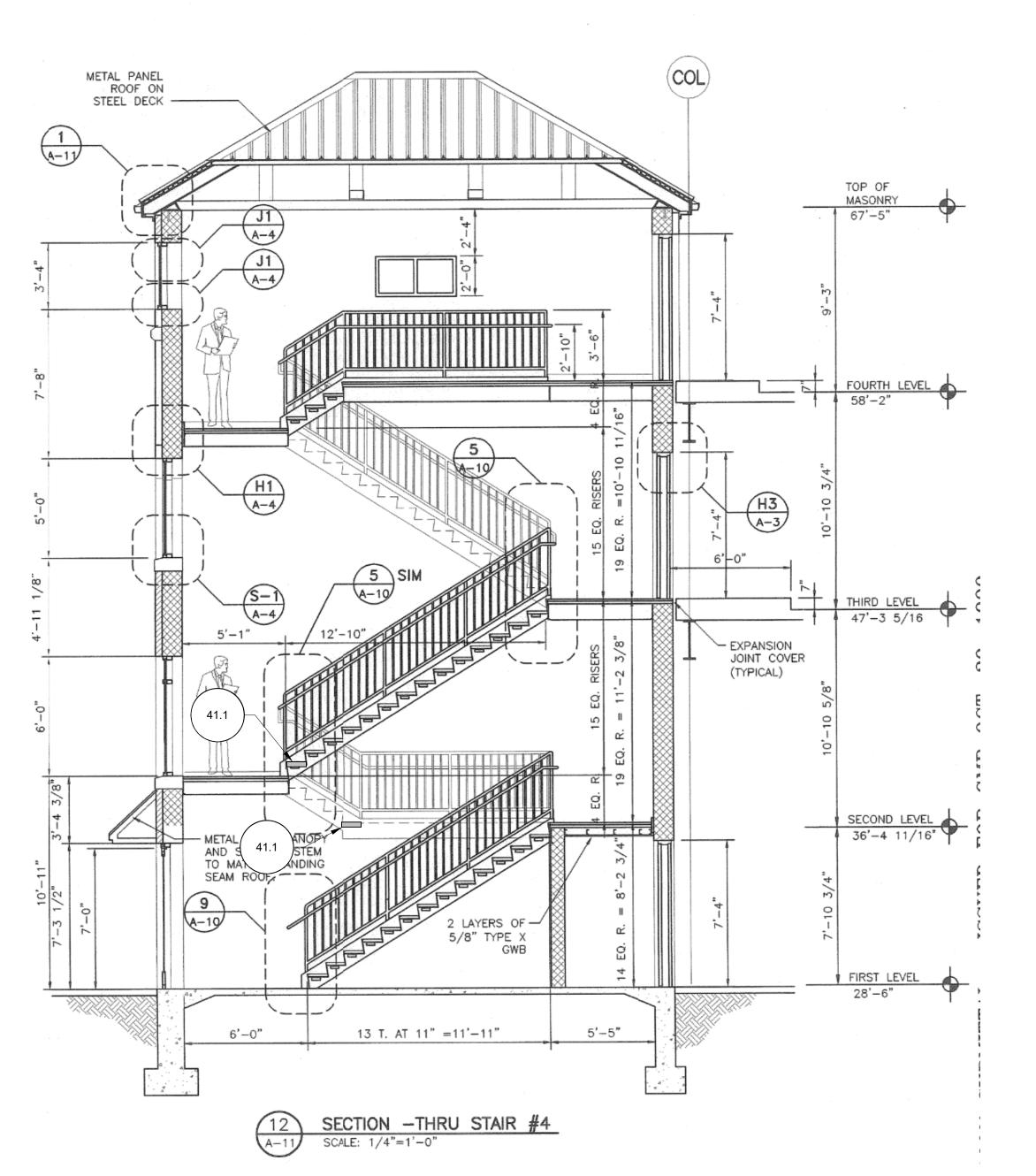


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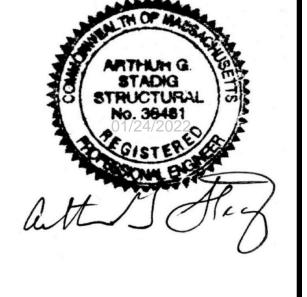


10 PLAN - STAIR #1 FIFTH LEVEL

SCALE: 1/4"=1'-0"

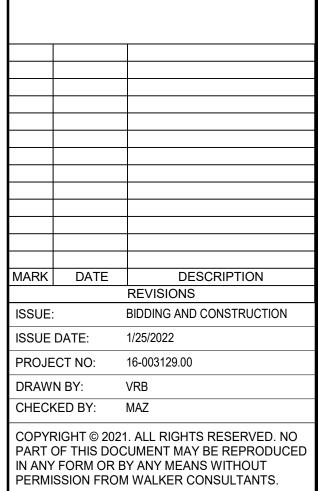


THE EXISTING DRAWING (SCREENED) ARE FROM THE GARAGE EXTENSION DRAWINGS TITLED CITY OF PORTSMOUTH HIGH-HANOVER PARKING FACILITY EXPANSION DATED OCTOBER 29, 1999 AND ARE FOR INFORMATION ONLY. THE ELEVATIONS SHOWS ARE INTENDED TO SHOW REPAIR AND MAINTENANCE WORK. (DO NOTE SCALE DRAWINGS)



HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET. PORTSMOUTH, NH 03801

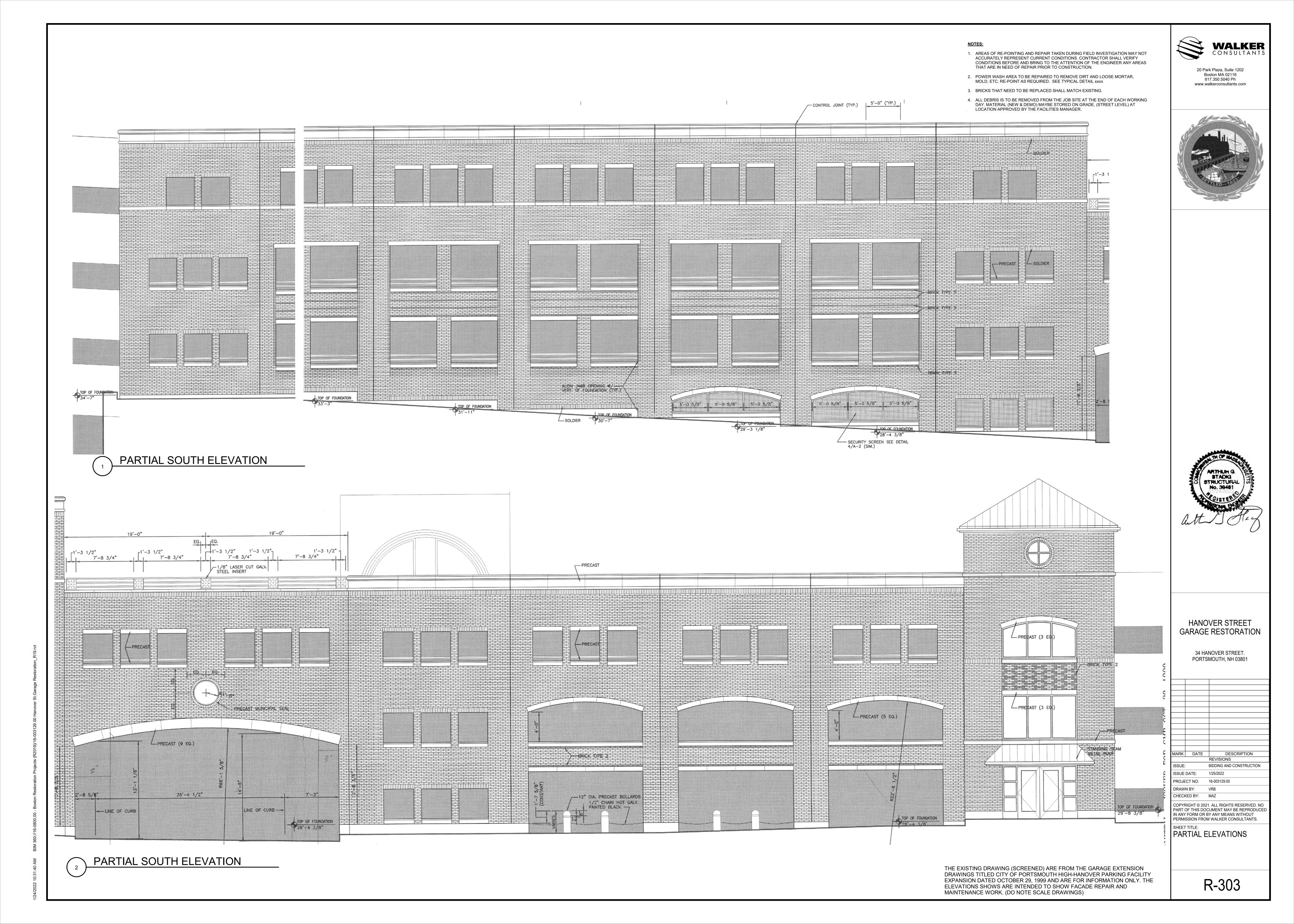


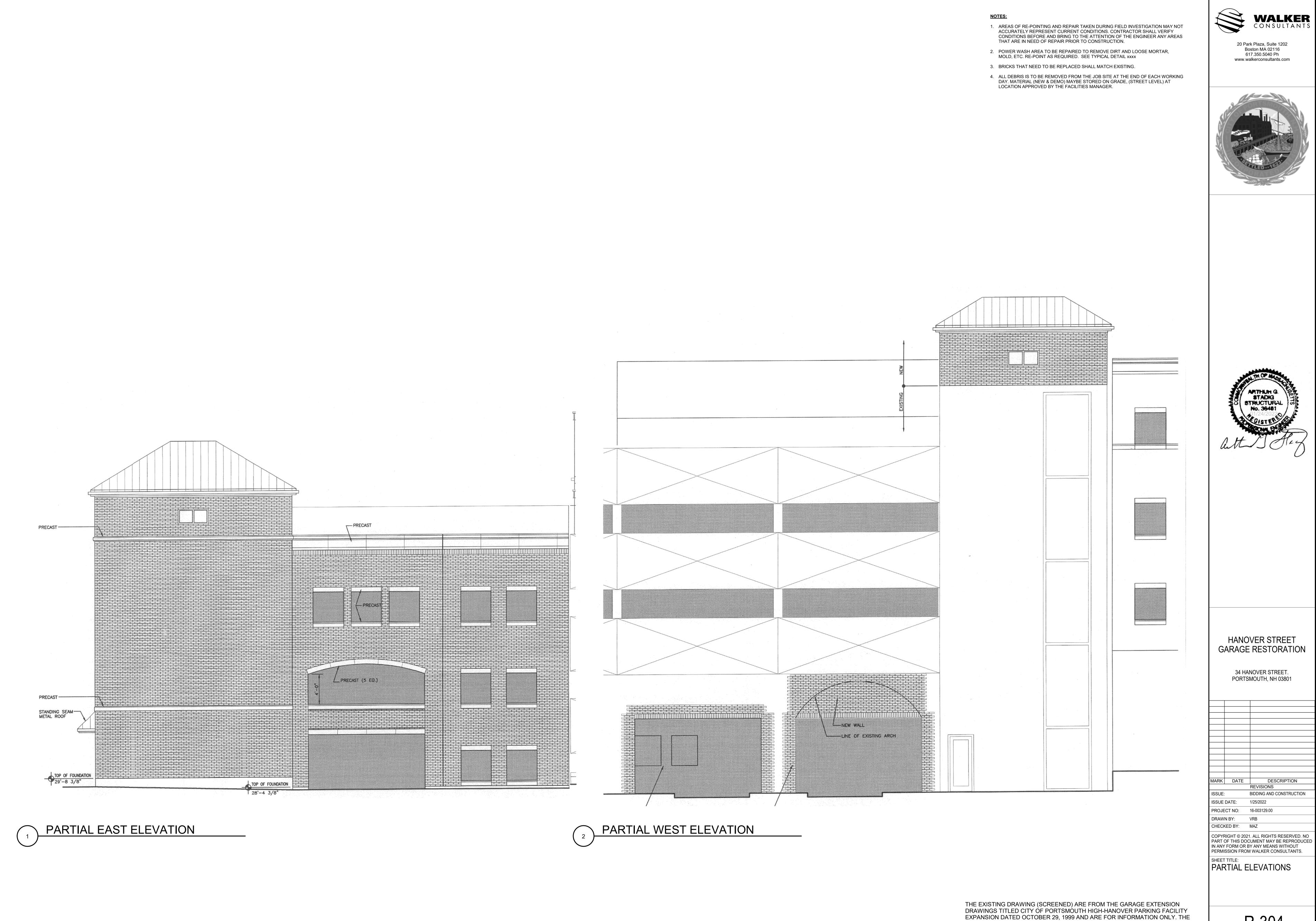
STAIR REPAIR @ STAIR 4

R-302

STAIR REPAIR AT STAIR 4

IIM 360://16-0800.00 - Boston Restoration Projects (R2019)/16-003129.00 Hanover St.Garage Restoration\_R19.rvt





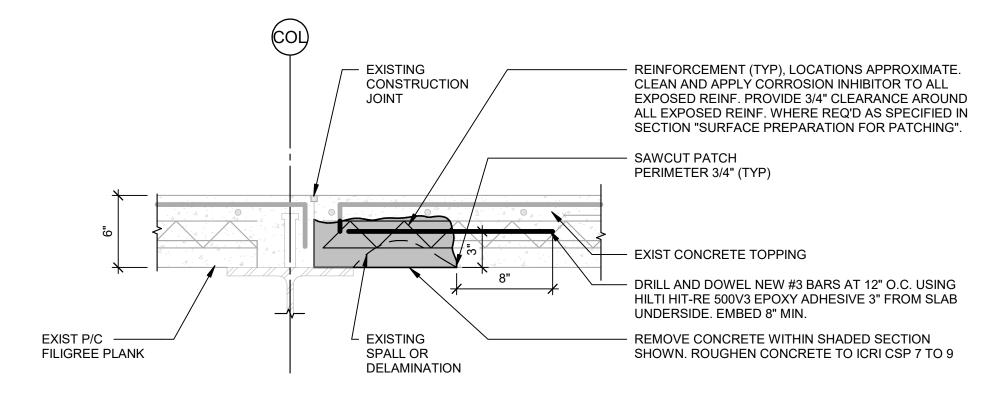
R-304

ELEVATIONS SHOWS ARE INTENDED TO SHOW FACADE REPAIR AND

MAINTENANCE WORK. (DO NOTE SCALE DRAWINGS)

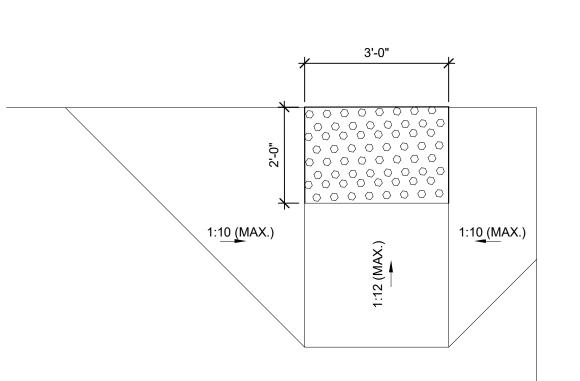
NOTES: 1. CONTRACTOR SHALL TEMPORARILY SHORE PLANKS AND BEAMS ON ALL LEVELS TO GRADE PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK. NUMBER AND LOCATION OF REINFORCEMENT SHOWN MAY DIFFER FROM ACTUAL FIELD CONDITIONS. COLUMN TIES WHICH HAVE LOST MORE THAN 15% OF ORIGINAL CROSS SECTIONAL AREA SHALL BE SUPPLEMENTED AS ENGINEER DIRECTS 4. DO NOT CUT OR DAMAGE REINFORCING. NOTIFY ENGINEER IF EXISTING REINFORCING STEEL APPEARS TO HAVE OVER 20% SECTION LOSS O TWO ADJACENT BARS HAVE 10% SECTION LOSS. ENGINEER SHALL DETERMINE DURING CONSTRUCTION IF REINFORCING REPAIRS ARE

### COLUMN REPAIR - PIER BASE

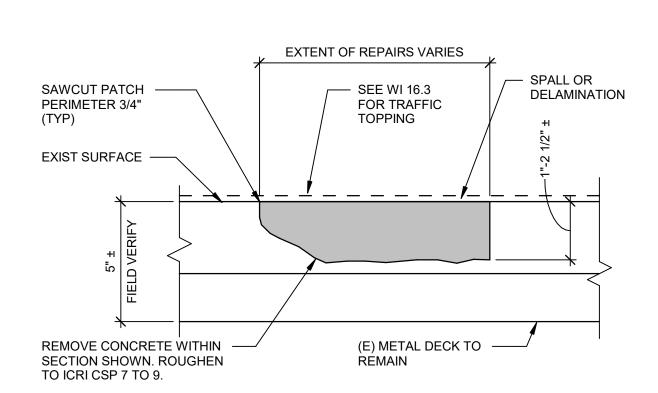


CONTRACTOR SHALL TEMPORARILY SHORE PLANKS PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK. 2. CONTRACTOR SHALL NOTIFY ENGINEER IF ONE REINFORCING BAR HAS LOST MORE THAN 20% CROSS SECTION OR IF TWO ADJACENT BARS HAVE LOST MORE THAN 10% CROSS SECTION EACH.

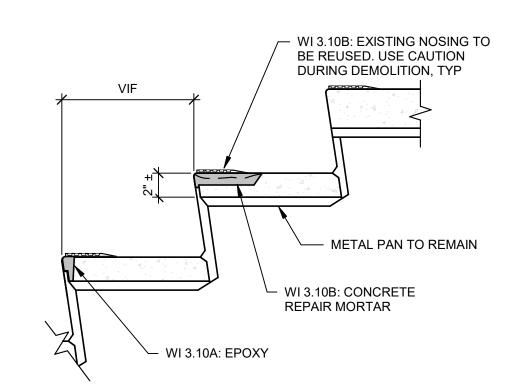
### **CEILING REPAIR - PARTIAL** DEPTH/DEEP



REMOVE AND REPLACE ADA CURB CUT



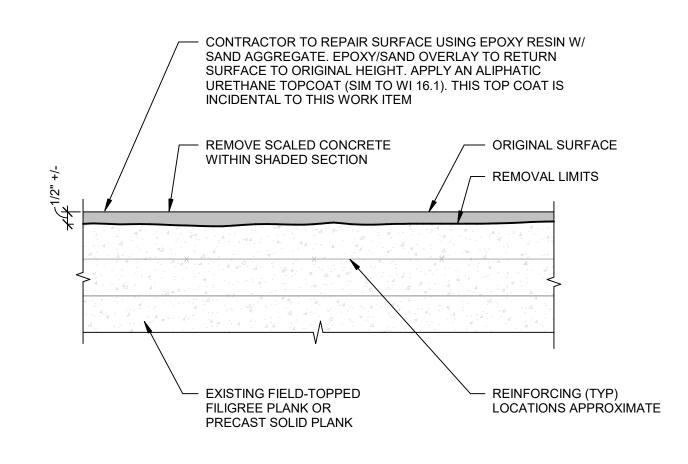
### FLOOR REPAIR-STAIR LANDING PARTIAL DEPTH



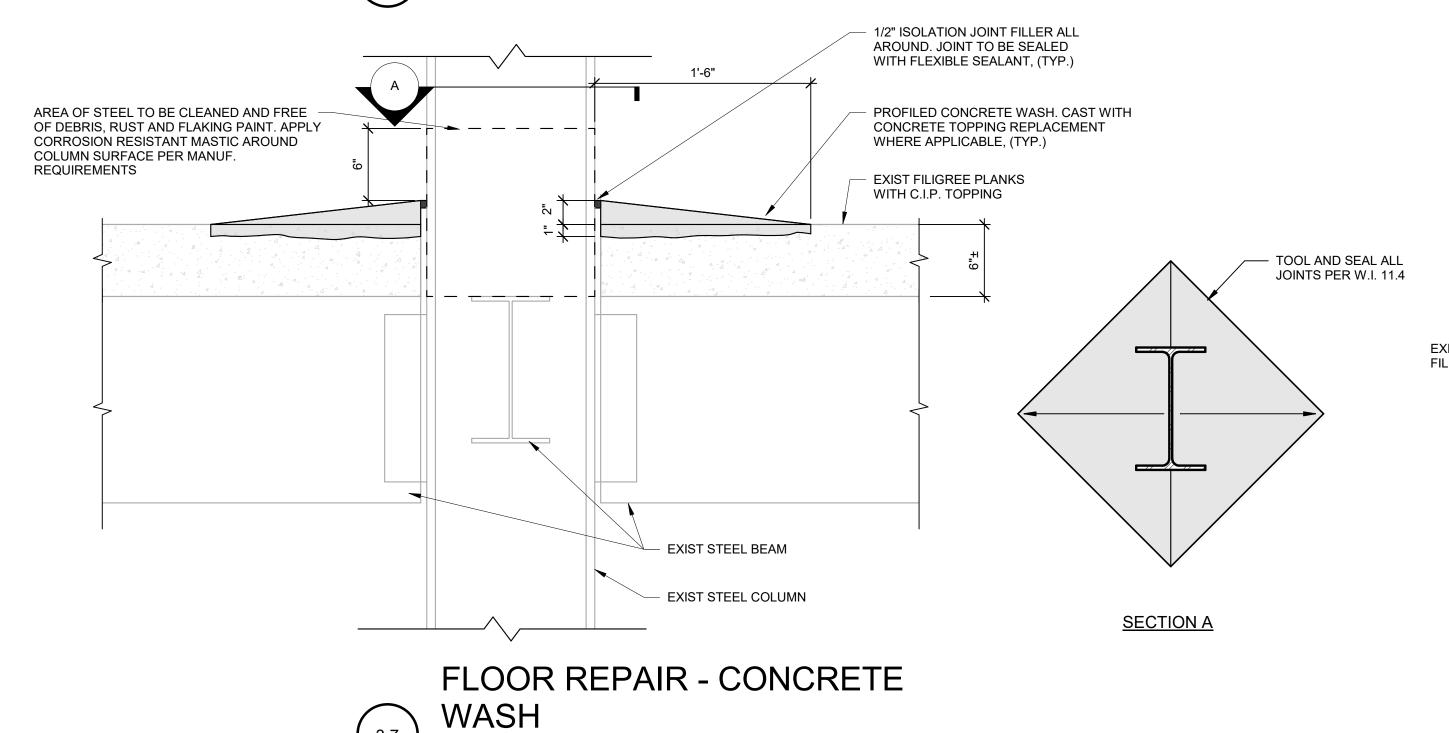
NOTES:

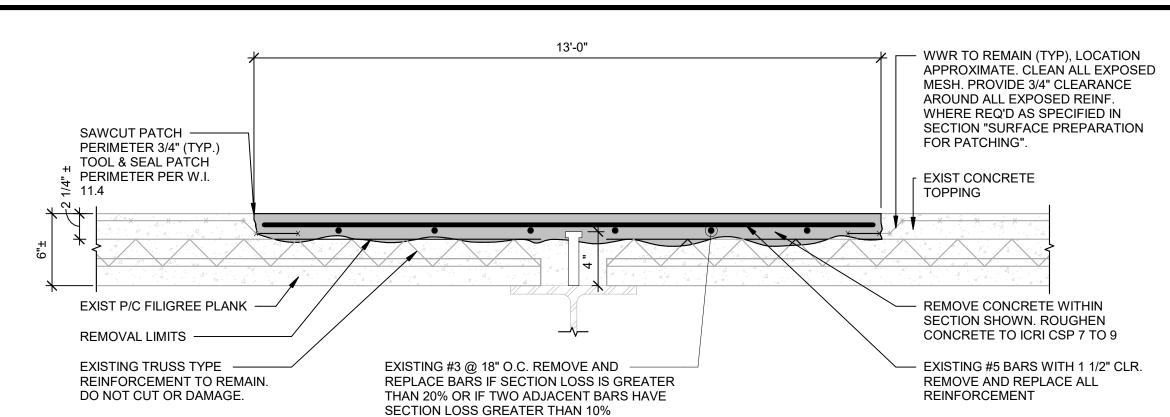
1. REMOVE AND REPLACE ALL SOUND AND UNSOUND CONCRETE WITHIN SECTION SHOWN SHADED. . APPLY BITUMINOUS COATING TO THE UNDERSIDE OF NOSING BEFORE INSERTION INTO STAIR STEP. 3. WI 3.10C: IF REQUIRED, REPLACE EXISTING NOSING IN KIND.

### STAIR NOSING REPAIR



### FLOOR REPAIR - SCALED CONCRETE SURFACE





SUGGESTED CONSTRUCTION SEQUENCE: I. TEMPORARILY SHORE THE EXISTING STEEL BEAM AND ADJACENT FILIGREE PLANKS SUCH THAT THE SHORING SUPPORTS THE DEAD LOAD OF THE EXISTING BEAM, PLANKS, AND C.I.P. CONCRETE TOPPING AND THE RECOMMENDED CONSTRUCTION LIVE LOAD. 2. REMOVE DETERIORATED CONCRETE AND REINFORCING STEEL FROM THE STRIP REPAIR AREA SHOWN.

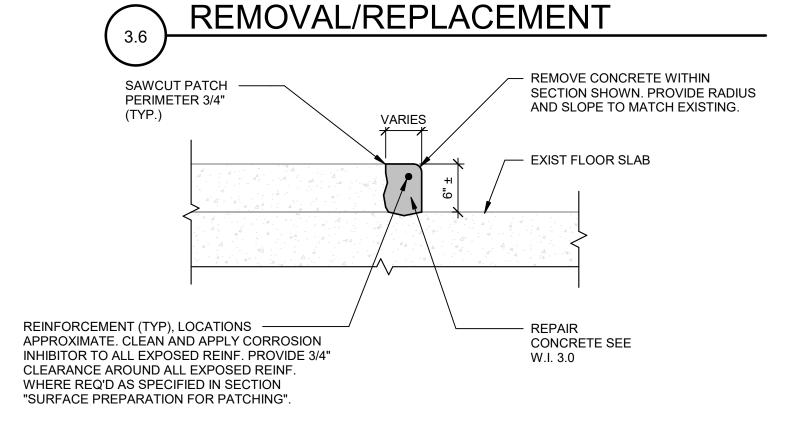
3. PREPARE THE CAVITY AND PLACE CONCRETE. 4. CURE NEW CONCRETE UNTIL F'C=5,000 PSI. 5. TEMPORARY SHORING CAN BE REMOVED.

NOTES:

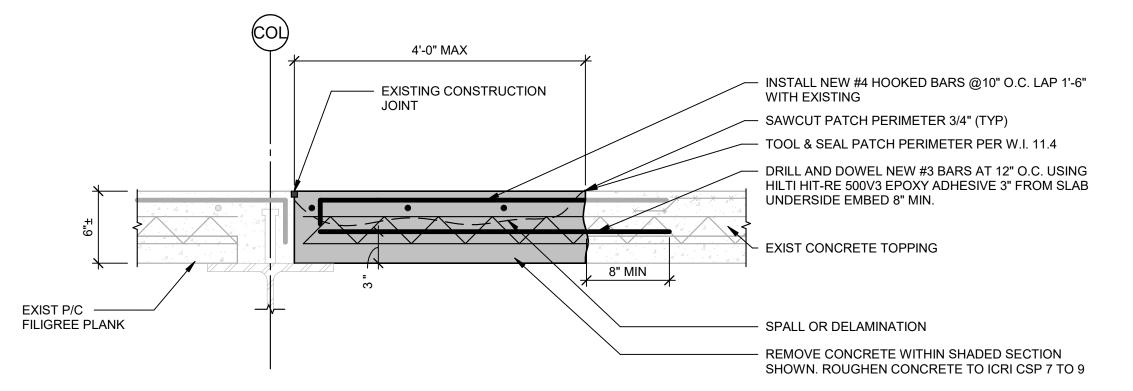
1. CONTRACTOR SHALL TEMPORARILY SHORE PLANKS AND BEAM PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. TEMPORARY SHORING SHALL REMAIN IN PLACE UNTIL CONCRETE REPAIRS HAVE FULLY CURED. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK. CONTRACTOR SHALL NOTIFY ENGINEER IF ONE REINFORCING BAR HAS LOST MORE THAN 20% CROSS SECTION OR IF TWO ADJACENT BARS HAVE LOST

MORE THAN 10% CROSS SECTION EACH. 3. TOOL & SEAL CONTROL JOINT IN PLASTIC CONCRETE OVER THE CENTERLINE OF THE STEEL BEAM. 4. TOOL & SEAL CONTROL JOINTS IN PLASTIC CONCRETE IN LINE WITH ALL PRECAST FILIGREE PANEL JOINTS BELOW.

### FLOOR REPAIR - PARTIAL DEPTH TOPPING STRIP



### FLOOR REPAIR - CURBS/WALKS

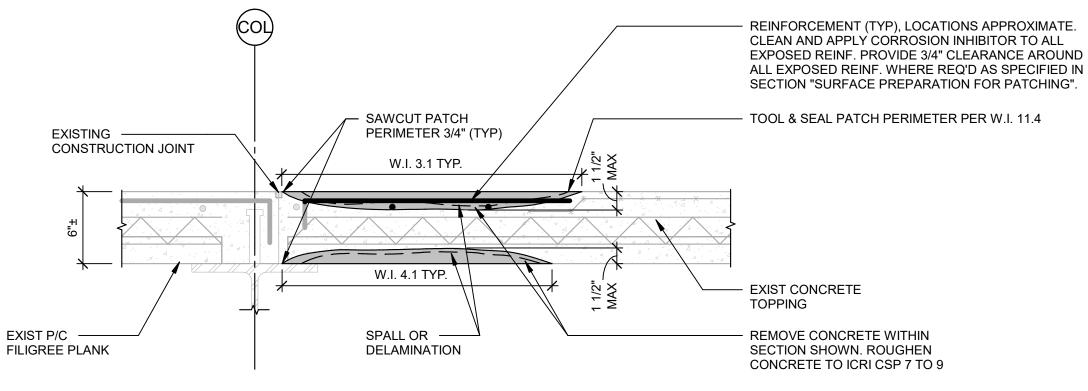


1. CONTRACTOR SHALL TEMPORARILY SHORE PLANKS PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. TEMPORARY SHORING SHALL REMAIN IN PLACE UNTIL CONCRETE REPAIRS HAVE FULLY CURED. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK.

2. CONTRACTOR SHALL NOTIFY ENGINEER IF CONCRETE REMOVAL LIMITS EXCEED 4'-0" FROM THE CONSTRUCTION JOINT AS SHOWN ON DRAWINGS. 3. CONTRACTOR SHALL NOTIFY ENGINEER IF ONE REINFORCING BAR HAS LOST MORE THAN 20% CROSS SECTION OR IF

TWO ADJACENT BARS HAVE LOST MORE THAN 10% CROSS SECTION EACH.

### FLOOR REPAIR - FULL DEPTH AT CONSTRUCTION JOINT



NOTES:

1. CONTRACTOR SHALL TEMPORARILY SHORE PLANKS PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. TEMPORARY SHORING SHALL REMAIN IN PLACE UNTIL CONCRETE REPAIRS HAVE FULLY CURED. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY

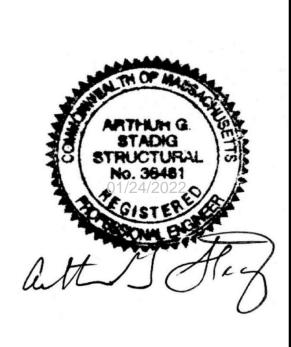
- REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK. 2. FLOOR REPAIR WORK, INCLUDING DEMOLITION, SHALL NOT BEGIN UNTIL UNDERSIDE REPAIRS HAVE BEEN COMPLETED
- AND HAVE FULLY CURED. 3. CONTRACTOR SHALL NOTIFY ENGINEER IF ONE REINFORCING BAR HAS LOST MORE THAN 20% CROSS SECTION OR IF TWO
- ADJACENT BARS HAVE LOST MORE THAN 10% CROSS SECTION EACH. 4. IF CONCRETE REMOVAL LIMITS EXCEED 1 1/2" FROM SLAB TOPSIDE SEE DETAIL 3.3. 5. IF CONCRETE REMOVAL LIMITS EXCEED 1 1/2" FROM SLAB UNDERSIDE SEE DETAIL 4.1

FLOOR AND CEILING REPAIRS PARTIAL DEPTH/SHALLOW AT **CONSTRUCTION JOINT** 



20 Park Plaza, Suite 1202 Boston MA 02116 617.350.5040 Ph www.walkerconsultants.com





HANOVER STREET **GARAGE RESTORATION** 

> 34 HANOVER STREET. PORTSMOUTH, NH 03801

MARK	DATE	DESCRIPTION
IVII U U C	Ditte	REVISIONS
ISSUE:		BIDDING AND CONSTRUCTION
ISSUE DATE:		1/25/2022
PROJECT NO:		16-003129.00
DRAWN BY:		NCH
CHECKED BY:		MAZ

REPAIR DETAILS

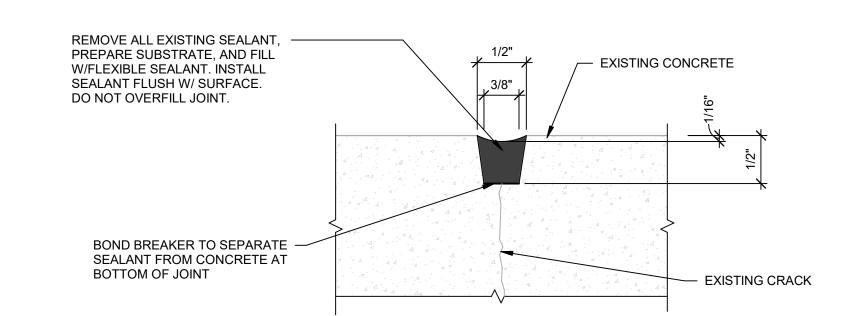
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PART OF THIS DOCUMENT MAY BE REPRODUCED

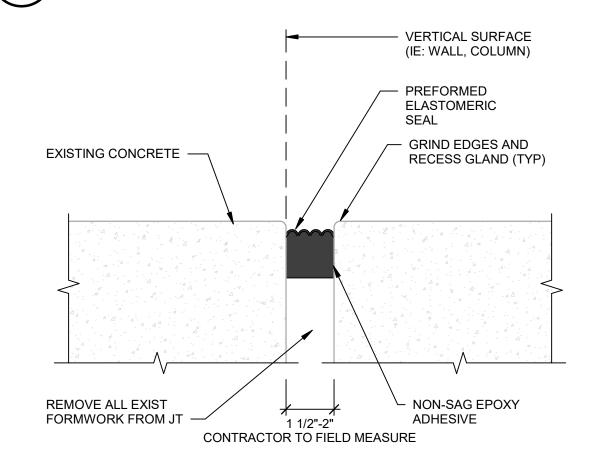
### REPLACE / INSTALL CRACK SEALANT (CRACKS 0.030" OR GREATER)



NOTES:

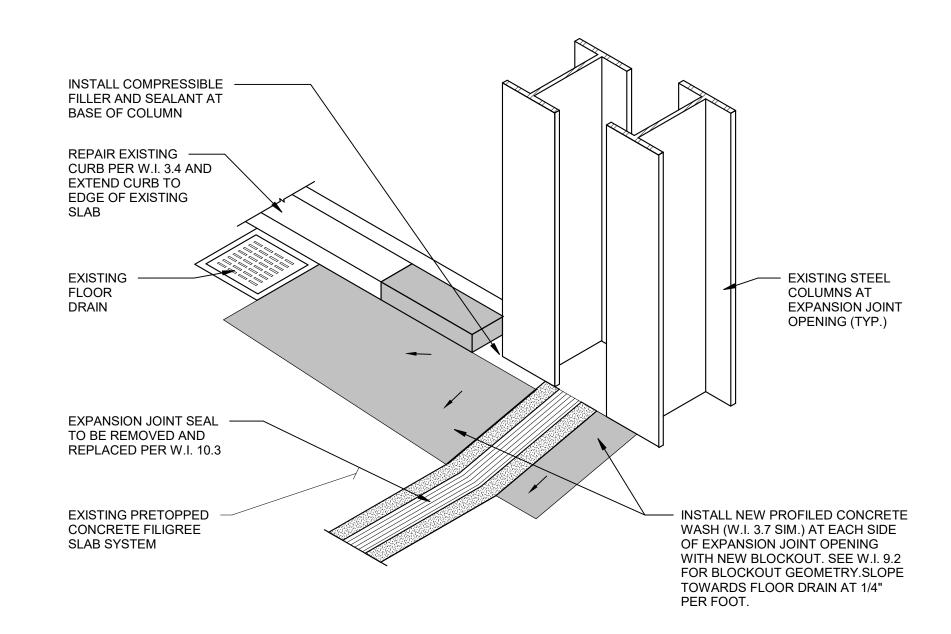
1. FOR 1/2" WIDE JOINT SEALANT REAPLCEMENT AT PRE-TOPPED FILIGREE PLANKS SEE W.I. 11.5 SIM.

### REMOVE AND REPLACE CONTROL JOINT SEALANT 1/2" WIDE

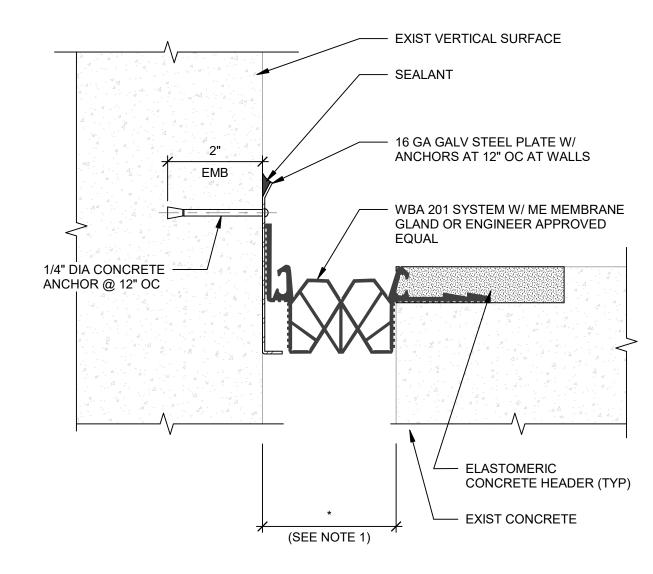


NOTES:
1. CONTRACTOR SHALL, WITH MANUFACTURER, VERIFY FIELD INSTALLATION WIDTH BASED ON TEMPERATURE CONDITIONS. 2. ALL INTERSECTIONS AND 90° CORNERS TO BE FIELD FABRICATED PER MANUFACTURER'S DETAILS AND WRITTEN INSTRUCTIONS.

### **EXPANSION JOINT- ADHERED**



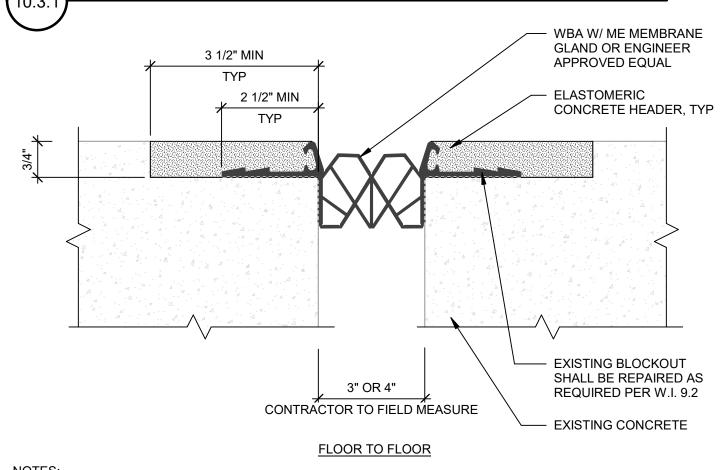
**EXPANSION JOINT TERMINATION** AT STEEL COLUMNS



FLOOR TO VERTICAL NOTES:

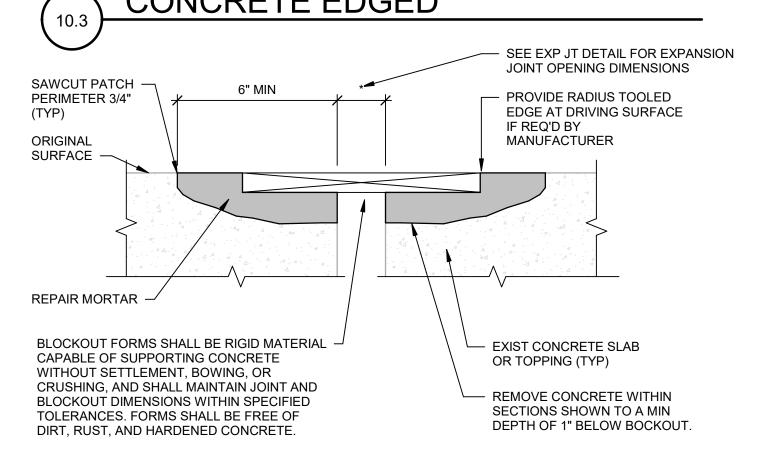
1. SEE DETAIL 10.3 FOR REMAINING INFORMATION.

### **EXPANSION JOINT- ELASTOMERIC** CONCRETE EDGED



NOTES:
1. CONTRACTOR SHALL, WITH MANUFACTURER, VERIFY FIELD INSTALLATION WIDTH BASED ON TEMPERATURE CONDITIONS. 2. PROVIDE SHOP DRAWINGS SHOWING JOINT INTERSECTIONS AND WALL TERMINATIONS.

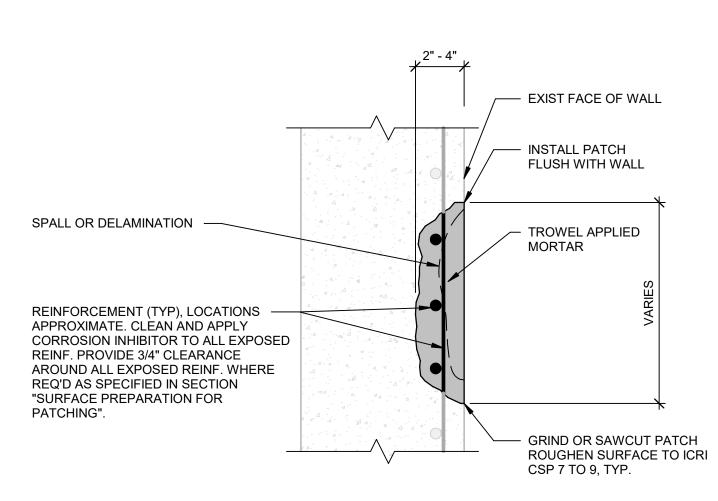
### **EXPANSION JOINT-ELASTOMERIC** CONCRETE EDGED



NOTES:

1. MAINTAIN EXPANSION JOINT OPENING CLEAR OF DEBRIS. VERTICAL SURFACE MUST BE PLUMB AND SHALL NOT VARY MORE THAN 1/8" IN 6". EDGE STRAIGHTNESS TOLERANCE SHALL NOT VARY MORE THAN 1/4" IN 20'-0" METHOD OF FORMING JOINT SHALL BE APPROVED BY ENGINEER PRIOR TO CONCRETE PLACEMENT. 4. CONCRETE ELEVATIONS ON EACH SIDE OF JOINT SHALL MATCH.

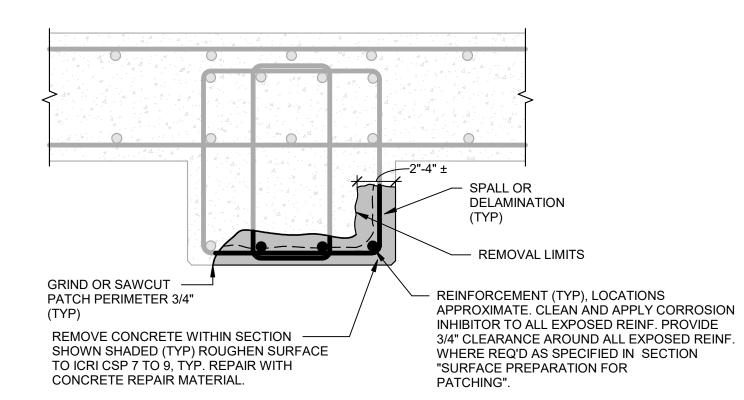
> **EXPANSION JOINT PREPARATION-**BLOCKOUT REPAIR



NOTES:

1. CONTRACTOR SHALL TEMPORARILY SHORE PLANKS AND BEAMS PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK.

### WALL REPAIR-PARTIAL DEPTH/SHALLOW

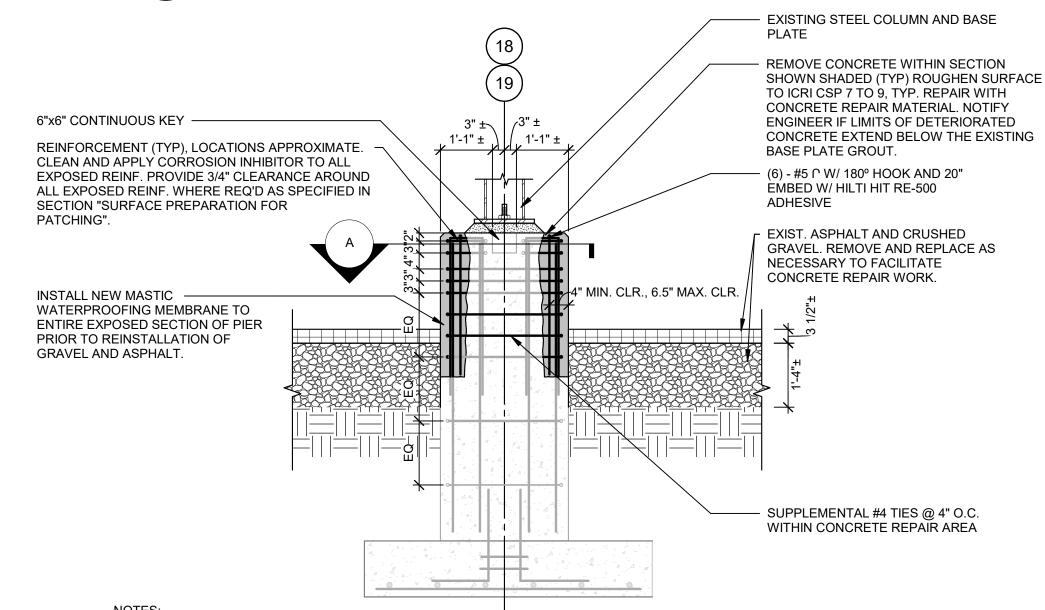


NOTES:

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- 2. COLUMN TIES WHICH HAVE LOST MORE THAN 15% OF ORIGINAL CROSS SECTIONAL AREA SHALL BE SUPPLEMENTED AS ENGINEER DIRECTS.
- 3. NUMBER AND LOCATION OF REINFORCEMENT SHOWN MAY DIFFER FROM ACTUAL FIELD CONDITIONS. 4. DO NOT CUT OR DAMAGE REINFORCING. NOTIFY ENGINEER IF EXISTING REINFORCING STEEL APPEARS TO HAVE OVER 20% SECTION LOSS OR TWO ADJACENT BARS HAVE 10% SECTION LOSS. ENGINEER SHALL DETERMINE DURING CONSTRUCTION IF REINFORCING REPAIRS ARE REQUIRED.

### COLUMN REPAIR-PILASTER

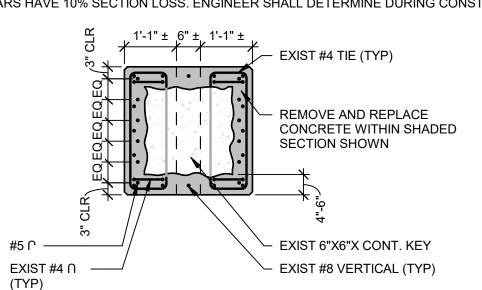


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- NUMBER AND LOCATION OF REINFORCEMENT SHOWN MAY DIFFER FROM ACTUAL FIELD CONDITIONS 3. COLUMN TIES WHICH HAVE LOST MORE THAN 15% OF ORIGINAL CROSS SECTIONAL AREA SHALL BE SUPPLEMENTED AS ENGINEER
- 4. DO NOT CUT OR DAMAGE REINFORCING. NOTIFY ENGINEER IF EXISTING REINFORCING STEEL APPEARS TO HAVE OVER 20% SECTION LOSS OR TWO ADJACENT BARS HAVE 10% SECTION LOSS. ENGINEER SHALL DETERMINE DURING CONSTRUCTION IF REINFORCING REPAIRS ARE REQUIRED.

**SECTION A** 

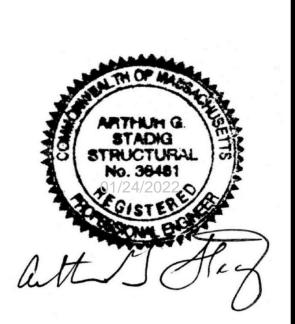


COLUMN REPAIR - PIER BASE AT BRACED FRAME



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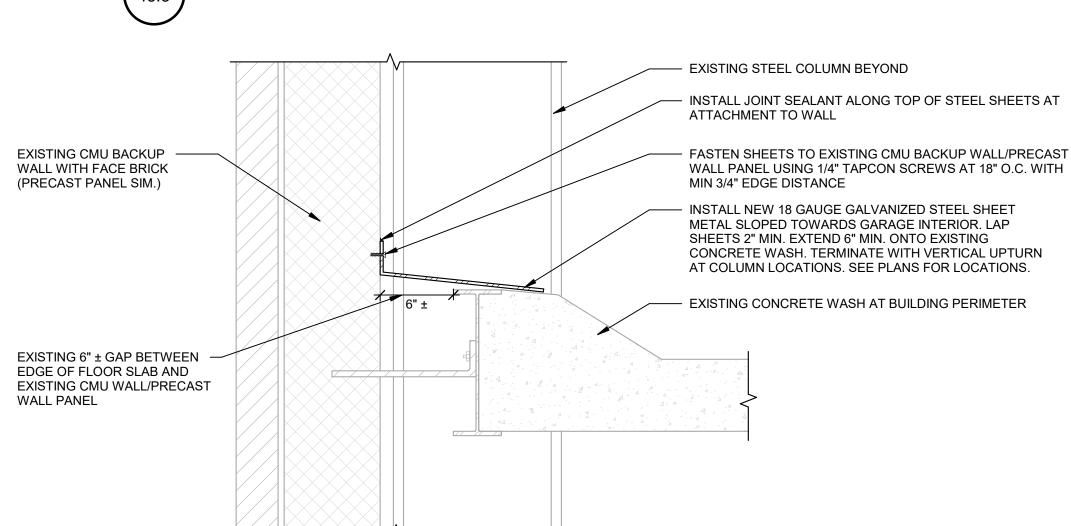
HANOVER STREET **GARAGE RESTORATION** 

34 HANOVER STREET. PORTSMOUTH, NH 03801

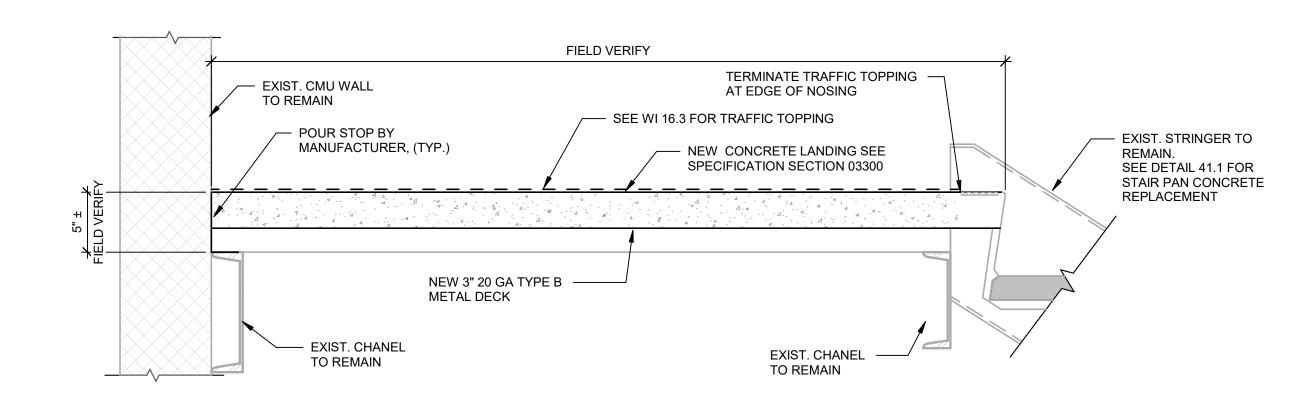
MARK	DATE	DESCRIPTION	
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ISSUE: BIDDING AND CONSTRUCTION			
ISSUE DATE:		1/25/2022	
PROJECT NO:		16-003129.00	
DRAWN BY:		NCH	
CHECKED BY:		MAZ	
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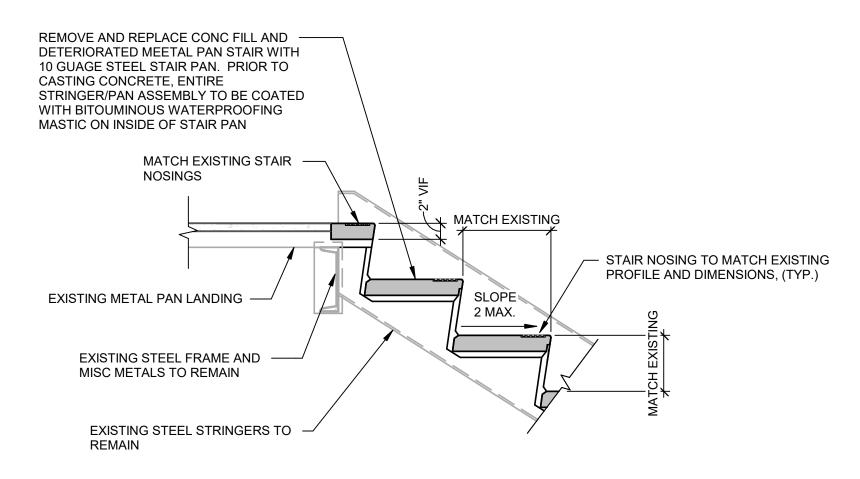


# INSTALL STEEL PLATE AT BUILDING PERIMETER GAP



CONTRACTOR SHALL INSTALL TEMPORARY SHORING PRIOR TO DEMOLITION PER WI 1.3. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD.
 VERIFY ALL DIMENSIONS IN FIELD PRIOR TO INSTALLATION OF NEW METAL DECK AND CONCRETE FILL.
 FASTEN NEW METAL DECK TO EXISTING STURCTURAL FRAMING PER MANUFACTURERES REQUIREMENTS.

REPLACE LANDING/FILL



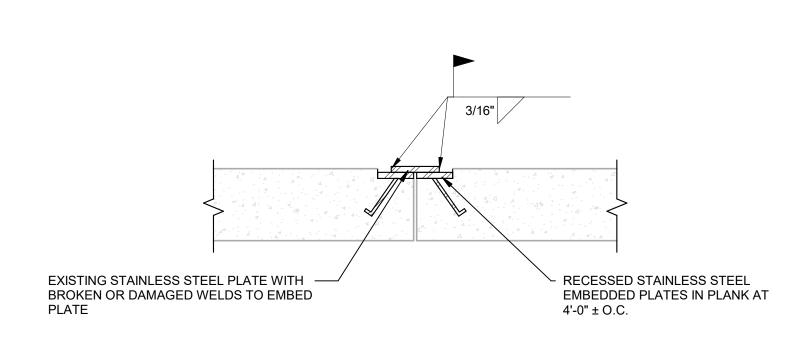
NOTES:

1. VERIFY ALL DIMENSIONS IN FIELD PRIOR TO FABRICATION OF METAL STAIR PANS.

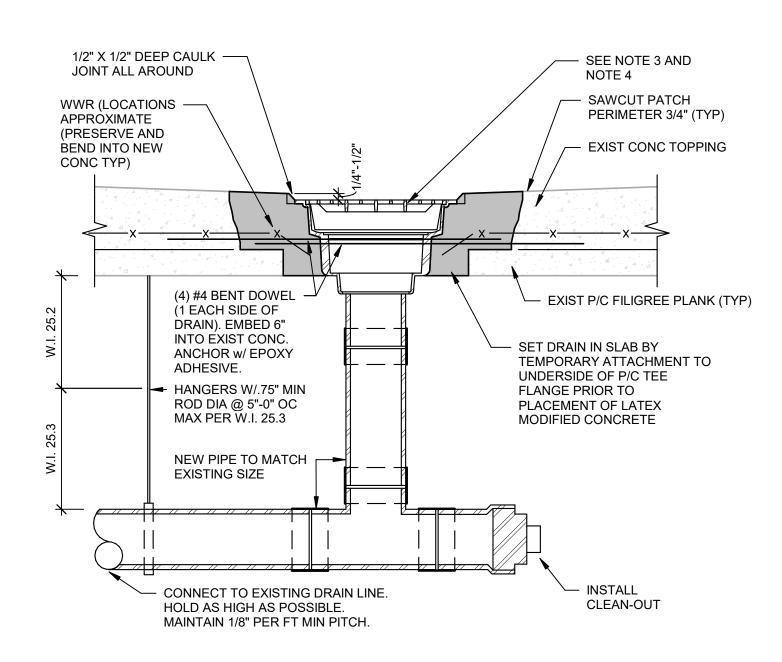
2. ALL IN PLACE CONSTRUCTION AFFECTED BY STAIR PAN REPLACEMENT TO BE CLEANED AND PAINTED. COLOR AND FINISH TO MATCH EXISTING.

3. FASTEN NEW STAIR PANS TO EXISTING STRUCTURAL STEEL PER MANUFACTURER'S

# REPLACE STAIR RISER /PAN CONCRETE FILL



### RE-WELD SHEAR CONNECTOR



NOTES:

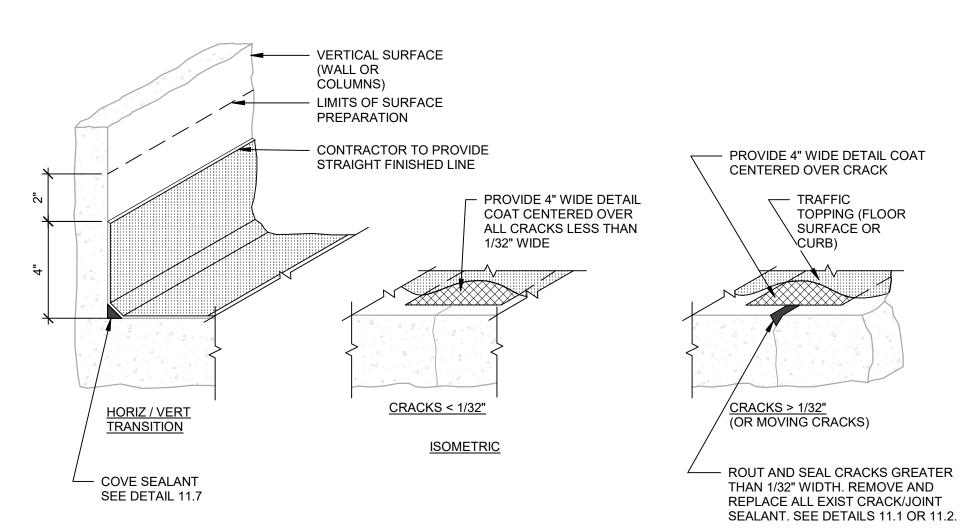
1. USE SERVICE WEIGHT CAST IRON PIPE. ALL MATERIALS SHALL BE IN ACCORDANCE WITH PREVAILING BUILDING

- INSTALL ADJUSTABLE HANGERS AS REQUIRED, VERIFY MINIMUM HEIGHT RESTRICTIONS OF PREVAILING BUILDING ORDINANCES PRIOR TO INSTALLATION.
- ALL DRAINS SHALL BE HOT DIP GALVANIZED.
   ORIGINAL GARAGE AREA 12" ROUND FLOOR DRAINS:
- A. ZURN Z610

  B. ENGINEER APPROVED EQUIVALENT

  CARACT EXPANSION APEA 40% COLLARS SLOOP PRAINS:
- 5. GARAGE EXPANSION AREA 12" SQUARE FLOOR DRAINS:
   A. ZURN Z662
   B. ENGINEER APPROVED EQUIVALENT
- 6. GARAGE EXPANSION AREA 6" SQUARE FLOOR DRAINS:
   A. WADE 1000 TX
   B. ENGINEER APPROVED EQUIVALENT

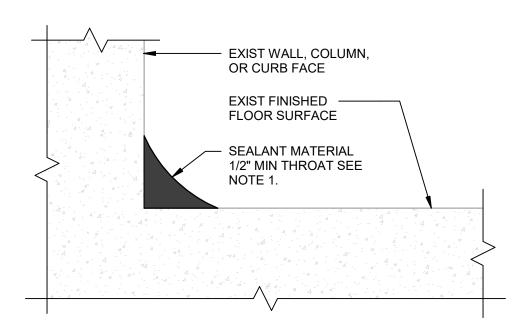
# MECHANICAL-SUPPLEMENTAL/REPLACEMENT FLOOR DRAIN/PIPE & HANGERS



NOTES:

1. QUANTITIES BASED ON HORIZONTAL APPLICATION AREA. VERTICAL DETAILING, ADDITIONAL DETAIL COAT OVER CRACKS, ROUTING AND SEALING CRACKS, INSTALLATION OF COVE SEALANT, AND REMOVAL AND REPLACEMENT OF EXISTING SEALANTS ARE INCIDENTAL TO THIS WORK.

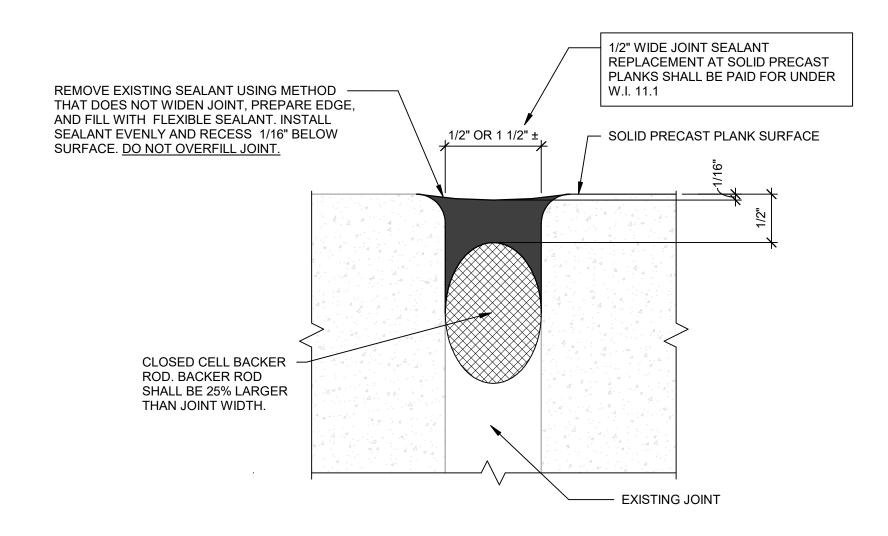
### TRAFFIC TOPPING



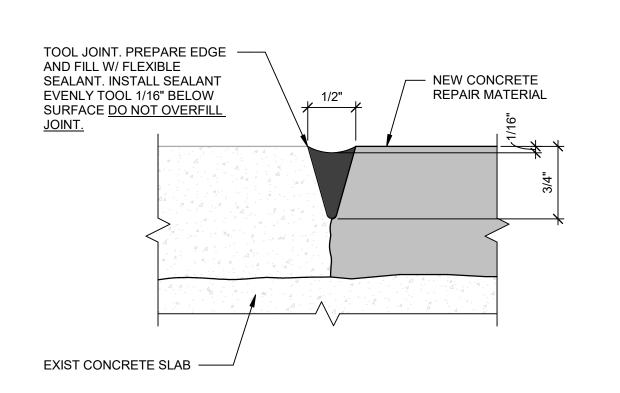
NOTES:

1. REMOVE EXISTING COVE SEALANT MATERIAL IF PRESENT. PREPARE SURFACE PER SPECIFICATIONS.

## (INCIDENTAL TO W.I. SERIES 16.0)



# REMOVE AND REPLACE CONTROL JOINT SEALANT 1 1/2" WIDE



NOTES:
 PLACE CONTROL JOINTS AS SHOWN ON PLANS.
 ADDITIONAL OVERLAY CONTROL JOINTS WILL BE REQUIRED OVER ALL ORIGINAL CONSTRUCTION JOINTS.
 CONTRACTOR SHALL LOCATE ALL SUCH JOINTS PRIOR TO OVERLAY INSTALLATION.
 CONTROL JOINT (C.J.) MUST BE TOOLED IN PLASTIC CONCRETE AND DIMENSIONS AS DETAILED WILL BE MAINTAINED IN SET CONCRETE. SAWCUTTING OF CONTROL JOINT IN HARDENED CONCRETE WILL NOT BE

# TOOL & SEAL PATCH PERIMETERS AND CONTROL JOINTS

(INCIDENTAL TO W.I. SERIES 3.0)

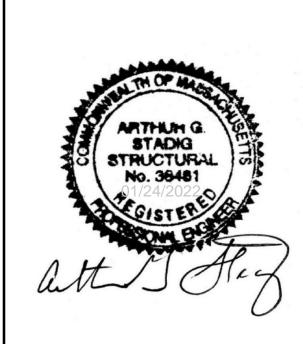
WALK CONSULT 20 Park Plaza, Suite 1202

Boston MA 02116

617.350.5040 Ph

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HANOVER STREET GARAGE RESTORATION

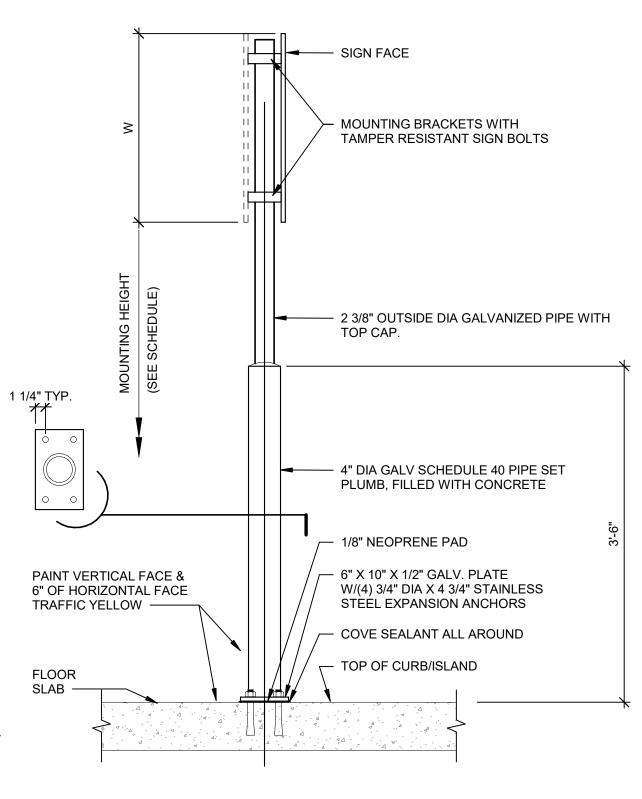
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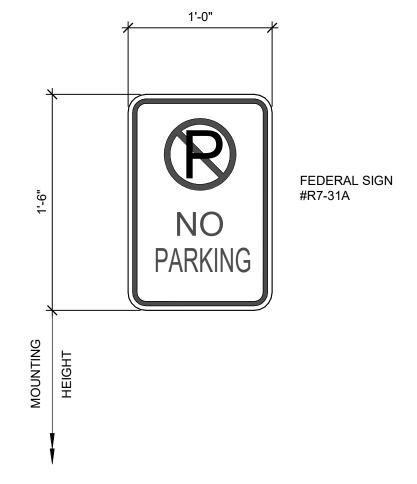
REPAIR DETAILS

R-503

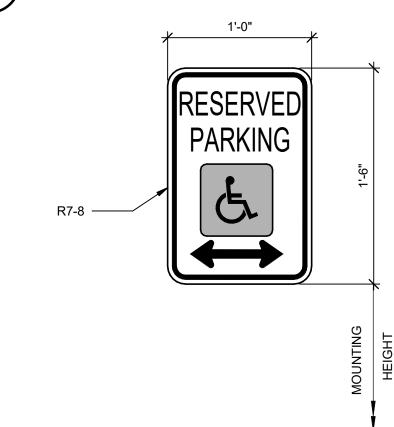
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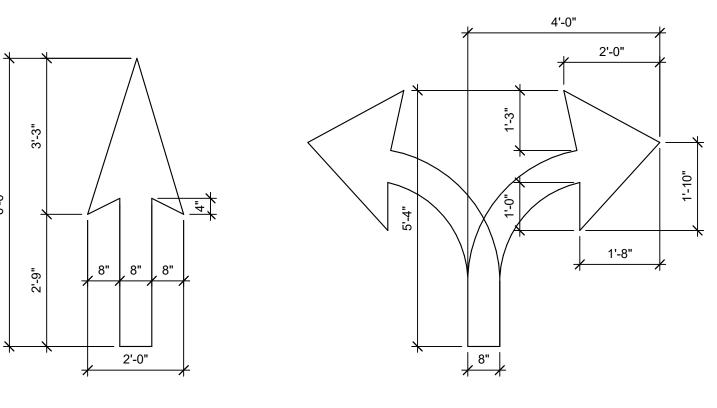
### SIGN DETAIL MOUNT - PIPE



NO PARKING (FEDERAL)

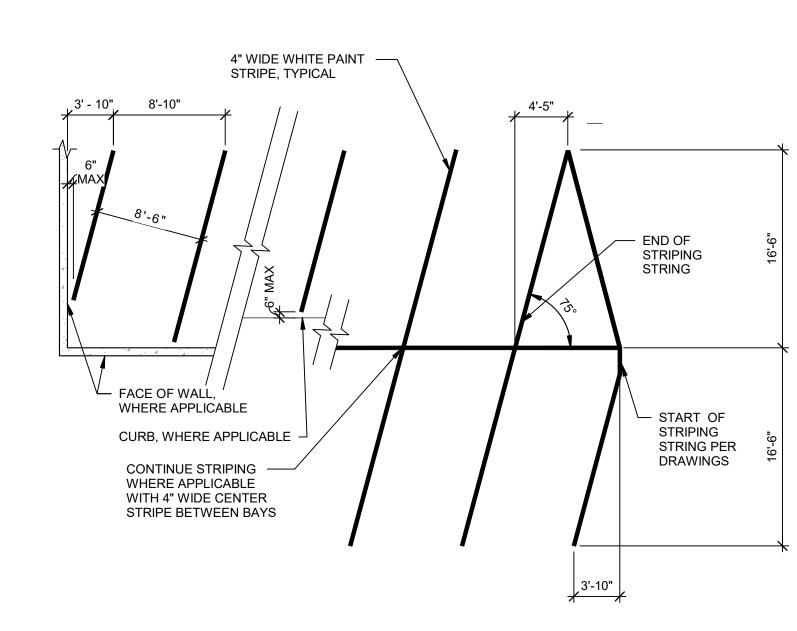


### RESERVED PARKING (FEDERAL)

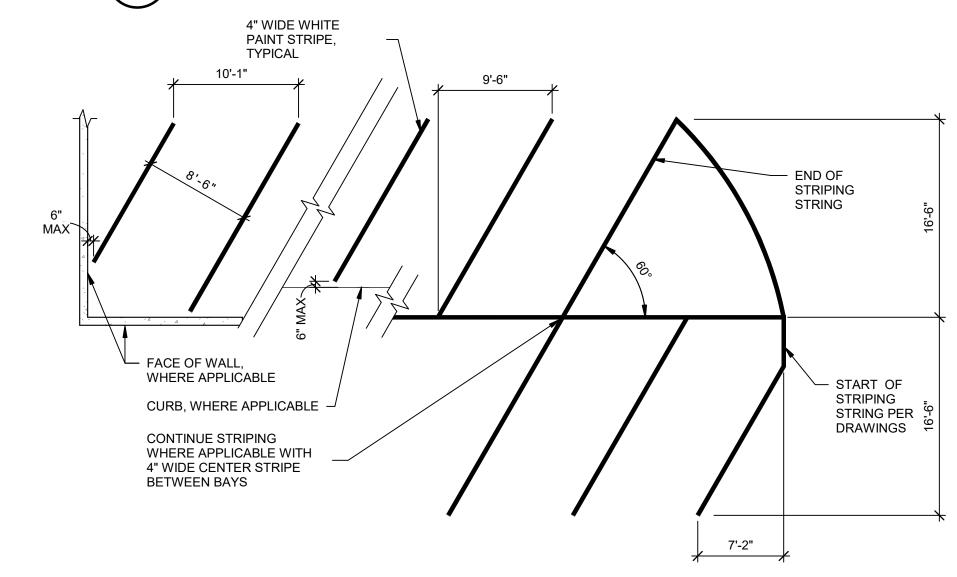


NOTE: PAINT ARROWS SOLID YELLOW, UNLESS NOTED OTHERWISE.

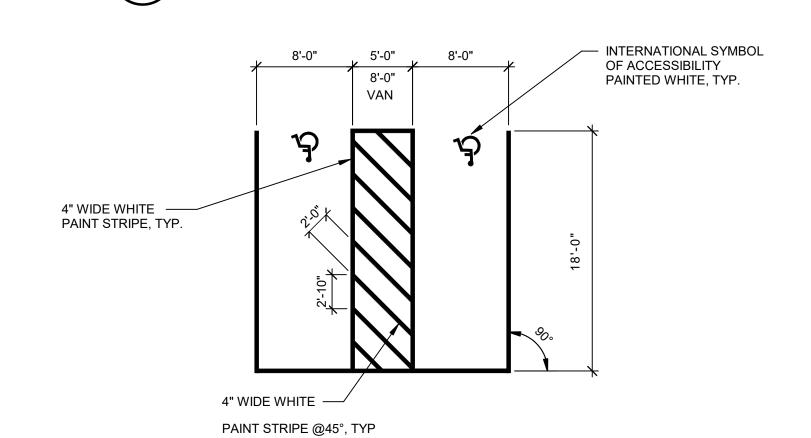
TYPICAL ARROW DETAIL



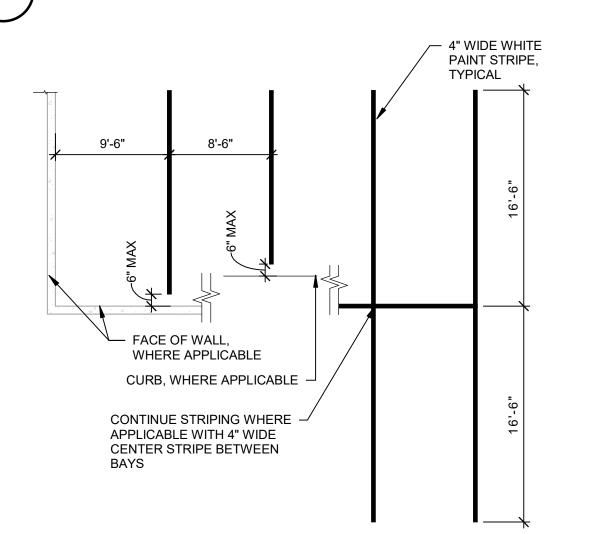




### 60° STRIPING DETAIL



### ACCESSIBLE STRIPING DETAIL

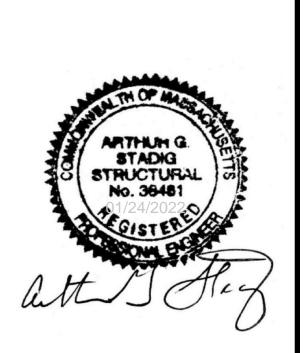


PAINT TRAFFIC MARKINGS





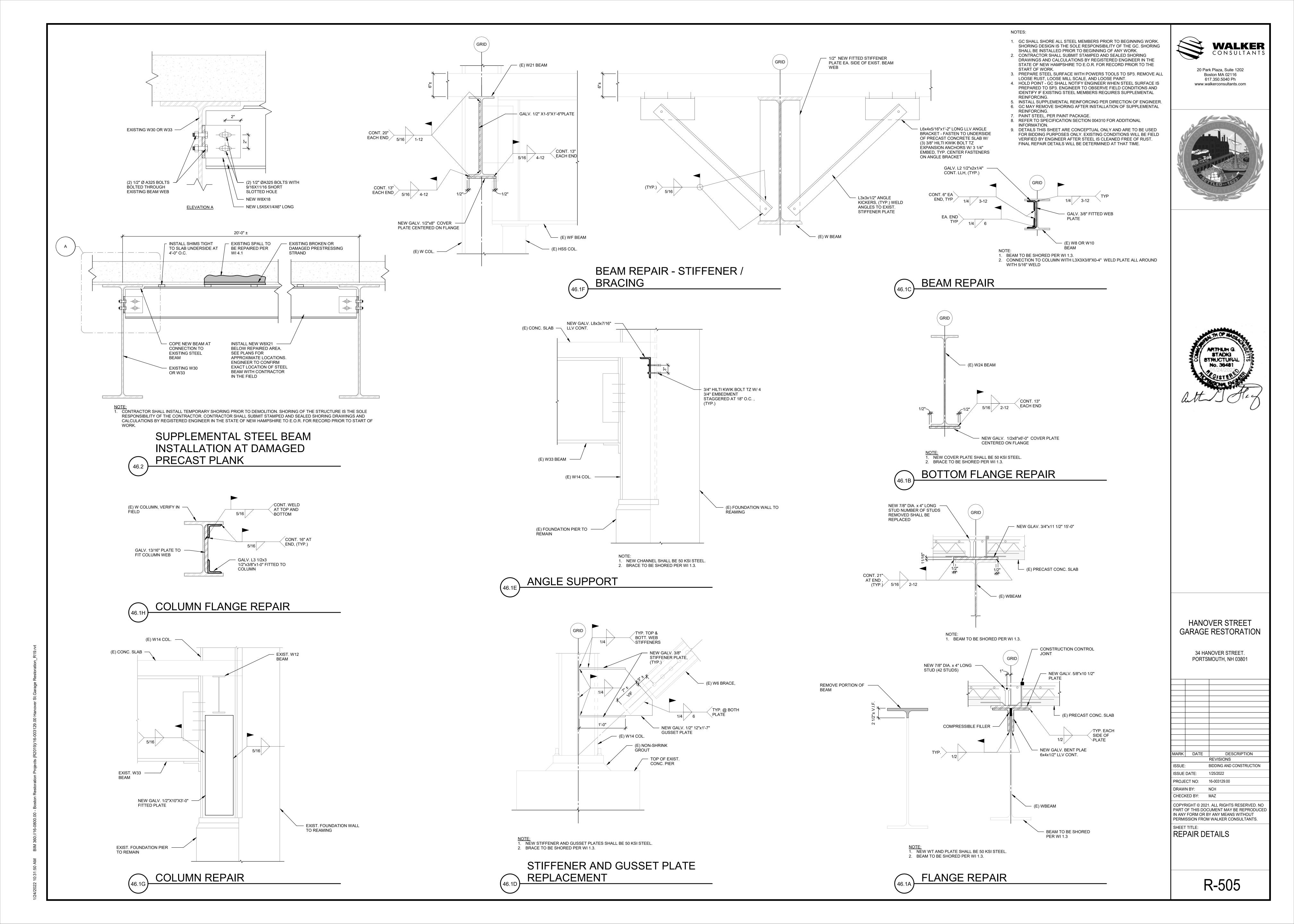
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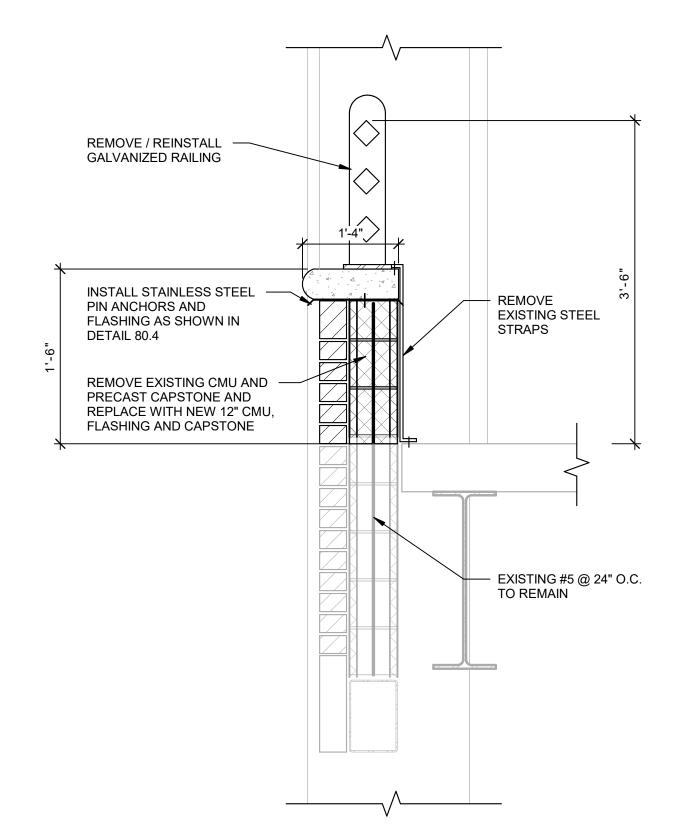
HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET. PORTSMOUTH, NH 03801

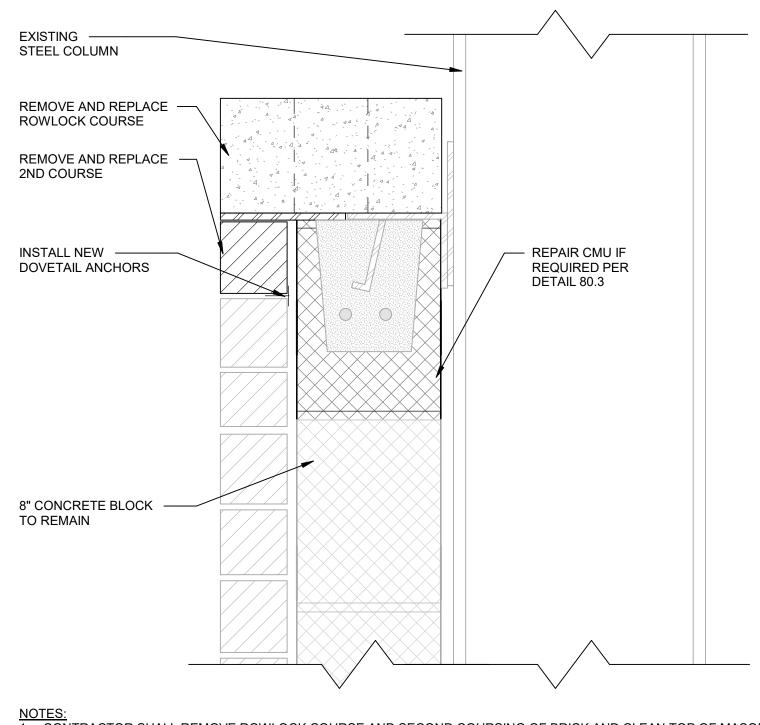
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· · · · ·	PAIR DE	ETAILS



# REPLACE WINDOW FRAME/FACADE PERIMETER JOINT



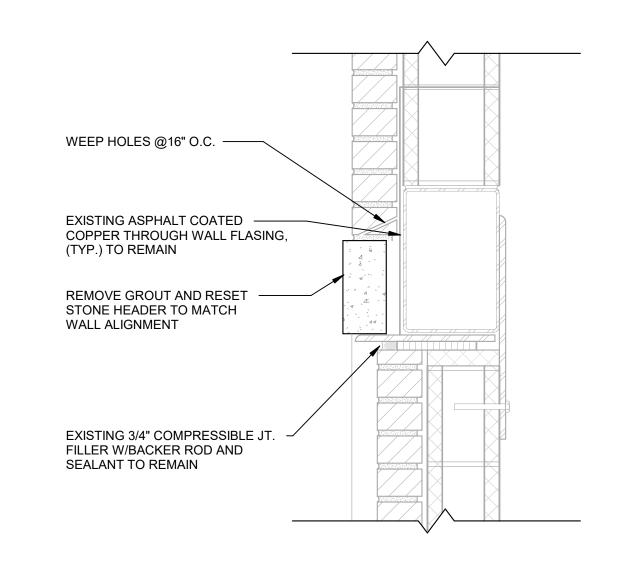
# REMOVE/REPLACE PARAPET WALL W/TOP RAIL



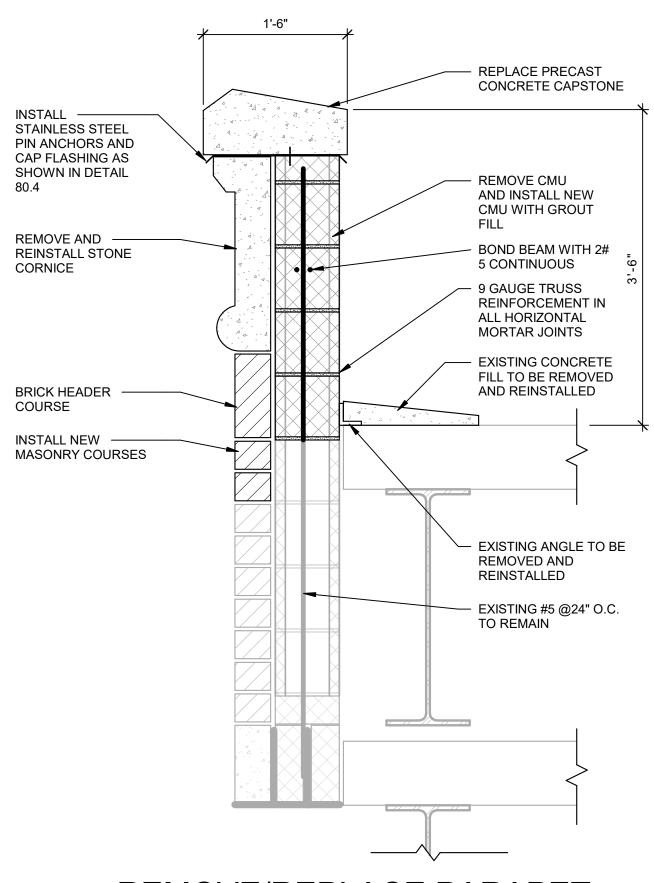
NOTES:

1. CONTRACTOR SHALL REMOVE ROWLOCK COURSE AND SECOND COURSING OF BRICK AND CLEAN TOP OF MASONRY WALL
2. CONTRACTOR TO INSERT GALVANIZED DOVETAIL ANCHOR INTO MASONRY JOINT AND ATTACH TO CMU BACKUP AT 24" O.C.
3. INSTALL BRICK COURSING CONSISTENT WITH EXISTING BRICK PATTERN. TOOL JOINTS TO MATCH EXISTING.
4. APPLY SEALER TO ROWLOCK COURSING FOLLOWING MORTAR CURE.

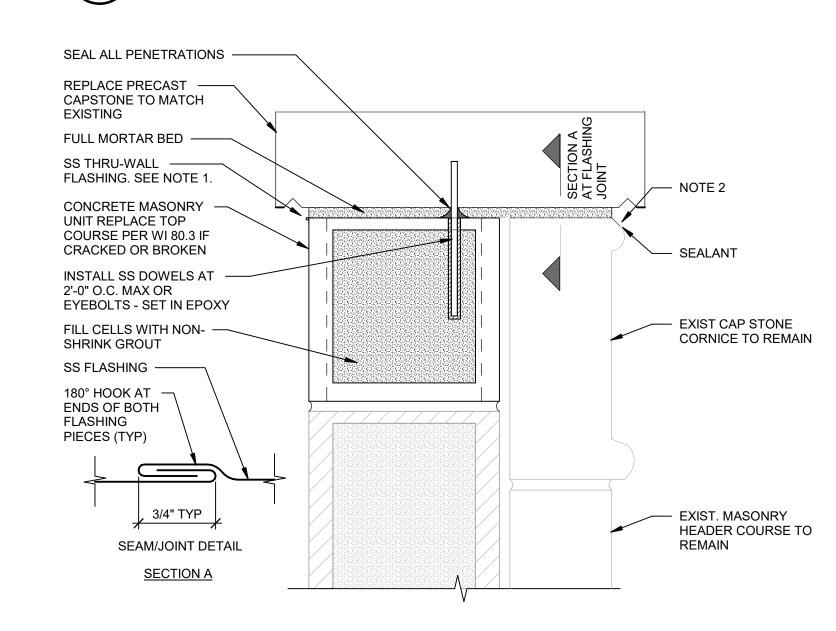
REMOVE AND REPLACE
ROWLOCK CAP



### RESET STONE UNIT



# REMOVE/REPLACE PARAPET WALL



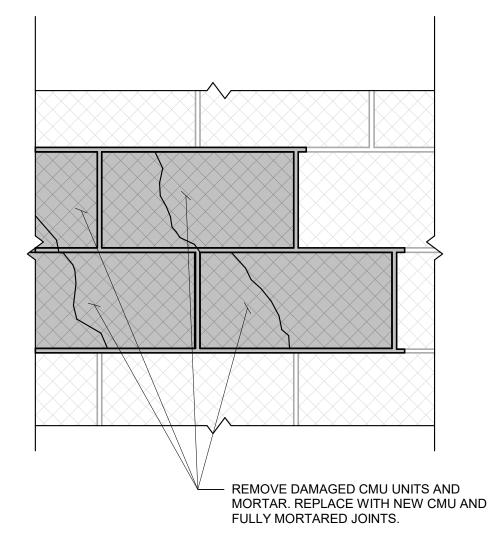
NOTES:

1. FLASHING SECTIONS SHALL BE ASSEMBLED WITH WATERTIGHT SEAMS TO FORM AN IMPERVIOUS BARRIER THAT ALLOWS WATER TO WEEP FROM FLASHING SURFACE.

2. BEND 1/2" EDGE PORTION OF FLASHING DOWNWARD TO FORM DRIP EDGE. DRIP EDGE TO

BE HEMMED.
3. CUT AND FIELD BEND FLASHING TO ACCOMMODATE PARAPET WIDTH VARIATIONS.
4. IN THE CASE THAT FOR ANY REASON SMOOTH (NON-INTERLOCKING) SS FLASHING IS USED, CONTRACTOR MUST PROVIDE (2) SS 3/8" DIA PINS PER CAPSTONE TO LATERALLY SECURE STONES. PROVIDE APPROVED MASTIC/SEALANT WHERE PINS PENETRATE FLASHING.

# REMOVE AND REPLACE CAPSTONE/FLASHING



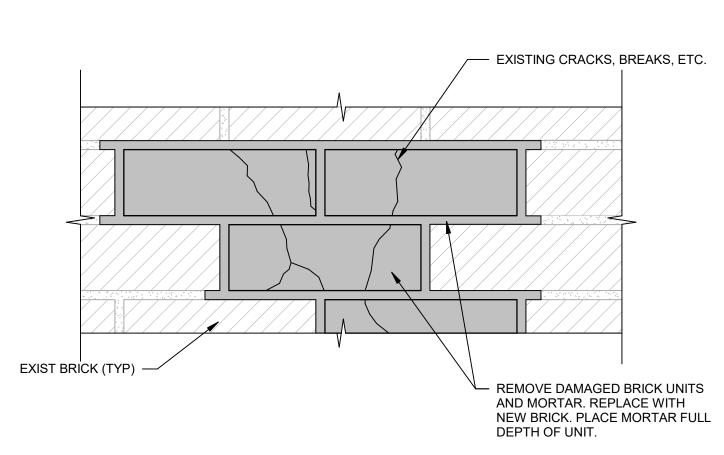
NOTES:

1. CONTRACTOR SHALL SUPPORT REMAINING AREAS OF CMU DURING REPAIRS.

2. PAY UNIT INCLUDES PROVIDING CORNER UNITS, BOND BEAM UNITS, ETC. AS REQUIRED TO REPAIR ALL DAMAGED CMU.

3. INCIDENTAL TO THIS WORK IS PROVIDING NEW TRUSS OR LADDER TYPE REINFORCEMENT TO MATCH EXISTING. LAP MINIMUM ONE BLOCK LENGTH.

# REMOVE AND REPLACE CONCRETE MASONRY UNIT



ELEVATION

NOTES:

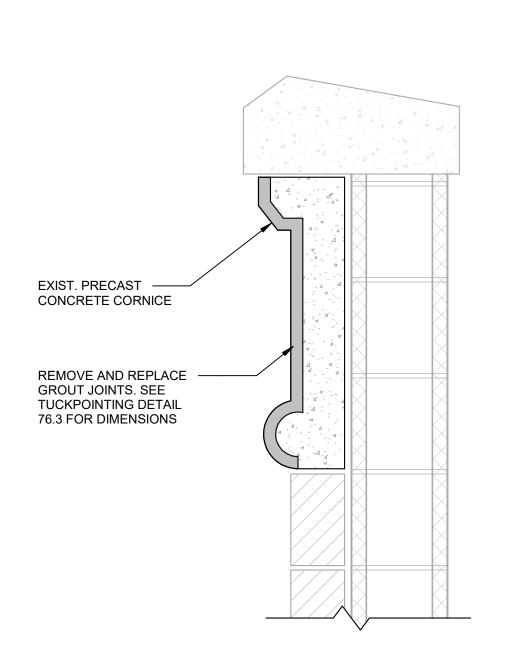
1. MORTAR JOINT REPLACEMENT AT BRICK MASONRY UNIT REPLACEMENT LOCATIONS IS INCIDENTAL, NO EXTRA PAYMENT UNDER TUCKPOINTING WORK ITEM.

2. DO NOT DAMAGE ADJACENT BRICK BY OVERCUTTING.

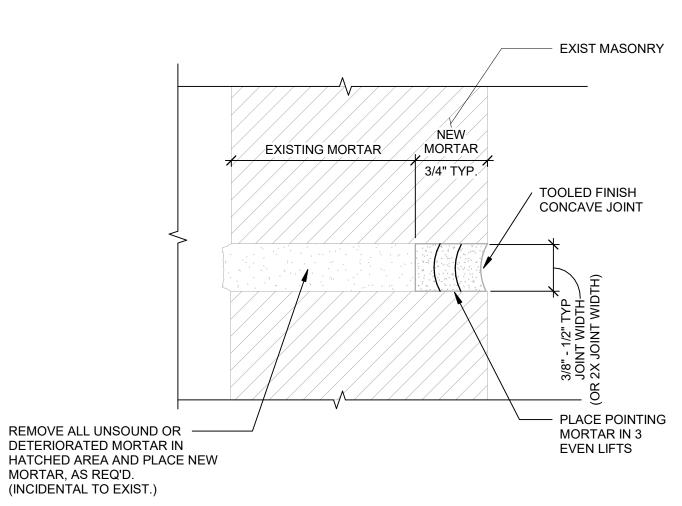
3. EXISTING BRCK TO MATCH EXISTING BRICK AND SAMPLE TO BE SUBMITTED AND APPROVED

# REMOVE AND REPLACE FACE BRICK

BY ENGINEER AND/OR OWNER PRIOR TO START OF WORK



GROUT JOINT REPAIR - CORNICE



NOTES:

1. REMOVE LOOSE MORTAR MATERIAL FROM JOINT.

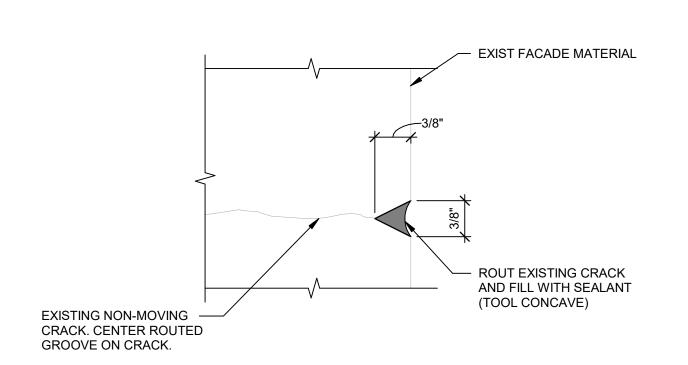
2. FILL AND VOIDS IN JOINT BEYOND POINTING WORK DEPTH.

3. GRIND BRICK SURFACES CLEAN OF EXISTING MORTAR. DO NOT DAMAGE ADJACENT MASONRY.

4. INSTALL MORTAR IN THREE LIFTS, COMPACTING EACH LIFT.

5. TOOL MORTAR JOINT CONCAVE.

### TUCKPOINTING



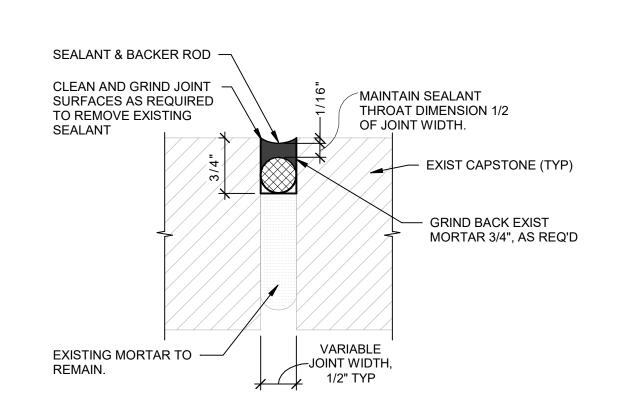
NOTES:

1. PRIME JOINT AS REQUIRED BY SEALANT MANUFACTURER.

2. INSTALL SEALANT EVENLY AND TOOL JOINT CONCAVE. WET TOOLING WILL NOT BE ALLOWED.

3. SUBMIT COLOR SAMPLES TO OWNER FOR APPROVAL PRIOR TO START OF WORK.

### ROUT AND SEAL FAÇADE CRACK



NOTES:

1. REMOVE ALL EXISTING SEALANT AND BACKER MATERIAL (IF PRESENT) AT EACH CAPSTONE JOINT.

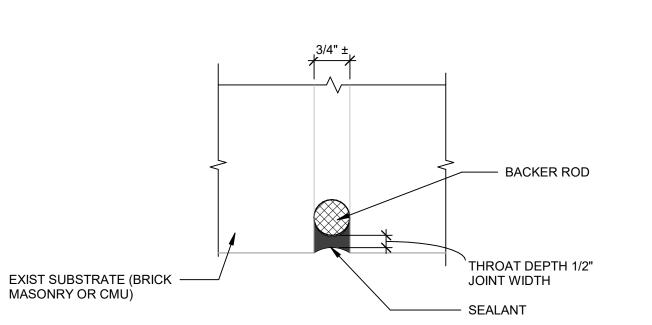
2. PRIME JOINT AS REQUIRED BY SEALANT MANUFACTURER.

3. INSTALL SEALANT AND TOOL JOINT CONCAVE 1/16" BELOW EXISTING SURFACE.

4. DO NOT SEAL CAPSTONE BED JOINT. WET TOOLING WILL NOT BE ALLOWED.

5. SUBMIT COLOR SAMPLES FOR APPROVAL PRIOR TO START OF WORK.

### CAPSTONE JOINT REPAIR



NOTES:
 REMOVE ALL EXISTING SEALANT AND BACKER ROD AS REQUIRED TO EXPOSE BASE FACADE. WIDEN JOINT AS REQUIRED TO MAINTAIN SEALANT WIDTH.
 CLEAN ALL DUST AND DEBRIS FROM JOINT, MAKE SURE JOINT IS CLEAN AND DRY.
 PREPARE AND PRIME SEALANT CAVITY ACCORDING TO SEALANT MANUFACTURER'S RECOMMENDATIONS.

ISOLATION JOINT SEALANT REPAIR







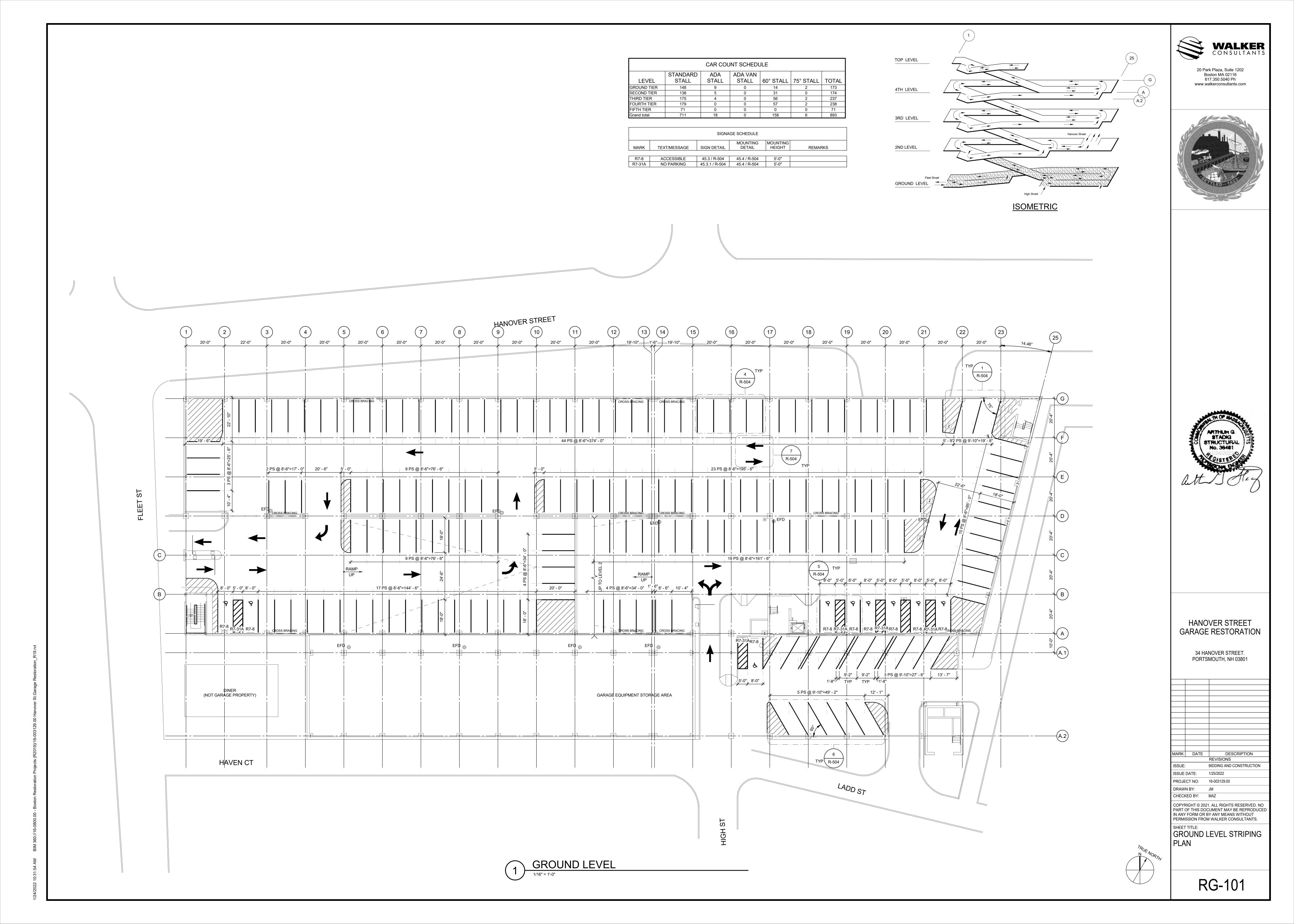
HANOVER STREET GARAGE RESTORATION

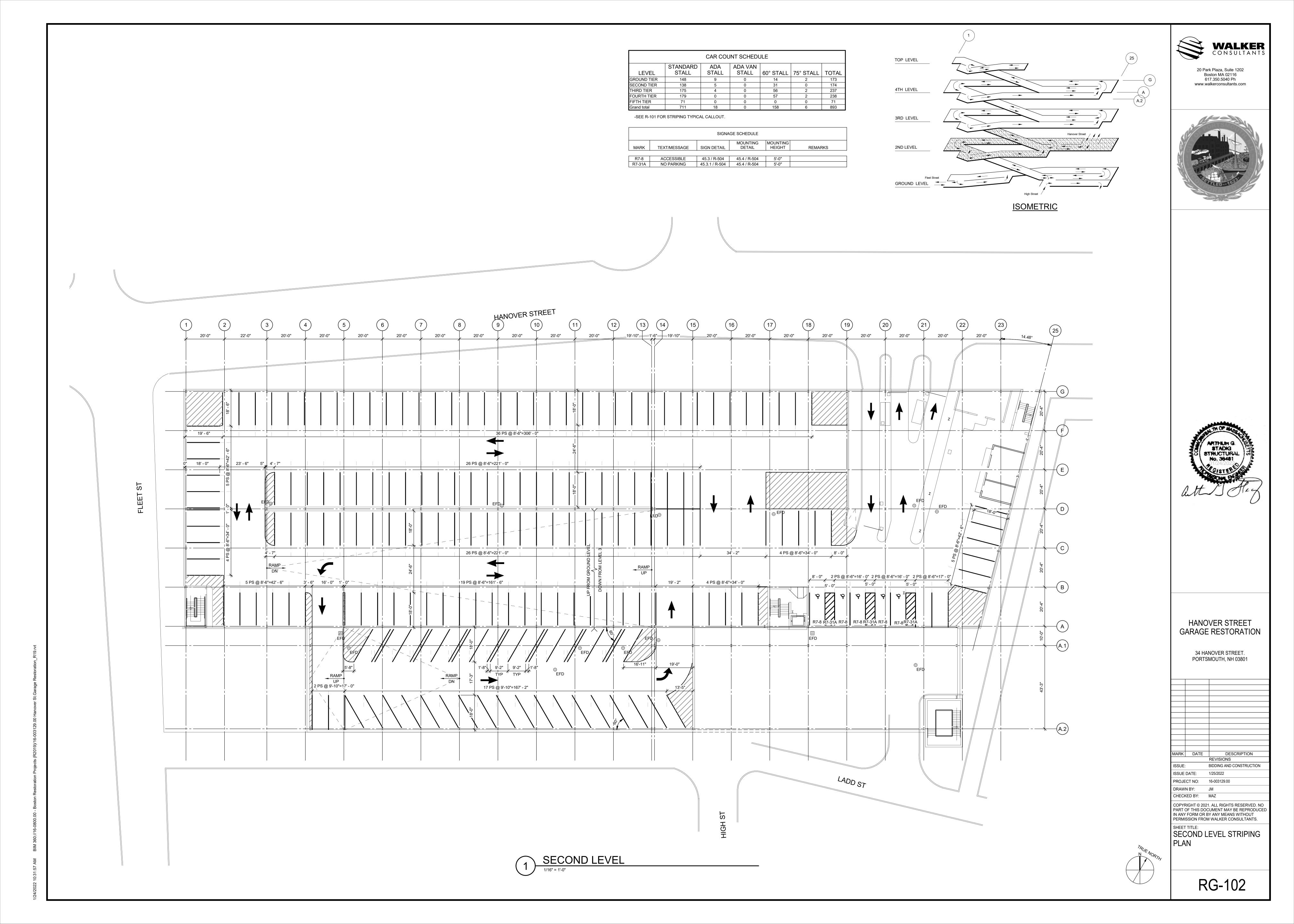
> 34 HANOVER STREET. PORTSMOUTH, NH 03801

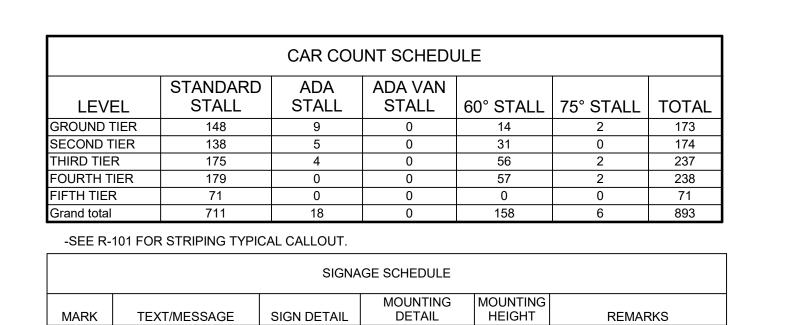
MARK	DATE	DESCRIPTION	
WARK	DATE		
REVISIONS			
ISSUE: BIDDING AND CONSTRUCTION			
ISSUE DATE: 1/25/2022			
PROJECT NO:		16-003129.00	
DRAWN BY:		NCH	
CHECKED BY: MA		MAZ	
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REMARKS

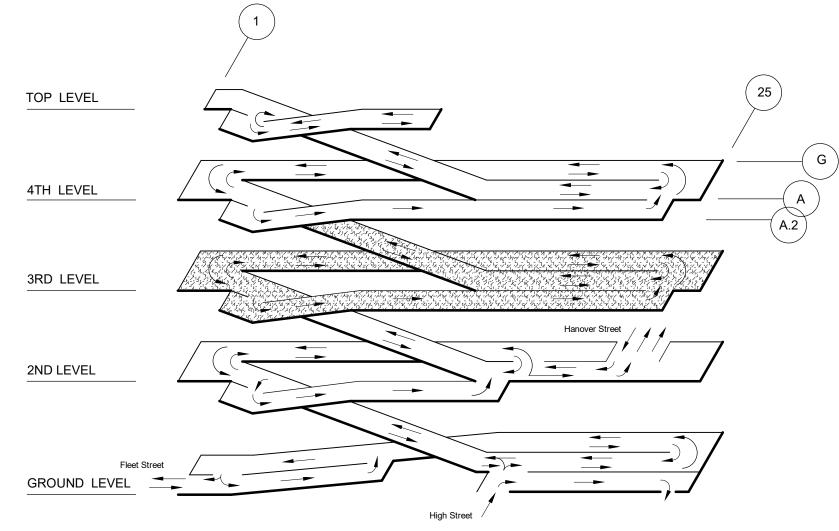
SIGN DETAIL

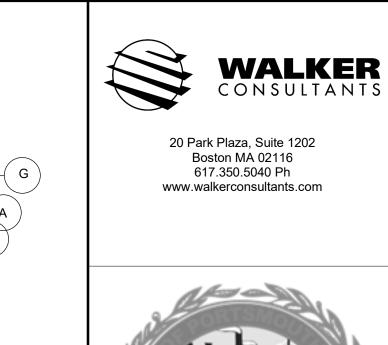
 R7-8
 ACCESSIBLE
 45.3 / R-504
 45.4 / R-504
 5'-0"

 R7-31A
 NO PARKING
 45.3.1 / R-504
 45.4 / R-504
 5'-0"

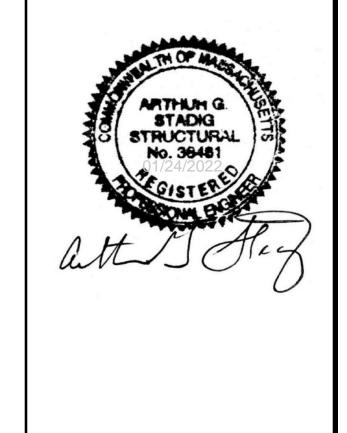
20'-0"

TEXT/MESSAGE









HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET. PORTSMOUTH, NH 03801

	MARK	DATE	DESCRIPTION
			REVISIONS
	ISSUE: ISSUE DATE: PROJECT NO:		BIDDING AND CONSTRUCTION
			1/25/2022
			16-003129.00
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	CHECKED BY:		MAZ

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SHEET TITLE:
THIRD LEVEL STRIPING

RG-103

75°- 2 PS @ 9'-10"=1/" - 8"  $\rightarrow$ 21' - 6" 5' - 0" 18' - 0" 40 PS @ 8'-6"=340' - 0" 5' - 6 1/2" 39 PS @ 8'-6"=331' - 6" RAMP \_\_\_\_ 2 - 2 8' - 0" 5' - 0" 8' - 0" 3 PS @ 8'-6"=25' - 6" 1' - 3" 23 PS @ 8'-6"=195' - 6" 8' - 0" 5' - 0" 8' - 0" 5 PS @ 8'-6"=42' - 6" 60°- 29 PS @ 9'-10"=285' - 2"

19'-10" 1'-6" 19'-10"

20'-0"

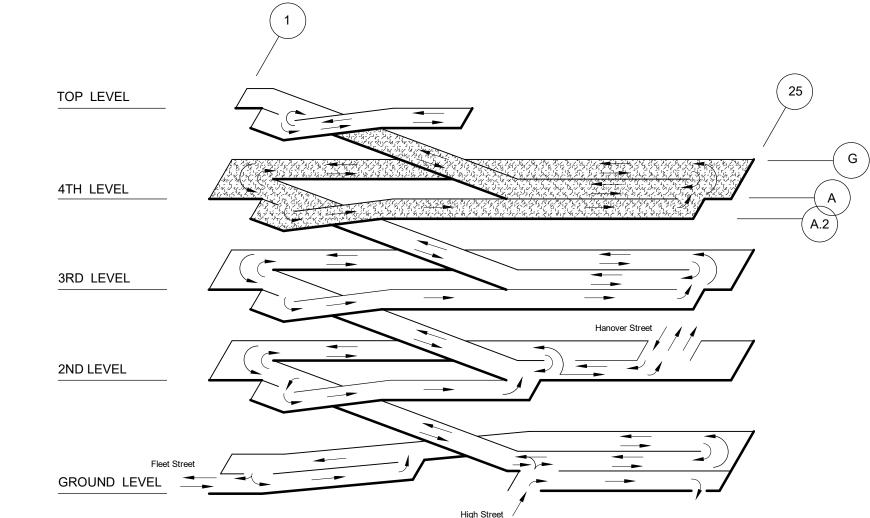
20'-0"

20'-0"

THIRD LEVEL

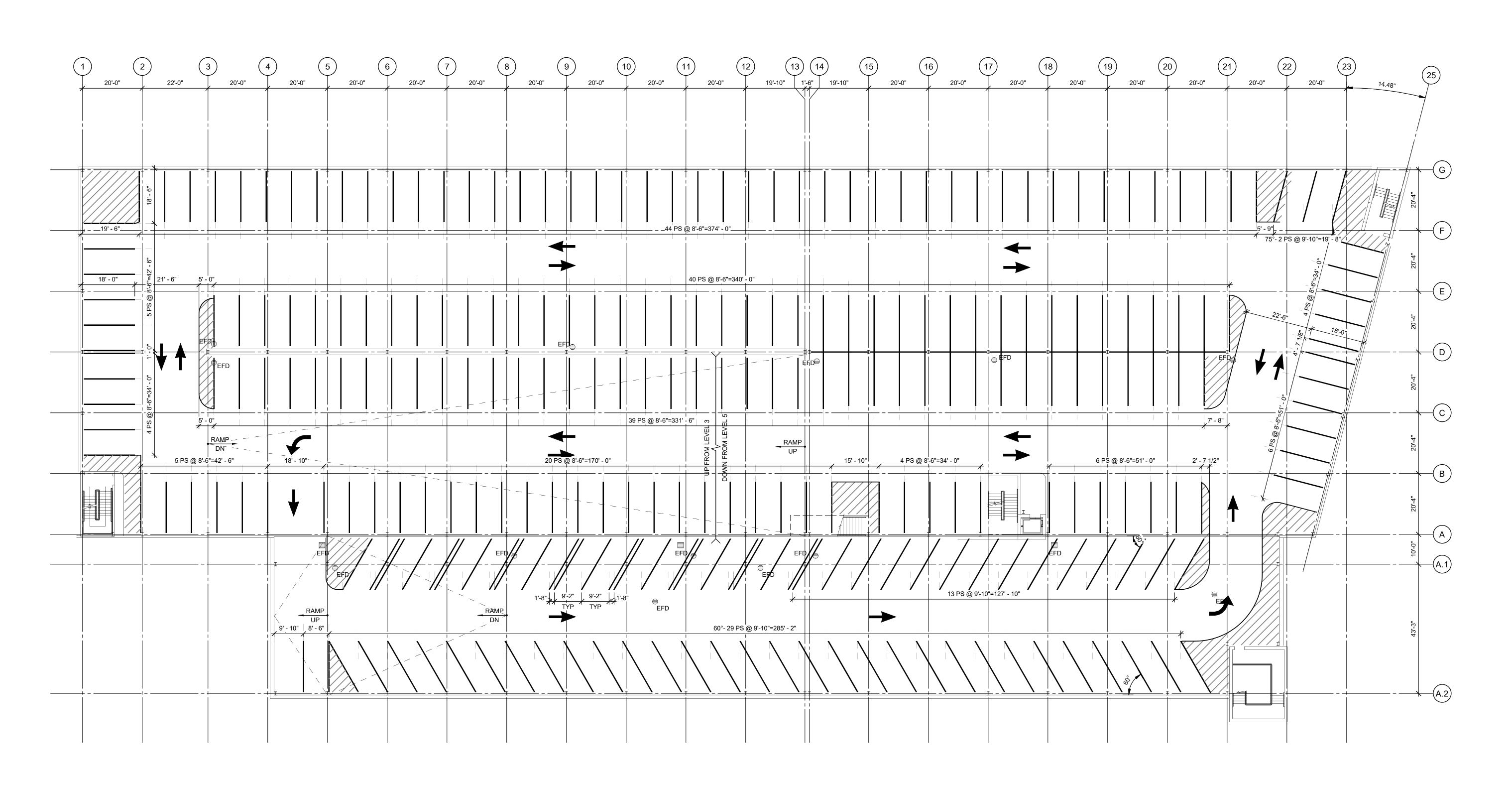
1/16" = 1'-0"

-SEE R-101 FOR STRIPING TYPICAL CALLOUT.



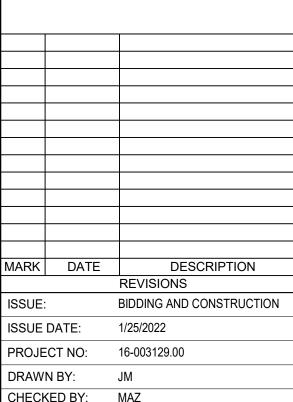






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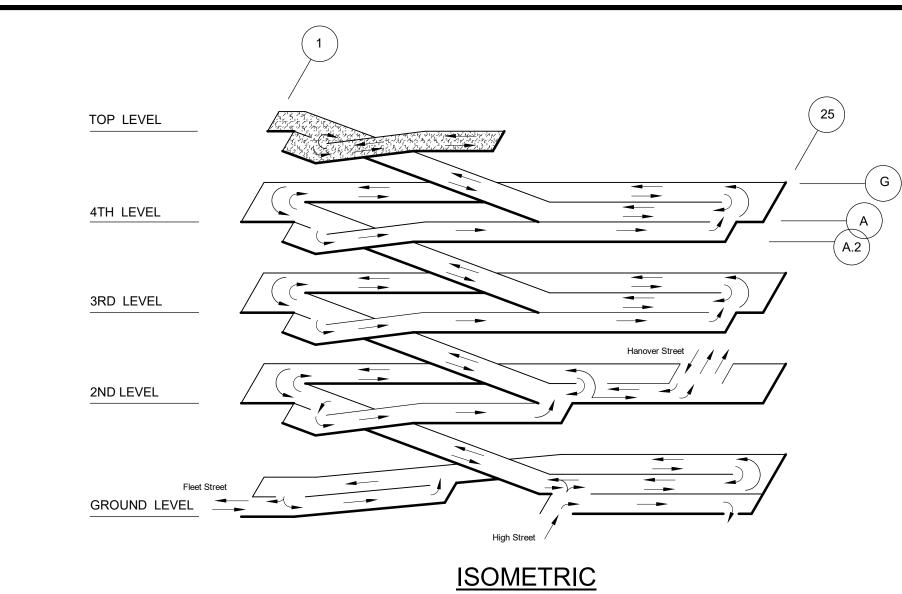


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SHEET TITLE:
FOURTH LEVEL STRIPING PLAN

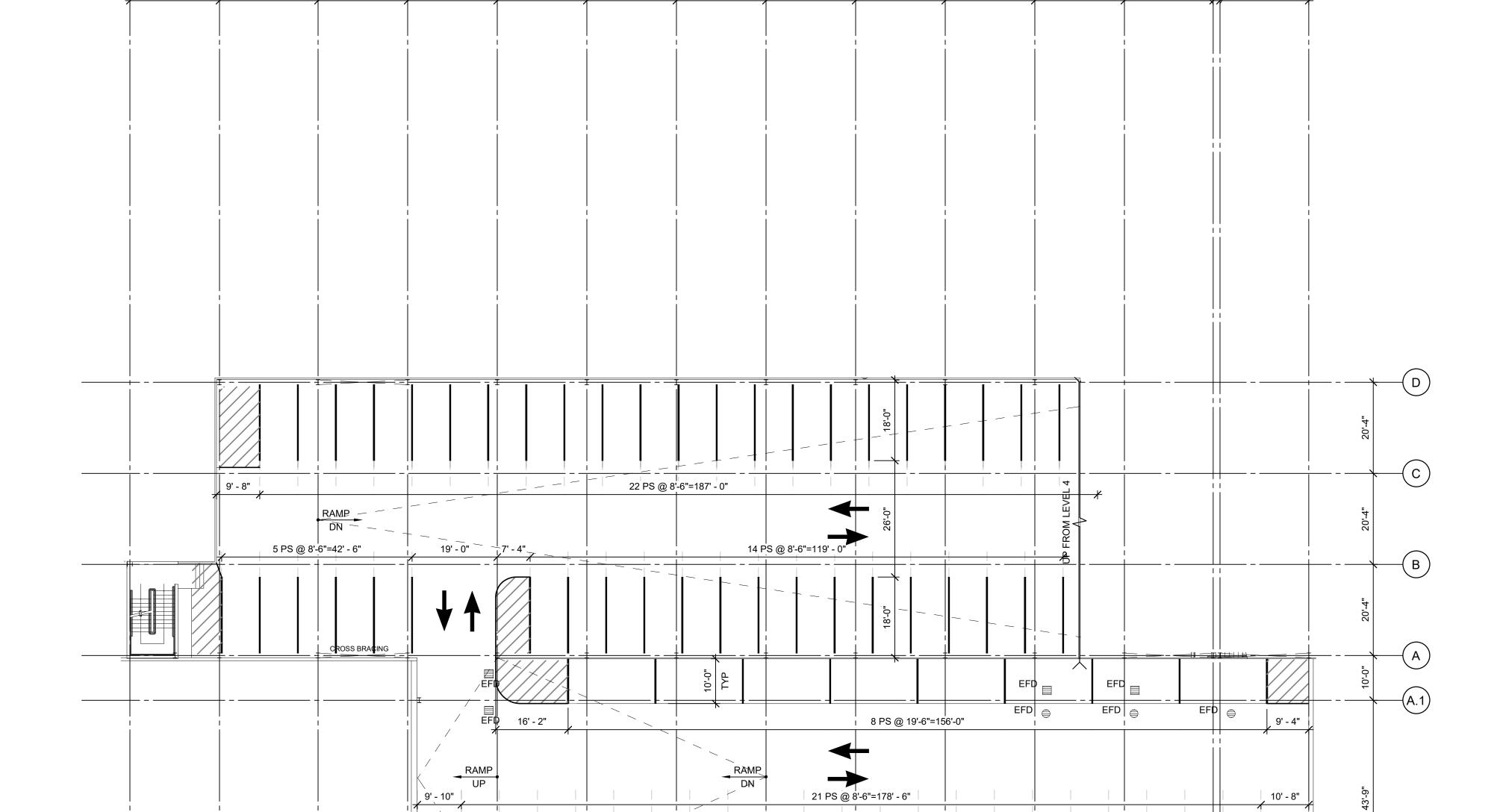
RG-104

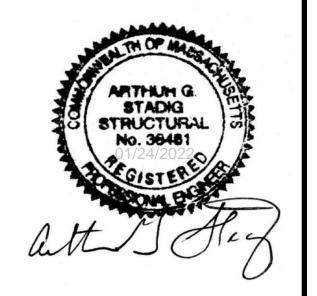
-SEE R-101 FOR STRIPING TYPICAL CALLOUT.











HANOVER STREET GARAGE RESTORATION

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SHEET TITLE:
FIFTH LEVEL STRIPING
PLAN

NORTH

RG-105