

HANOVER STREET GARAGE RESTORATION



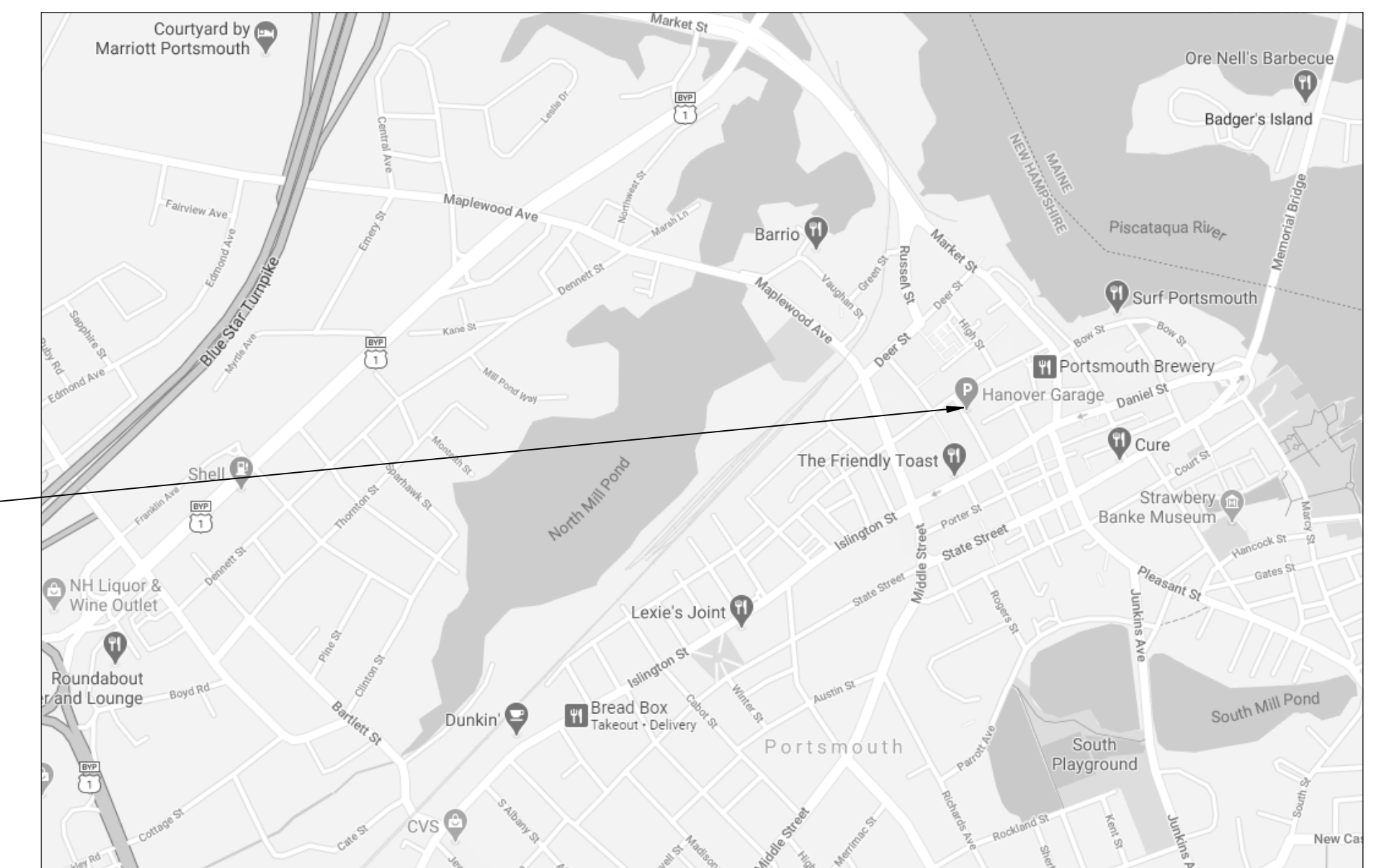
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34 HANOVER STREET.
PORTSMOUTH, NH 03801

PROJECT NO: 16-003129.00

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SITE LOCATION



SITE LOCATION MAP



Arthur G. Stading

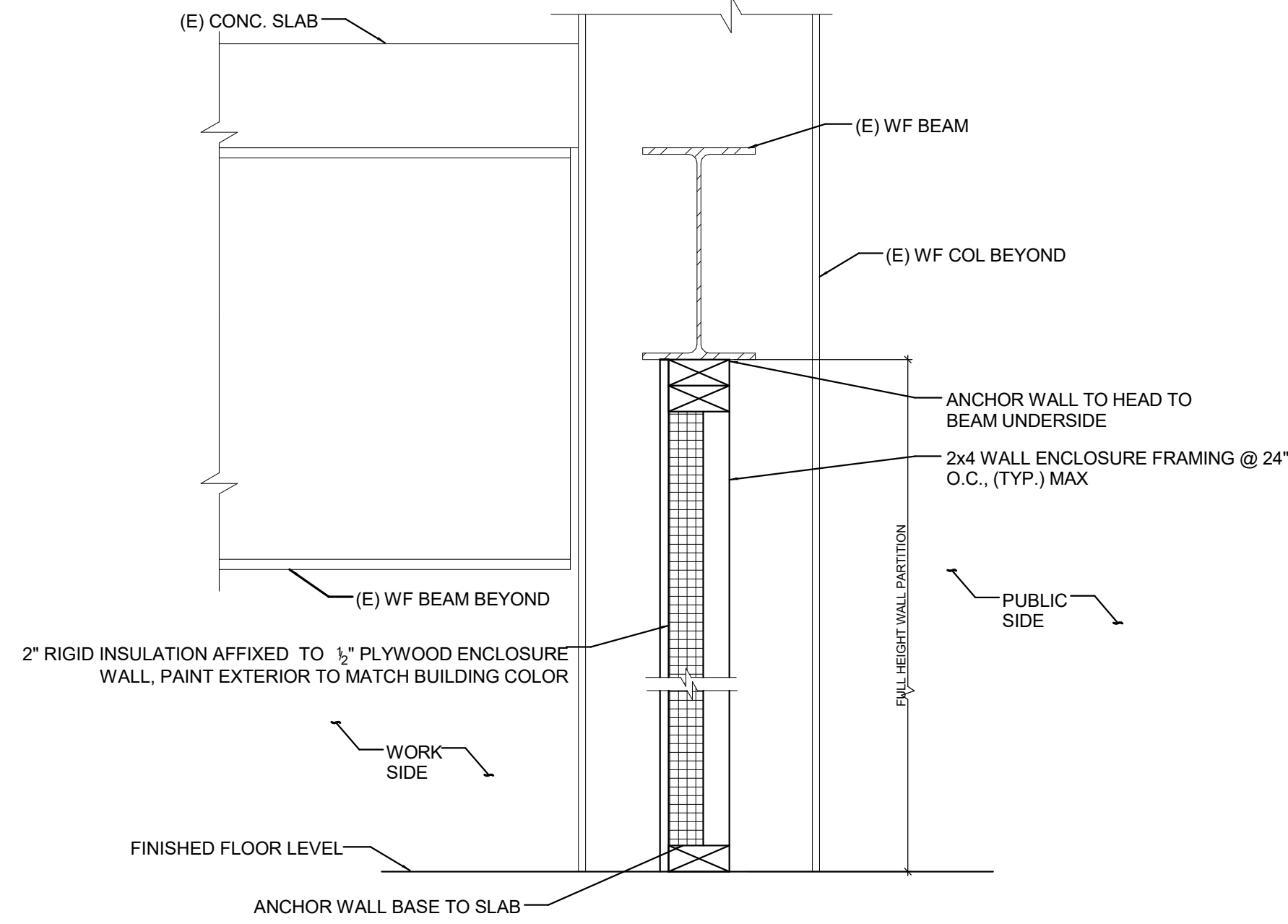
ISSUED FOR BIDS AND CONSTRUCTION
1/25/2022

- PHASING NOTES:**
- WORK ZONE:** THIS IS AN AREA THAT IS CAPTURED PER THE PHASING PLAN FOR THE PURPOSE OF PERFORMING PROJECT RELATED WORK ITEMS. THE AREA SHALL BE BARRICADED TO PREVENT GARAGE PATRONS/VEHICLES FROM ENTERING/PARKING WHILE CONTRACTOR IS IN CONTROL OF THE AREA.
 - PROTECTION ZONE:** THIS IS AN AREA THAT IS TAKEN OUT OF SERVICE AND ISOLATED FROM THE GARAGES NORMAL PEDESTRIAN AND VEHICLE CIRCULATION TO PROTECT THE PATRONS FROM HAZARDS RESULTING FROM WORK BEING PERFORMED ABOVE.
 - THE MAXIMUM NUMBER OF PARKING STALLS OUT OF SERVICE AT ONE SHALL BE LIMITED TO THE AMOUNT PER PHASE AS DICTATED IN THE SHEET NOTES ON R-001.**
 - GARAGE SHALL REMAIN OPERATIONAL AT ALL TIMES.
 - THE CONTRACTOR SHALL PROVIDE UNOBSTRUCTED PEDESTRIAN ACCESS (PROTECTED WHEN NECESSARY) TO ALL EMERGENCY EGRESS STAIRS AS WELL AS THE MAIN STAIR ELEVATOR TOWER AT ALL TIMES. THE CONTRACTOR SHALL INSTALL AND MAINTAIN SIGNAGE THAT CLEARLY DIRECTS PATRONS TO AND FROM THESE DESTINATIONS. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY LIGHTING FOR THESE PATHS IN THE EVENT THE GARAGE LIGHTING IS BLOCKED OR REDUCED BY PROJECT RELATED WORK.
 - THE CONTRACTOR SHALL SUBMIT DETAILED PHASING PLANS TO THE OWNER AND ENGINEER FOR EACH PHASE OF WORK. THE PHASING PLANS SHALL BE KEYED INTO A PROJECT SCHEDULE. THE PHASING PLANS SHALL INCLUDE BUT NOT BE LIMITED TO THE FOLLOWING:
 - LIMITS OF WORK ZONE INCLUDING DIMENSIONS TO BARRICADES.
 - LIMITS OF PROTECTION ZONE INCLUDING DIMENSIONS TO BARRICADES.
 - TEMPORARY PEDESTRIAN PATHS TO EMERGENCY EGRESS STAIRS AND ELEVATOR.
 - TEMPORARY VEHICLE CIRCULATION FOR ALL AREAS AFFECTED BY THE PHASE.
 - SIGNAGE LAYOUT FOR TEMPORARY PEDESTRIAN AND VEHICLE CIRCULATION.
 - INCLUDE KEY FOR TEXT TO FACILITATE OWNER REVIEW.
 - FLAGMEN POSITIONS (WHEN REQUIRED).
 - PHASING PLANS SHALL BE SUBMITTED TO THE ENGINEER/OWNER A MINIMUM OF THREE WEEKS BEFORE THE START OF WORK IN A PHASE AREA TO ALLOW SUFFICIENT TIME FOR REVIEW AND CAPTURE OF AREA.**
 - WORK ZONE AND PROTECTION ZONE BARRICADES SHALL BE OF SUFFICIENT CONSTRUCTION TO PREVENT INADVERTENT ACCESS BY PATRON VEHICLES AND PEDESTRIANS. THE BARRICADES CAN BE INCORPORATED INTO THE WORK ZONE PROTECTION PARTITIONS THAT ARE INTENDED TO PREVENT THE ESCAPE OF DUST AND OTHER DEBRIS FROM THE WORK ZONE AND PROTECTION ZONE.
 - THE FOLLOWING PARAMETERS SHALL BE CONSIDERED WHEN DEVELOPING PHASING PLANS.
 - PEDESTRIAN PATH WIDTH (MINIMUM): 8'-0"
 - PEDESTRIAN PATH HEADROOM (MINIMUM): 7'-6"
 - TEMPORARY VEHICLE ONE-WAY TRAVEL LANE (MINIMUM): 12'-0"
 - TEMPORARY VEHICLE TWO-WAY TRAVEL LANE (MINIMUM): 24'-0"
 - TEMPORARY VEHICLE LANE HEADROOM HEIGHT: AS POSTED AT GARAGE ENTRANCE

*ADD 2'-0" TO THE LANE DIMENSION AT TURNING AREAS.

J. MISCELLANEOUS NOTES

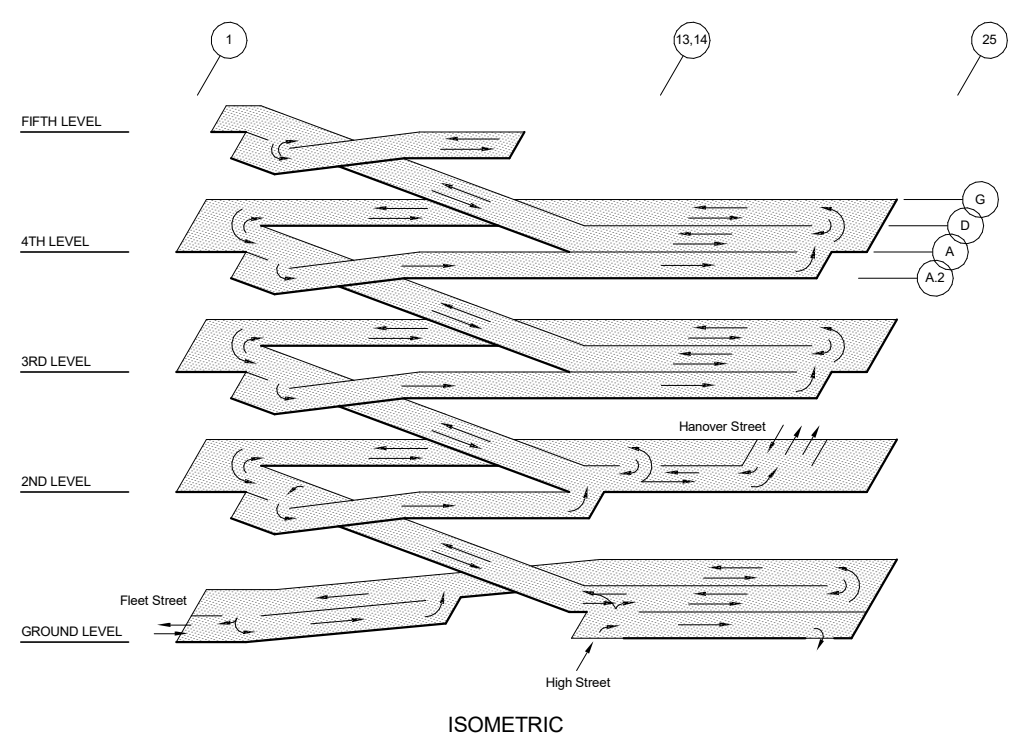
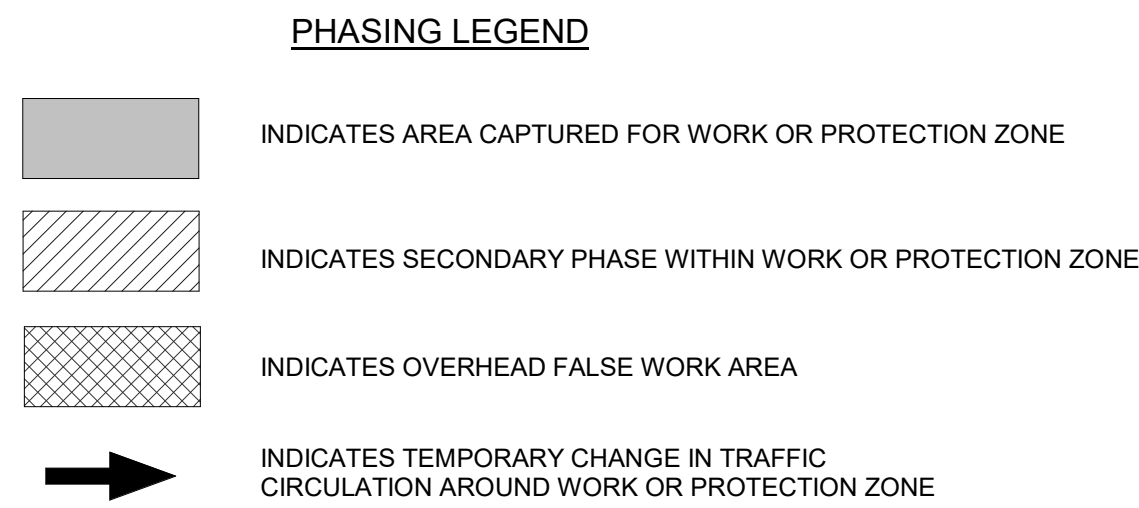
- OWNER AND THE ENGINEER RESERVE THE RIGHT TO ISSUE A STOP WORK ORDER FOR SPECIFIC AREAS / WORK ITEMS IF IN THE OPINION OF EITHER ONGOING WORK DOES NOT MEET THE PHASING PARAMETERS OF THE PROJECTS OR PRESENTS HAZARD TO THE PATRONS OF THE GARAGE. THE CONTRACTOR SHALL NOT BE ENTITLED TO ADDITIONAL COMPENSATION.
- WORK ZONES SHALL NOT BE RETURNED TO SERVICE WHEN HAZARDOUS CONDITIONS ARE PRESENT.
- DO NOT RETURN AREAS TO SERVICE UNLESS EXISTING STRIPING AFFECTED BY WORK HAS BEEN RESTORED OR NEW STRIPING HAS BEEN APPLIED.
- CONTRACTOR SHALL PROVIDE FULL HEIGHT PARTITIONING WITHIN WORK AND SAFE ZONES PRIOR TO START OF WORK. ALL PARTITIONING, FALSEWORK, SHORING, BARRICADES, SIGNAGE, ENCLOSURES, AND METHODS OF CONTROLLING VEHICLE TRAVEL AND PARKING AROUND THE WORK AND SAFETY ZONE MUST BE SUBMITTED WITH DETAILS TO THE ENGINEER FOR APPROVAL PRIOR TO THE START OF INSTALLATION WORK.
- ALL PARTITIONS SHALL HAVE SOUND DEADENING MATERIAL TO REDUCE DEMOLITION NOISE OUTPUT OUTSIDE THE STRUCTURE.
- THE ENCLOSED AREAS SHALL BE PROVIDED WITH VENTILATION AND FILTRATION DEVICES TO PREVENT AIRBORNE DEBRIS FROM EXITING THE ENCLOSED AREA(S).
- THE ENCLOSED AREAS SHALL BE PROVIDED WITH ACCESS DOORWAYS FOR ENTRY AND SECURITY OF THE AREAS UNDER CONTROL BY THE CONTRACTOR.
- TRAFFIC CONTROL SIGNAGE SHALL BE PROVIDED AND INSTALLED IN A PLAIN LINE OF SIGHT TO INSTRUCT PATRONS THROUGH AND AROUND THE GARAGE CONSTRUCTION AREAS.
- SUSPENDED CONSTRUCTION FALSE WORK SHALL BE PROVIDED OVER ALL VEHICLE AND PEDESTRIAN ENTRY AND EXIT PORTALS TO ALLOW ACCESS TO THESE AREAS UNTIL CONSTRUCTION WORK IS UNDERTAKEN IN THESE AREAS MUST BE CLOSED.
- CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE TEMPORARY LIGHTING AS NEEDED TO PROVIDE PROPER LIGHTING AROUND WORK AND SAFETY ZONES AND WHERE FALSE WORK IS INSTALLED OVER PATRON ACCESS AREAS NOTED IN NOTE I ABOVE.
- CONTRACTOR SHALL SUPPLY AND USE INDUSTRIAL MISTING FANS WITHIN THE WORK ZONES TO MINIMIZE AIRBORNE DEBRIS DURING CONCRETE DEMOLITION. CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE AIR VENTILATION AND FILTRATION FROM WORK ZONE ENCLOSED AREAS TO CONTROL AIRBORNE DEBRIS FROM EXHAUSTING OUT OF THE WORK ZONE DURING DEMOLITION.
- CONTRACTOR SHALL TEMPORARILY RELOCATE ALL ADA PARKING SPACES CAPTURED IN WORK OR PROTECTION ZONES SUCH THAT THE TOTAL NUMBER OF ADA SPACES WITHIN THE GARAGE IS MAINTAINED AT ALL TIMES.
- CONTRACTOR SHALL PROVIDE A CONSTRUCTION SIGNAGE SCHEDULE AND CONSTRUCTION TRAFFIC MANAGEMENT PLAN SHOWING THE LOCATIONS OF TEMPORARY SIGNAGE THAT IS TO BE INSTALLED IN THE GARAGE DURING CONSTRUCTION.
- CONTRACTOR'S PHASING PLAN FOR WATERPROOFING COATING APPLICATION MAY DIFFER FROM THE CONCEPTUAL PHASING SCHEMES SHOWN AND WOULD NOT REQUIRE THE ENCLOSED PERIMETER SOUND DEADENING WALLS. HOWEVER, CONTRACTOR MUST PROVIDE POSITIVE BARRICADES AND SIGNAGE TO PREVENT PATRONS FROM ENTERING THESE WORK ZONES DURING WATERPROOFING APPLICATION.



- NOTE:
- WALL SHALL FORM ENTIRE ENCLOSED PERIMETER OF WORK AND PROTECTION ZONES WITH SOUND DEADENING MATERIALS

TYPICAL CONSTRUCTION BARRIER DETAIL

TEMPORARY CONSTRUCTION SIGN SCHEDULE						
SIZE		TEXT/MESSAGE	BACKGROUND COLOR	LETTERS/SYMBOLS		REMARKS
W	L			H	COLOR	
2'-0"	3'-0"	CAUTION UNDER CONSTRUCTION WATCH FOR SIGNS	ORANGE	4"	BLACK	
2'-0"	3'-0"	NO PARKING THIS AREA	ORANGE	4"	BLACK	
2'-0"	3'-0"	PARKING	ORANGE	6"	BLACK	MANUFACTURE SIGN WITH A ROTATABLE ARROW THAT CAN BE LOCKED INTO POSITION.
2'-0"	3'-0"	EXIT	ORANGE	6"	BLACK	MANUFACTURE SIGN WITH A ROTATABLE ARROW THAT CAN BE LOCKED INTO POSITION.
2'-0"	3'-0"	STAIR CLOSED USE OTHER STAIR	WHITE	4"	BLACK	
3'-0"	3'-0"	DO NOT ENTER	RED	4"	WHITE	
2'-0"	3'-0"	CAUTION TWO-WAY TRAFFIC AHEAD	ORANGE	6"	BLACK	
2'-0"	3'-0"	LEFT TURN ONLY	ORANGE	6"	BLACK	
2'-0"	3'-0"	RIGHT TURN ONLY	ORANGE	6"	BLACK	
2'-0"	3'-0"	ONE WAY	ORANGE	6"	BLACK	
2'-0"	3'-0"	CONSTRUCTION PERSONNEL ONLY	ORANGE	6"	BLACK	



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STRUCTURAL
No. 36481
REGISTERED
ENGINEER

Arthur G. Stodg

HANOVER STREET GARAGE RESTORATION

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MARK	DATE	DESCRIPTION
		REVISIONS
		BIDDING AND CONSTRUCTION

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PROJECT NO: 16-003129.00
DRAWN BY: NCH
CHECKED BY: MAZ

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SHEET TITLE:
PHASING NOTES AND LEGENDS

R-002



HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET
PORTSMOUTH, NH 03801

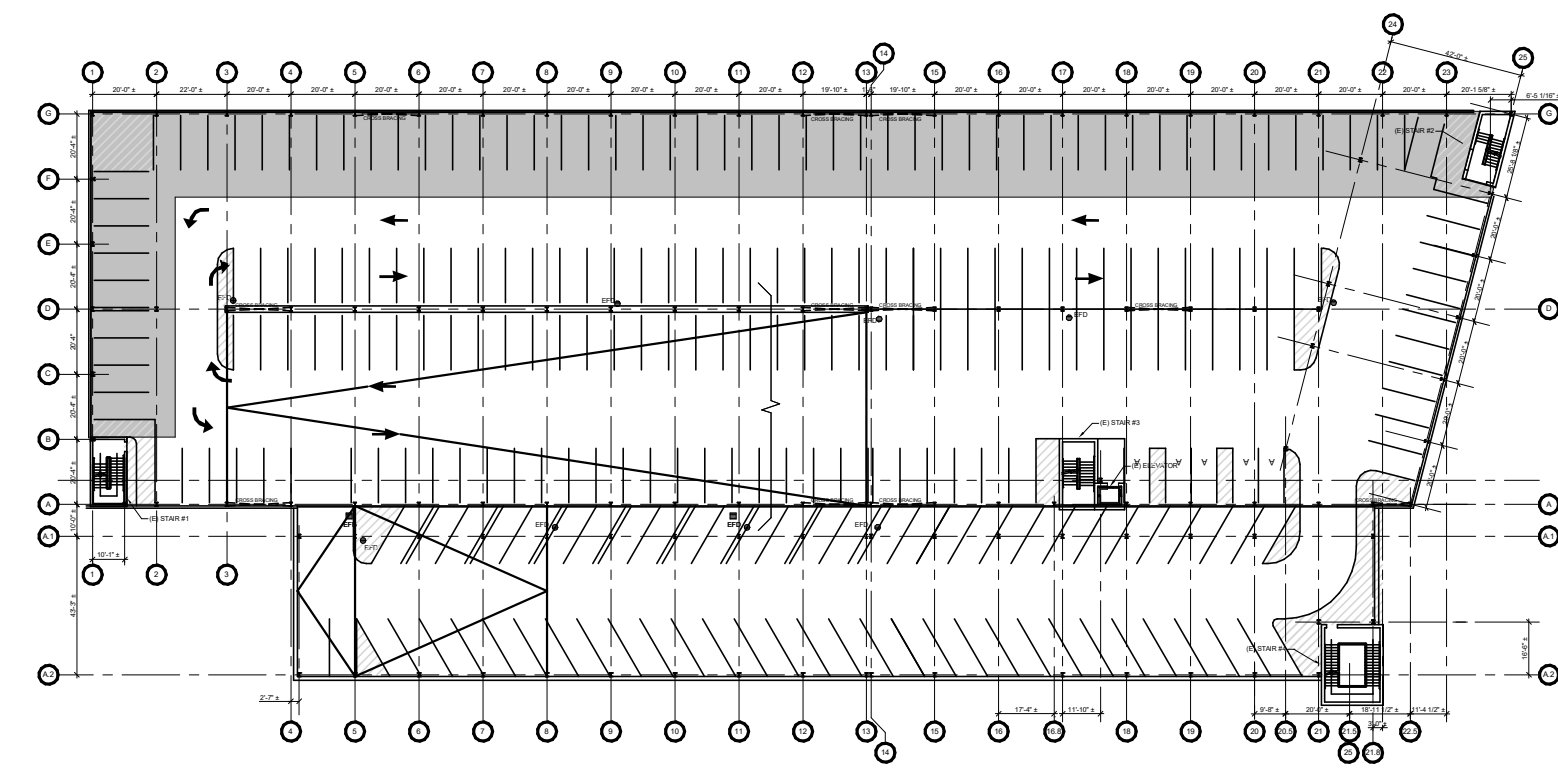
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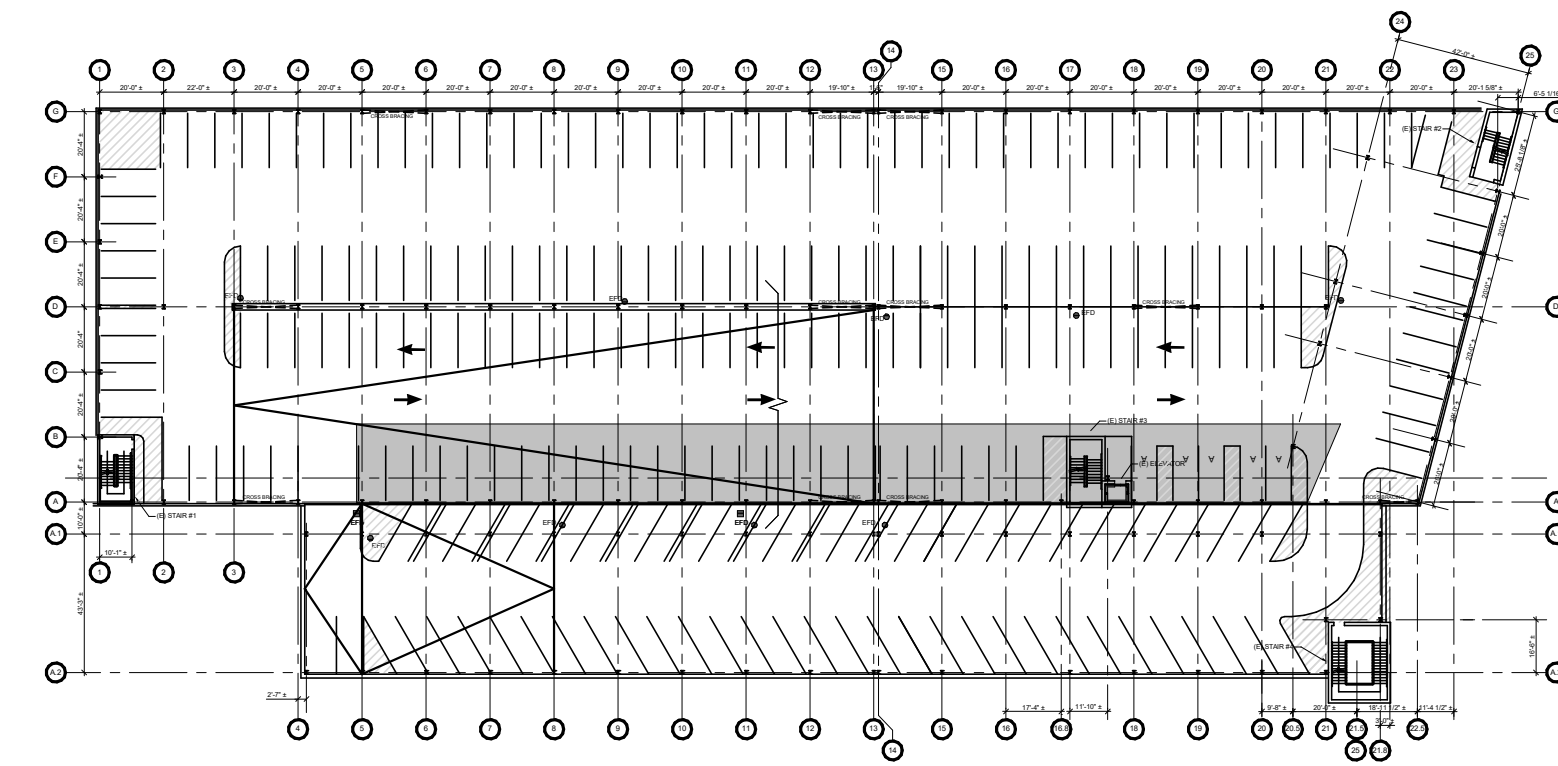
SHEET TITLE
2022 PHASING PLANS

NOTE:

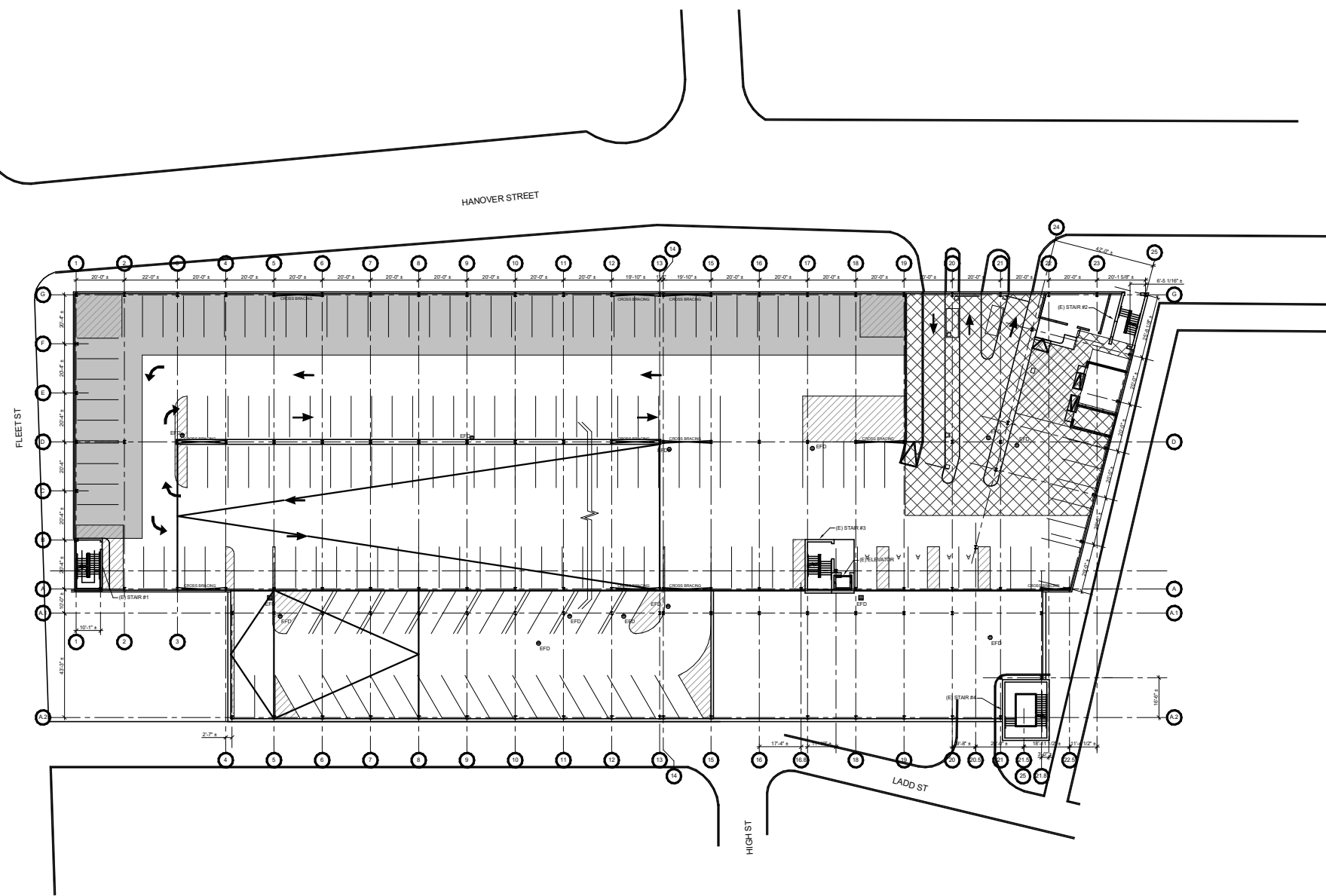
- SEE R-002 FOR PHASING NOTES, CONSTRUCTION SIGNAGE SCHEDULE, AND TYPICAL CONSTRUCTION BARRIER DETAIL.



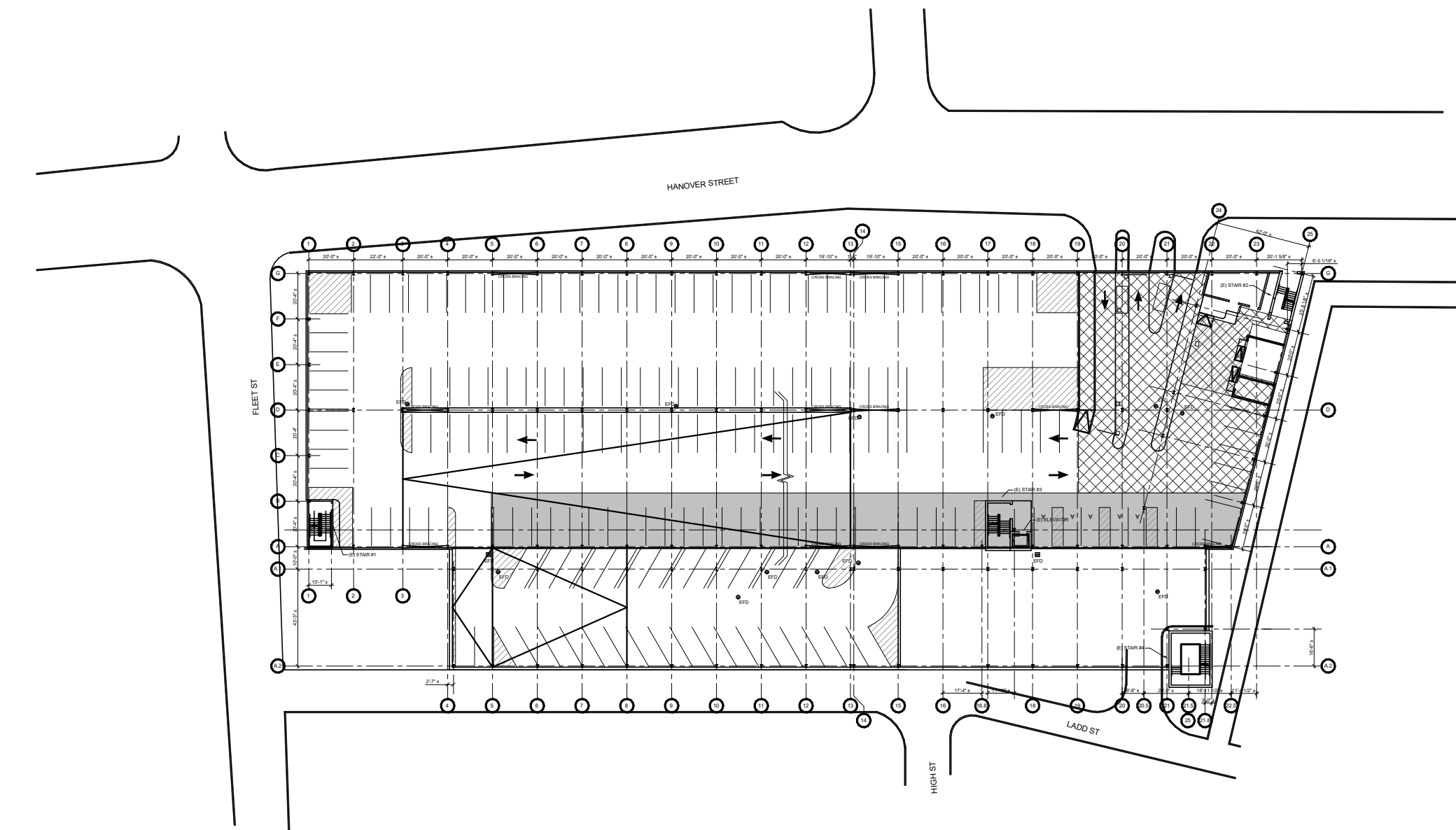
THIRD LEVEL PHASE 1 WORK ZONE
1W



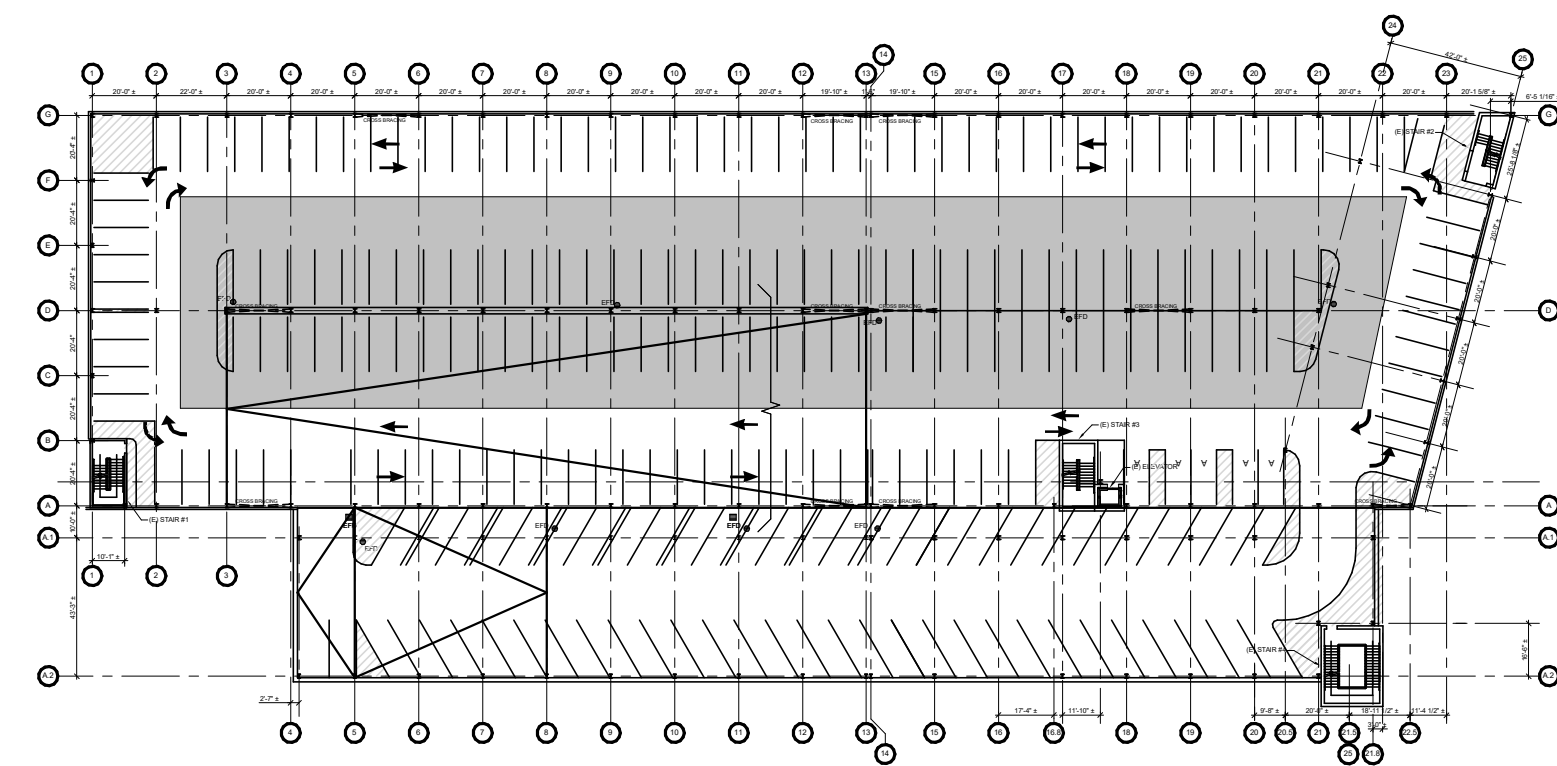
THIRD LEVEL PHASE 3 WORK ZONE
3W



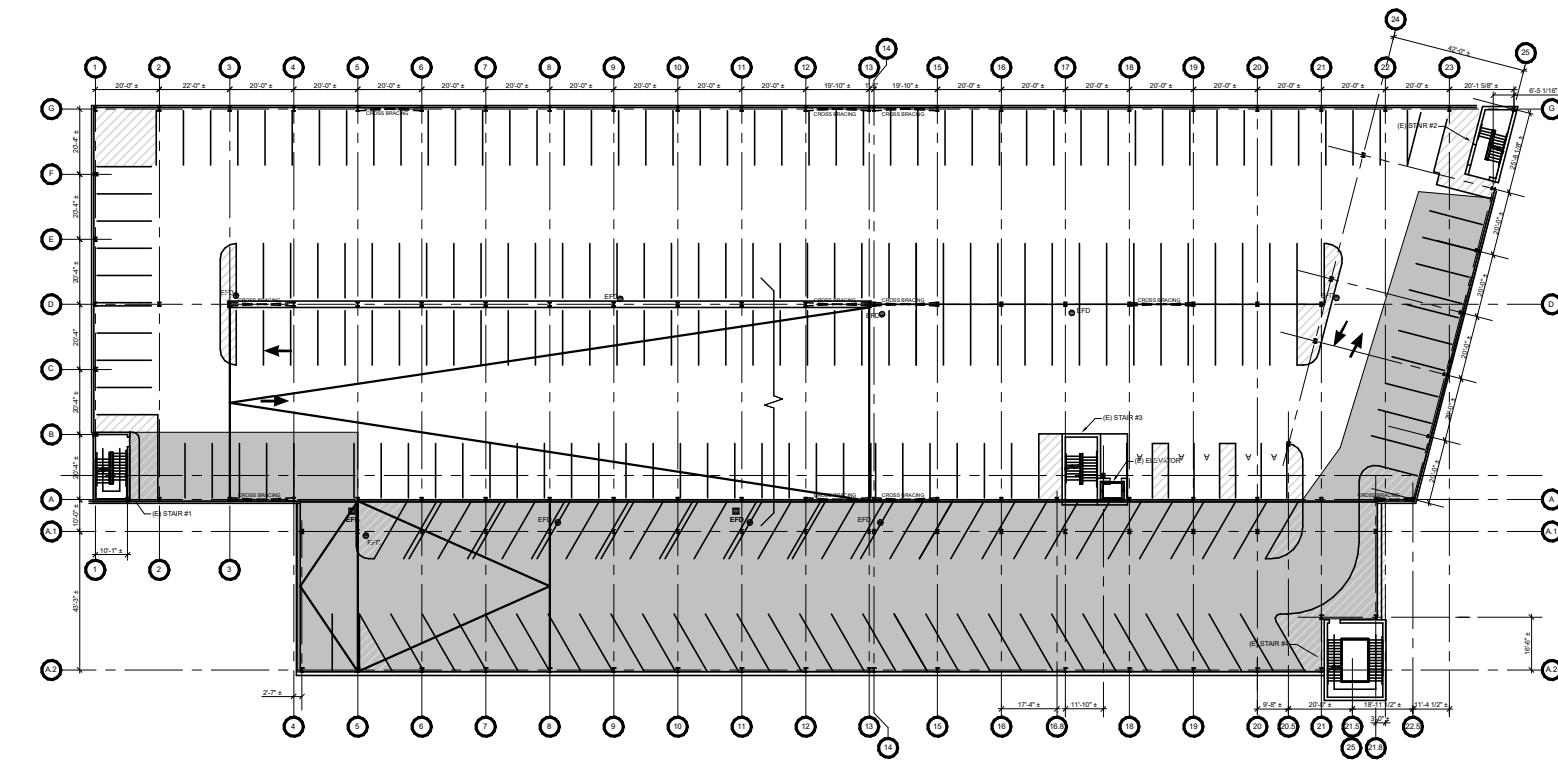
SECOND LEVEL PHASE 1 PROTECTION ZONE
1P



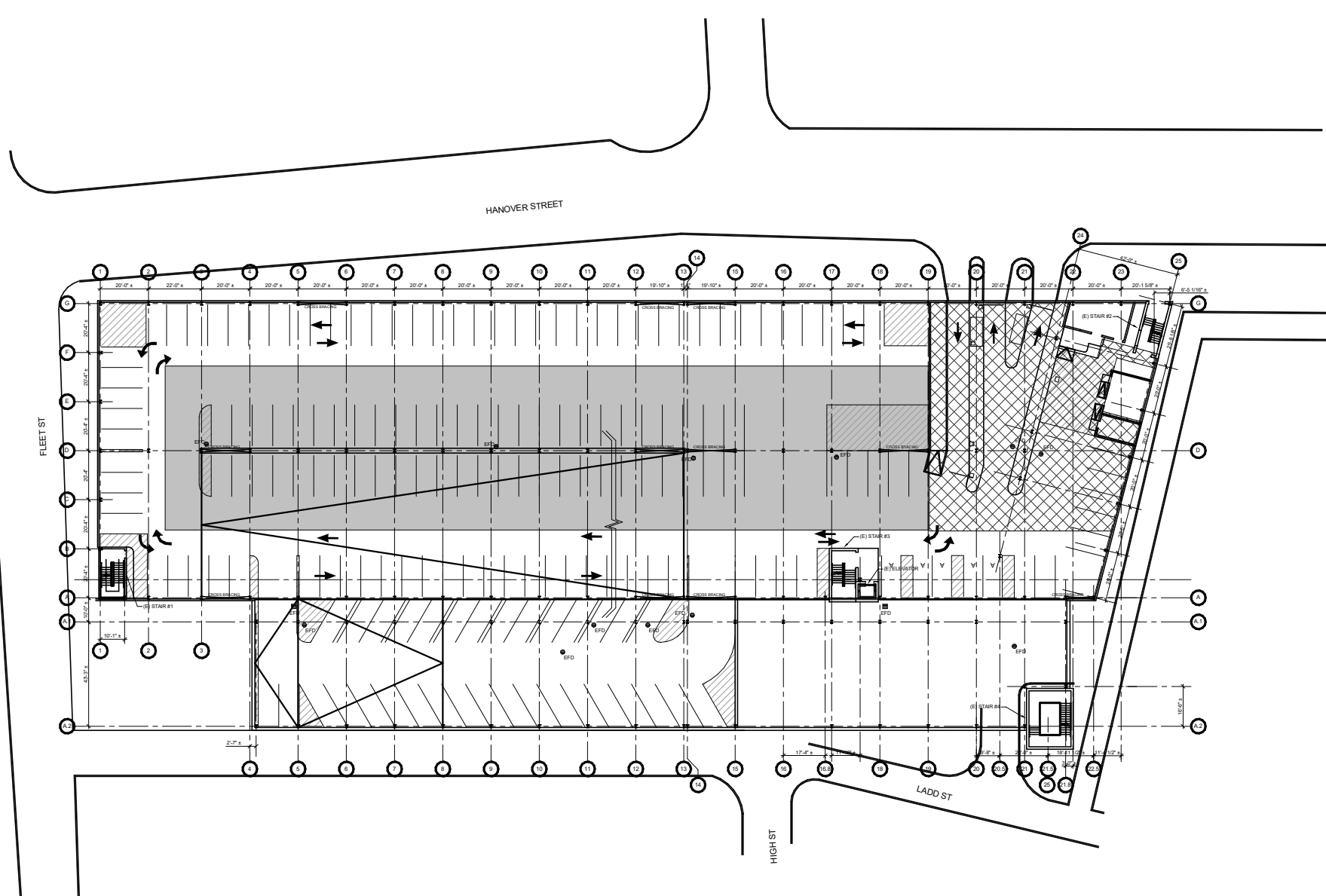
SECOND LEVEL PHASE 3 PROTECTION ZONE
3P



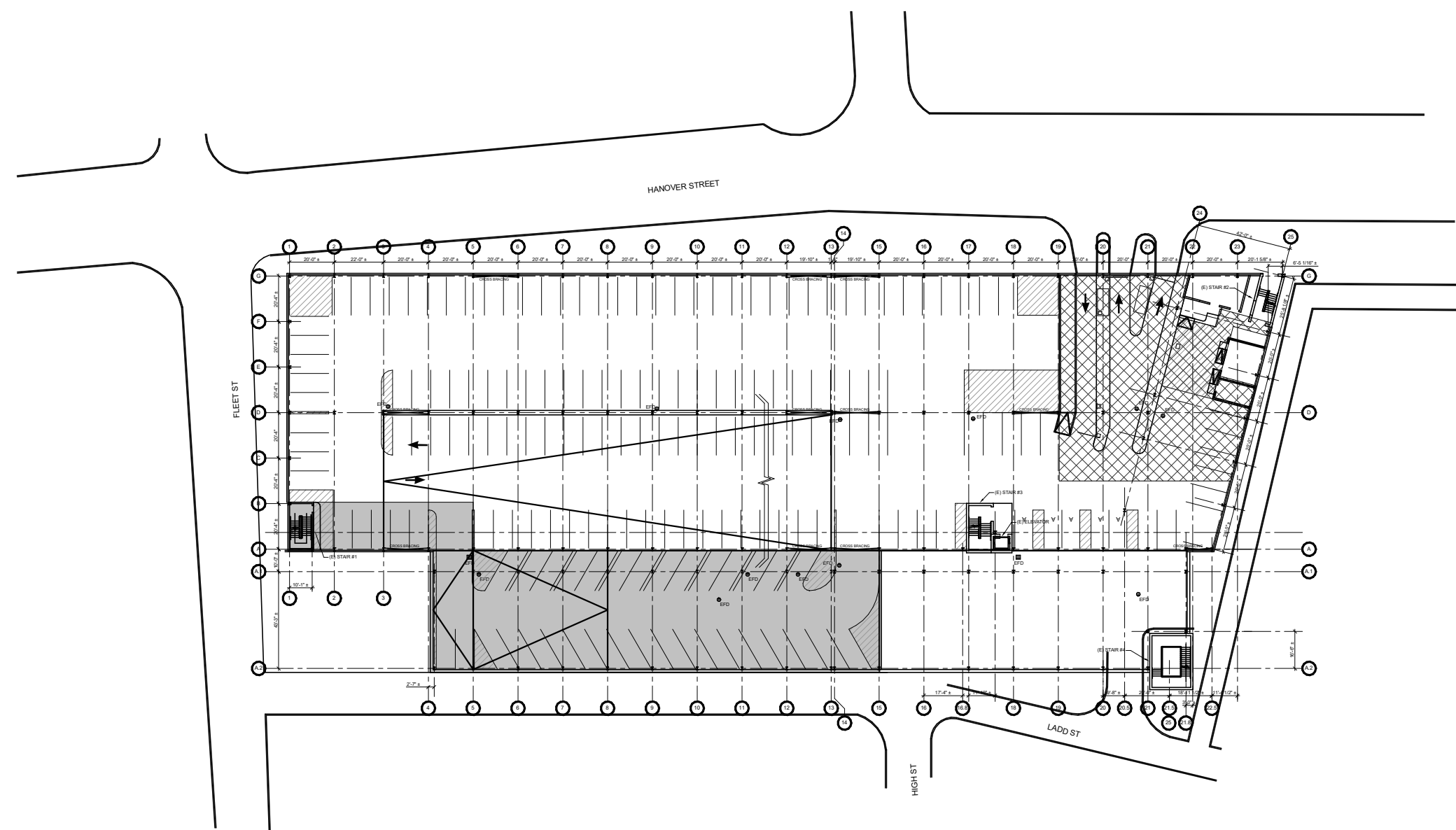
THIRD LEVEL PHASE 2 WORK ZONE
2W



THIRD LEVEL PHASE 4 WORK ZONE
4W



SECOND LEVEL PHASE 2 PROTECTION ZONE
2P



SECOND LEVEL PHASE 4 PROTECTION ZONE
4P

PHASING LEGEND

- INDICATES AREA CAPTURED FOR WORK OR PROTECTION ZONE
- INDICATES SECONDARY PHASE WITHIN WORK OR PROTECTION ZONE
- INDICATES OVERHEAD FALSE WORK AREA
- INDICATES TEMPORARY CHANGE IN TRAFFIC CIRCULATION AROUND WORK OR PROTECTION ZONE

1/24/2022 10:31:30 AM BIM 350/116-0000.00 - Boston Restoration Projects (R2019)16-00129 00 Hanover St Garage Restoration - R10.rvt

NOTE:
1. SEE R-002 FOR PHASING NOTES, CONSTRUCTION SIGNAGE SCHEDULE, AND TYPICAL CONSTRUCTION BARRIER DETAIL.



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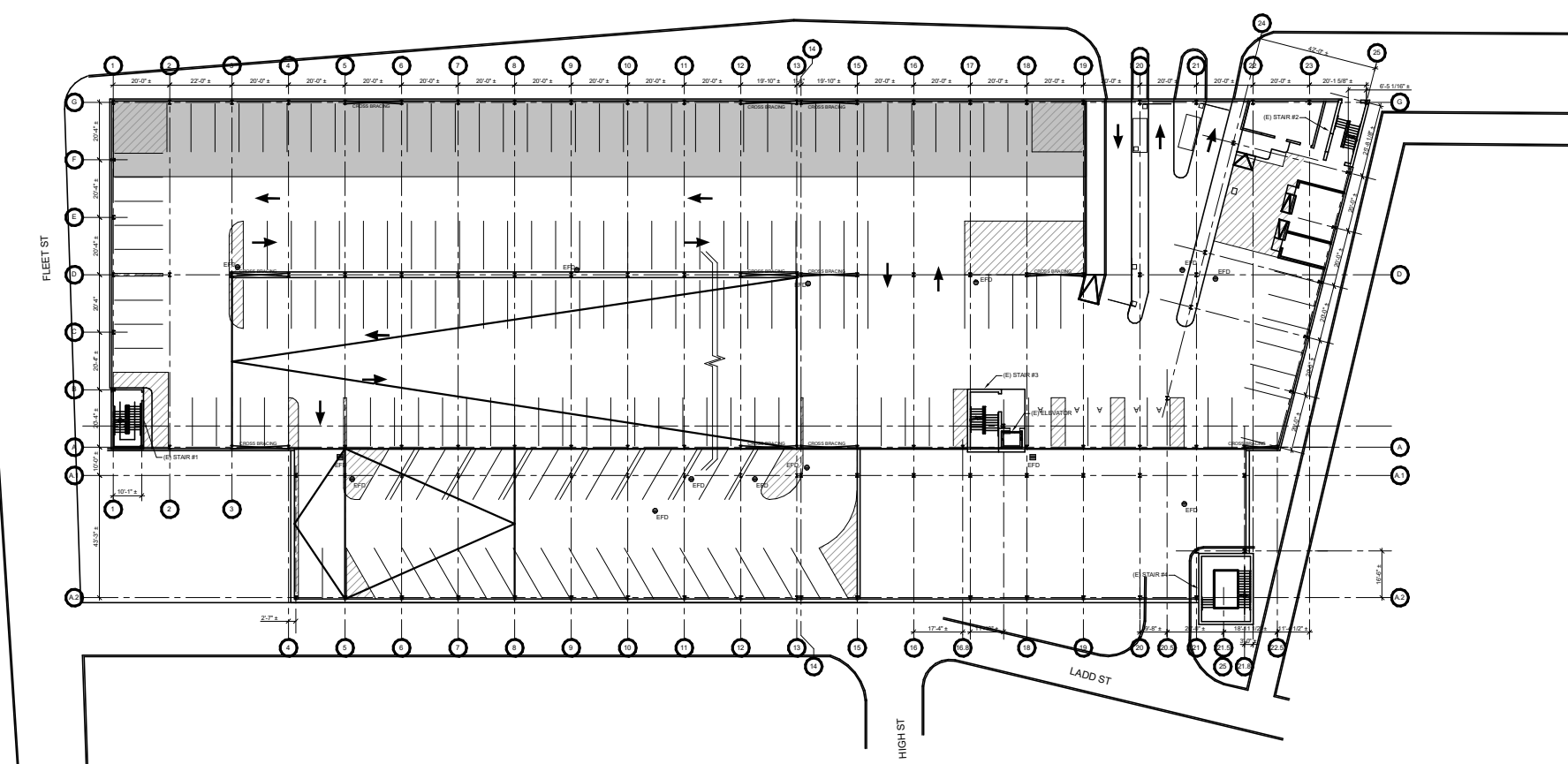
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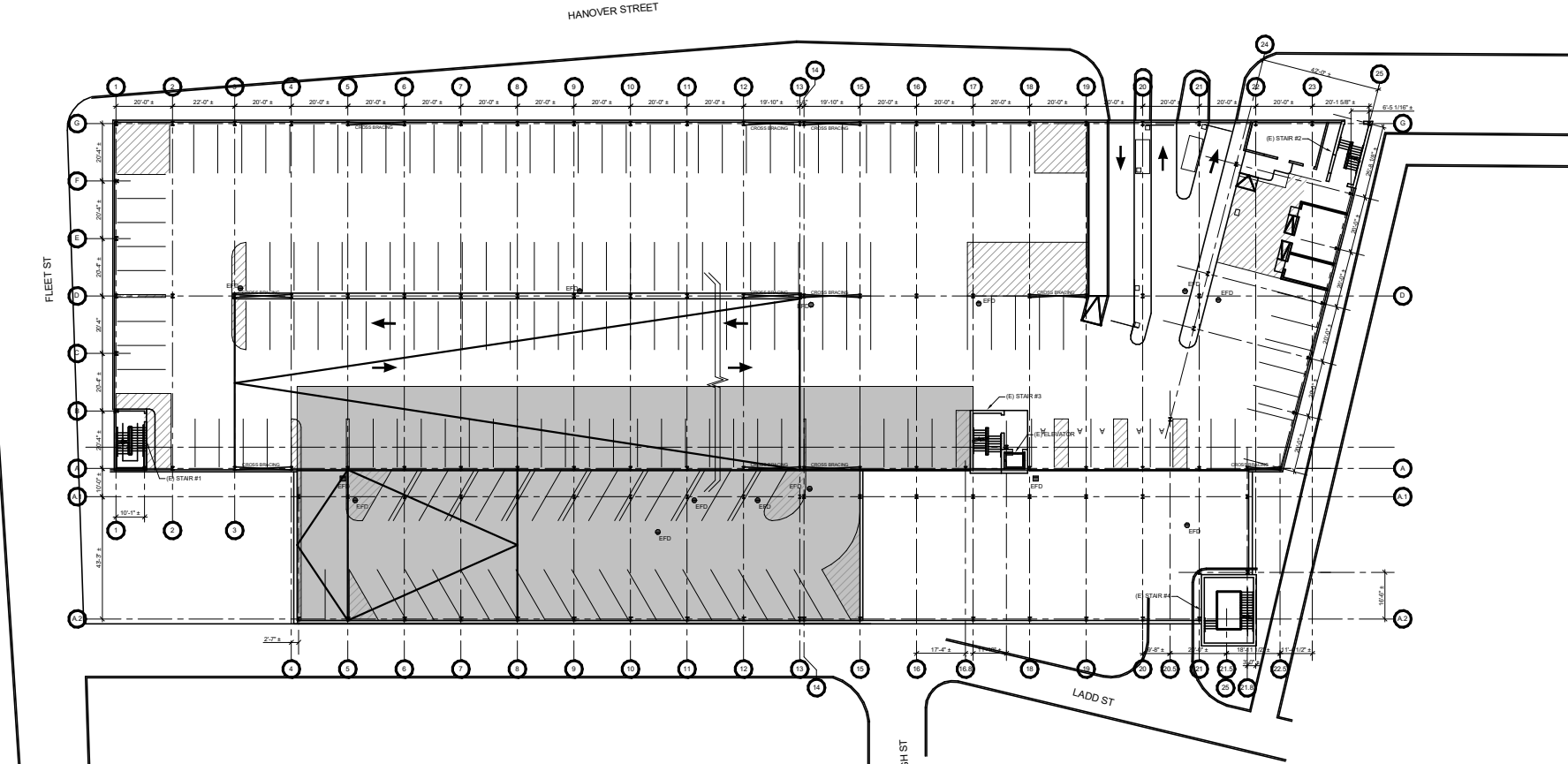
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SHEET TITLE
2023 PHASING PLANS

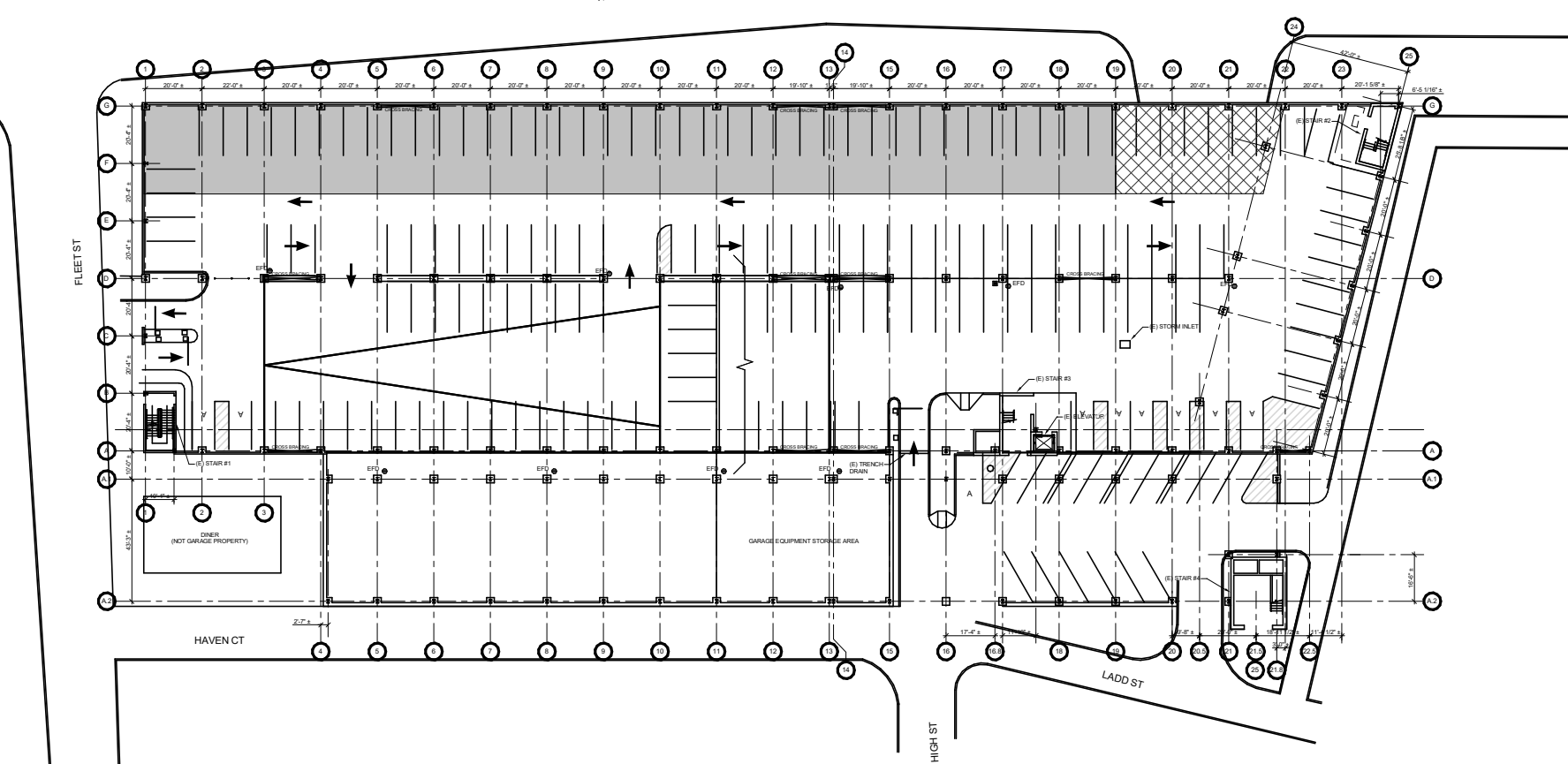
R-004



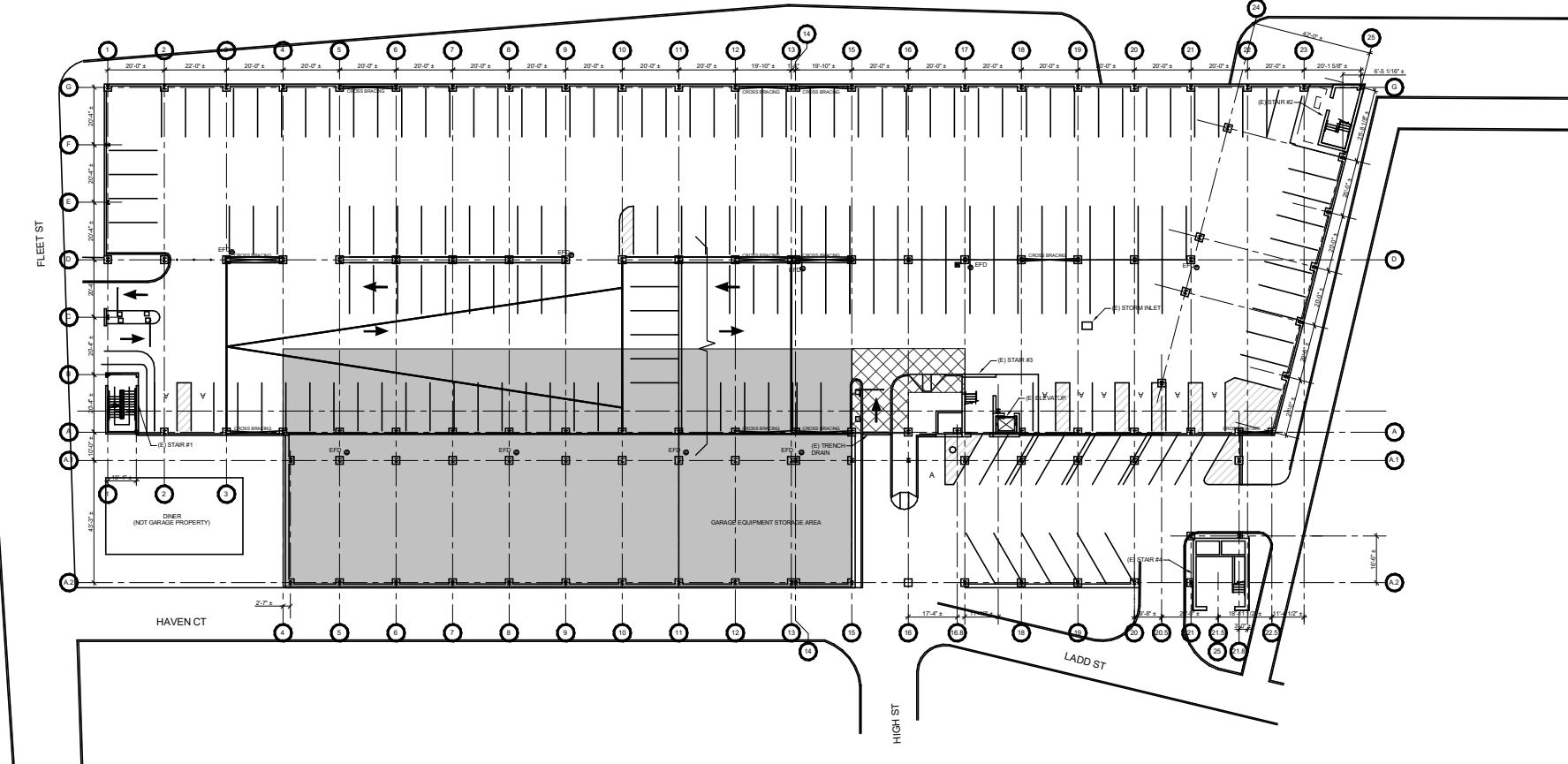
SECOND LEVEL PHASE 1 WORK ZONE



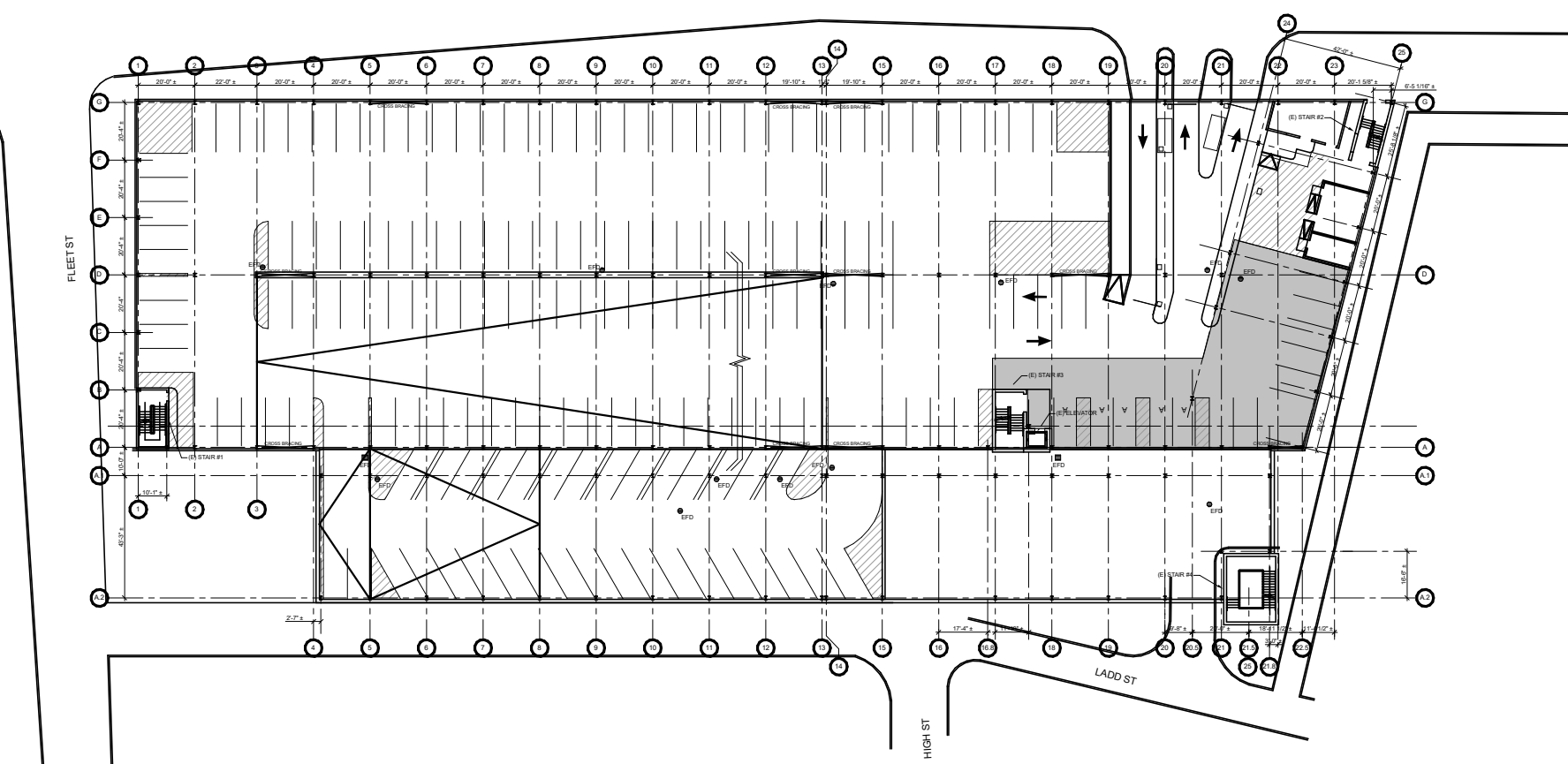
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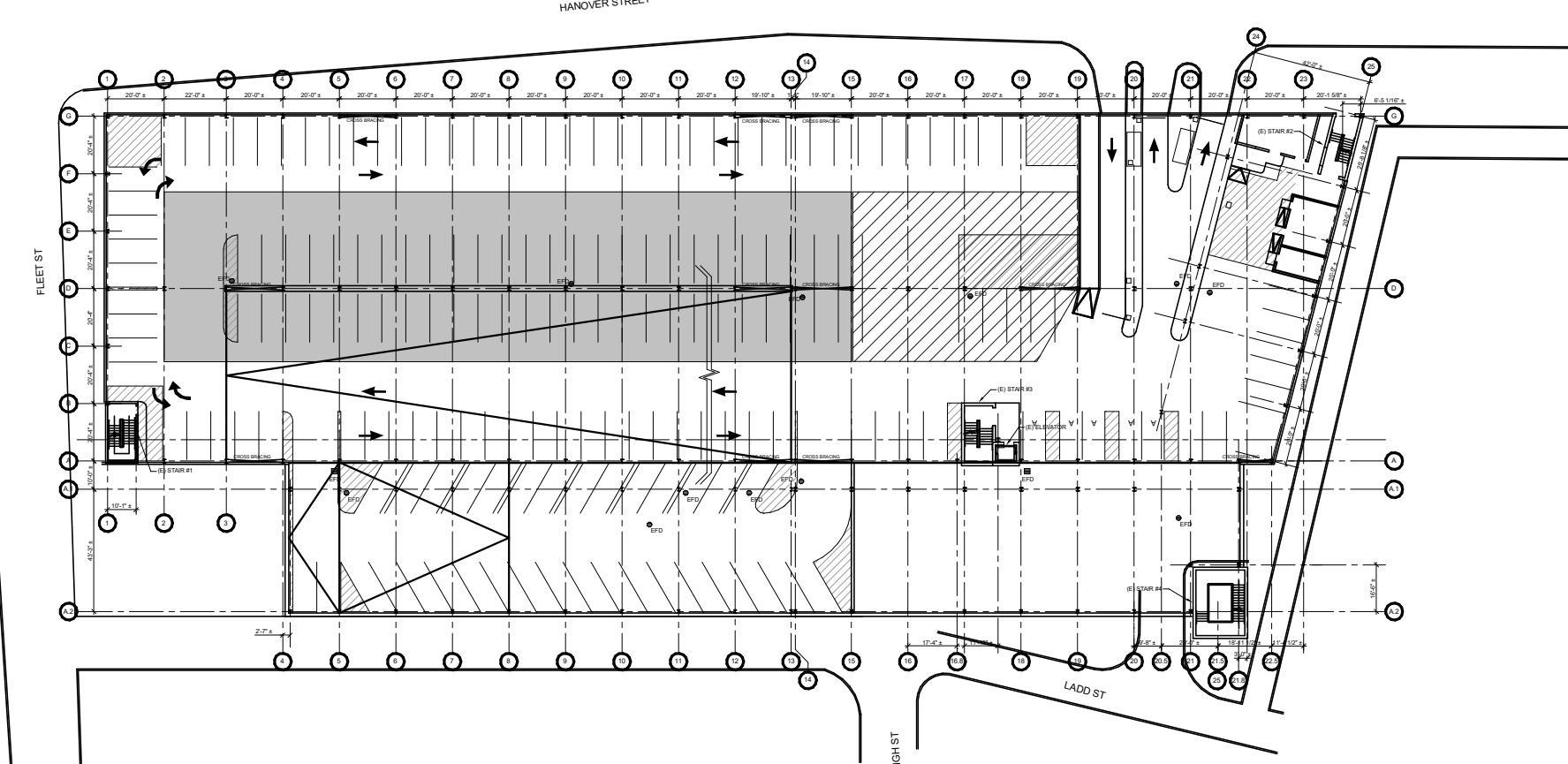
FIRST LEVEL PHASE 1 PROTECTION ZONE



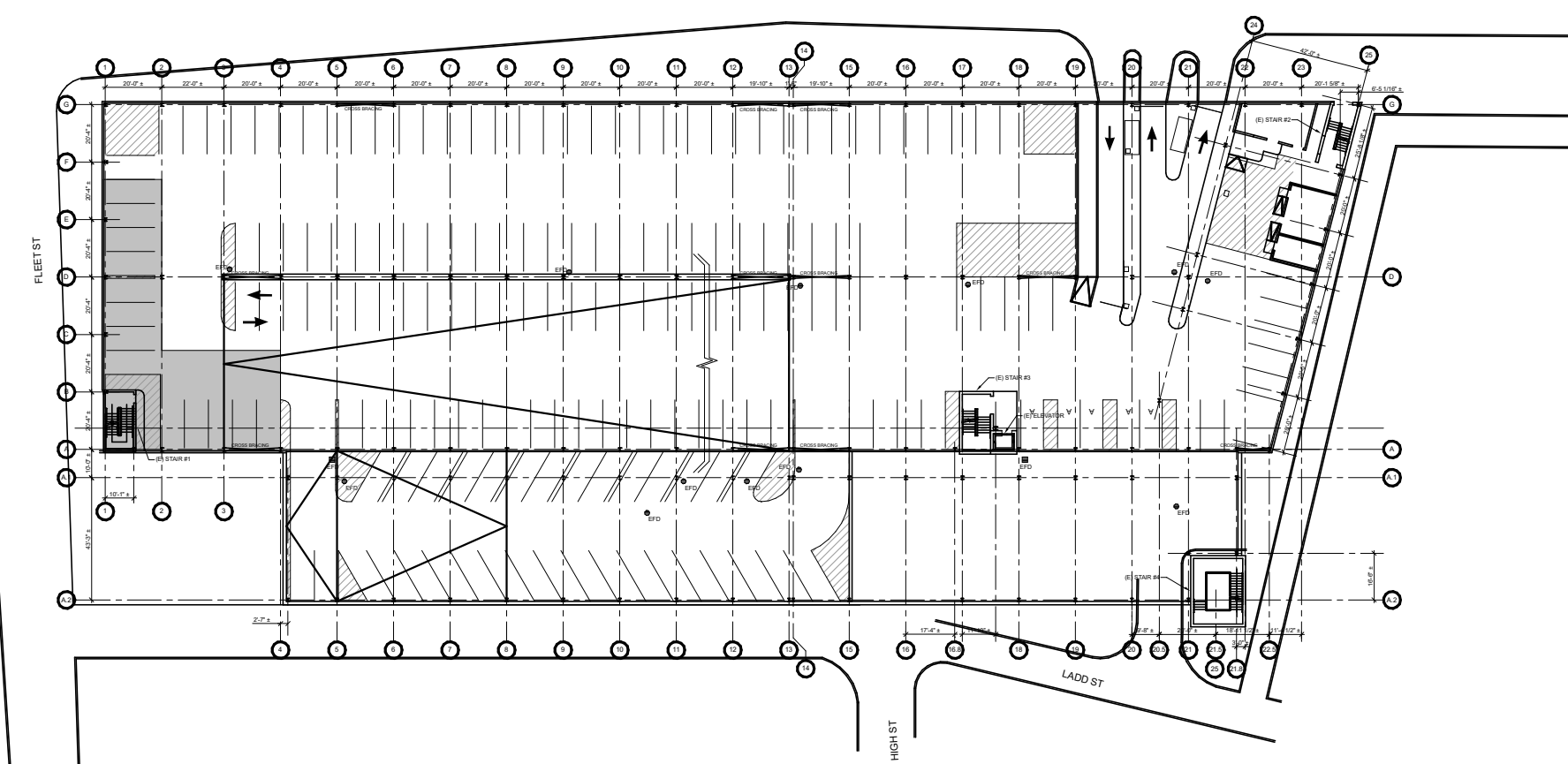
FIRST LEVEL PHASE 3 PROTECTION ZONE



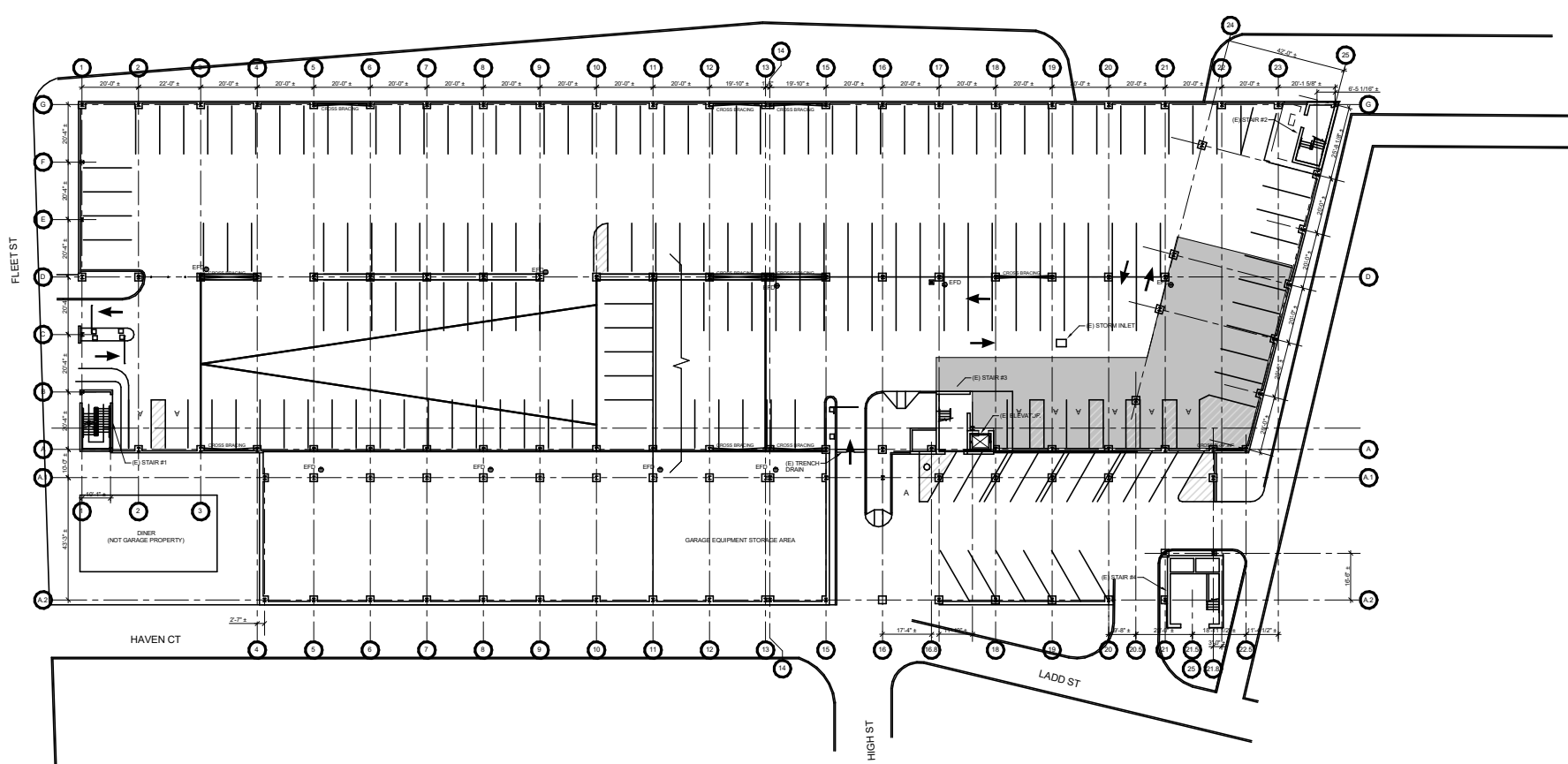
SECOND LEVEL PHASE 5 WORK ZONE



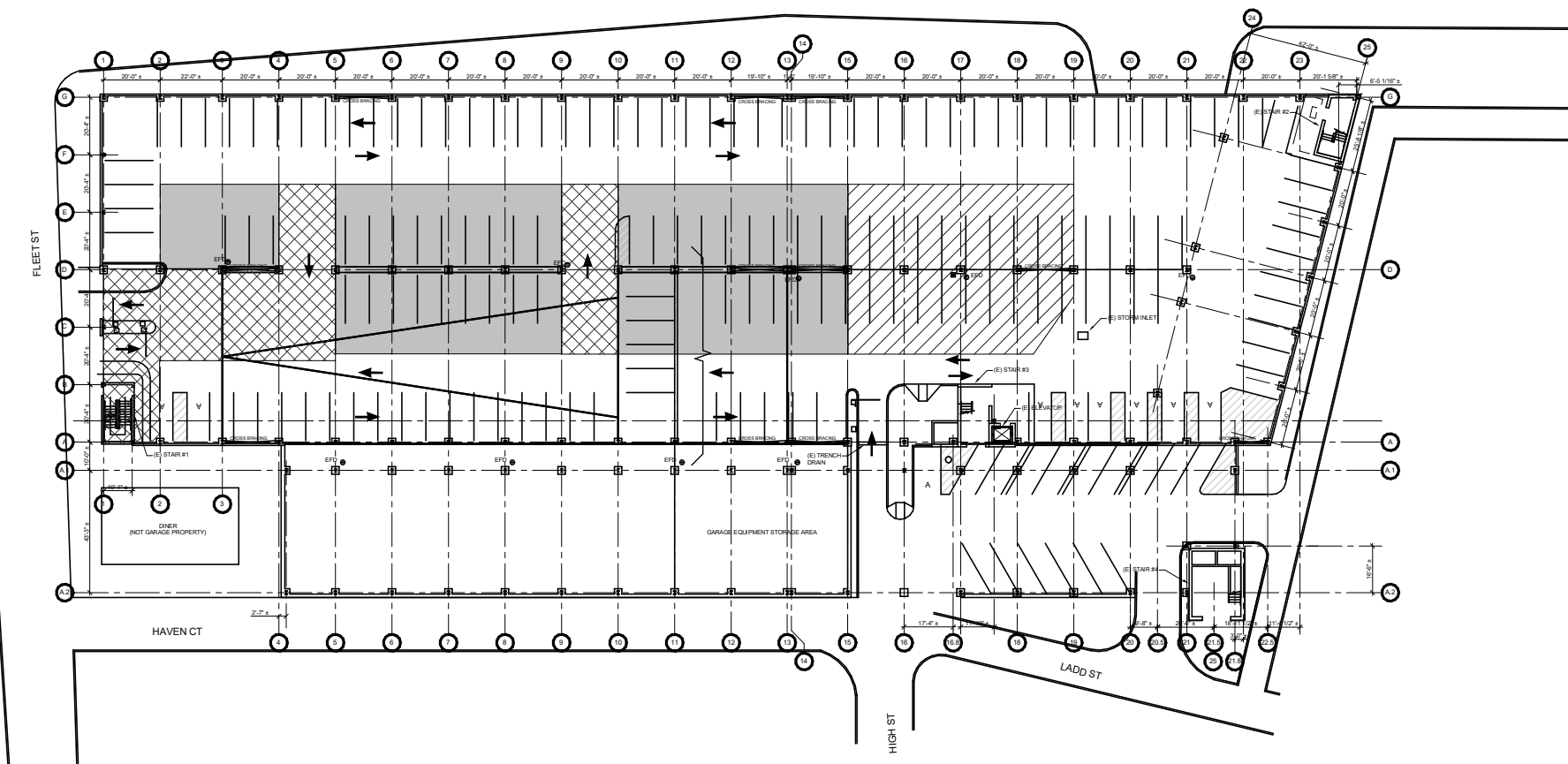
SECOND LEVEL PHASE 2 WORK ZONE



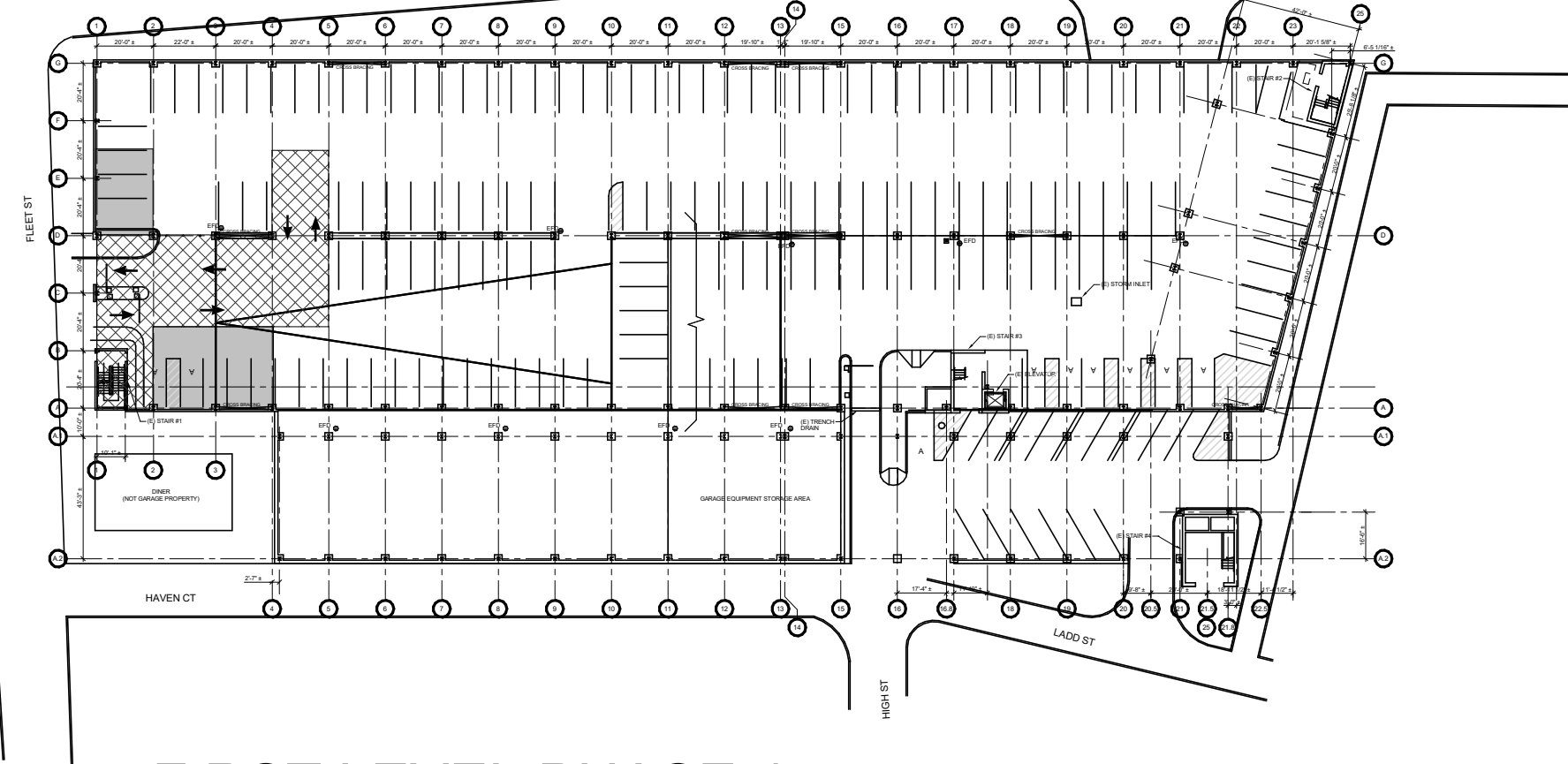
SECOND LEVEL PHASE 4 WORK ZONE



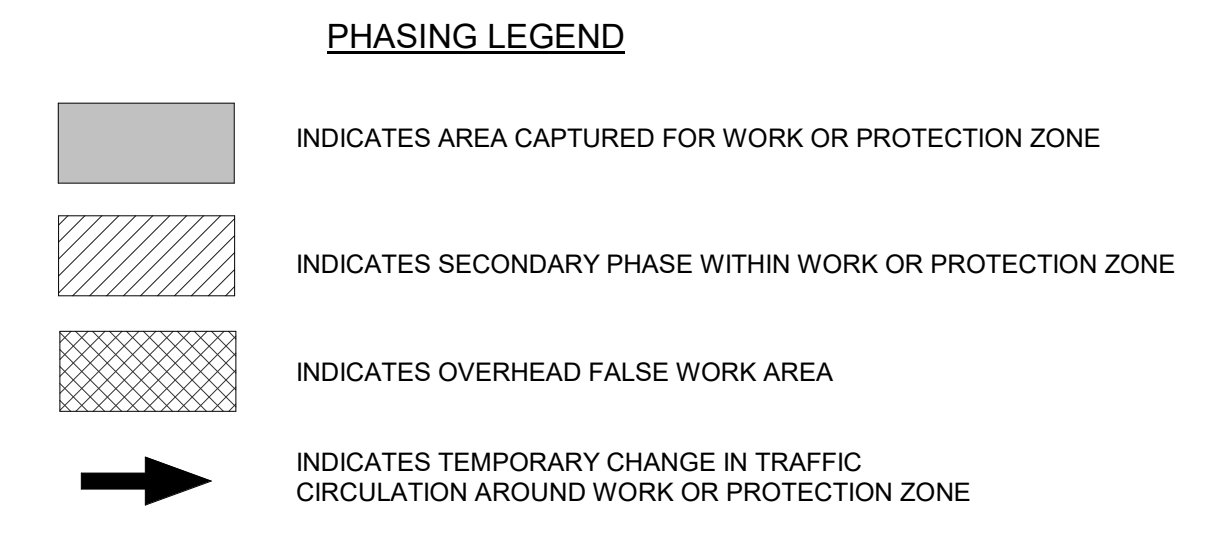
FIRST LEVEL PHASE 5 PROTECTION ZONE



FIRST LEVEL PHASE 2 PROTECTION ZONE



FIRST LEVEL PHASE 4 PROTECTION ZONE





Arthur G. Stadig

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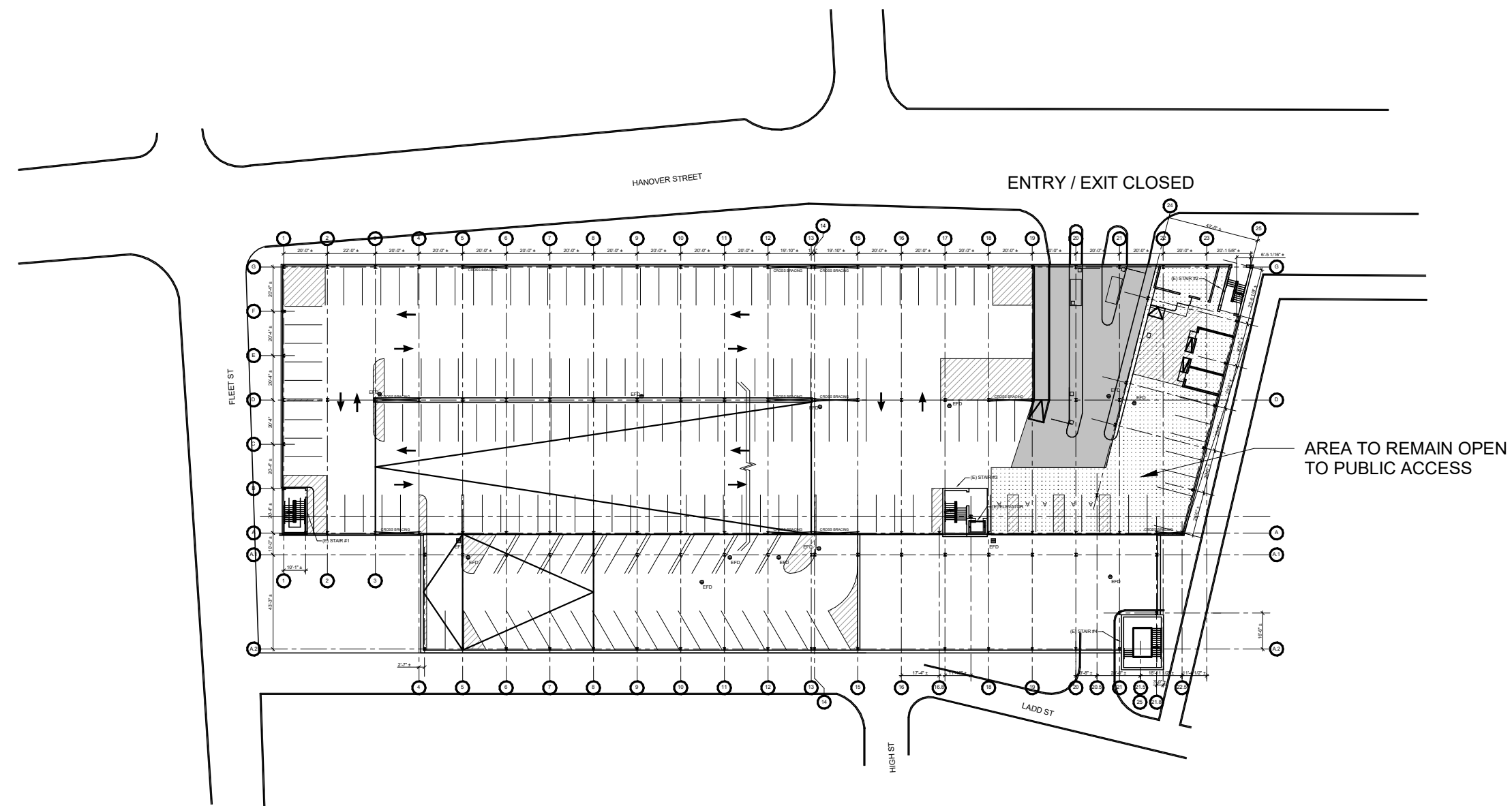
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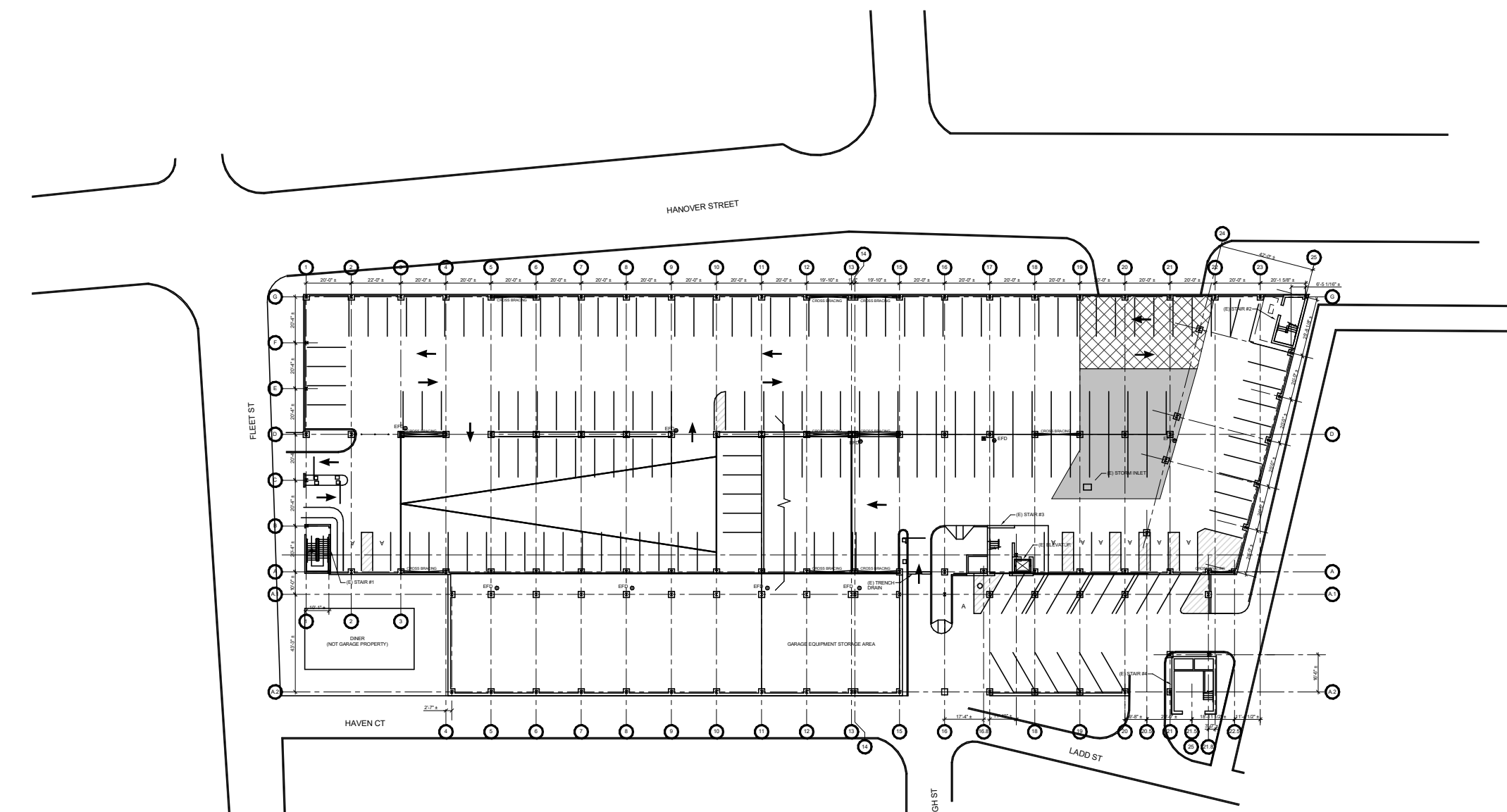
SHEET TITLE:
2023 PHASING PLANS

NOTE:

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SECOND LEVEL PHASE 6 WORK ZONE
6W

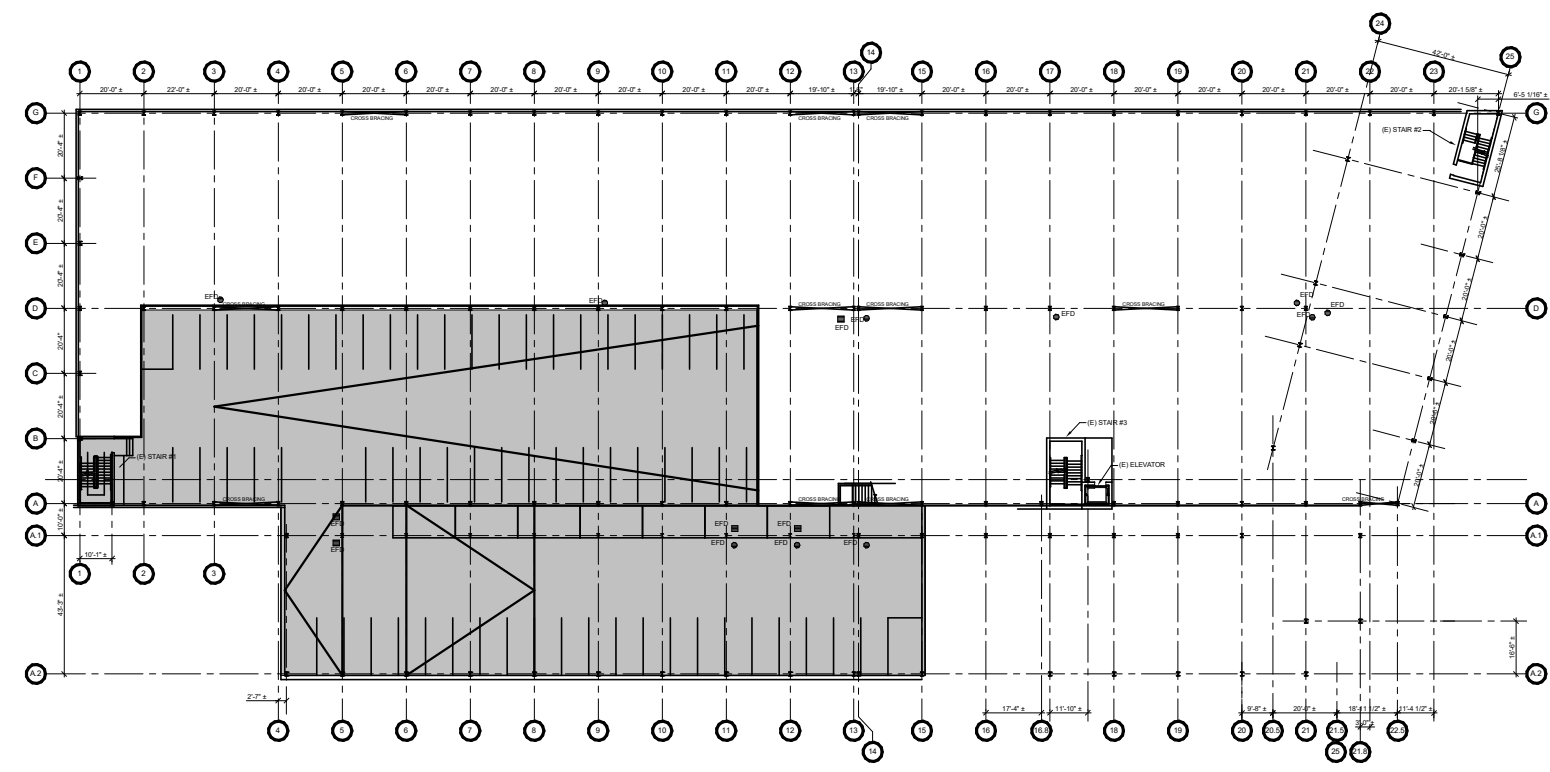


FIRST LEVEL PHASE 6 PROTECTION ZONE
6P

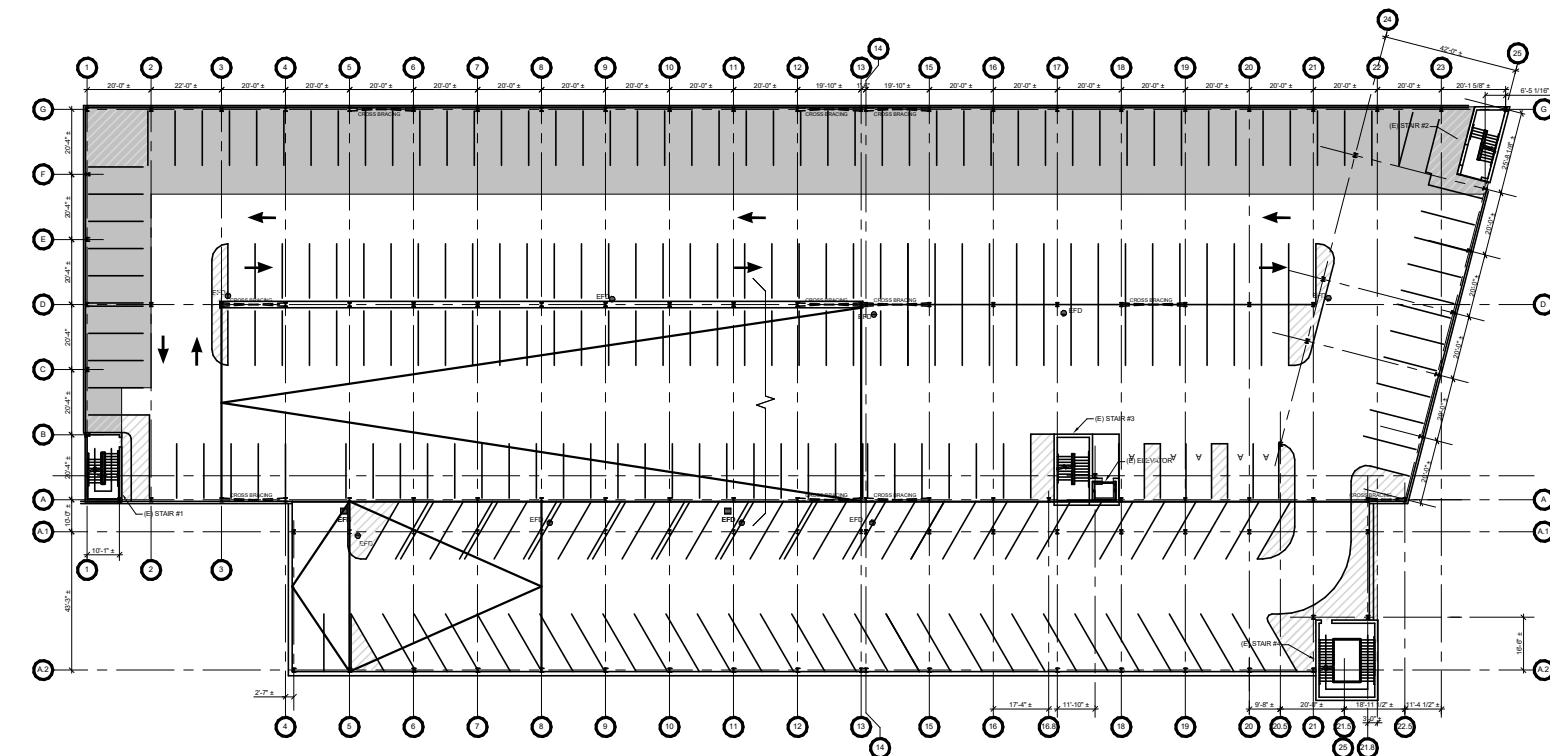
PHASING LEGEND

- INDICATES AREA CAPTURED FOR WORK OR PROTECTION ZONE
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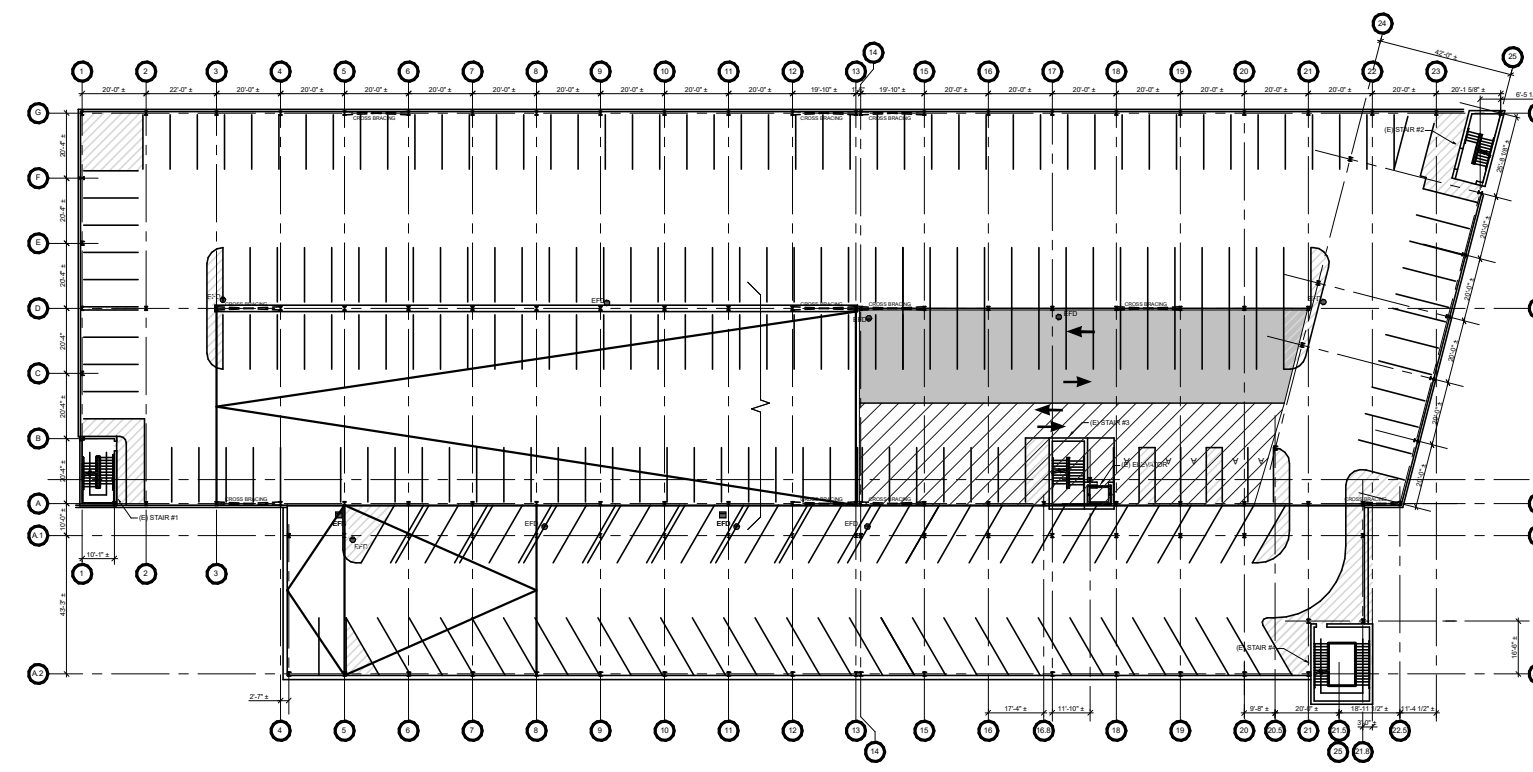
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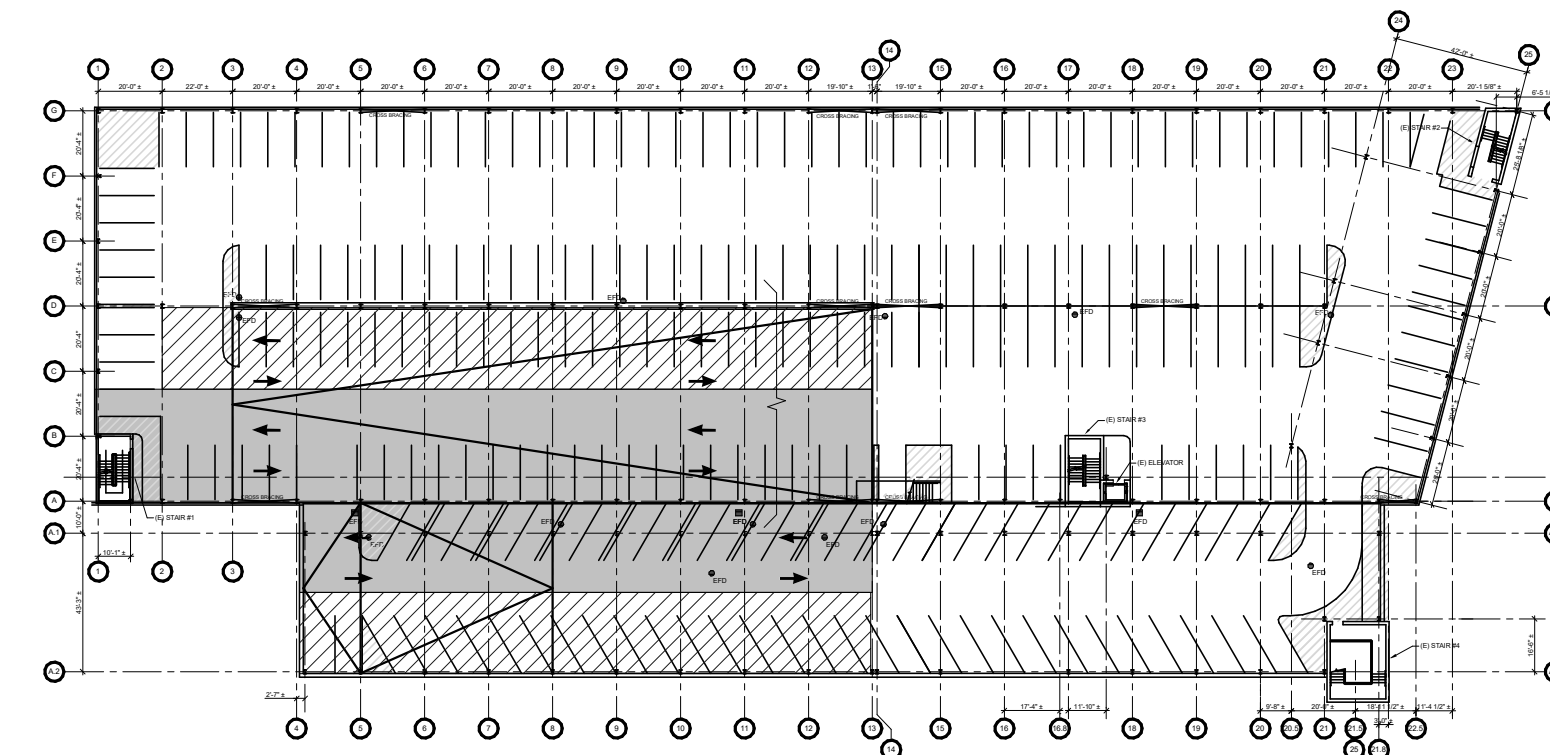
1W FIFTH LEVEL PHASE 1 WORK ZONE



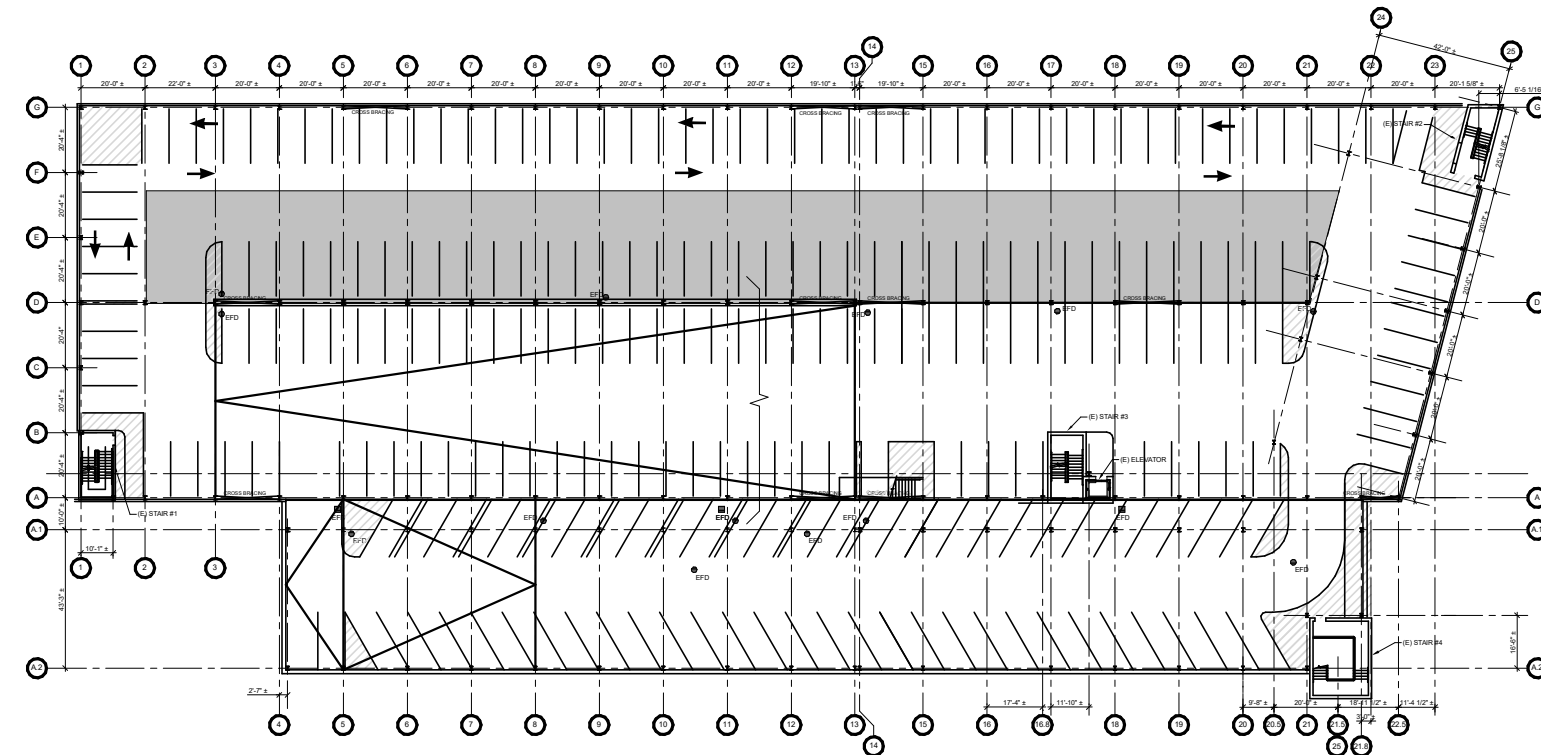
2P THIRD LEVEL PHASE 2 PROTECTION ZONE



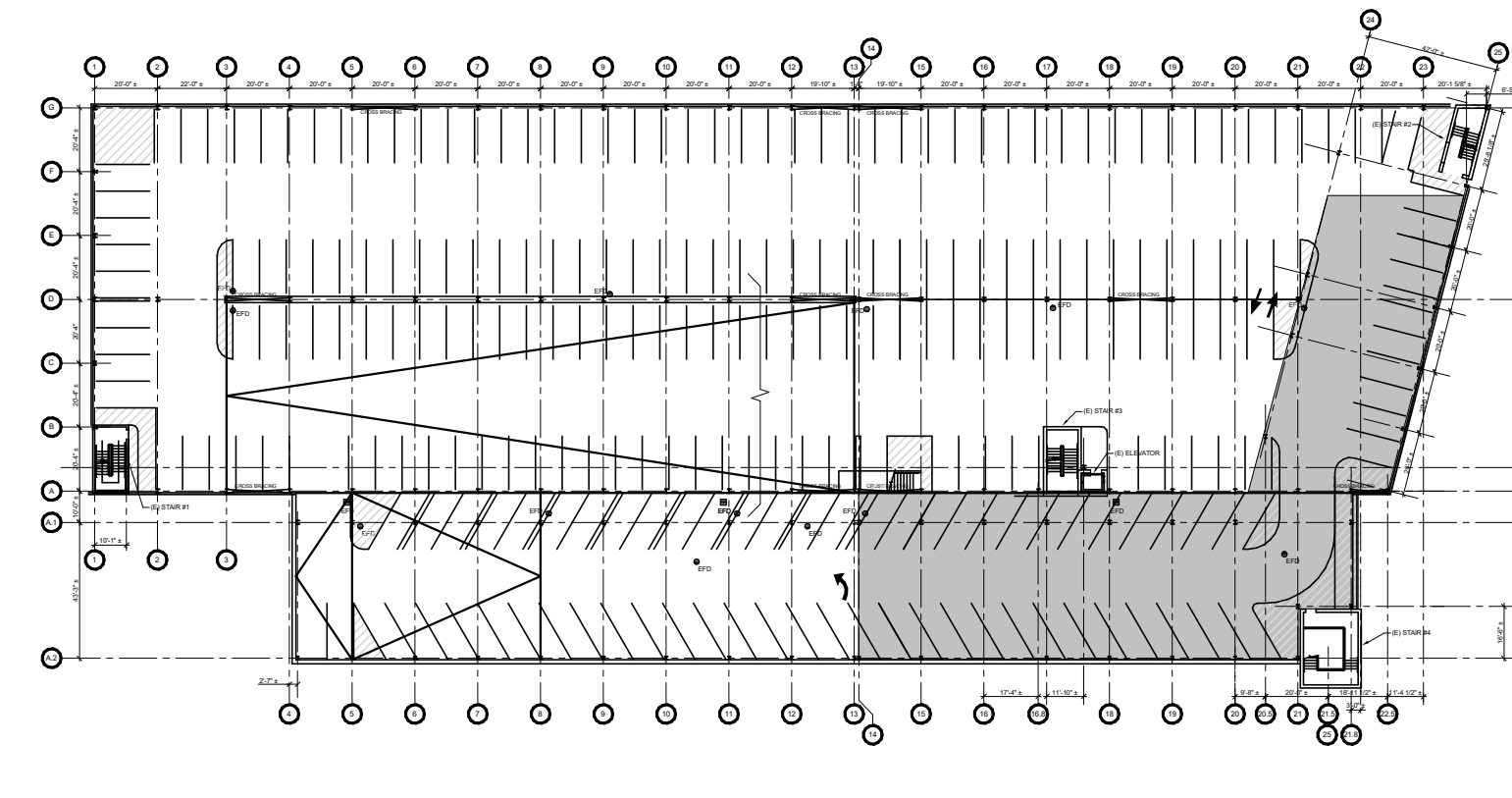
4P THIRD LEVEL PHASE 4 PROTECTION ZONE



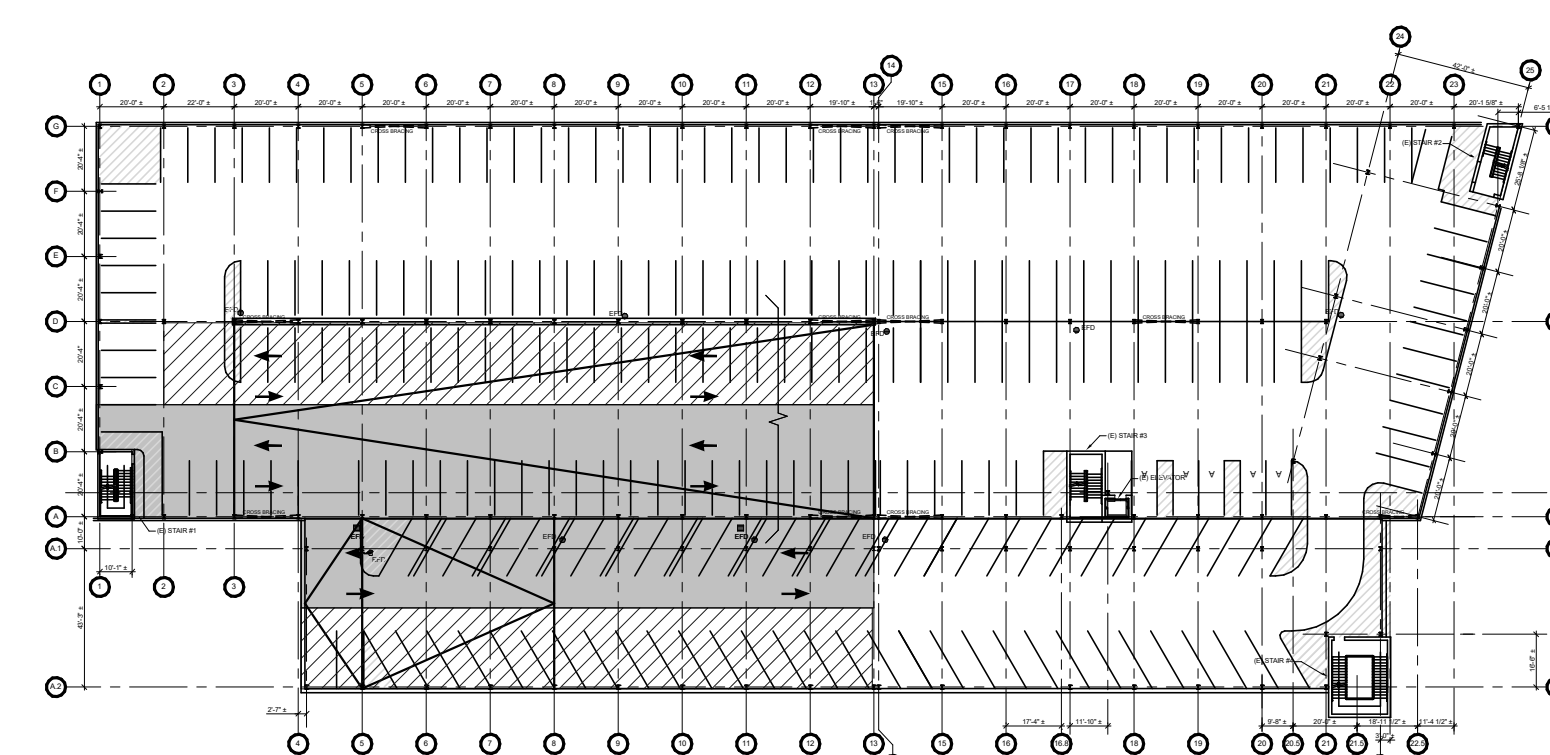
1W/1P FOURTH LEVEL PHASE 1 PROTECTION / WORK ZONE



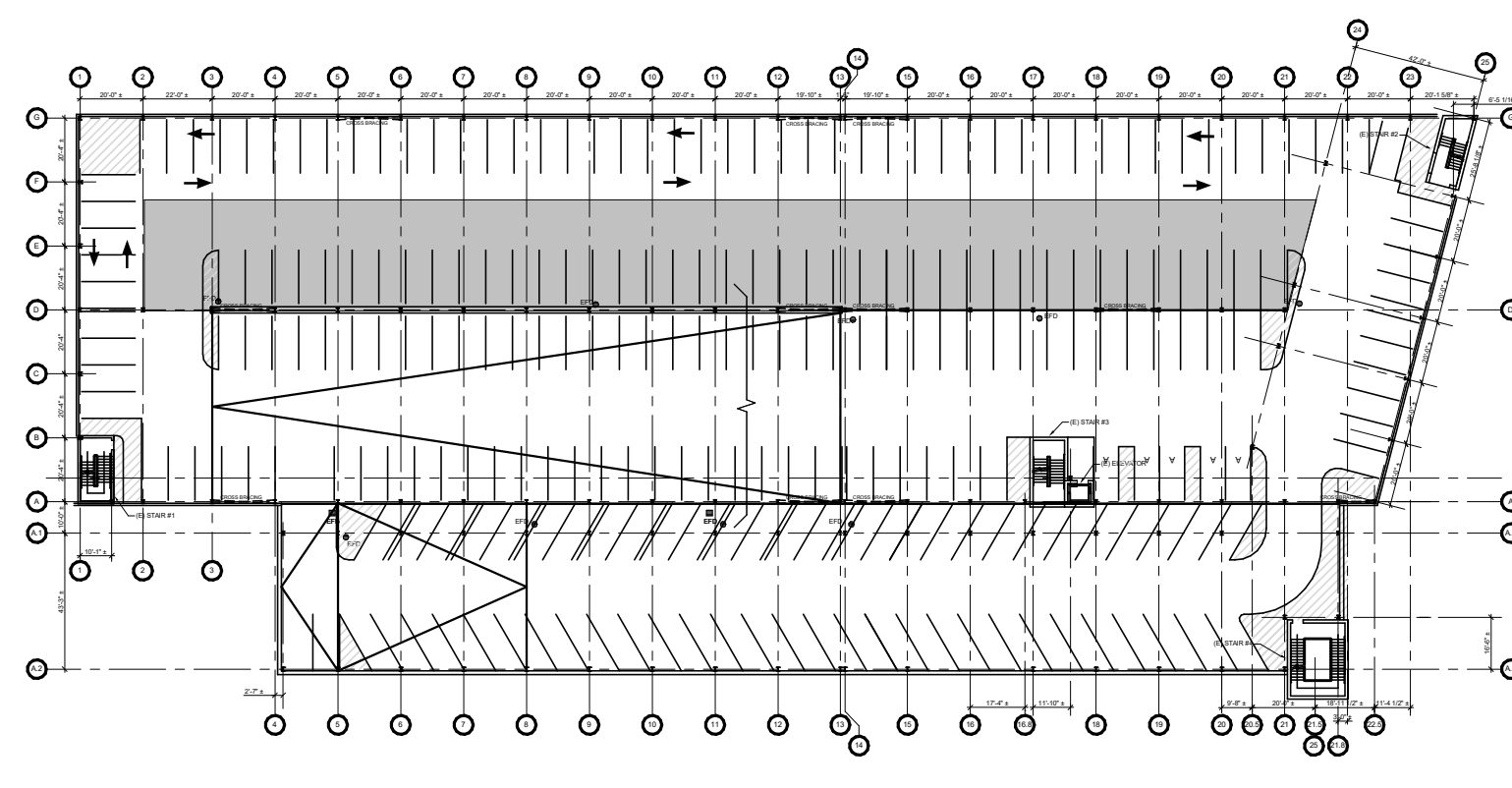
3W FOURTH LEVEL PHASE 3 WORK ZONE



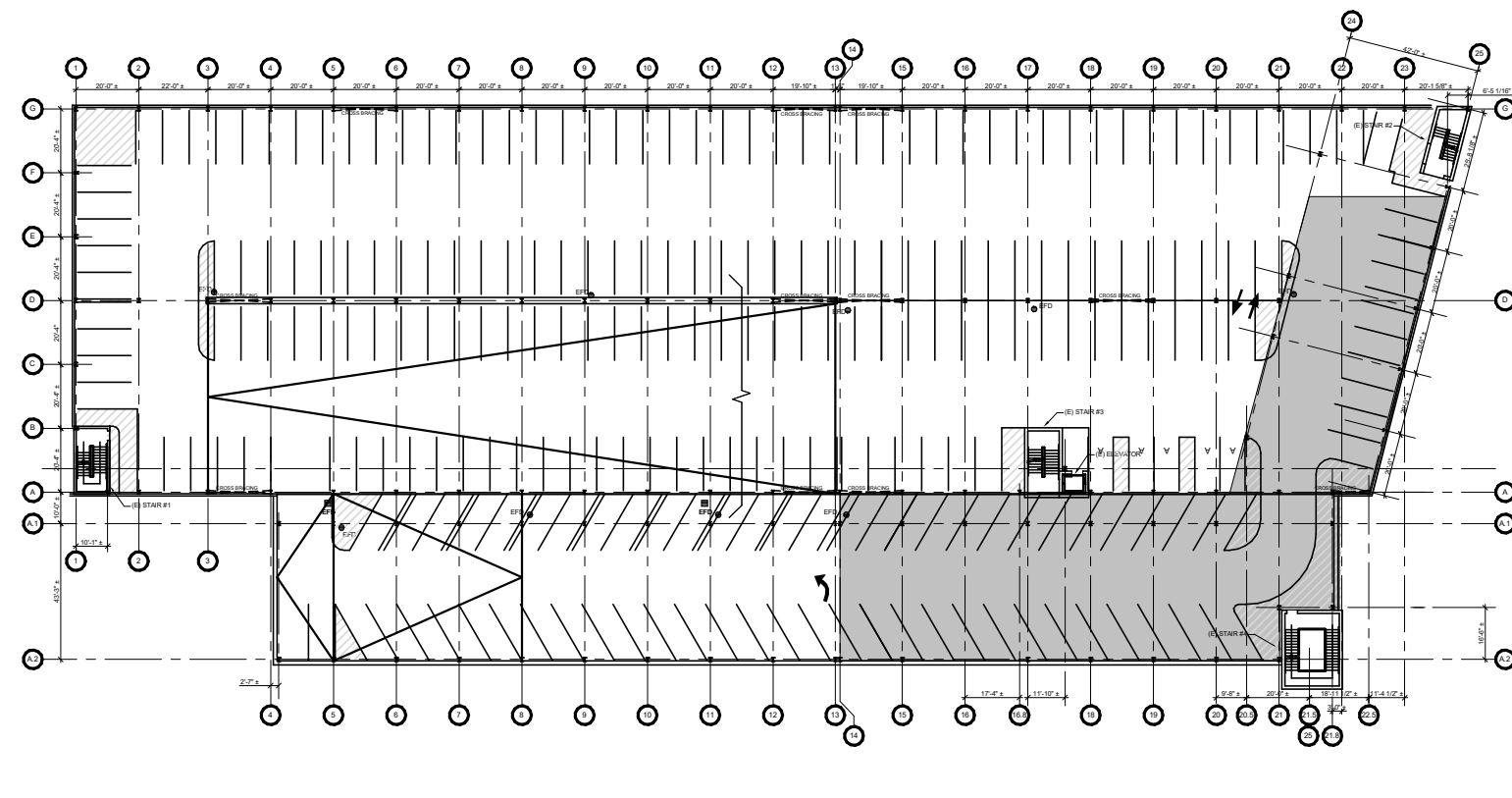
5W FOURTH LEVEL PHASE 5 WORK ZONE



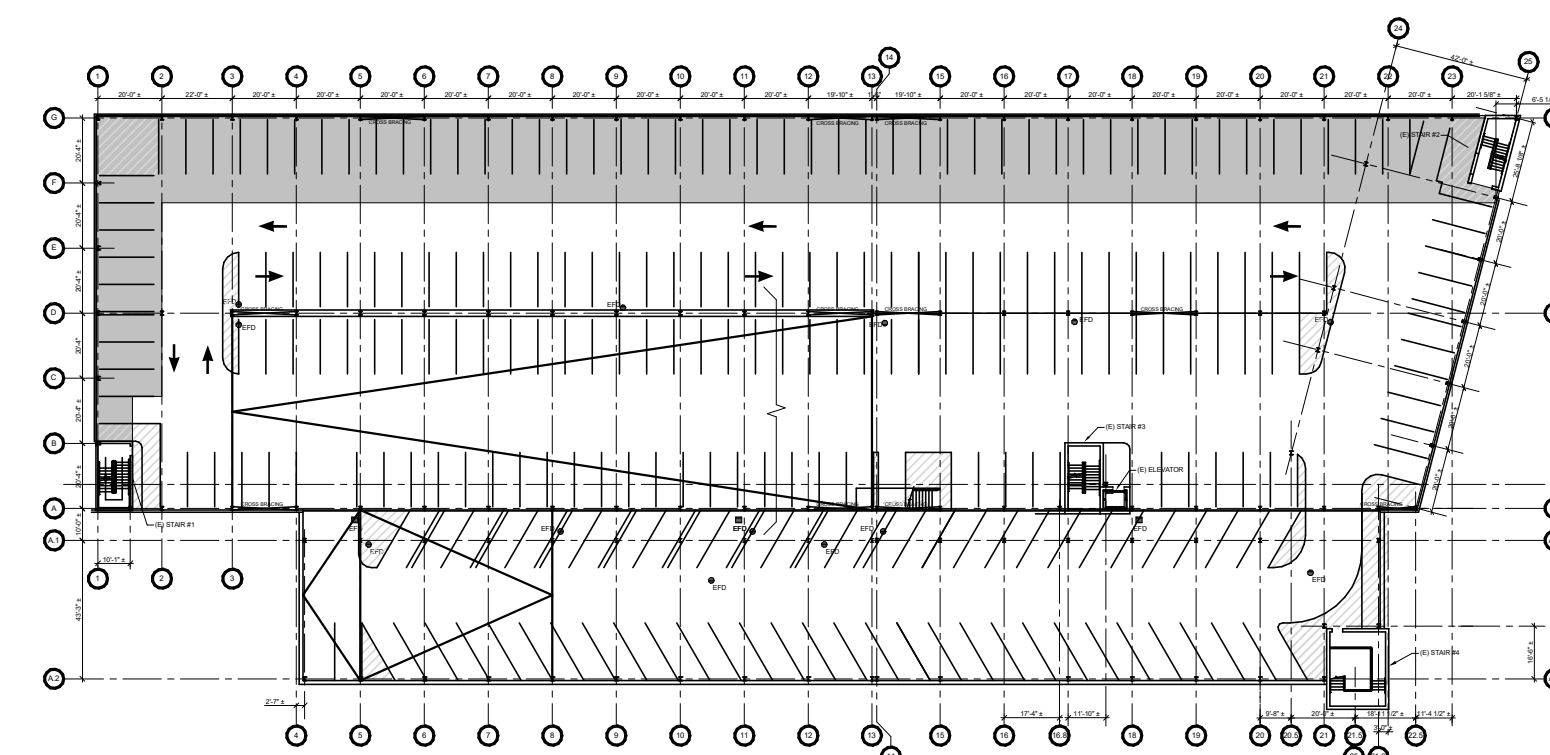
1P THIRD LEVEL PHASE 1 PROTECTION ZONE



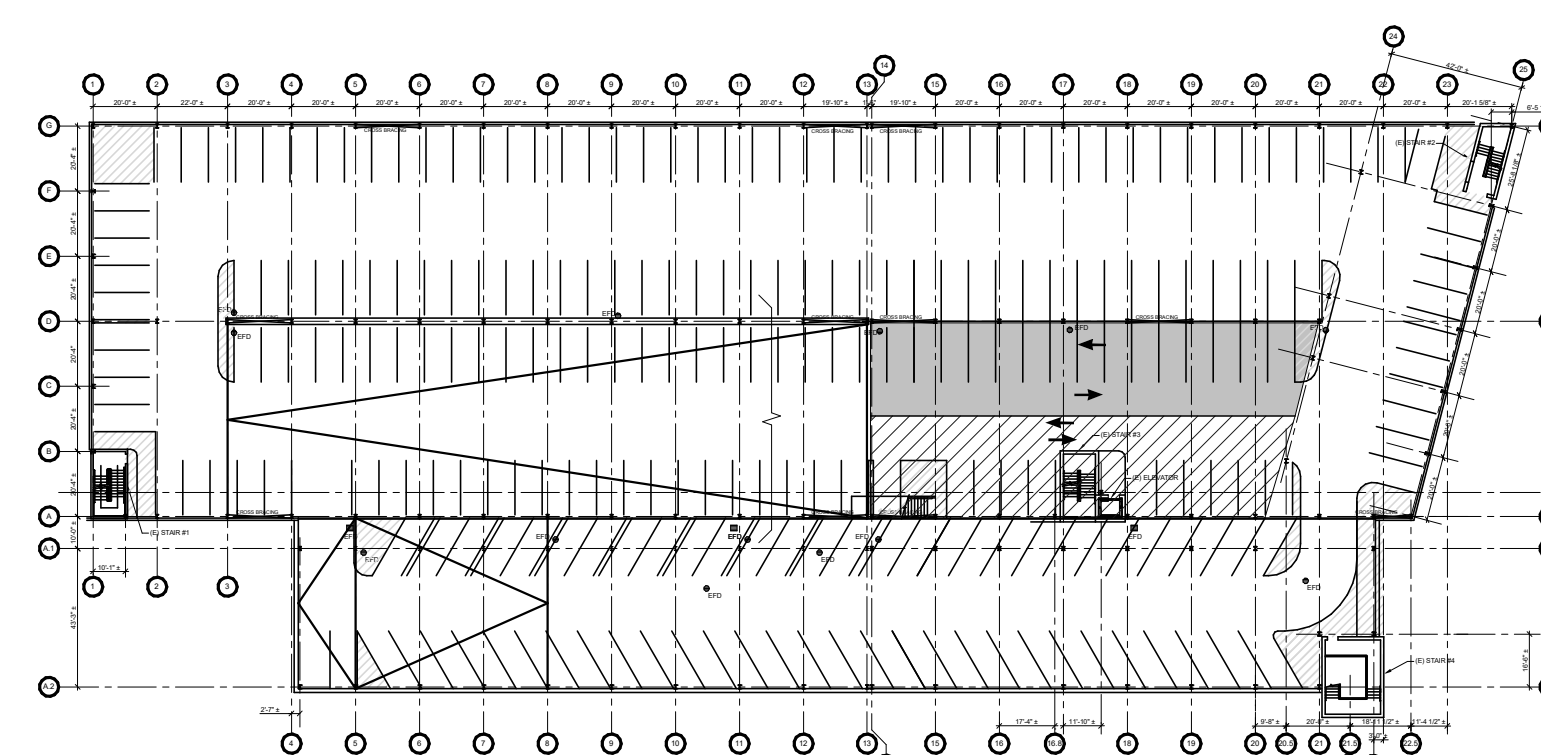
3P THIRD LEVEL PHASE 3 PROTECTION ZONE



5P THIRD LEVEL PHASE 5 PROTECTION ZONE



2W FOURTH LEVEL PHASE 2 WORK ZONE



4W FOURTH LEVEL PHASE 4 WORK ZONE

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SHEET TITLE:
2024 PHASING PLANS

PHASING LEGEND

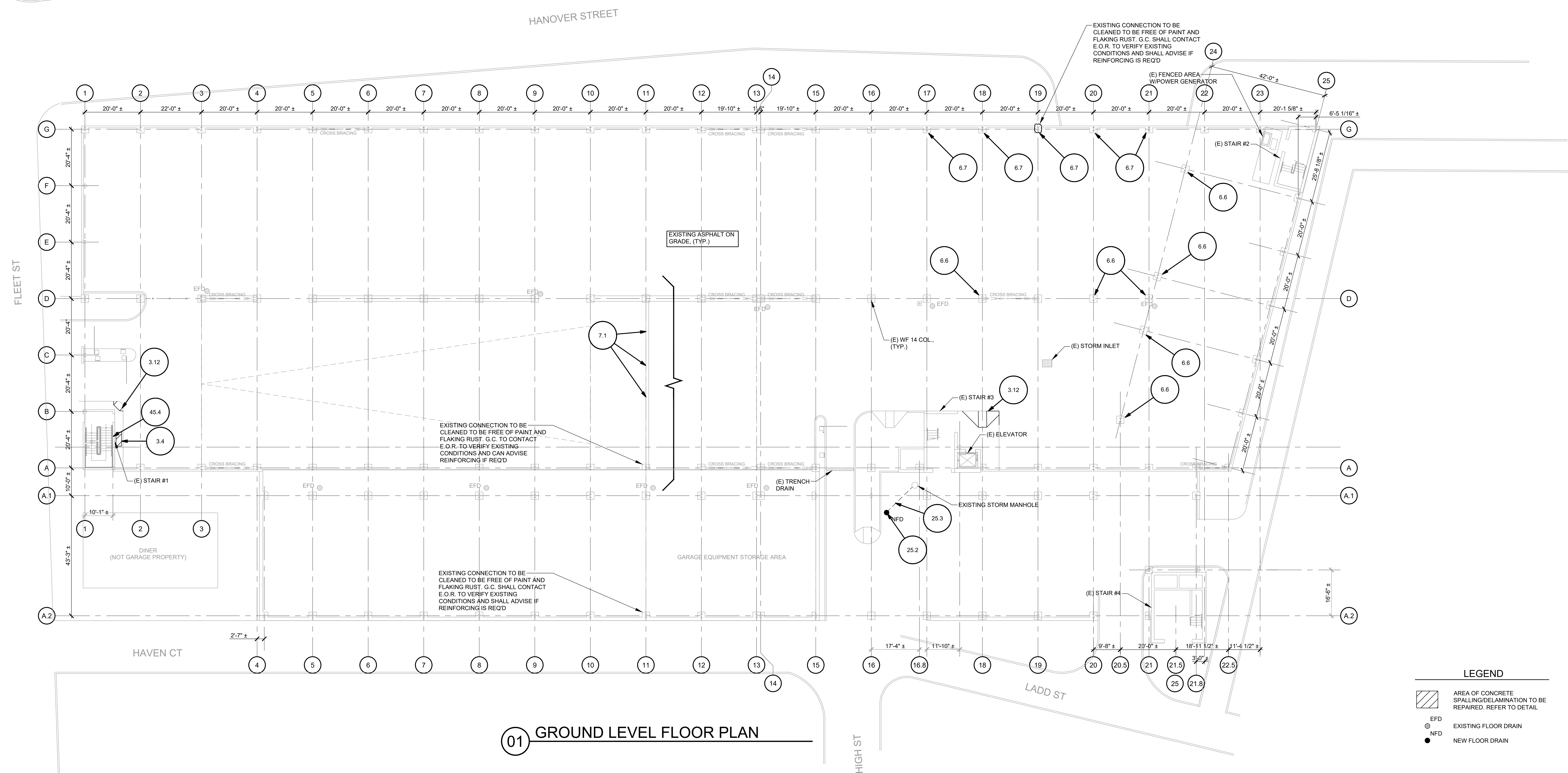
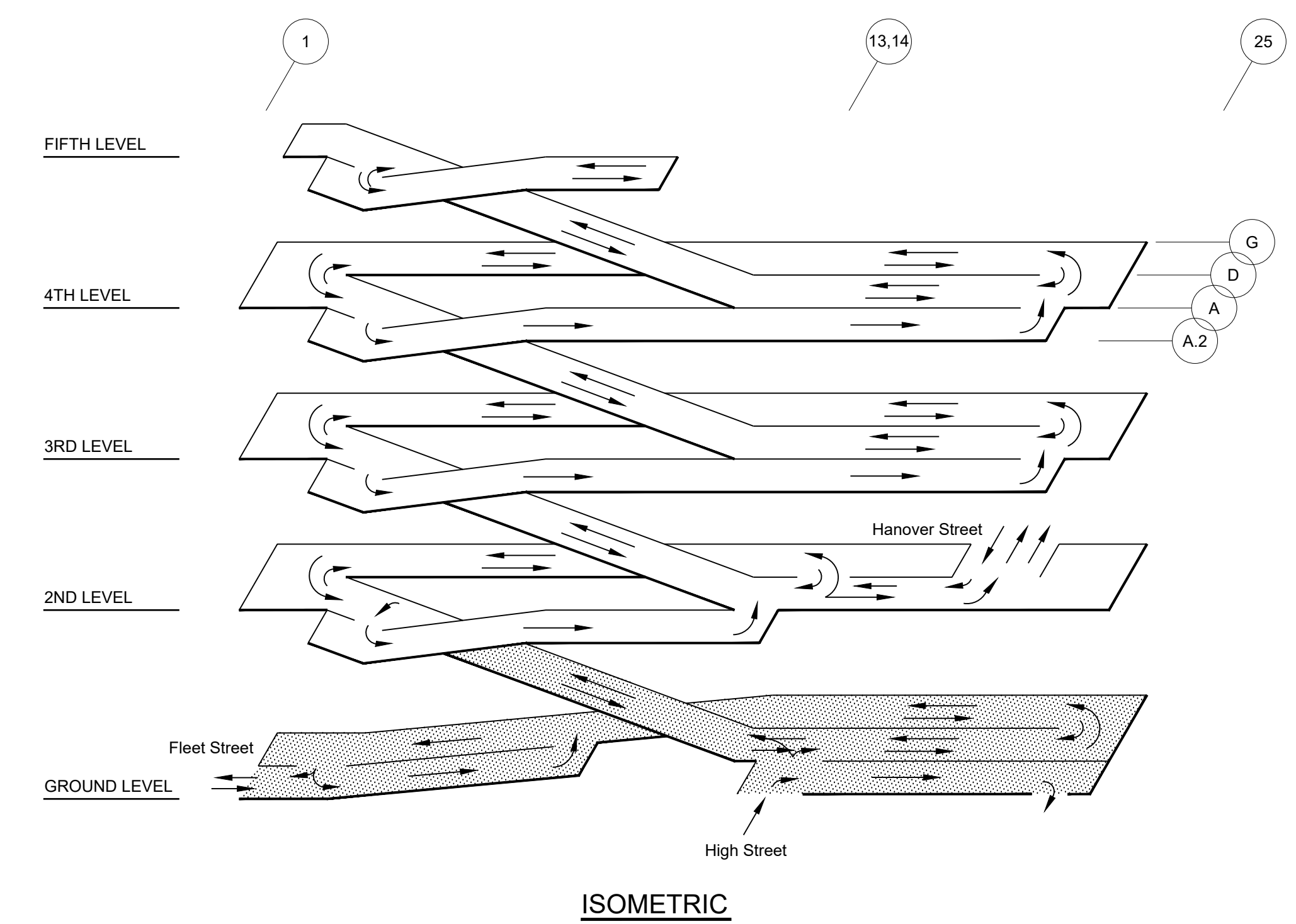
- INDICATES AREA CAPTURED FOR WORK OR PROTECTION ZONE
- INDICATES SECONDARY PHASE WITHIN WORK OR PROTECTION ZONE
- INDICATES OVERHEAD FALSE WORK AREA
- INDICATES TEMPORARY CHANGE IN TRAFFIC CIRCULATION AROUND WORK OR PROTECTION ZONE

R-006

SHEET NOTES:

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- SEE SHEET SERIES R-300 FOR STAIR REPAIRS.
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- REMOVE AND REPLACE ALL EXISTING HORIZONTAL JOINT SEALANT, REFER TO WORK ITEM SERIES 11.0

NOTE:
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HANOVER STREET GARAGE RESTORATION
 34 HANOVER STREET
 PORTSMOUTH, NH 03801

MARK	DATE	DESCRIPTION
REVISIONS		
ISSUE: BIDDING AND CONSTRUCTION		
ISSUE DATE: 1/25/2022		
PROJECT NO: 16-003129.00		
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CHECKED BY: MAZ		

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 SHEET TITLE:
GROUND LEVEL FLOOR PLAN

R-101A

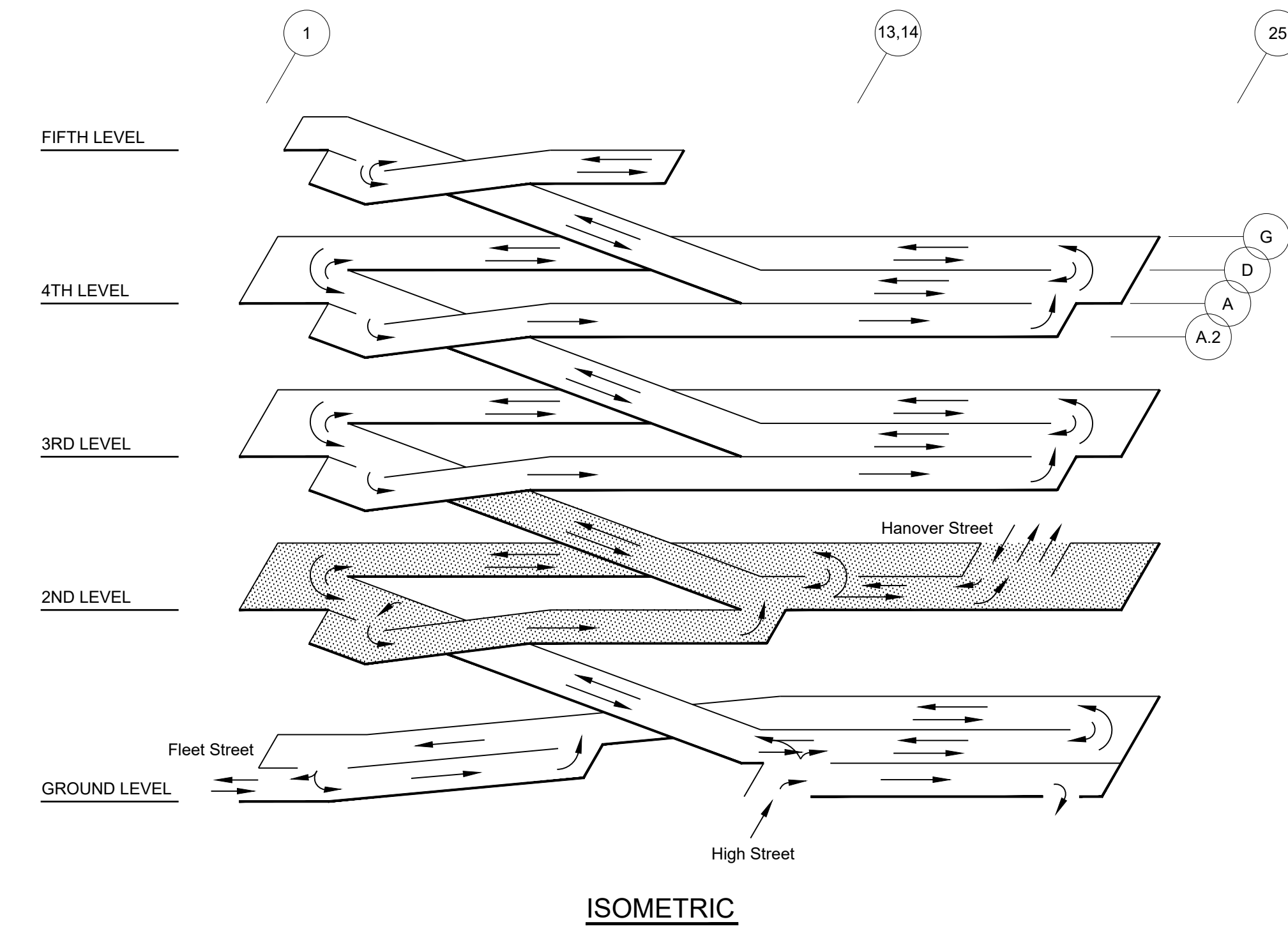
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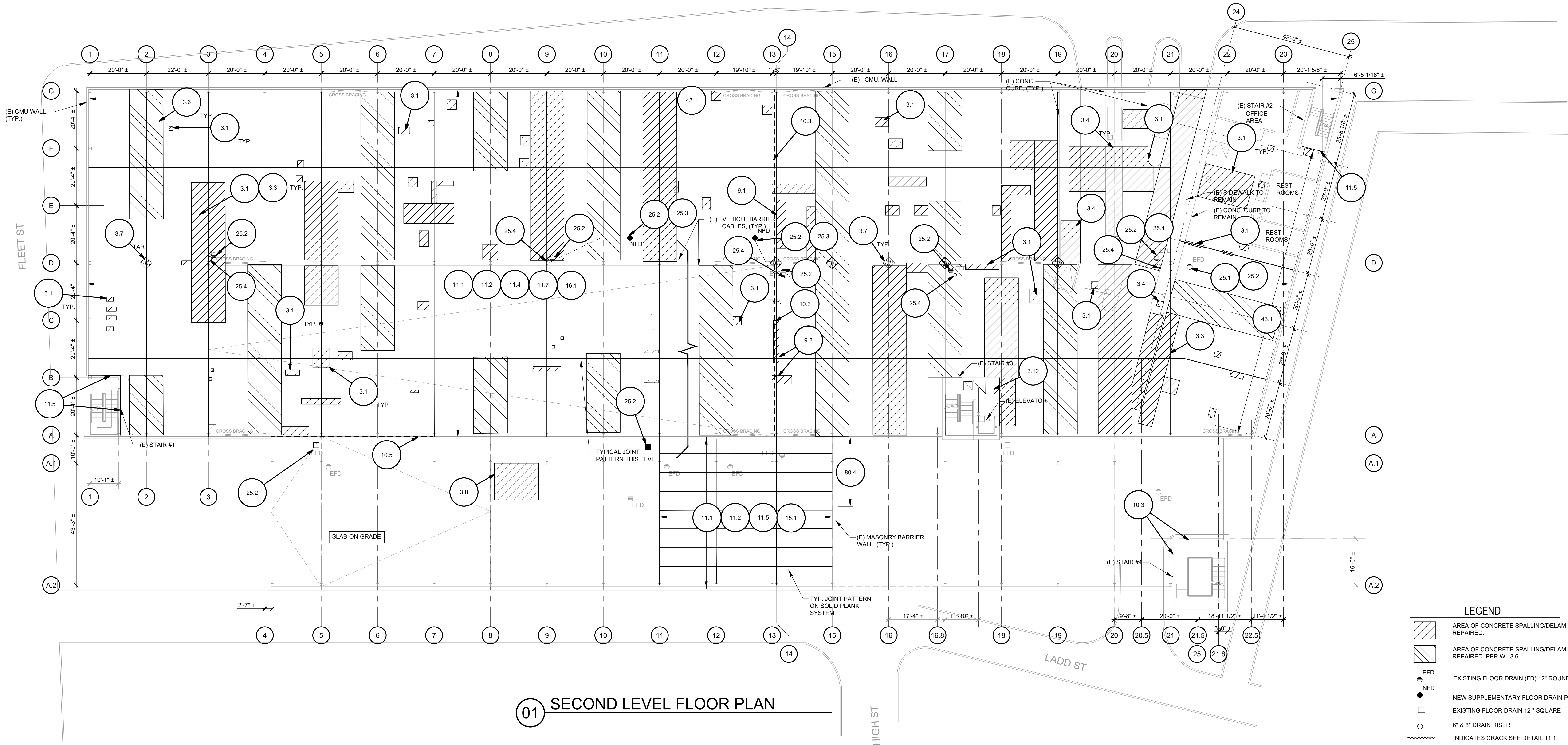
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HANOVER STREET



Arthur G. Stacks

HANOVER STREET GARAGE RESTORATION

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SHEET TITLE:
SECOND LEVEL FLOOR PLAN

R-102A

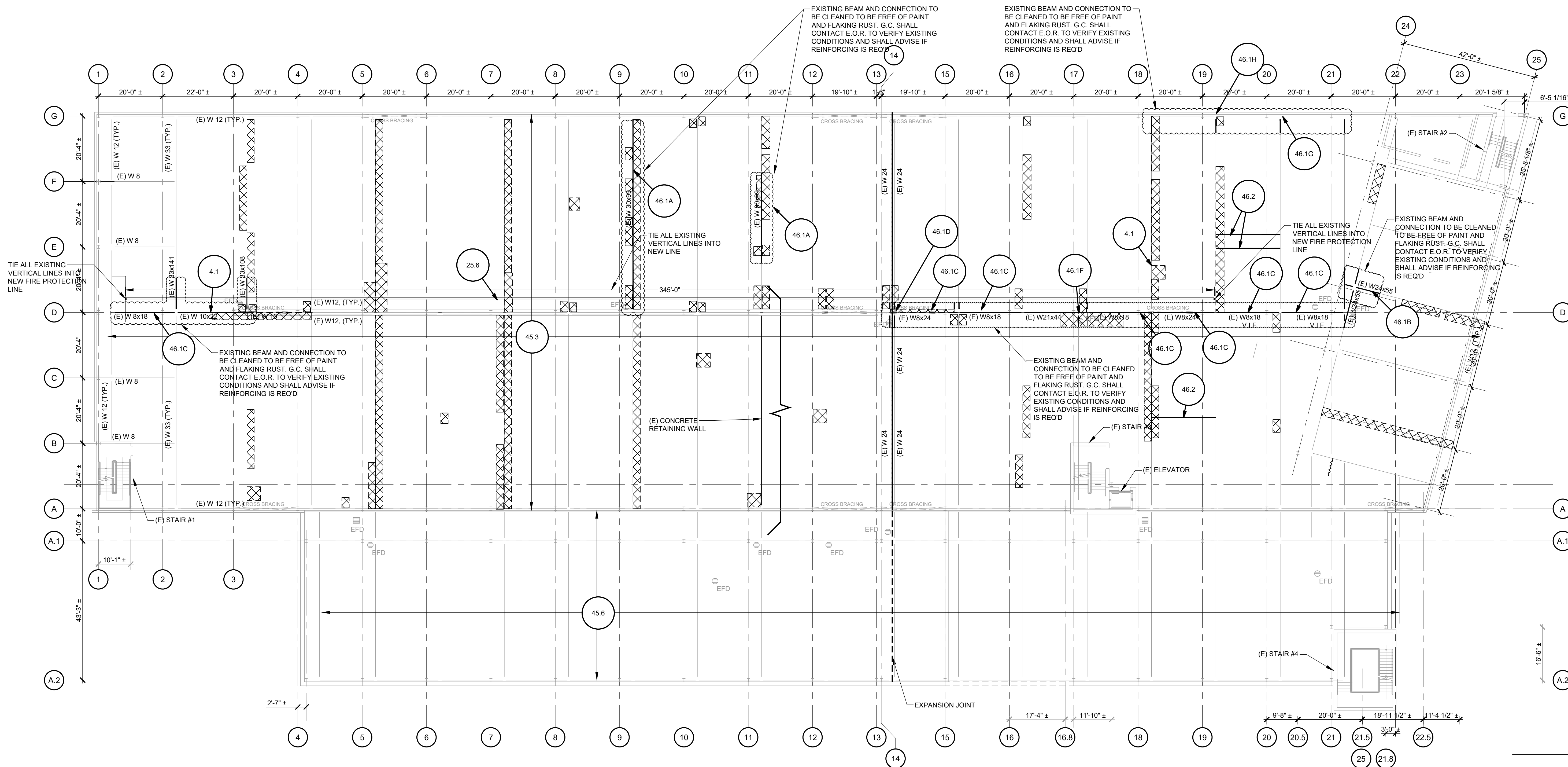
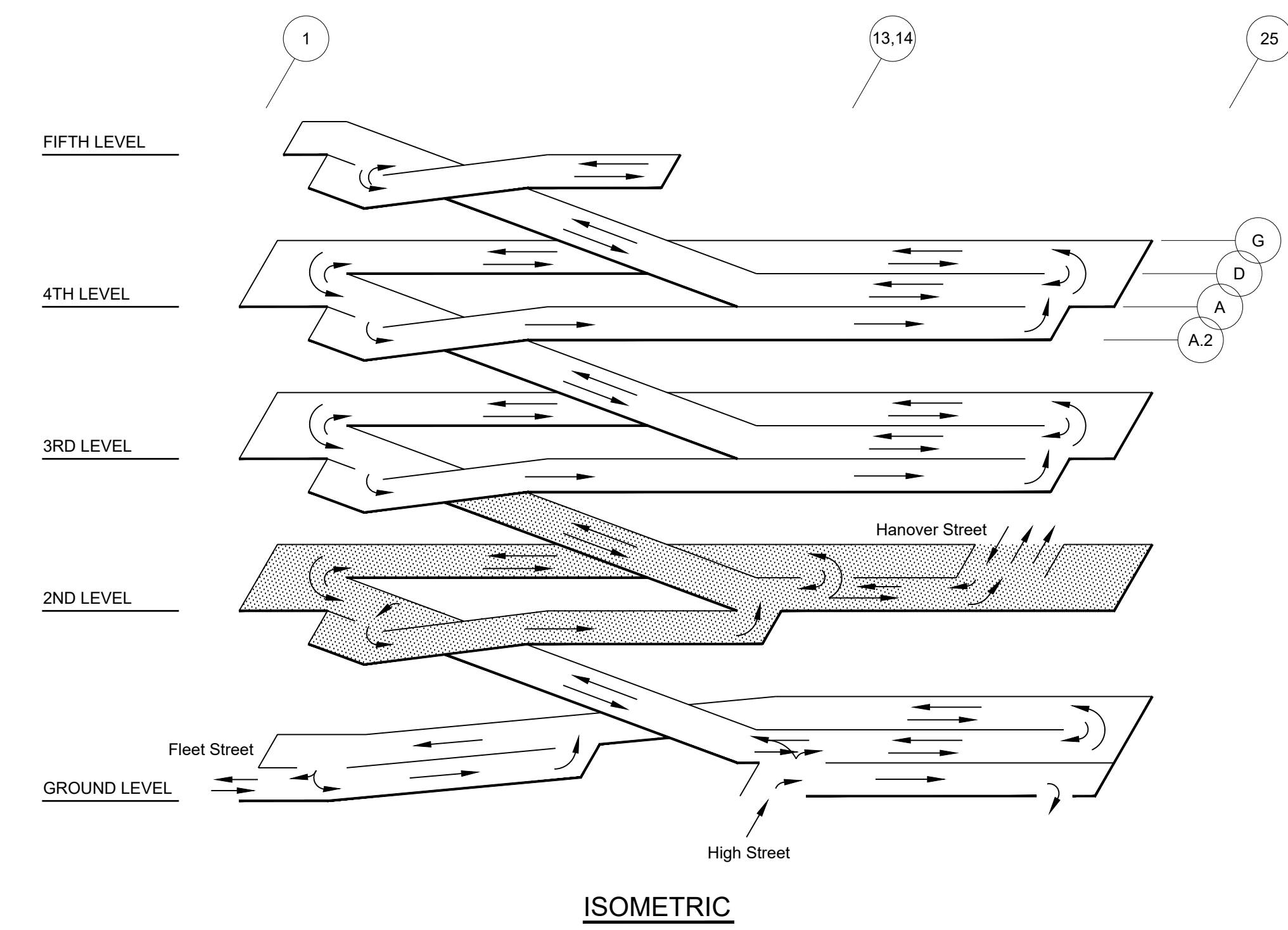
- LEGEND**
- AREA OF CONCRETE SPALLING/DELAMINATION TO BE REPAIRED.
 - AREA OF CONCRETE SPALLING/DELAMINATION TO BE REPAIRED, PER WI. 3.6
 - EXISTING FLOOR DRAIN (FD) 12" ROUND
 - NEW SUPPLEMENTARY FLOOR DRAIN PER WI 25.2
 - EXISTING FLOOR DRAIN 12" SQUARE
 - 6" & 8" DRAIN RISER
 - INDICATES CRACK SEE DETAIL 11.1

SHEET NOTES:

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LEGEND

	AREA OF CONCRETE SPALLING/DELAMINATION TO BE REPAIRED PER W.I. 4.1 & 4.2
	EXISTING FLOOR DRAIN



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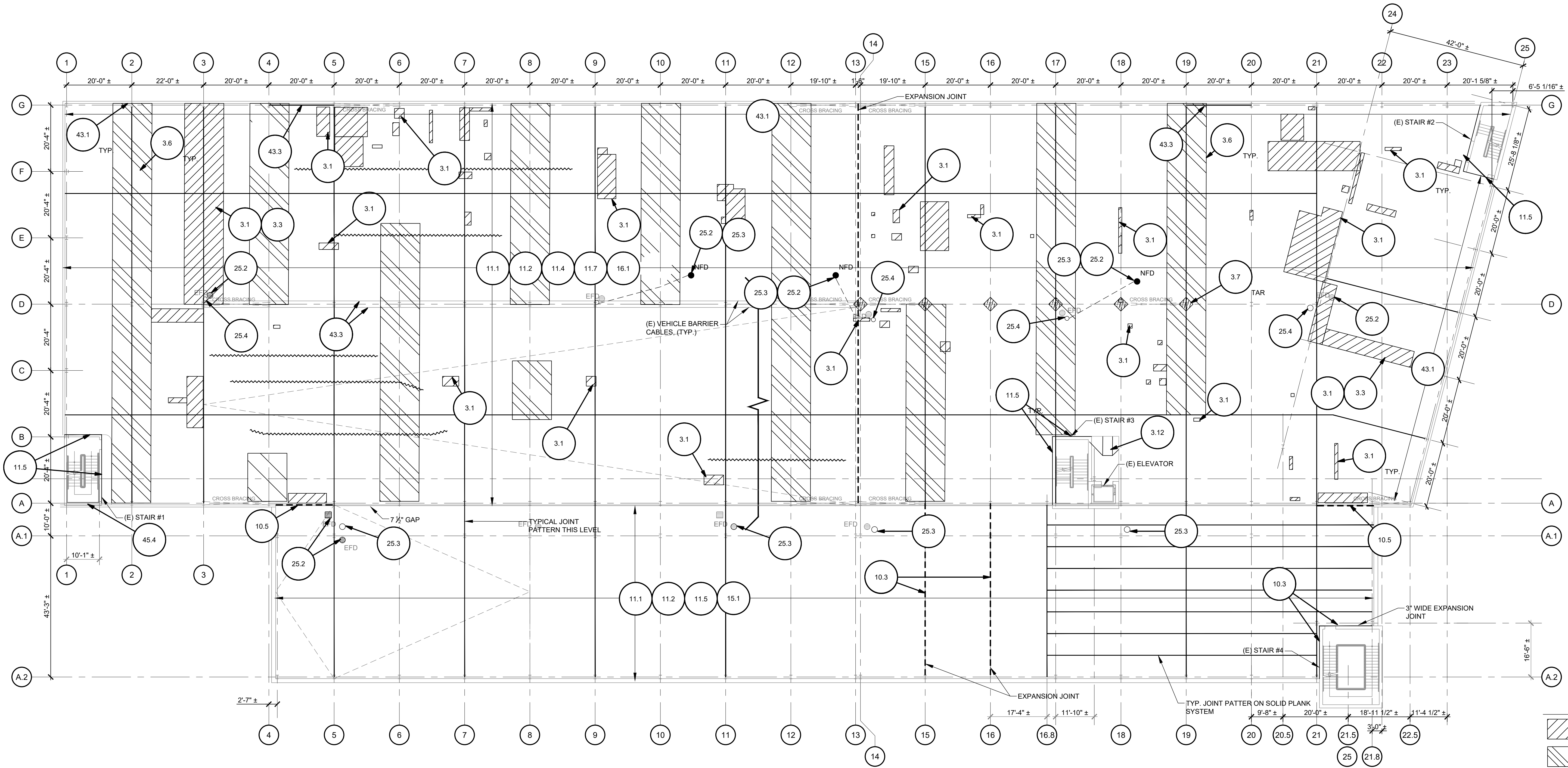
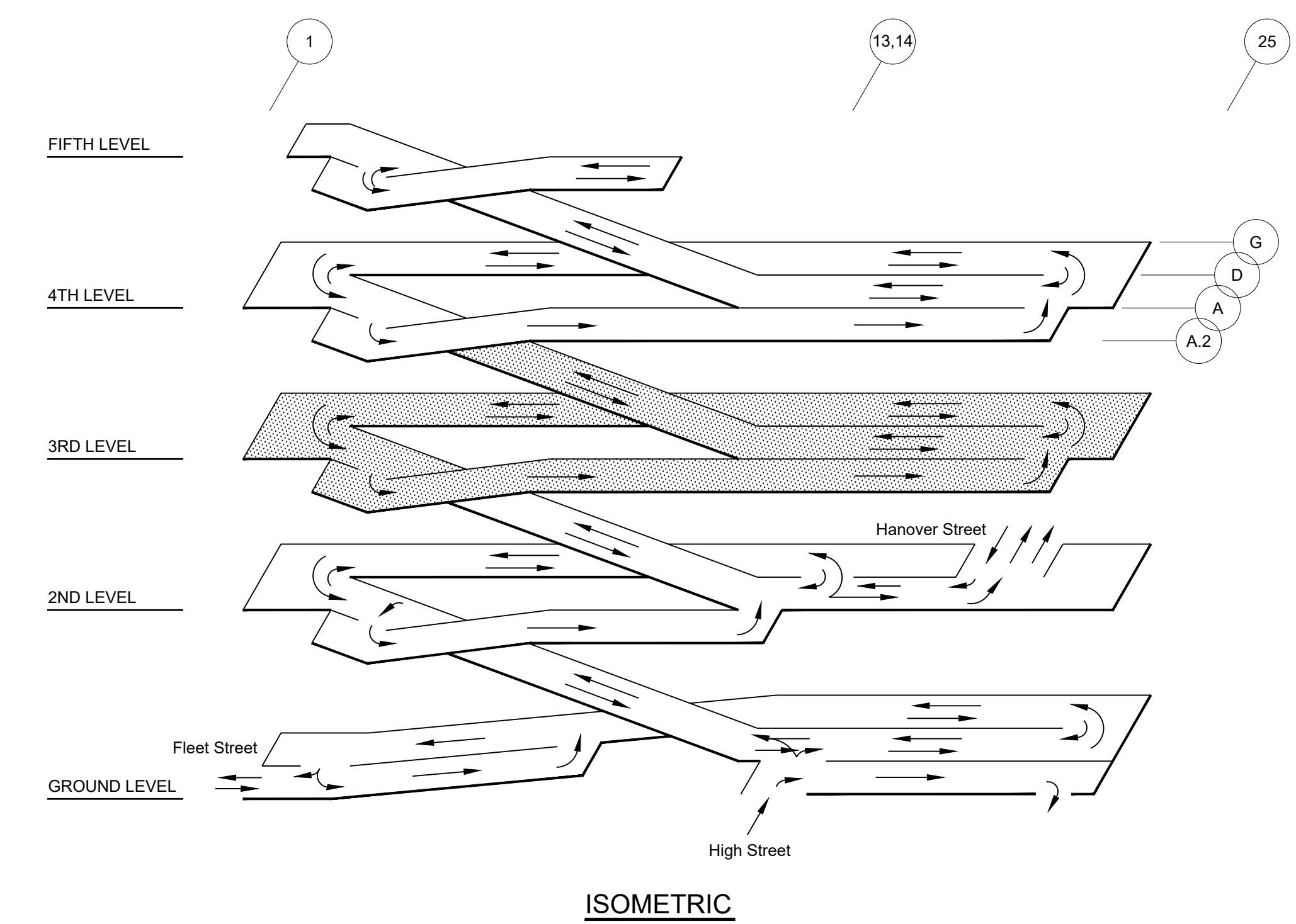
SHEET TITLE:
SECOND LEVEL UNDERSIDE PLAN

R-102B

SHEET NOTES:

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01 THIRD LEVEL FLOOR PLAN

LEGEND

- AREA OF CONCRETE SPALLING/DELAMINATION TO BE REPAIRED
- AREA OF CONCRETE SPALLING/DELAMINATION TO BE REPAIRED, PER WI. 3.6
- EXISTING FLOOR DRAIN (FD) 12" ROUND
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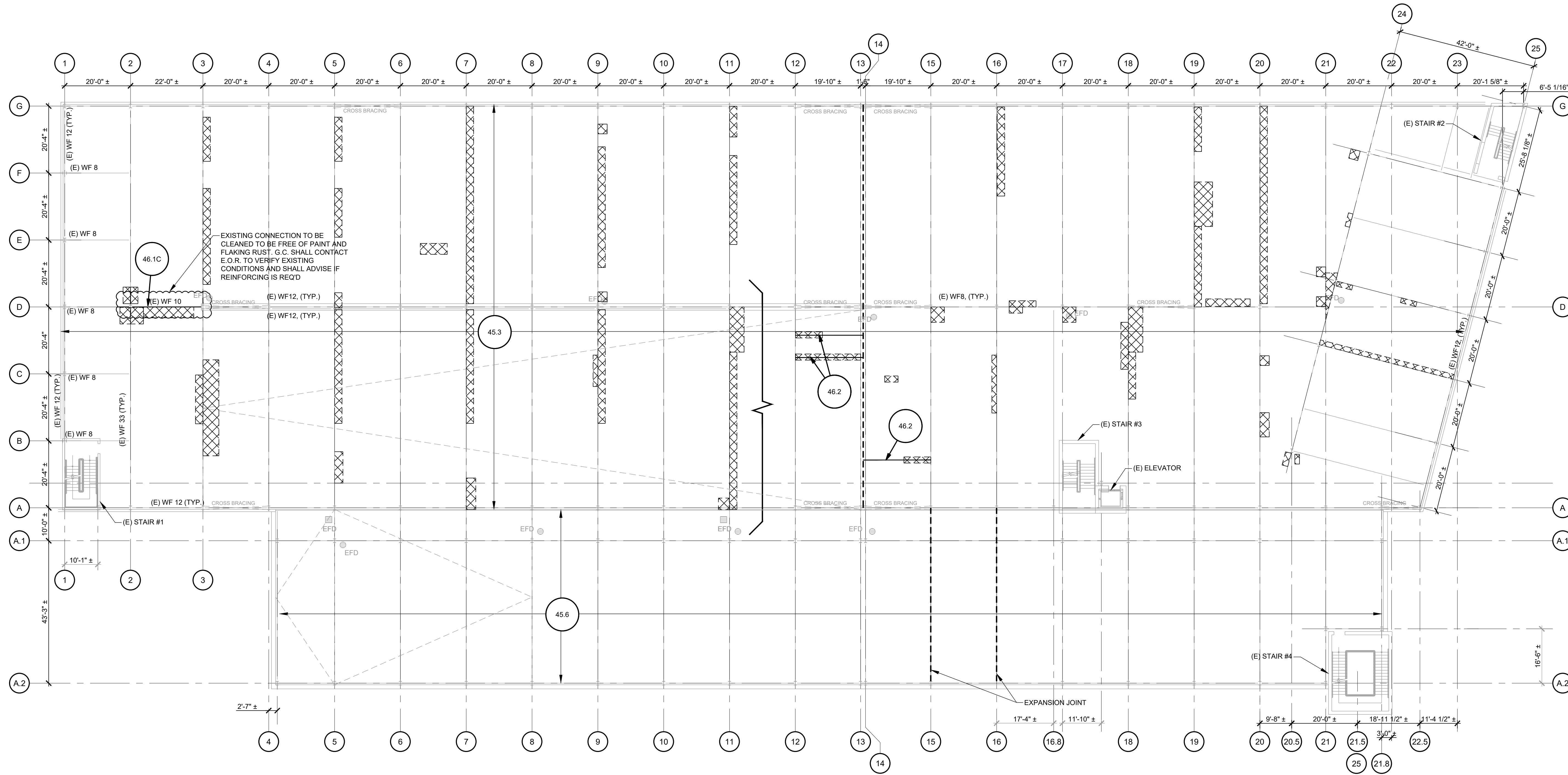
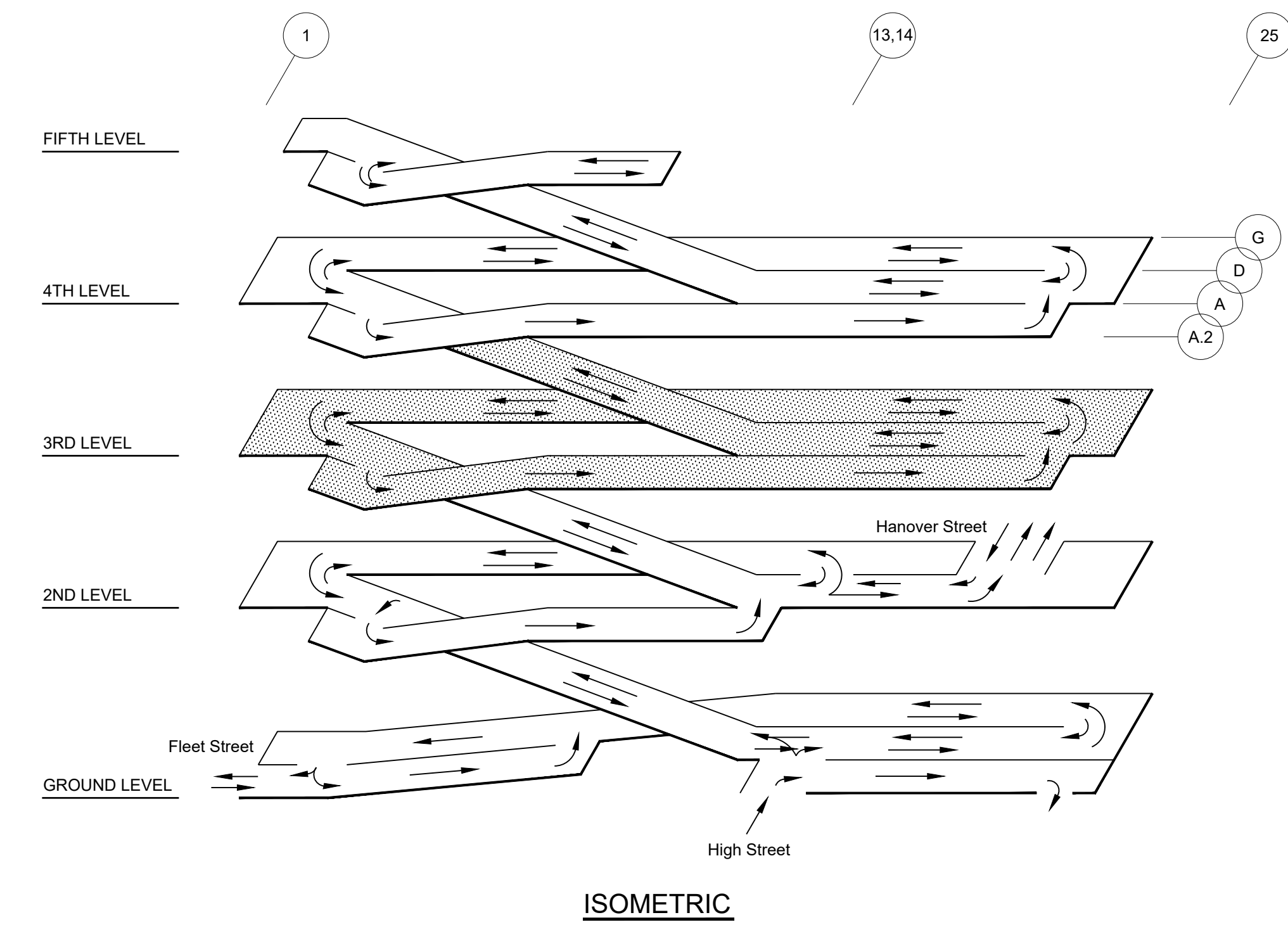
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 SHEET TITLE:
THIRD LEVEL FLOOR PLAN

R-103A

SHEET NOTES:

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01 THIRD LEVEL UNDERSIDE PLAN

LEGEND

	AREA OF CONCRETE SPALLING/DELMINATION TO BE REPAIRED PER W.I. 4.1 & 4.2
	EXISTING FLOOR DRAIN



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 34 HANOVER STREET
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THIRD LEVEL UNDERSIDE PLAN

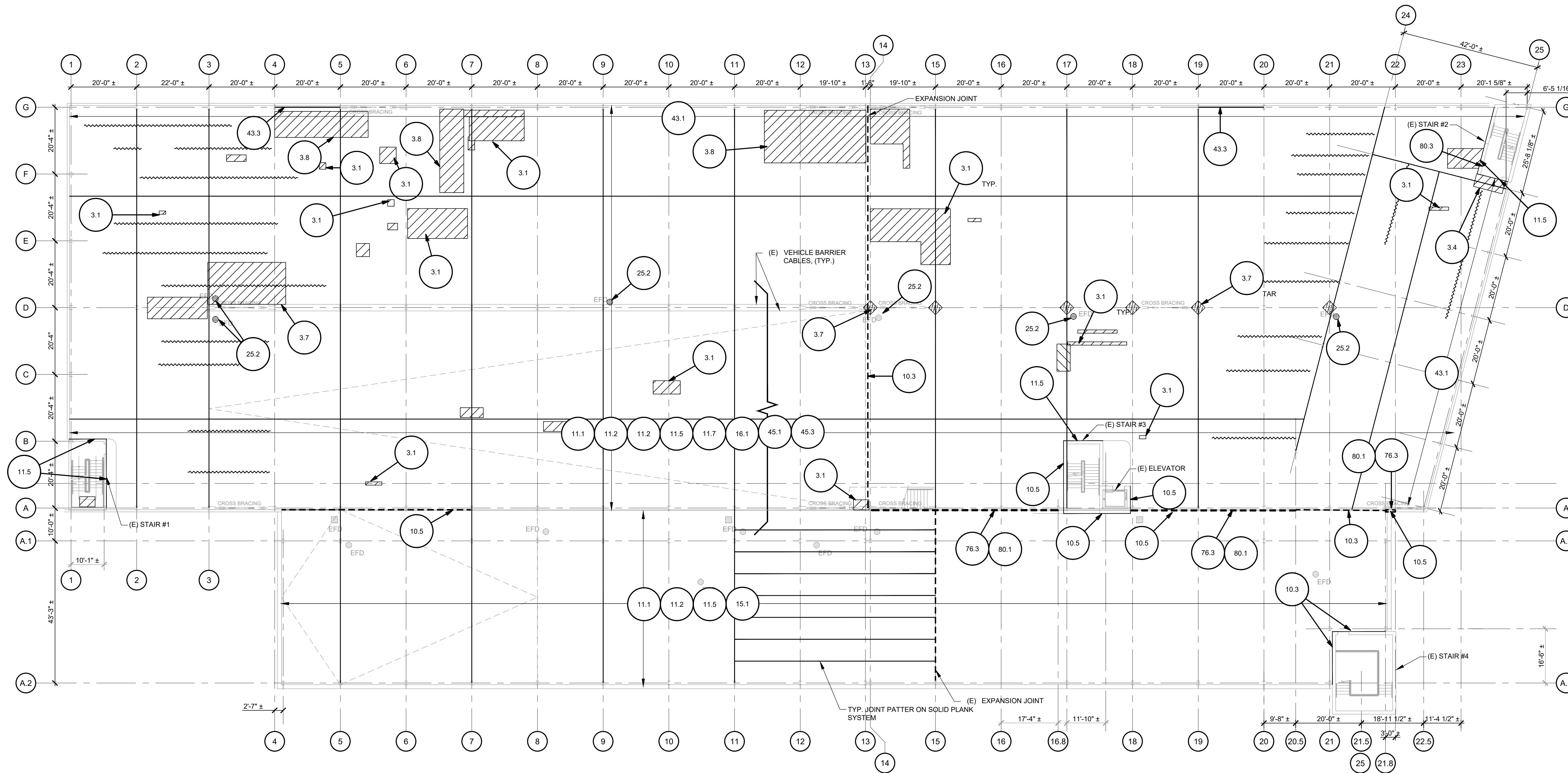
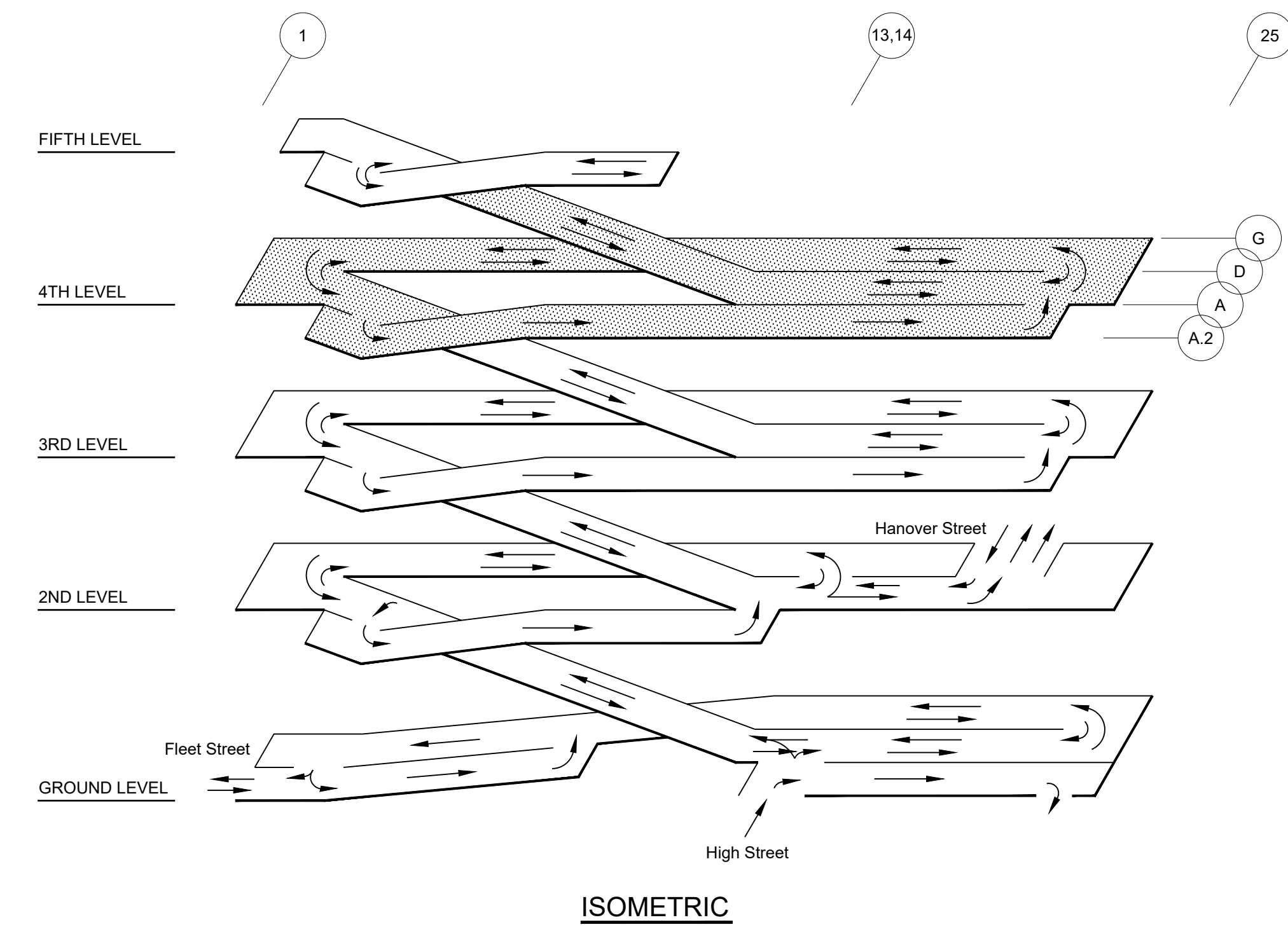
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01 FOURTH LEVEL FLOOR PLAN

- LEGEND**
- AREA OF CONCRETE SPALLING/DELAMINATION TO BE REPAIRED.
 - EXISTING FLOOR DRAIN (FD) 12" ROUND
 - NEW SUPPLEMENTARY FLOOR DRAIN PER WI 25.2
 - EXISTING FLOOR DRAIN 12" SQUARE
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 - INDICATES CRACK SEE DETAIL 11.1



Arthur G. Stacks

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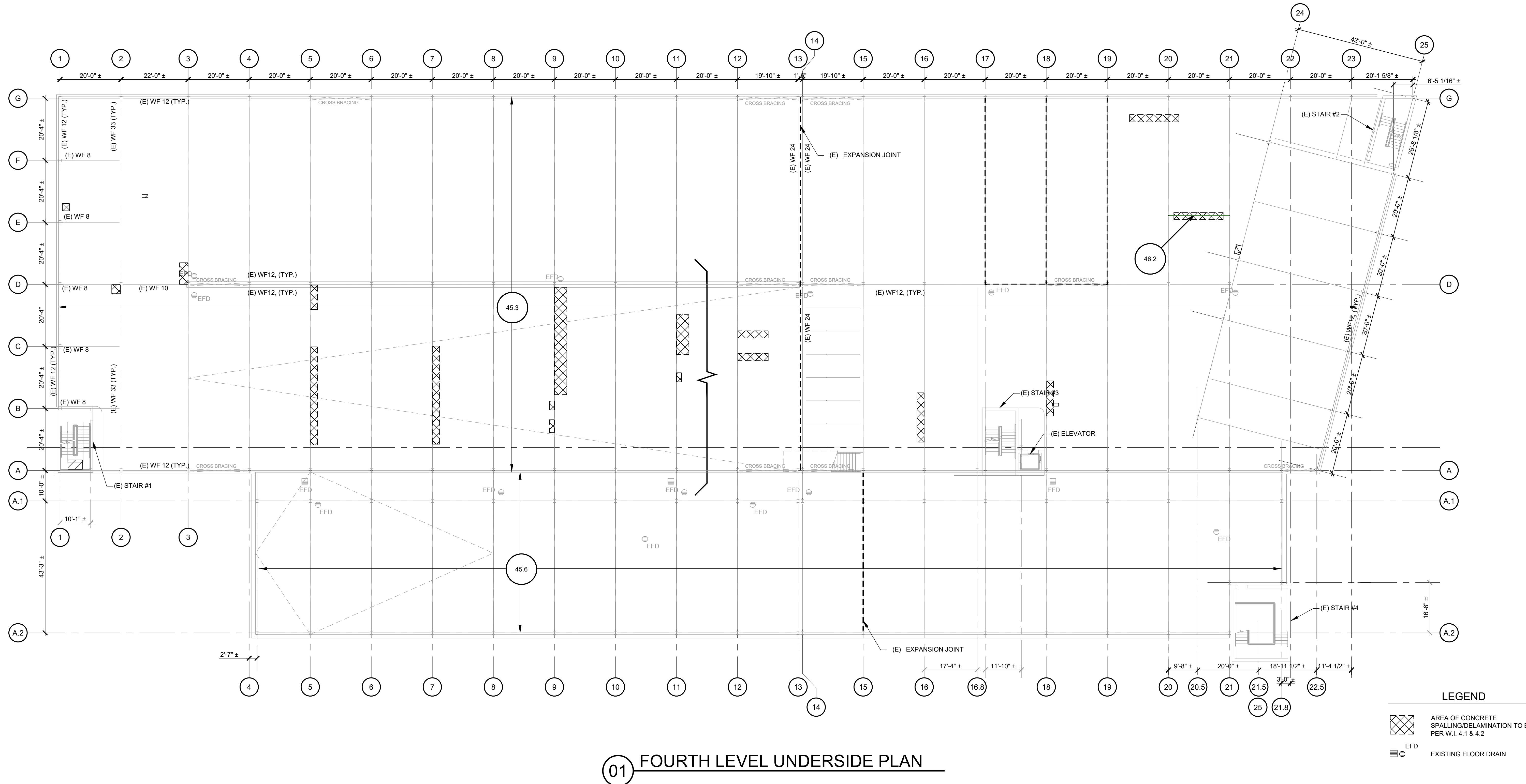
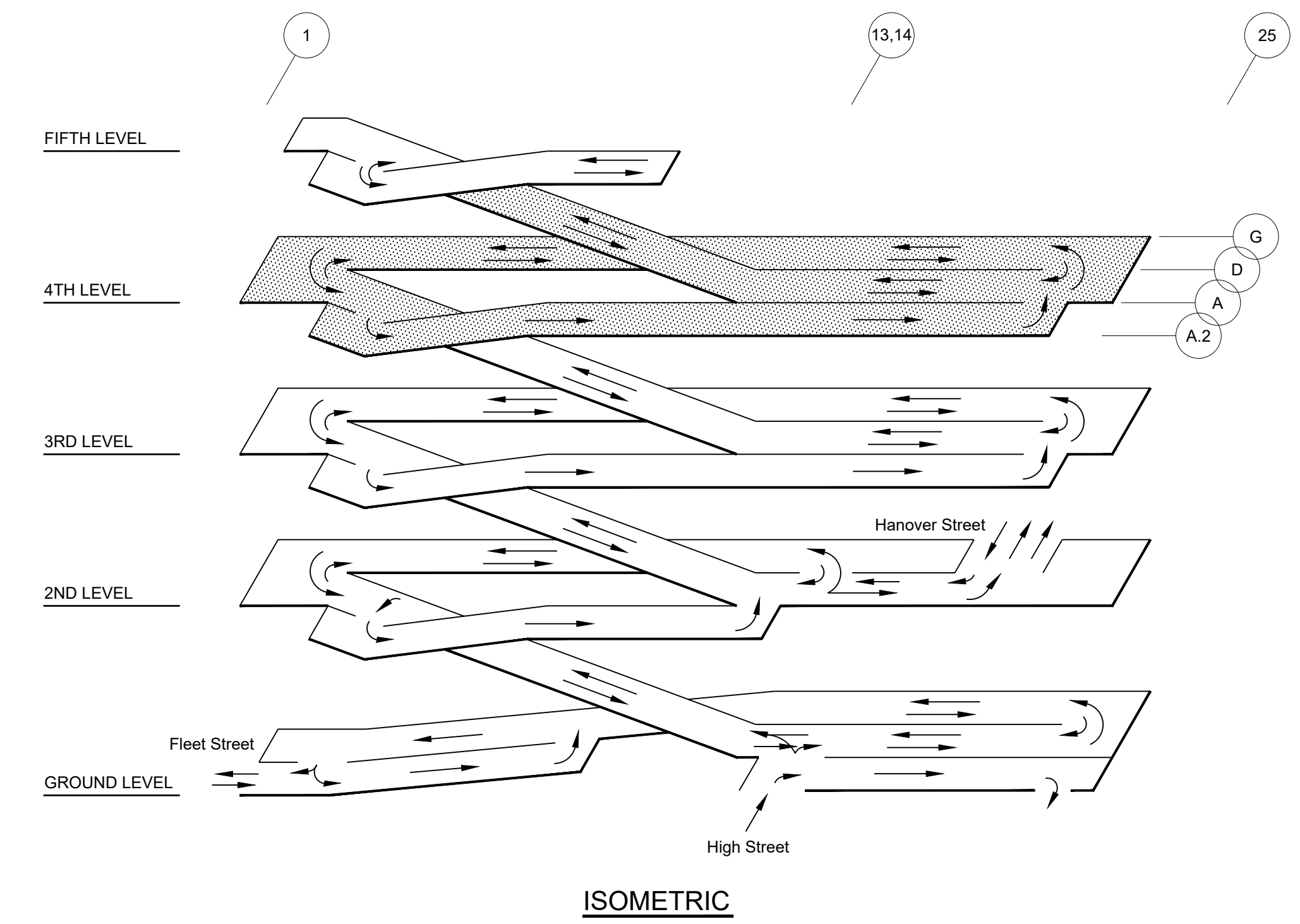
SHEET TITLE:
FOURTH LEVEL FLOOR PLAN

R-104A

SHEET NOTES:

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01 FOURTH LEVEL UNDERSIDE PLAN

LEGEND

	AREA OF CONCRETE SPALLING/DELMINATION TO BE REPAIRED PER W.I. 4.1 & 4.2
	EXISTING FLOOR DRAIN



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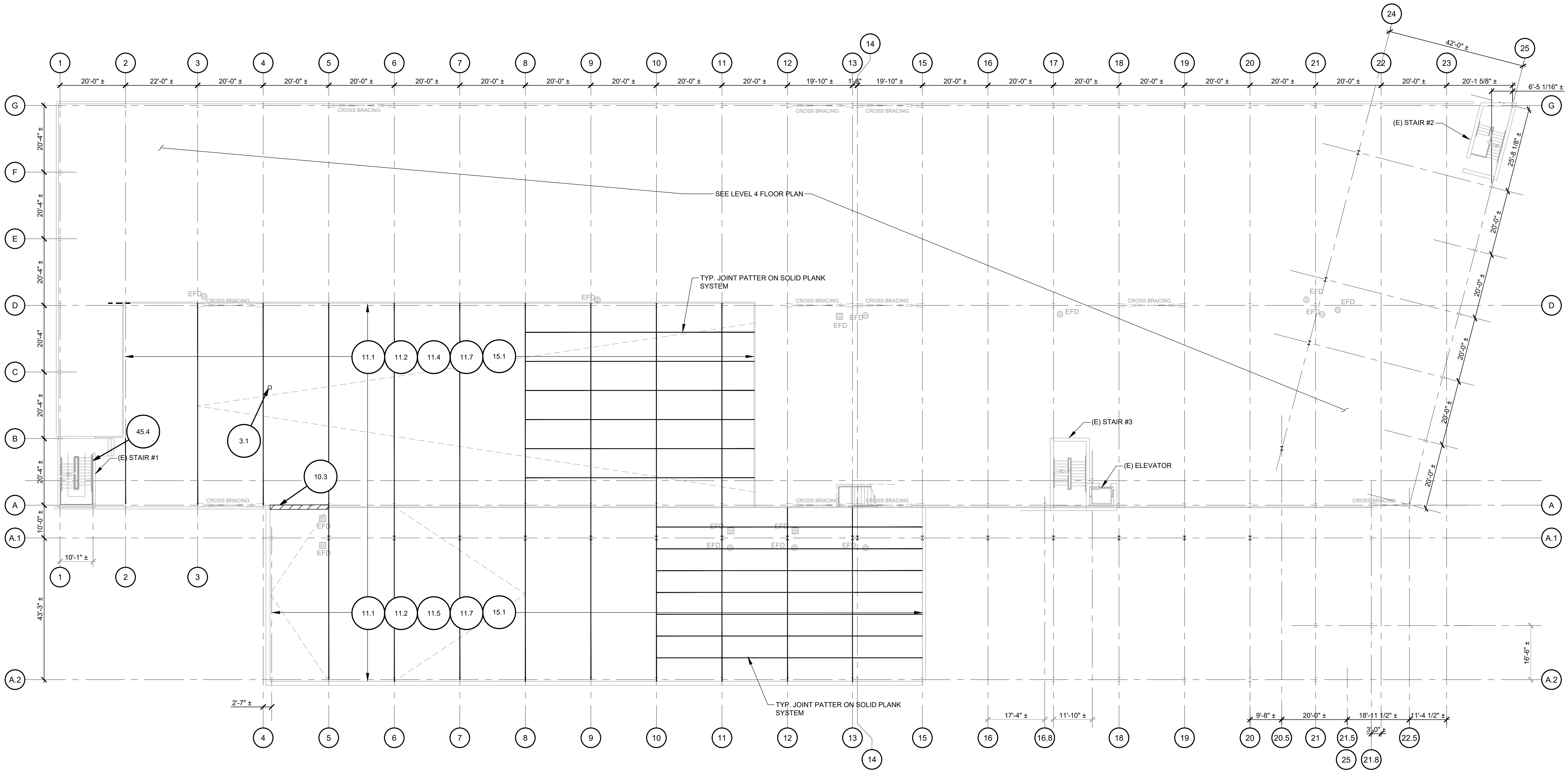
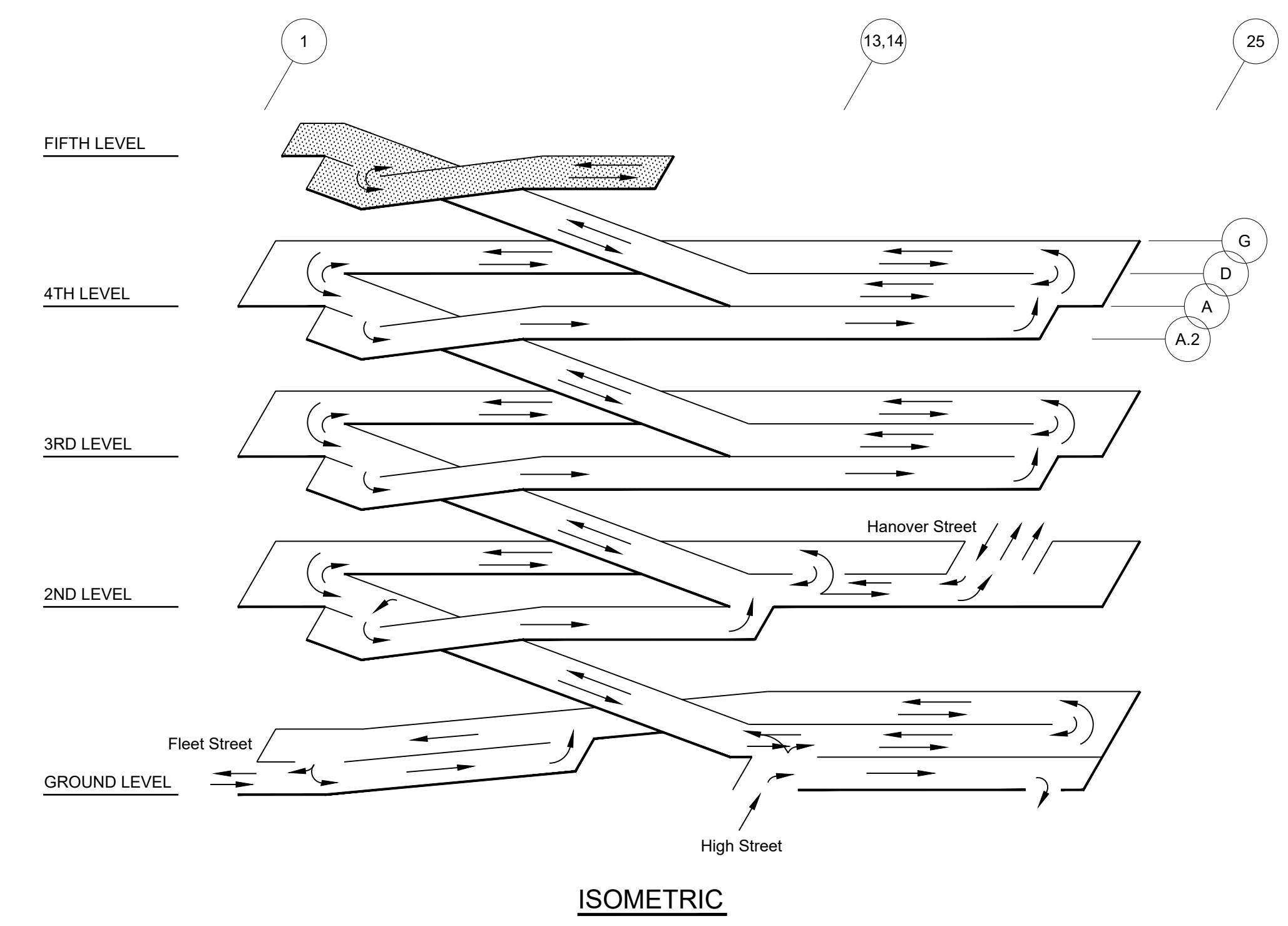
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- ALL REPAIRS SHALL BE COMPLETED IN A PHASE PRIOR TO MOVING INTO THE NEXT PHASE. JOINT SEALANT AND TRAFFIC TOPPING INSTALLATION CAN BE CARRIED OUT AFTER CONCRETE HAS FULLY CURED FOR 28 DAYS.
- REMOVE AND REPLACE ALL EXISTING HORIZONTAL JOINT SEALANT, REFER TO WORK ITEM SERIES 11.0.
- THE CONCRETE SLAB IS COMPRISED OF SOLID PRECAST PLANKS ON THIS LEVEL.

NOTE:
 LOCATIONS OF REPAIRS SHOWN ARE FOR GRAPHICAL REPRESENTATIONAL ONLY. CONTRACTOR IS RESPONSIBLE FOR DETERMINING ACTUAL EXTENT AND LOCATIONS OF REPAIR AREAS IN ACCORDANCE WITH THE SPECIFICATIONS. WORK ITEM IS SHOWN ONLY TO REPRESENT THE TYPES OF DETERIORATION.



01 FIFTH LEVEL FLOOR PLAN

LEGEND

	AREA OF CONCRETE SPALLING/DELAMINATION TO BE REPAIRED.
	EXISTING FLOOR DRAIN
	INDICATES CRACK SEE DETAIL 11.1



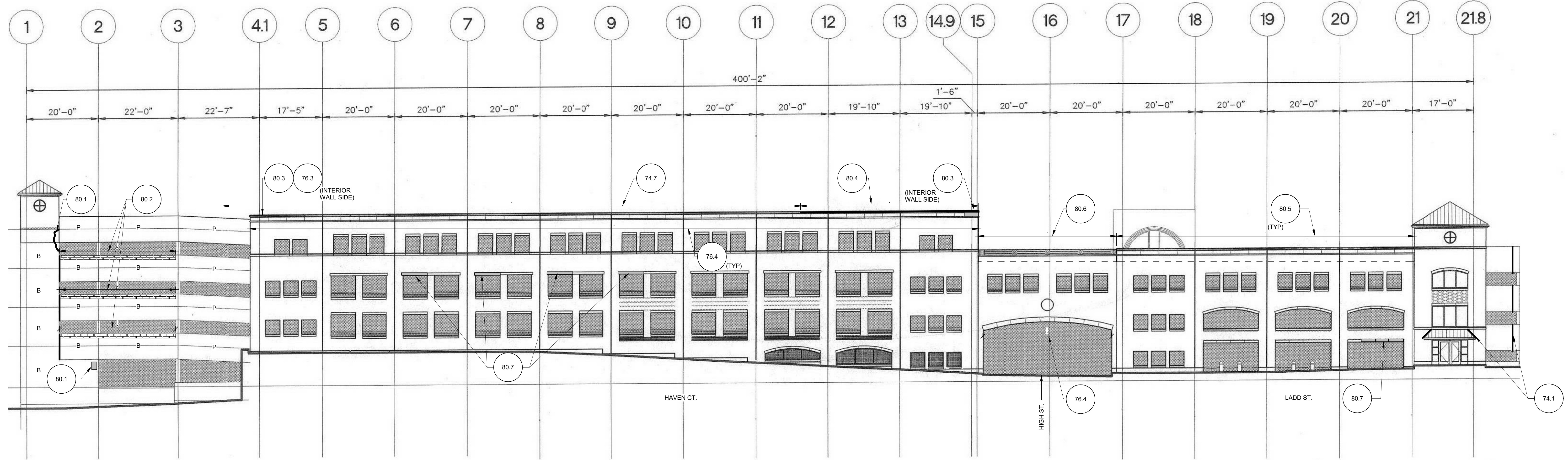
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 34 HANOVER STREET
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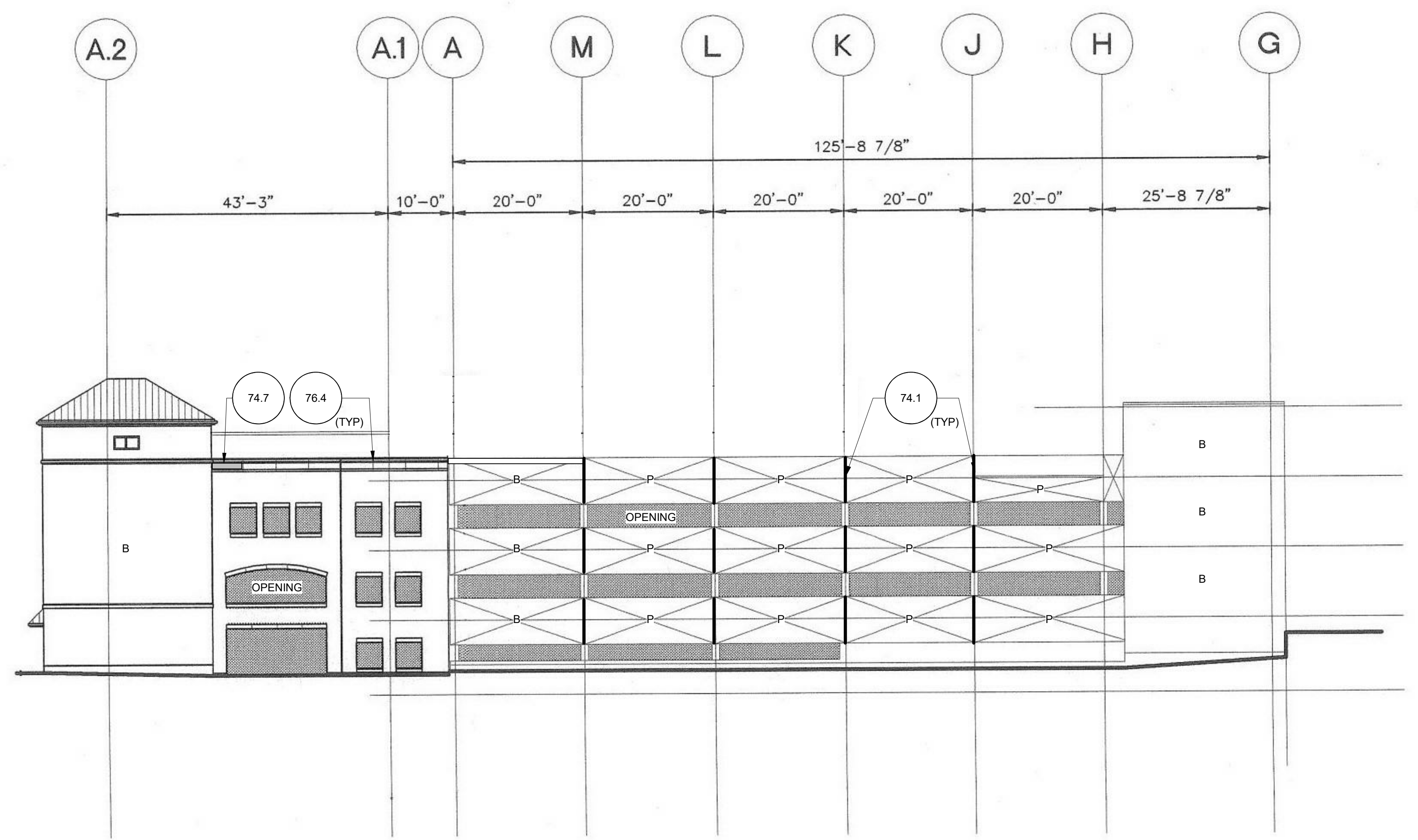
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SHEET TITLE:
FIFTH LEVEL FLOOR PLAN

R-105A



2 SOUTH ELEVATION



1 EAST ELEVATION



Arthur G. Stodk

HANOVER STREET GARAGE RESTORATION

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LEGEND

- P - PRECAST WALL PANEL
- B OR BRICK - BRICK VENEER
- - WALL OPENING
- (TAS) - INDICATES TYPICAL AS SHOWN

SHEET TITLE
BUILDING ELEVATIONS



Arthur G. Stadig

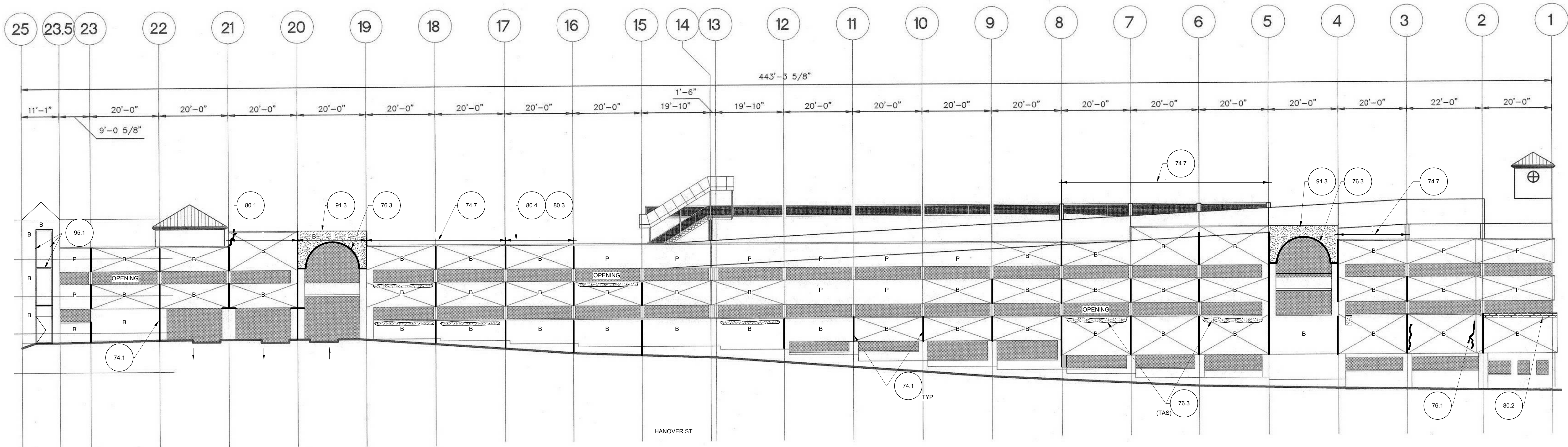
HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET
PORTSMOUTH, NH 03801

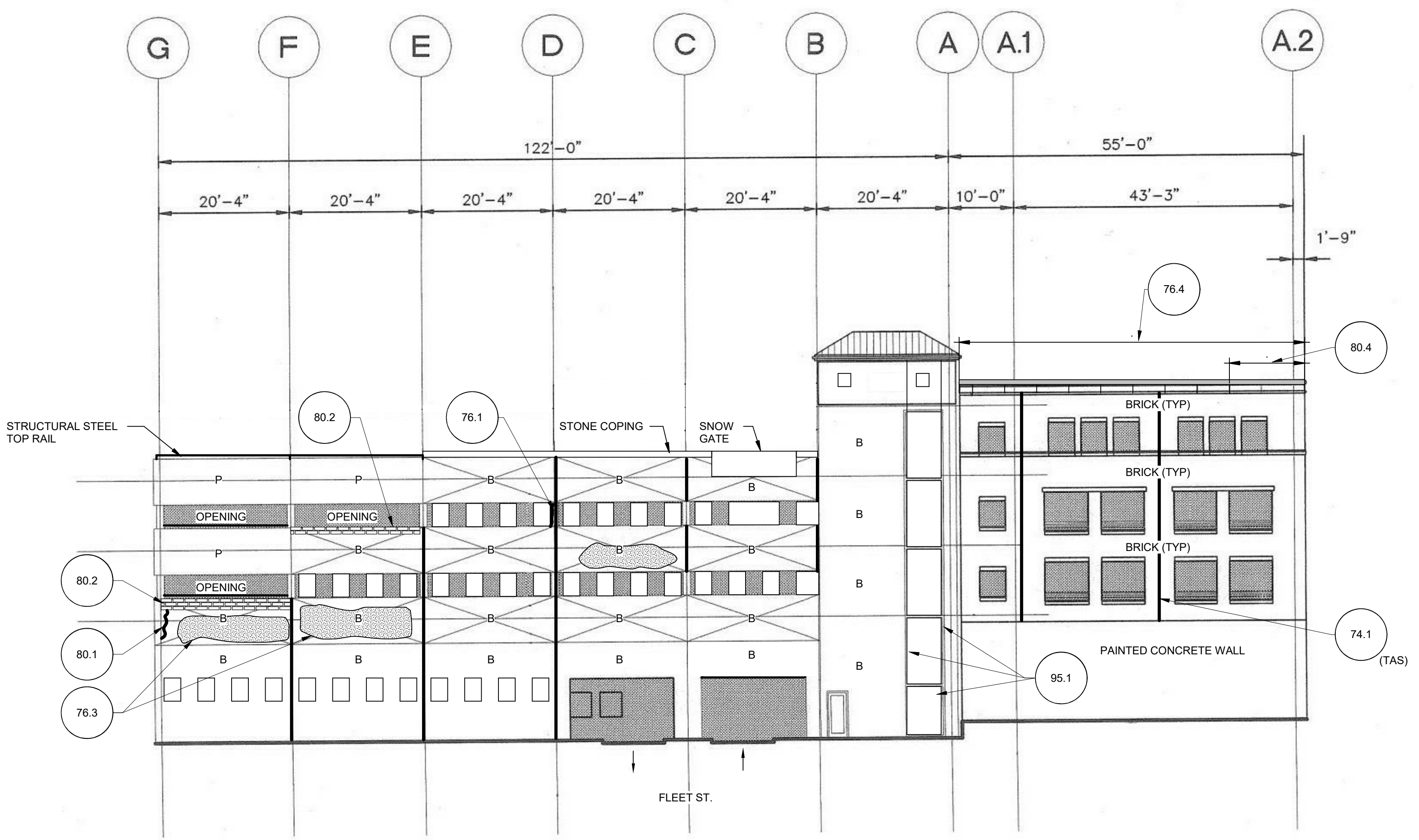
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SHEET TITLE
BUILDING ELEVATIONS



2 NORTH ELEVATION



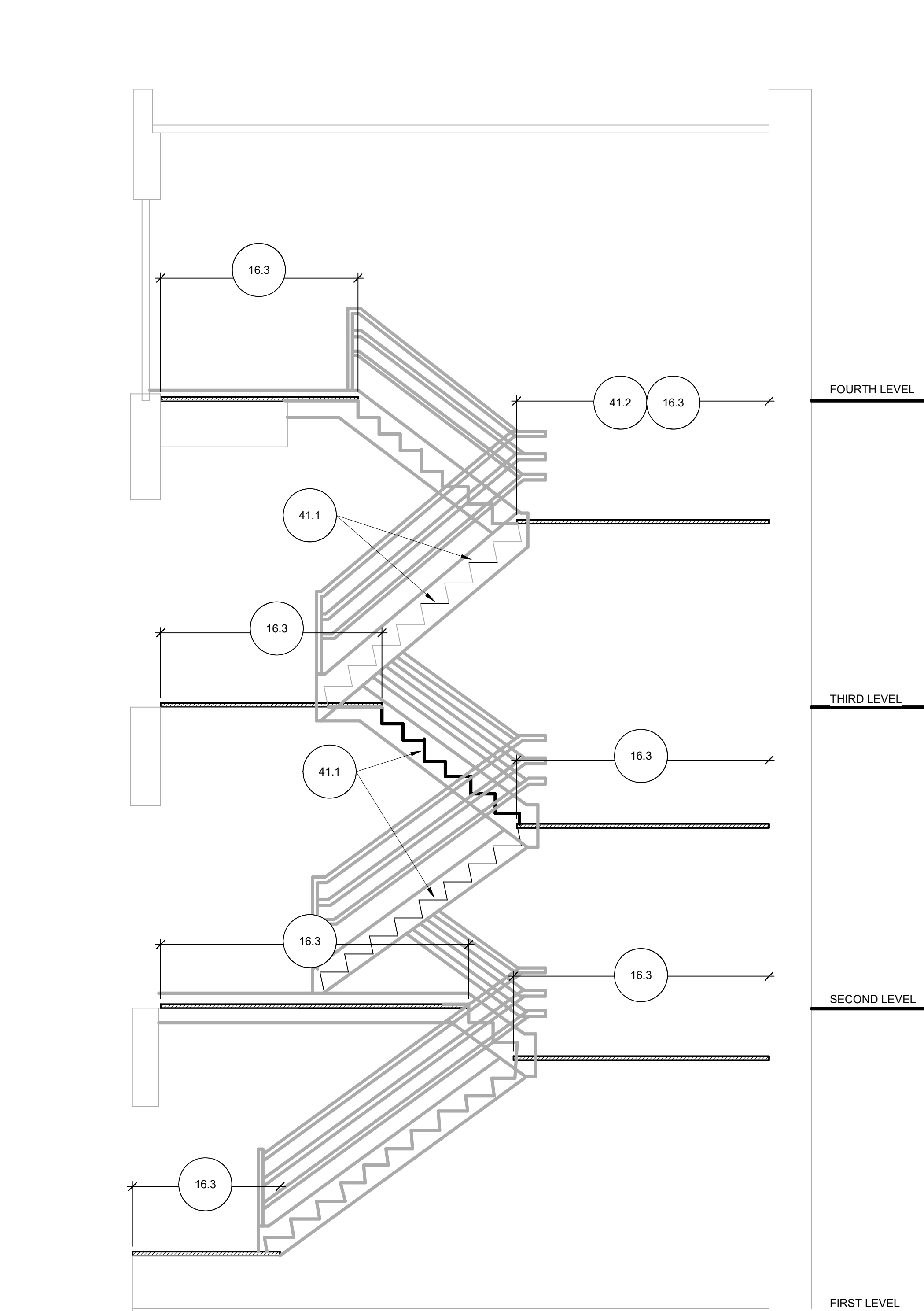
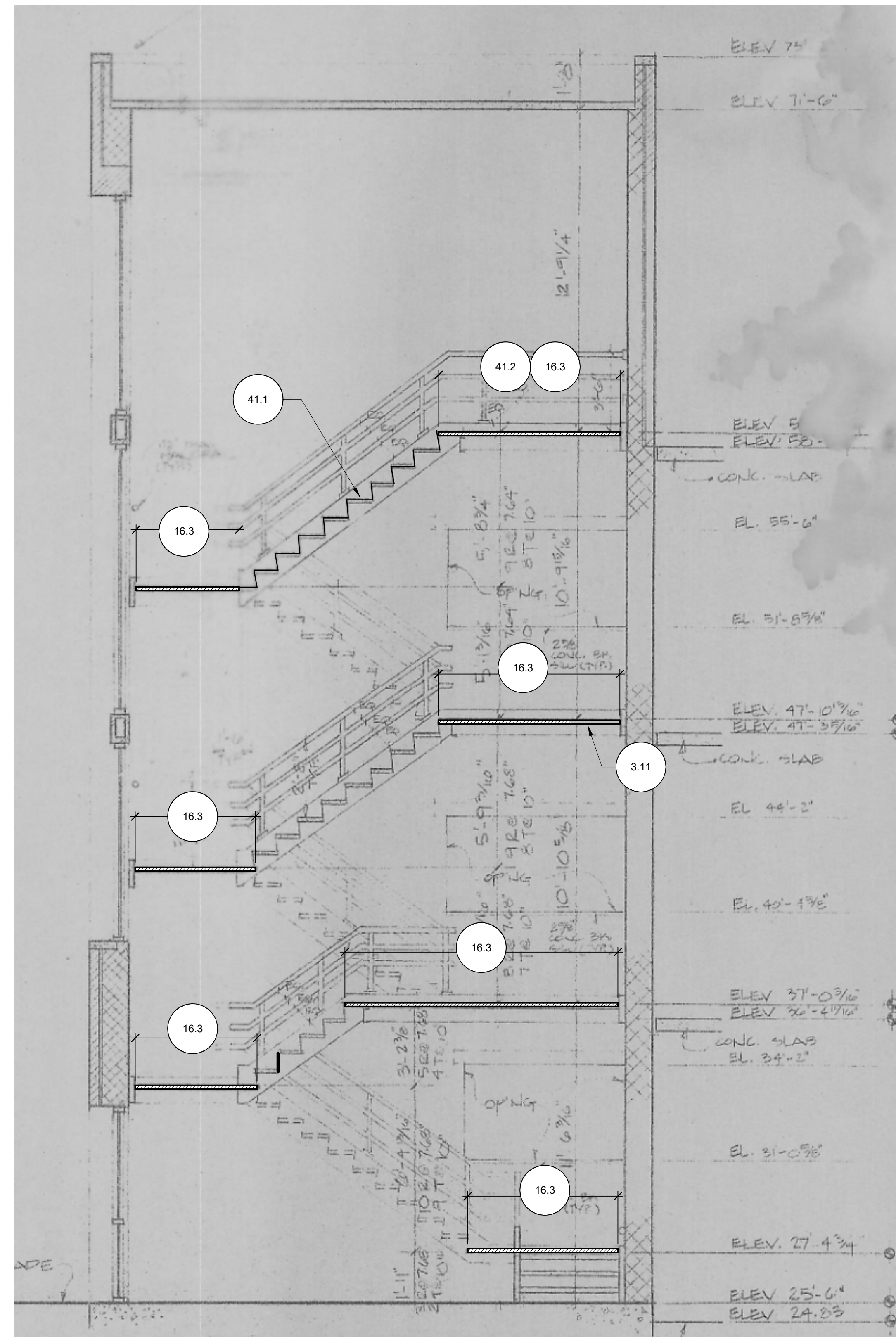
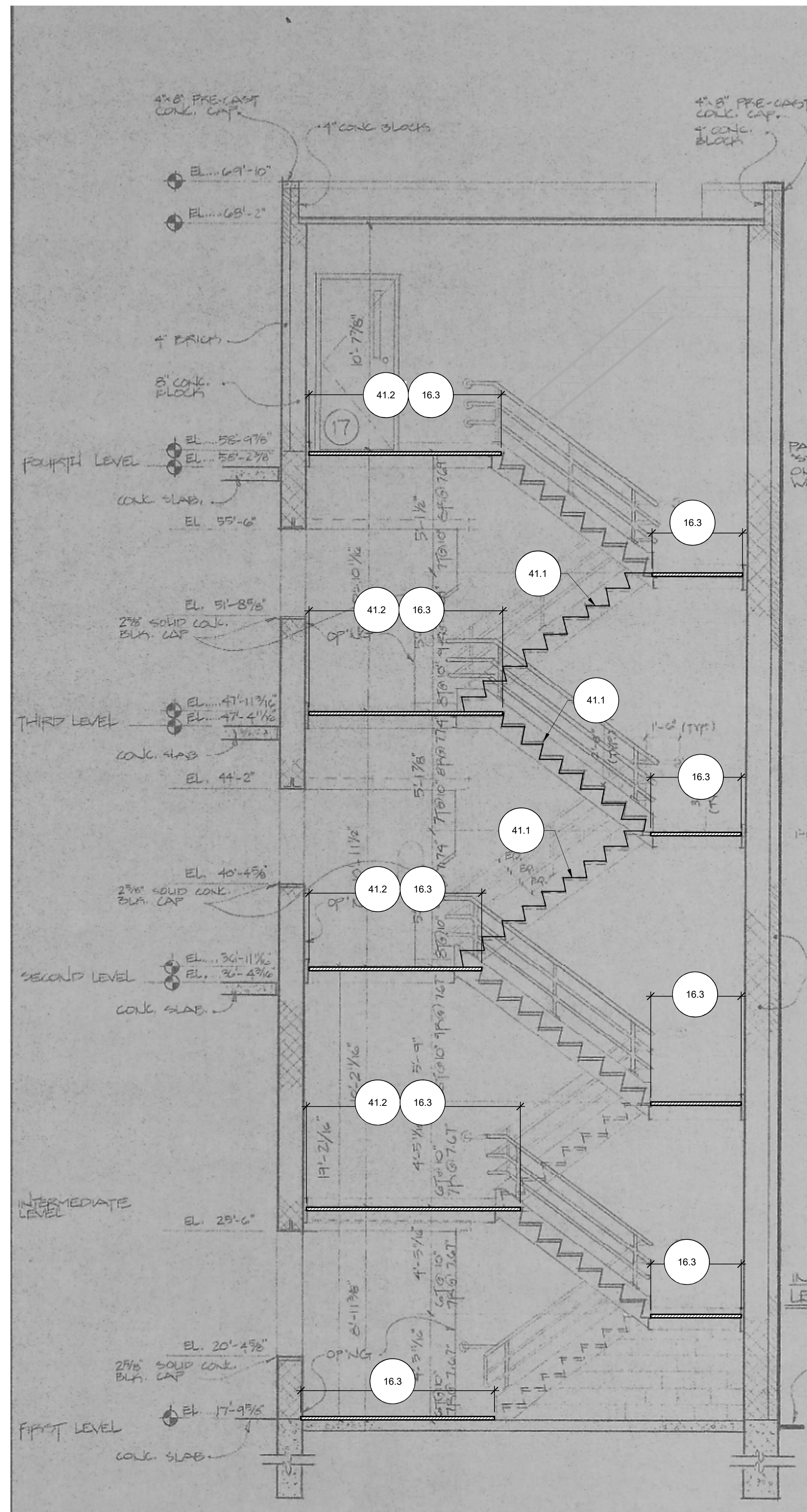
1 WEST ELEVATION

LEGEND

- P - PRECAST WALL PANEL
- B OR BRICK - BRICK VENEER
- - WALL OPENING
- (TAS) - INDICATES TYPICAL AS SHOWN



- GENERAL NOTES:
- REFER TO SHEET R-001 FOR GENERAL NOTES AND WORK ITEM SCHEDULE
 - THIS DRAWING REPRESENTS THE EXISTING STAIR ELEVATION. THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFYING ALL DIMENSIONS AND EXISTING CONDITIONS PRIOR TO CONSTRUCTION AND DEMOLITION. DO NOT SCALE DRAWINGS.
 - CONTRACTOR IS TO DEMOLISH EXISTING CONCRETE LANDING SHOWING SIGNS OF RUSTING AND CRACKING CONNECTING TO STAIR STRINGERS. CONTRACTOR IS TO FIELD VERIFY LIMITS OF CONCRETE LANDING NEEDING REPLACEMENT PRIOR TO DEMOLITION OF EXISTING CONCRETE
 - CONTRACTOR IS SOLELY RESPONSIBLE FOR THE DESIGN AND INSTALLATION OF TEMPORARY SHORING NECESSARY TO COMPLETE THE WORK.
 - ALL REBAR THAT IS EXPOSED SHALL BE CLEANED AND COATED WITH A CORROSION INHIBITOR.
 - DAMAGE THAT OCCURS OUTSIDE OF THE WORK AREA SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR ANY COST THAT IS INCURRED AT NO COST TO THE OWNER.
 - CONTRACTORS PROCEDURES, BRACING, MEANS, METHODS AND SAFETY PRECAUTIONS ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR DOING THE WORK.
 - DIMENSIONS SHOWN ON THE PLAN ARE AT THE TIME OF CONSTRUCTION AND SHOULD BE FIELD VERIFIED BEFORE FABRICATION AND INSTALLATION OF NEW STAIR LANDINGS AND STAIR TREADS.



1 REPAIRS AT STAIR 1

2 REPAIRS AT STAIR 2

3 REPAIRS AT STAIR 3



HANOVER STREET GARAGE RESTORATION

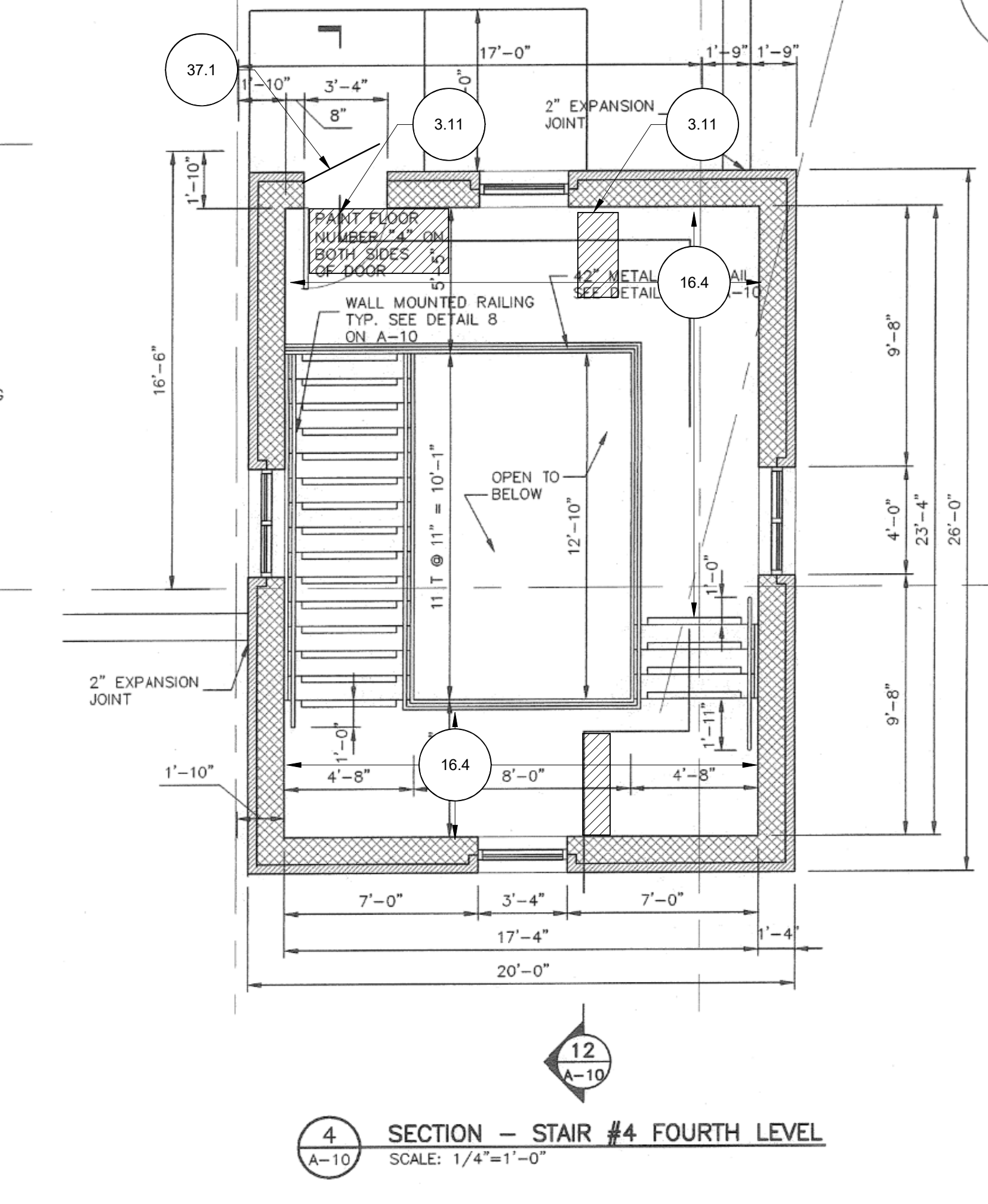
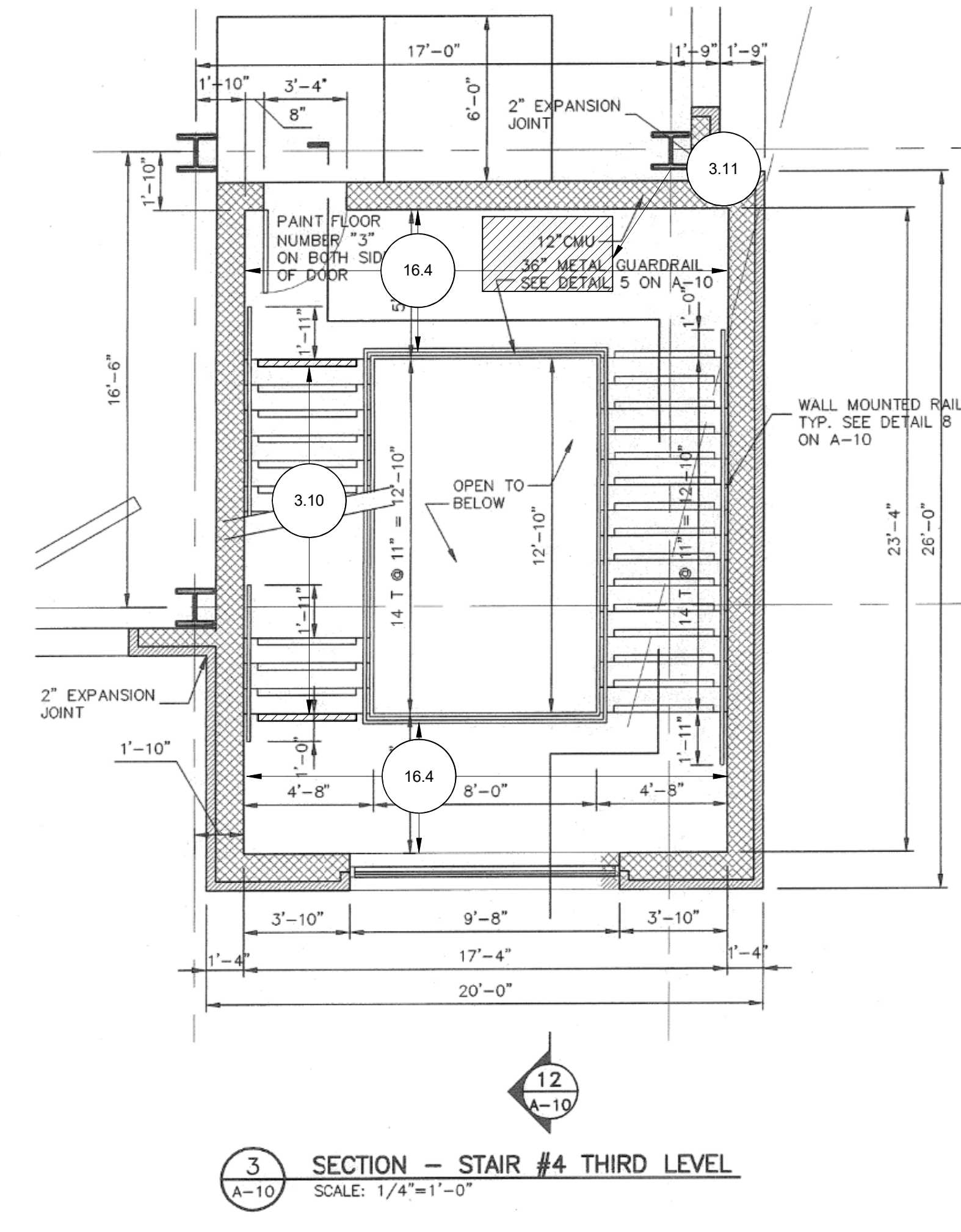
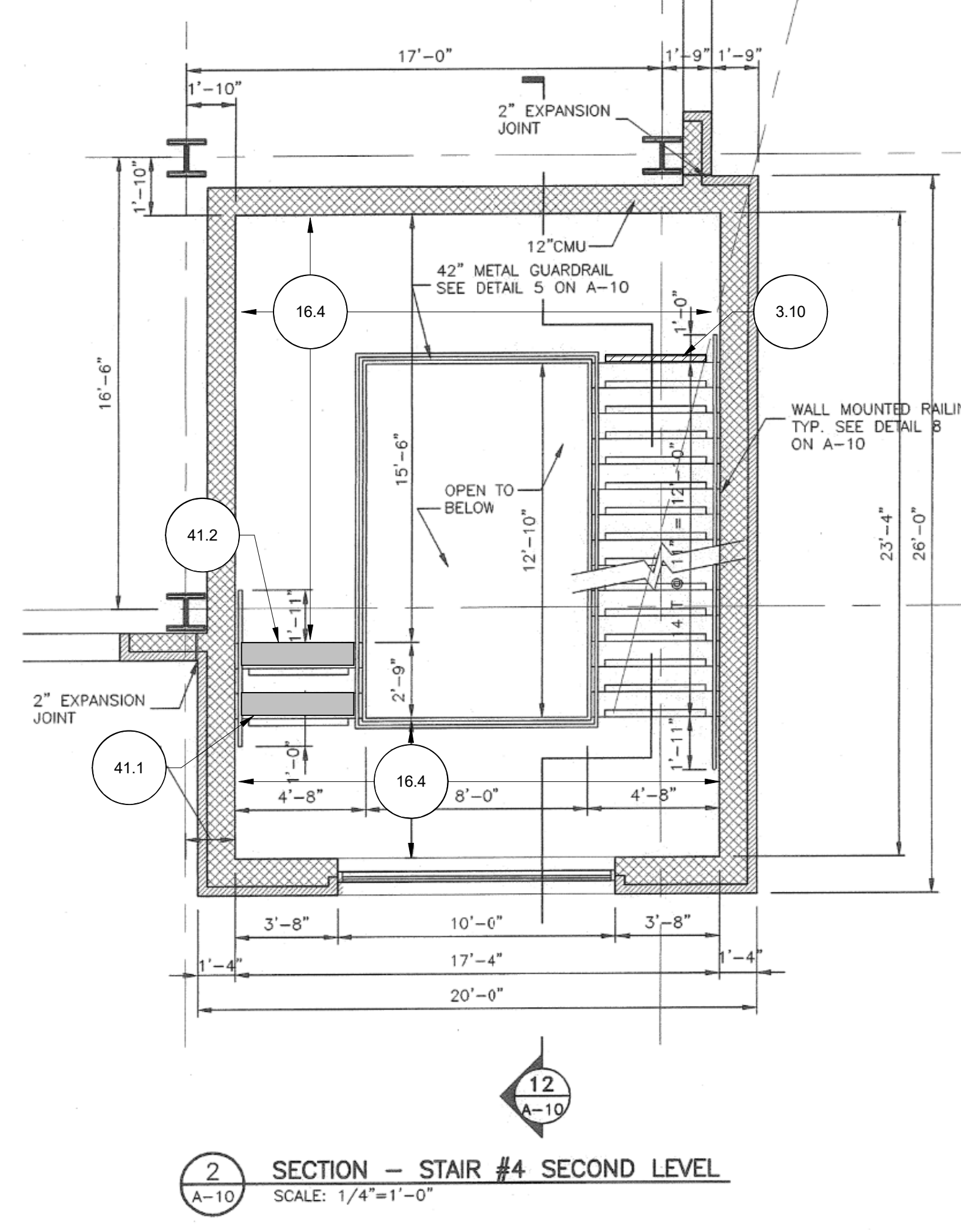
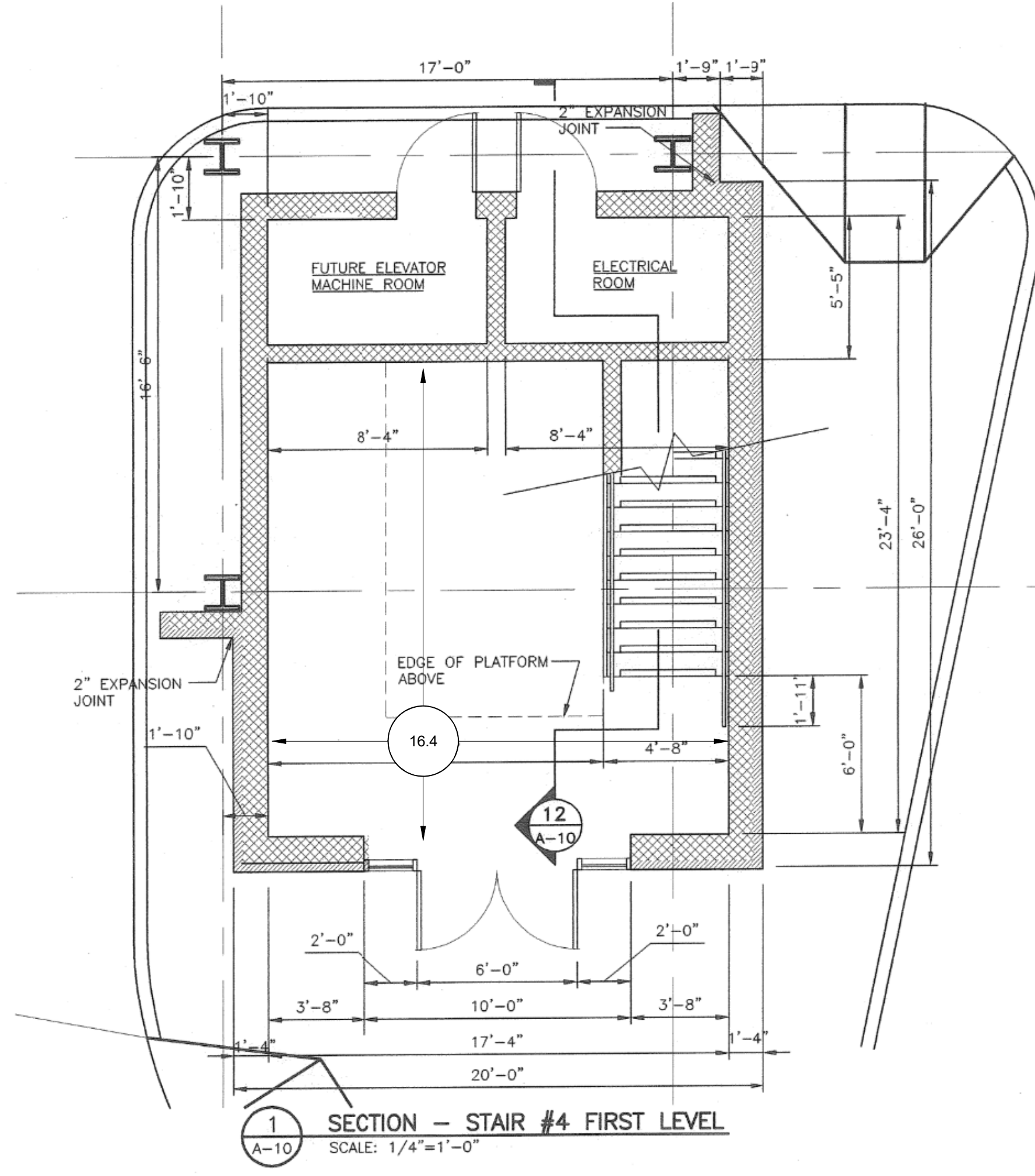
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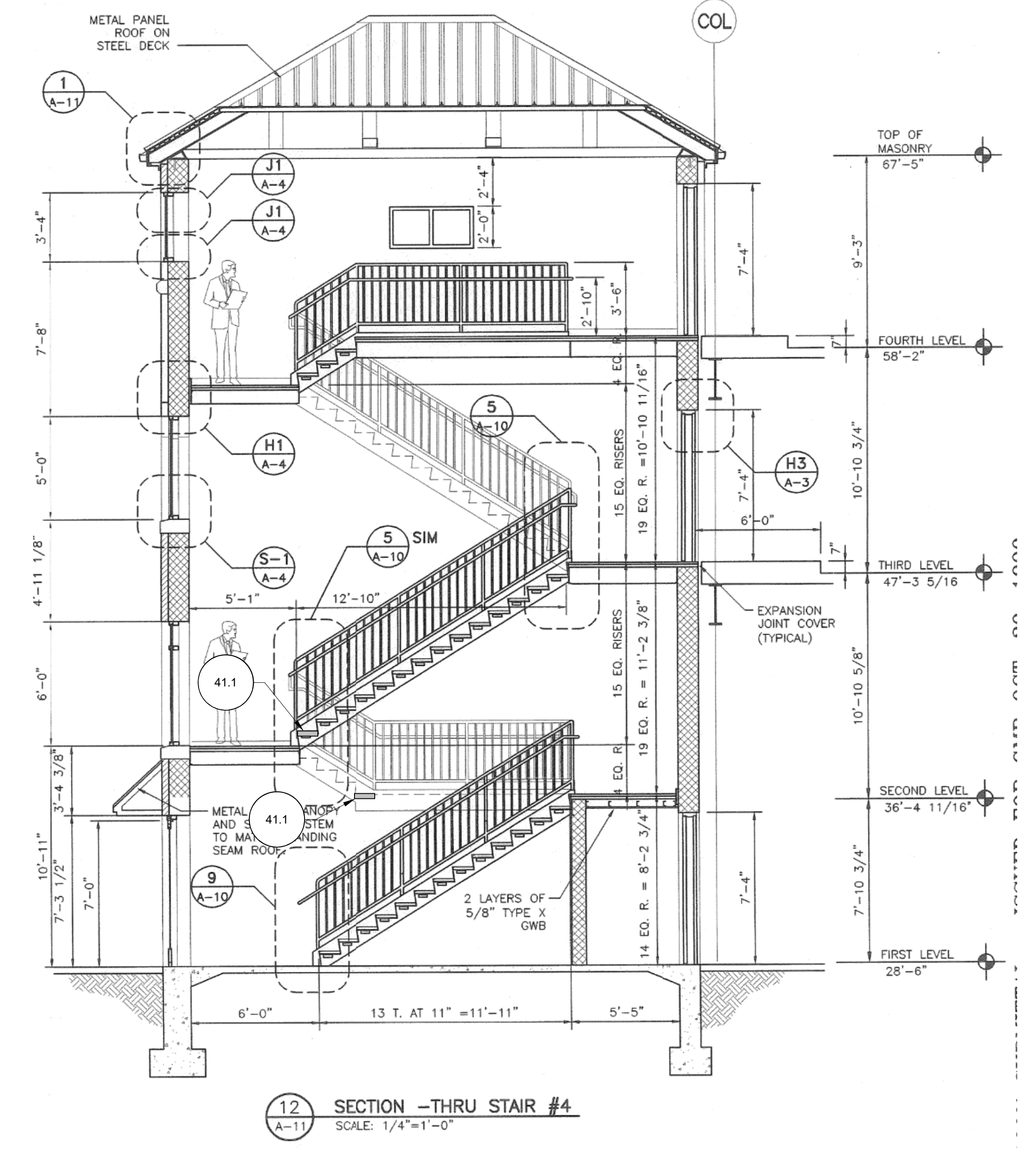
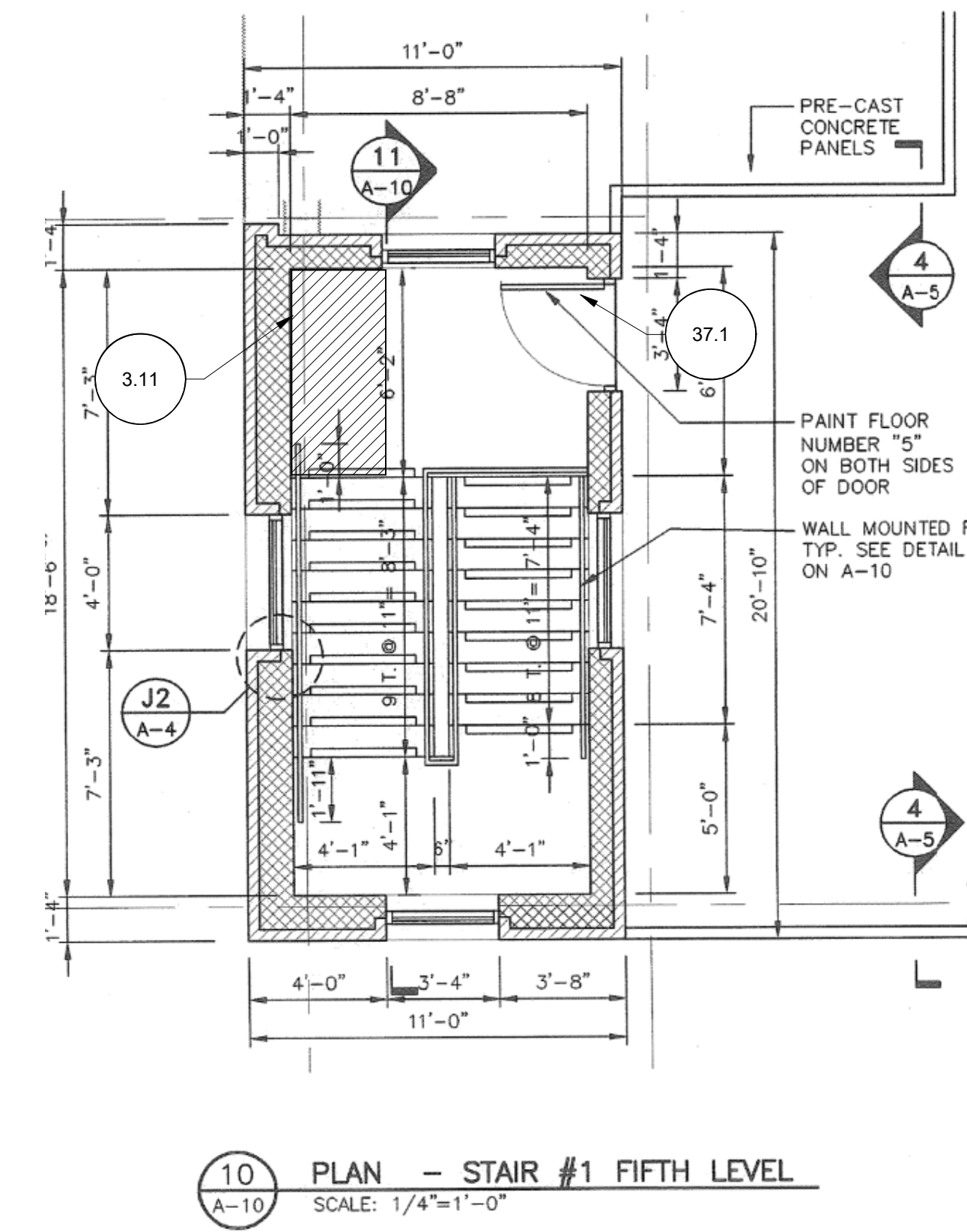
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SHEET TITLE
STAIR REPAIR @ STAIR 1, 2 & 3

THE EXISTING DRAWING (SCREENED) ARE FROM PARKING FACILITY HIGH AND HANOVER STREETS FOR THE CITY OF PORTSMOUTH DATED DECEMBER 21, 1984 AND ARE FOR INFORMATION ONLY. THE ELEVATIONS SHOWN ARE INTENDED TO SHOW REPAIR AND MAINTENANCE WORK. (DO NOT SCALE DRAWINGS)



- GENERAL NOTES:
- REFER TO SHEET R-001 FOR GENERAL NOTES AND WORK ITEM SCHEDULE.
 - THIS DRAWING REPRESENTS THE EXISTING STAIR ELEVATION. THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFYING ALL DIMENSIONS AND EXISTING CONDITIONS PRIOR TO CONSTRUCTION AND DEMOLITION. DO NOT SCALE DRAWINGS.
 - CONTRACTOR IS TO DEMOLISH EXISTING CONCRETE LANDING SHOWING SIGNS OF RUSTING AND CRACKING CONNECTING TO STAIR STRINGERS. CONTRACTOR IS TO FIELD VERIFY LIMITS OF CONCRETE LANDING NEEDING REPLACEMENT PRIOR TO DEMOLITION OF EXISTING CONCRETE.
 - CONTRACTOR IS SOLELY RESPONSIBLE FOR THE DESIGN AND INSTALLATION OF TEMPORARY SHORING NECESSARY TO COMPLETE THE WORK.
 - ALL REBAR THAT IS EXPOSED SHALL BE CLEANED AND COATED WITH A CORROSION INHIBITOR.
 - DAMAGE THAT OCCURS OUTSIDE OF THE WORK AREA SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR ANY COST THAT IS INCURRED AT NO COST TO THE OWNER.
 - CONTRACTOR'S PROCEDURES, BRACING, MEANS, METHODS AND SAFETY PRECAUTIONS ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR DURING THE WORK.
 - DIMENSIONS SHOWN ON THE PLAN ARE AT THE TIME OF CONSTRUCTION AND SHOULD BE FIELD VERIFIED BEFORE FABRICATION AND INSTALLATION OF NEW STAIR LANDINGS AND STAIR TREADS.



Arthur G. Stading

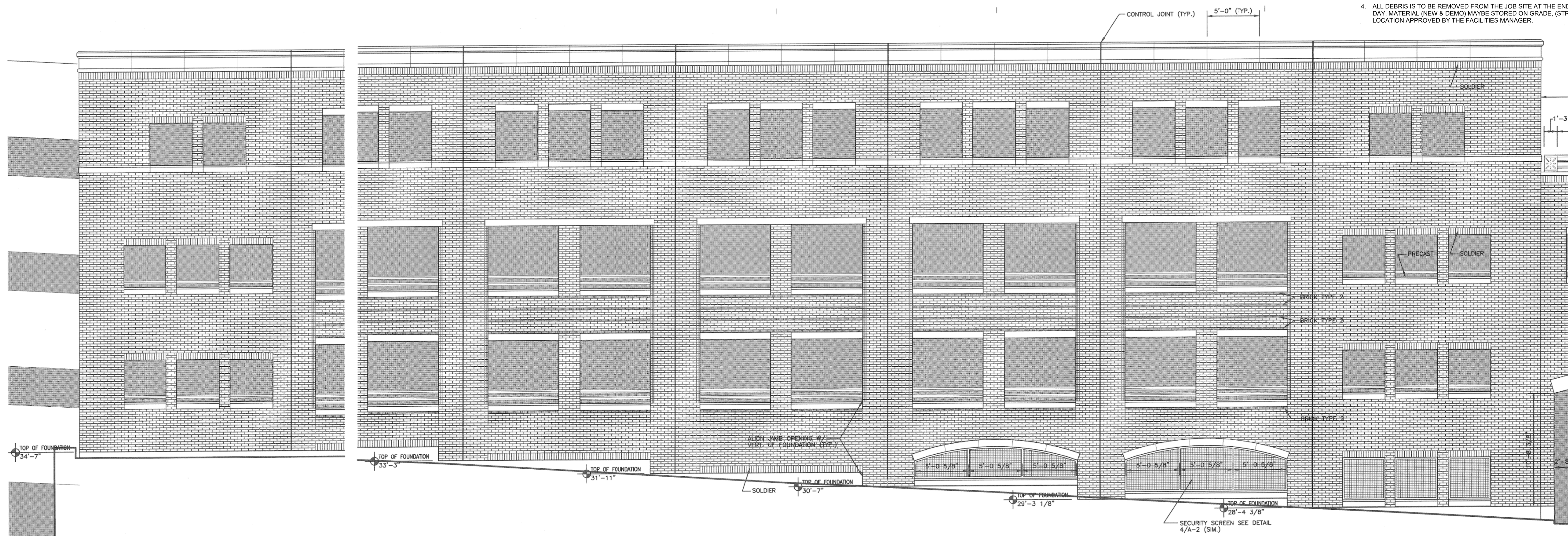
HANOVER STREET GARAGE RESTORATION
34 HANOVER STREET
PORTSMOUTH, NH 03801

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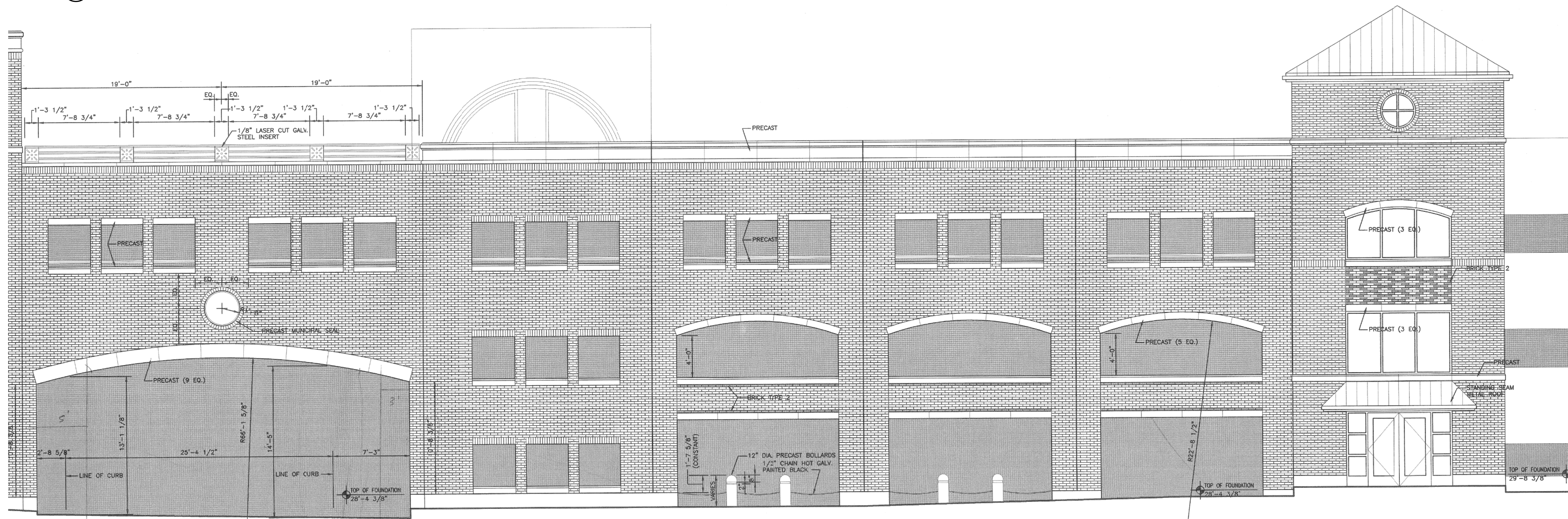
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SHEET TITLE
STAIR REPAIR @ STAIR 4



- NOTES:**
1. AREAS OF RE-POINTING AND REPAIR TAKEN DURING FIELD INVESTIGATION MAY NOT ACCURATELY REPRESENT CURRENT CONDITIONS. CONTRACTOR SHALL VERIFY CONDITIONS BEFORE AND BRING TO THE ATTENTION OF THE ENGINEER ANY AREAS THAT ARE IN NEED OF REPAIR PRIOR TO CONSTRUCTION.
 2. POWER WASH AREA TO BE REPAIRED TO REMOVE DIRT AND LOOSE MORTAR, MOLD, ETC. RE-POINT AS REQUIRED. SEE TYPICAL DETAIL. xxx
 3. BRICKS THAT NEED TO BE REPLACED SHALL MATCH EXISTING.
 4. ALL DEBRIS IS TO BE REMOVED FROM THE JOB SITE AT THE END OF EACH WORKING DAY. MATERIAL NEW & DEMO MAY BE STORED ON GRADE, (STREET LEVEL) AT LOCATION APPROVED BY THE FACILITIES MANAGER.



1 PARTIAL SOUTH ELEVATION



2 PARTIAL SOUTH ELEVATION



Arthur G. Stacks

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34 HANOVER STREET,
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SHEET TITLE: PARTIAL ELEVATIONS

THE EXISTING DRAWING (SCREENED) ARE FROM THE GARAGE EXTENSION DRAWINGS TITLED CITY OF PORTSMOUTH HIGH-HANOVER PARKING FACILITY EXPANSION DATED OCTOBER 29, 1999 AND ARE FOR INFORMATION ONLY. THE ELEVATIONS SHOWS ARE INTENDED TO SHOW FACADE REPAIR AND MAINTENANCE WORK. (DO NOTE SCALE DRAWINGS)



Arthur G. Stading

HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET
PORTSMOUTH, NH 03801

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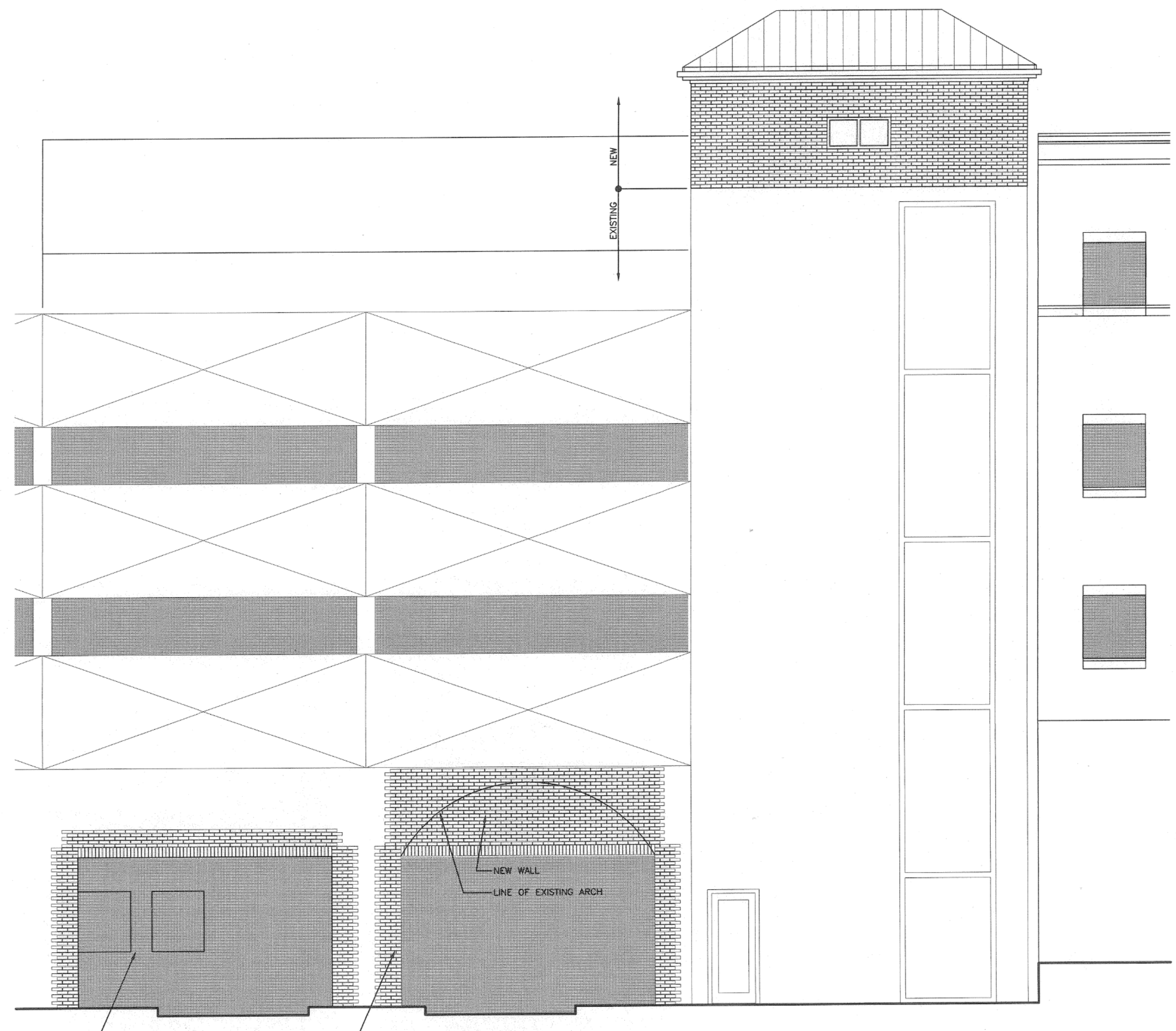
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SHEET TITLE:
PARTIAL ELEVATIONS

- NOTES:**
- AREAS OF RE-POINTING AND REPAIR TAKEN DURING FIELD INVESTIGATION MAY NOT ACCURATELY REPRESENT CURRENT CONDITIONS. CONTRACTOR SHALL VERIFY CONDITIONS BEFORE AND BRING TO THE ATTENTION OF THE ENGINEER ANY AREAS THAT ARE IN NEED OF REPAIR PRIOR TO CONSTRUCTION.
 - POWER WASH AREA TO BE REPAIRED TO REMOVE DIRT AND LOOSE MORTAR, MOLD, ETC. RE-POINT AS REQUIRED. SEE TYPICAL DETAIL xxxx
 - BRICKS THAT NEED TO BE REPLACED SHALL MATCH EXISTING.
 - ALL DEBRIS IS TO BE REMOVED FROM THE JOB SITE AT THE END OF EACH WORKING DAY. MATERIAL (NEW & DEMO) MAY BE STORED ON GRADE, (STREET LEVEL) AT LOCATION APPROVED BY THE FACILITIES MANAGER.



1 PARTIAL EAST ELEVATION



2 PARTIAL WEST ELEVATION

THE EXISTING DRAWING (SCREENED) ARE FROM THE GARAGE EXTENSION DRAWINGS TITLED CITY OF PORTSMOUTH HIGH-HANOVER PARKING FACILITY EXPANSION DATED OCTOBER 29, 1999 AND ARE FOR INFORMATION ONLY. THE ELEVATIONS SHOWS ARE INTENDED TO SHOW FACADE REPAIR AND MAINTENANCE WORK. (DO NOTE SCALE DRAWINGS)



Arthur G. Stadio

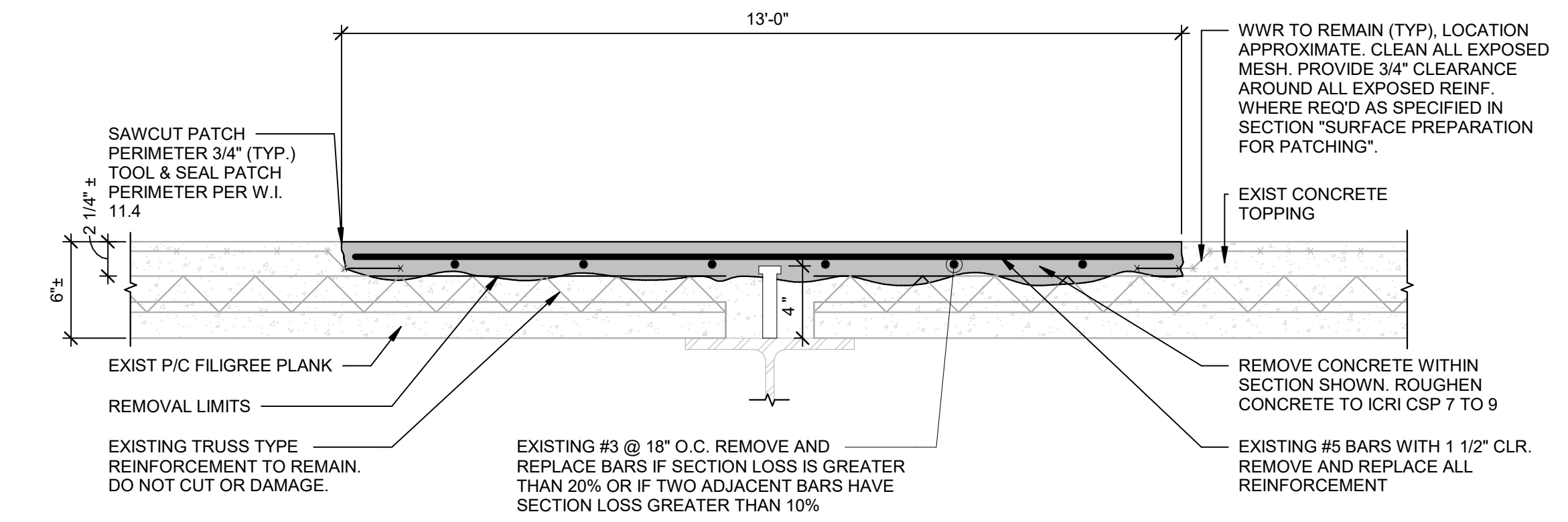
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SHEET TITLE:
REPAIR DETAILS

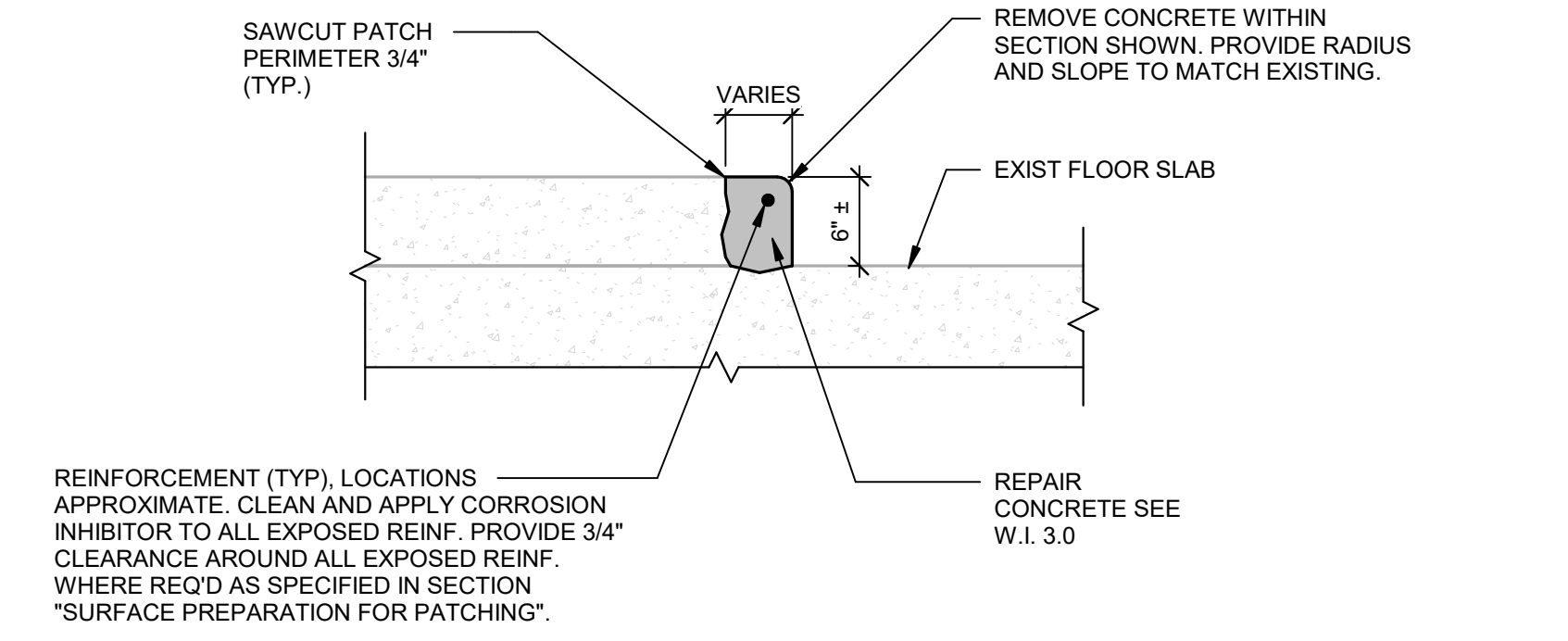


- SUGGESTED CONSTRUCTION SEQUENCE:**
1. TEMPORARILY SHORE THE EXISTING STEEL BEAM AND ADJACENT FILIGREE PLANKS SUCH THAT THE SHORING SUPPORTS THE DEAD LOAD OF THE EXISTING BEAM, PLANKS, AND C.I.P. CONCRETE TOPPING AND THE RECOMMENDED CONSTRUCTION LIVE LOAD.
 2. REMOVE DETERIORATED CONCRETE AND REINFORCING STEEL FROM THE STRIP REPAIR AREA SHOWN.
 3. PREPARE THE CAVITY AND PLACE CONCRETE.
 4. CURE NEW CONCRETE UNTIL F'c=5,000 PSI.
 5. TEMPORARY SHORING CAN BE REMOVED.

- NOTES:**
1. CONTRACTOR SHALL TEMPORARILY SHORE PLANKS AND BEAM PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. TEMPORARY SHORING SHALL REMAIN IN PLACE UNTIL CONCRETE REPAIRS HAVE FULLY CURED. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK.
 2. CONTRACTOR SHALL NOTIFY ENGINEER IF ONE REINFORCING BAR HAS LOST MORE THAN 20% CROSS SECTION OR IF TWO ADJACENT BARS HAVE LOST MORE THAN 10% CROSS SECTION EACH.
 3. TOOL & SEAL CONTROL JOINT IN PLASTIC CONCRETE OVER THE CENTERLINE OF THE STEEL BEAM.
 4. TOOL & SEAL CONTROL JOINTS IN PLASTIC CONCRETE IN LINE WITH ALL PRECAST FILIGREE PANEL JOINTS BELOW.

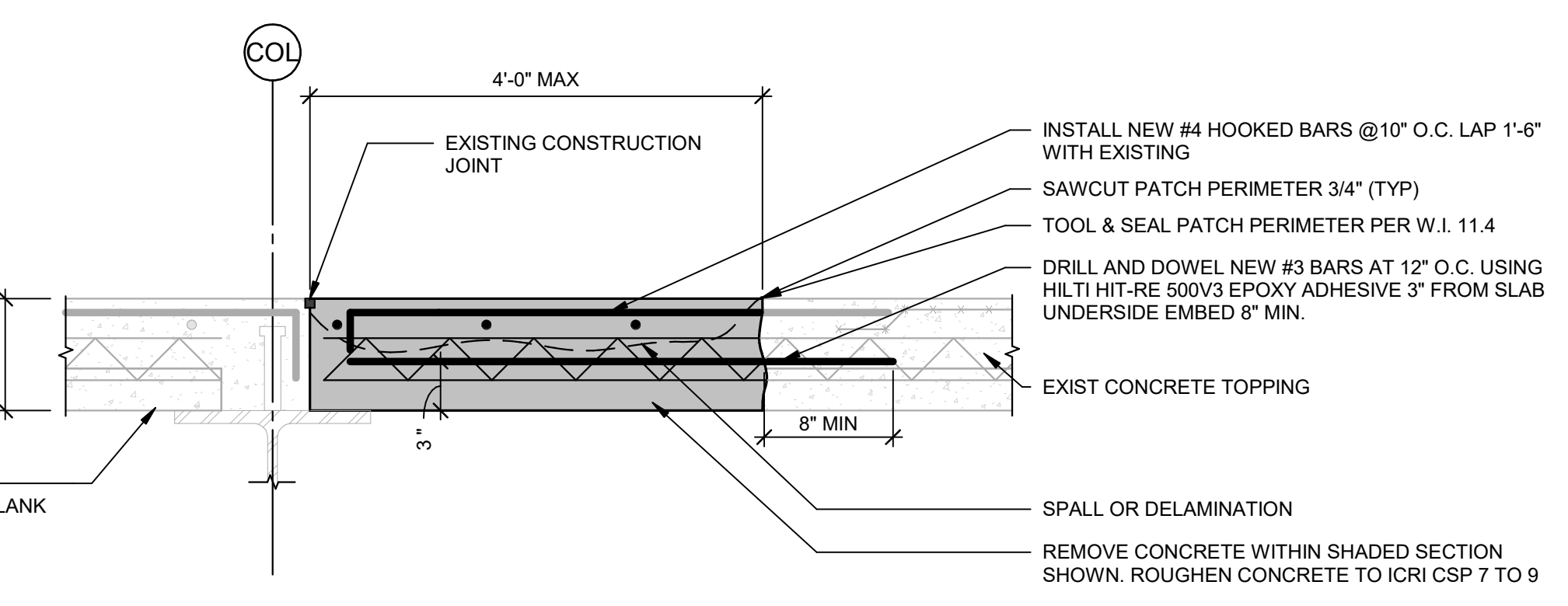
FLOOR REPAIR - PARTIAL DEPTH TOPPING STRIP REMOVAL/REPLACEMENT

3.6



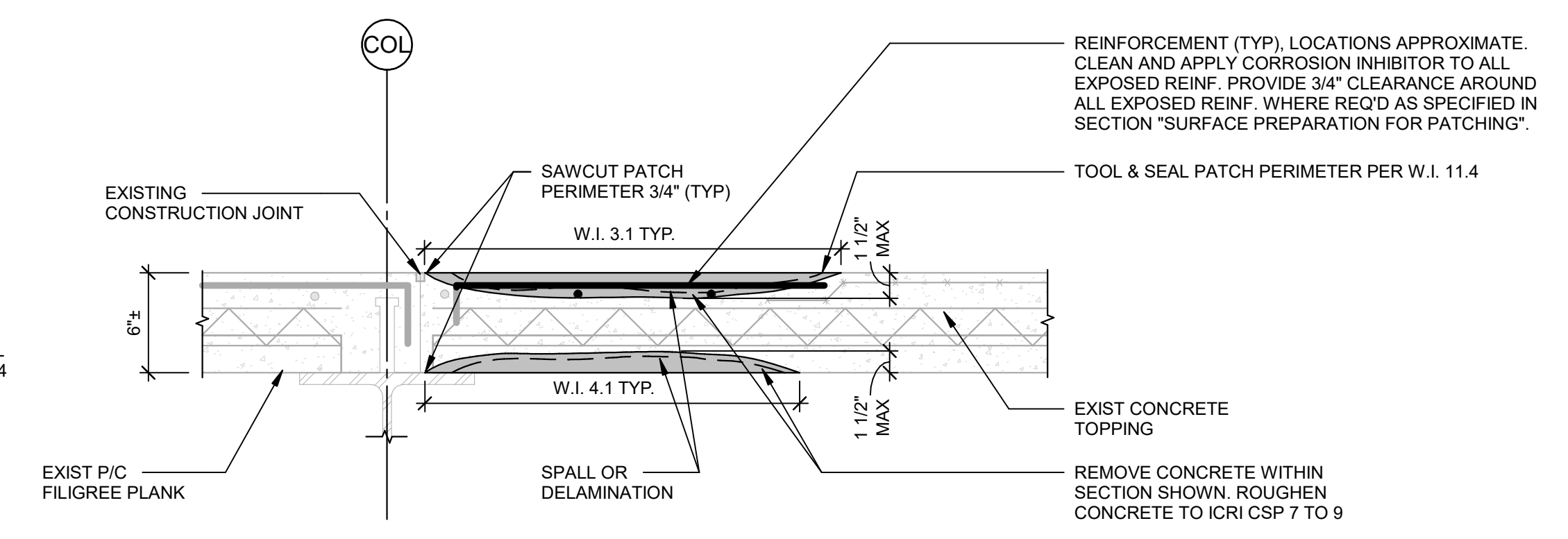
FLOOR REPAIR - CURBS/WALKS

3.4



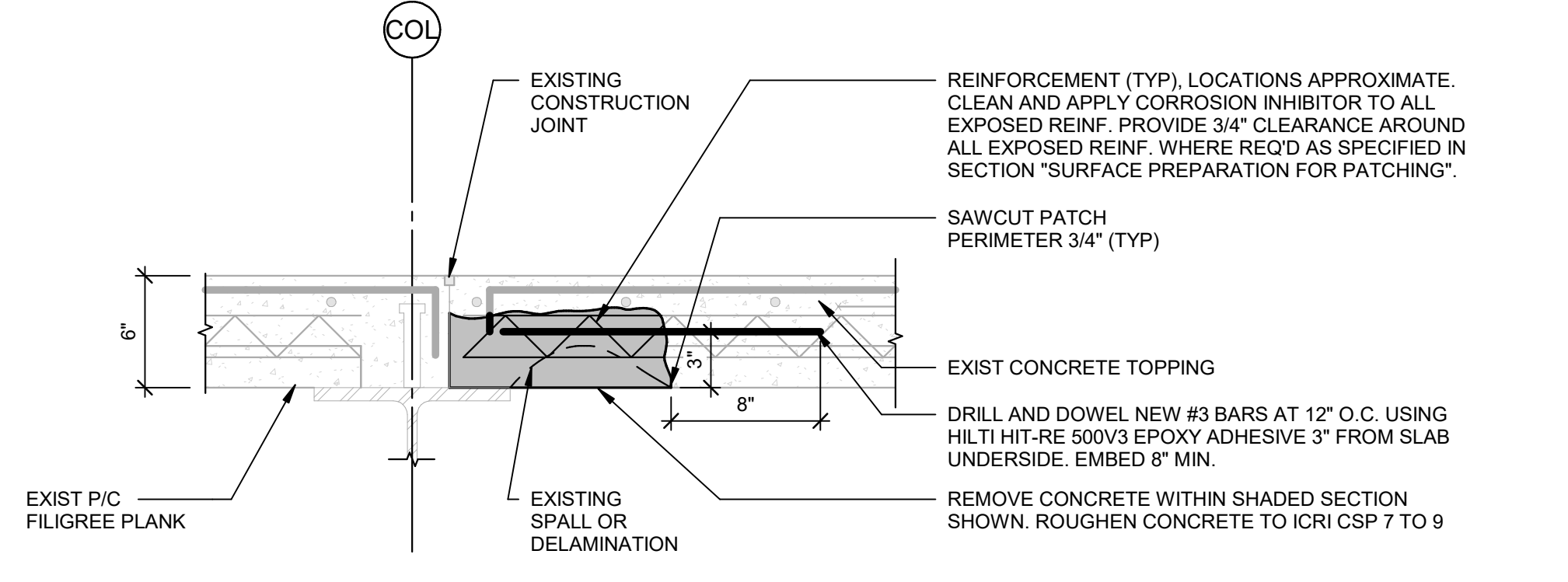
FLOOR REPAIR - FULL DEPTH AT CONSTRUCTION JOINT

3.3



CEILING REPAIR - PARTIAL DEPTH/DEEP

4.2



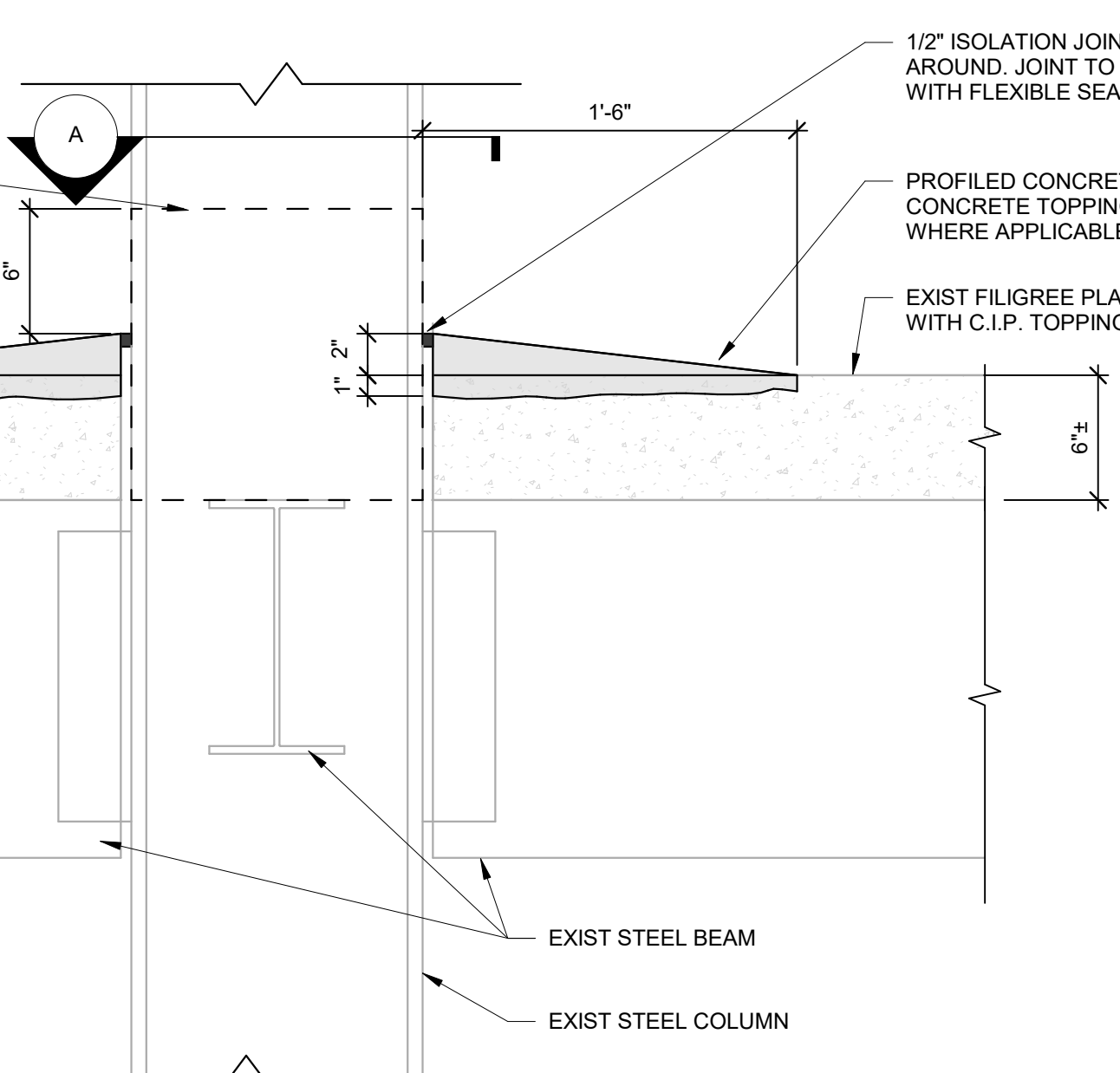
- NOTES:**
1. CONTRACTOR SHALL TEMPORARILY SHORE PLANKS AND BEAMS ON ALL LEVELS TO GRADE PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK.
 2. NUMBER AND LOCATION OF REINFORCEMENT SHOWN MAY DIFFER FROM ACTUAL FIELD CONDITIONS.
 3. COLUMN TIES WHICH HAVE LOST MORE THAN 15% OF ORIGINAL CROSS SECTIONAL AREA SHALL BE SUPPLEMENTED AS ENGINEER DIRECTS.
 4. DO NOT CUT OR DAMAGE REINFORCING. NOTIFY ENGINEER IF EXISTING REINFORCING STEEL APPEARS TO HAVE OVER 20% SECTION LOSS OR TWO ADJACENT BARS HAVE 10% SECTION LOSS. ENGINEER SHALL DETERMINE DURING CONSTRUCTION IF REINFORCING REPAIRS ARE REQUIRED.

REMOVE AND REPLACE ADA CURB CUT

3.12

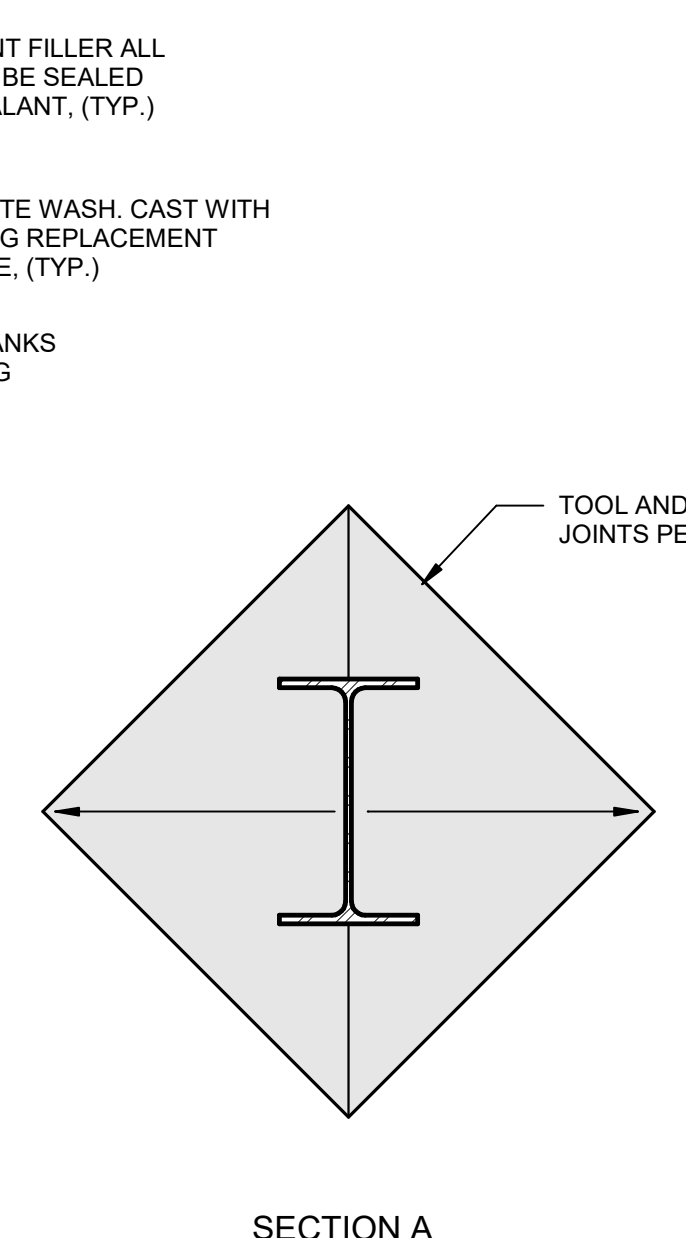
FLOOR REPAIR - SCALED CONCRETE SURFACE

3.8



FLOOR REPAIR - CONCRETE WASH

3.7



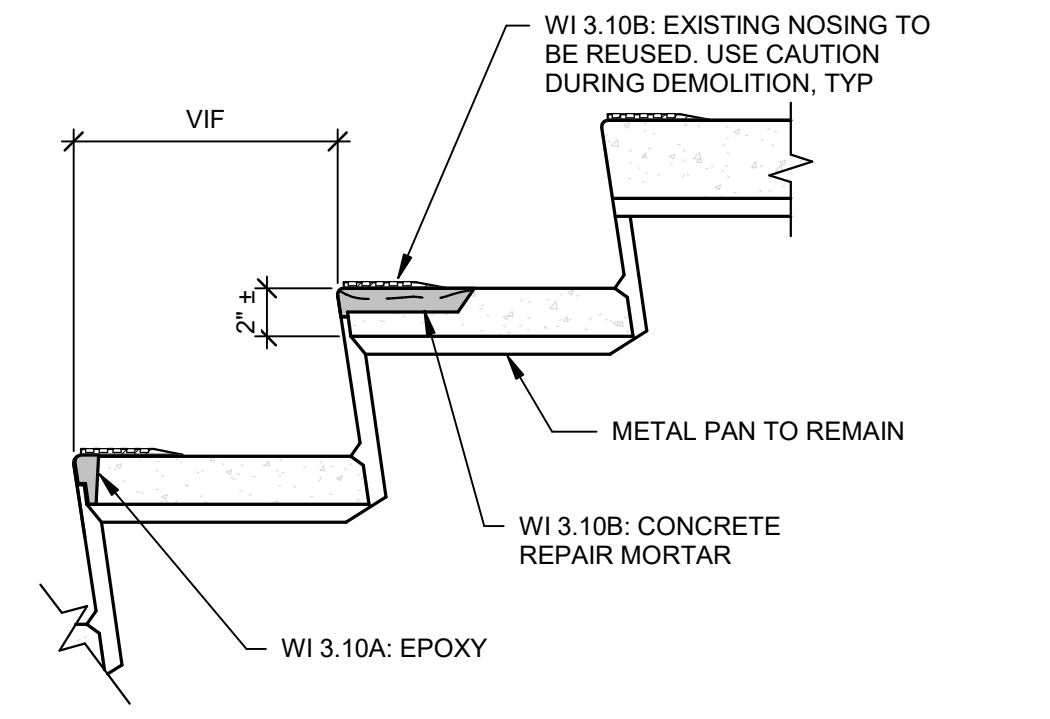
- NOTES:**
1. REMOVE AND REPLACE ALL SOUND AND UNSOUND CONCRETE WITHIN SECTION SHOWN SHADED.
 2. APPLY BITUMINOUS COATING TO THE UNDERSIDE OF NOSING BEFORE INSERTION INTO STAIR STEP.
 3. WI 3.10C: IF REQUIRED, REPLACE EXISTING NOSING IN KIND.

STAIR NOSING REPAIR

3.10

FLOOR REPAIR-STAIR LANDING PARTIAL DEPTH

3.11



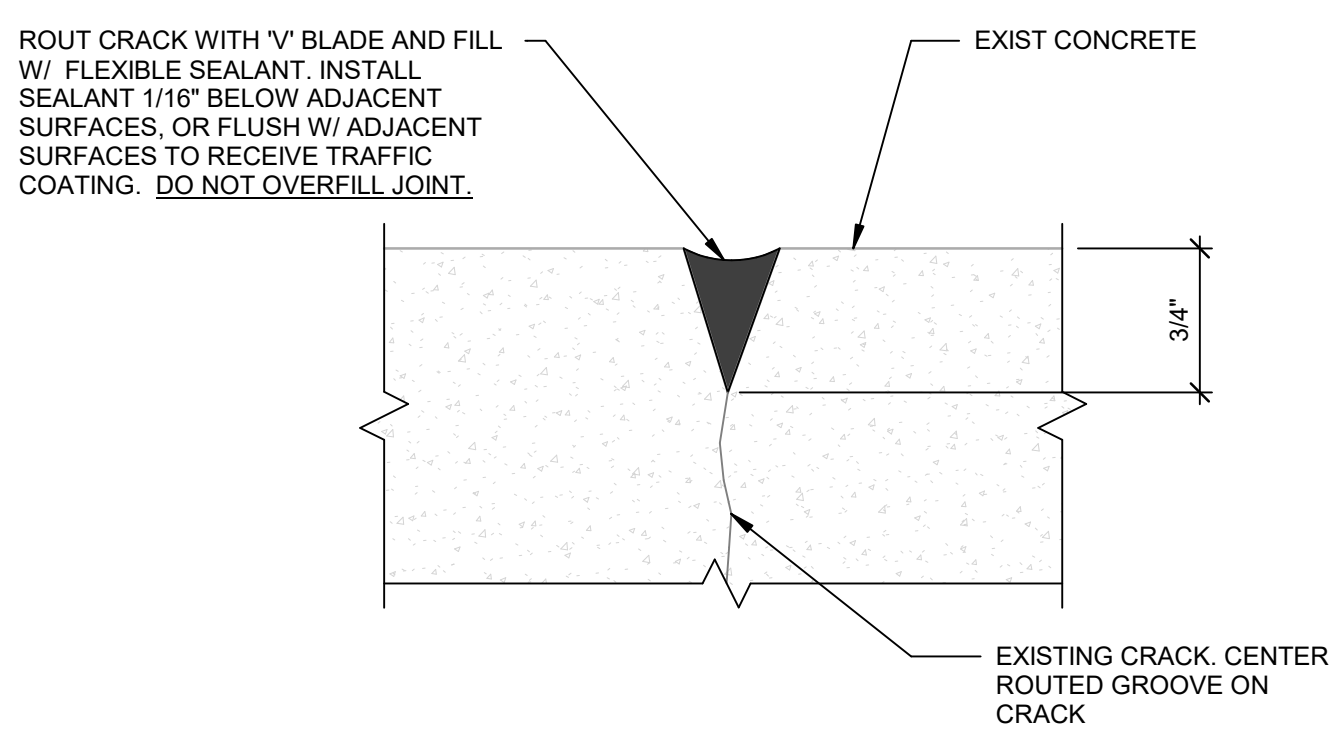
FLOOR AND CEILING REPAIRS PARTIAL DEPTH/SALLOW AT CONSTRUCTION JOINT

3.1
4.1

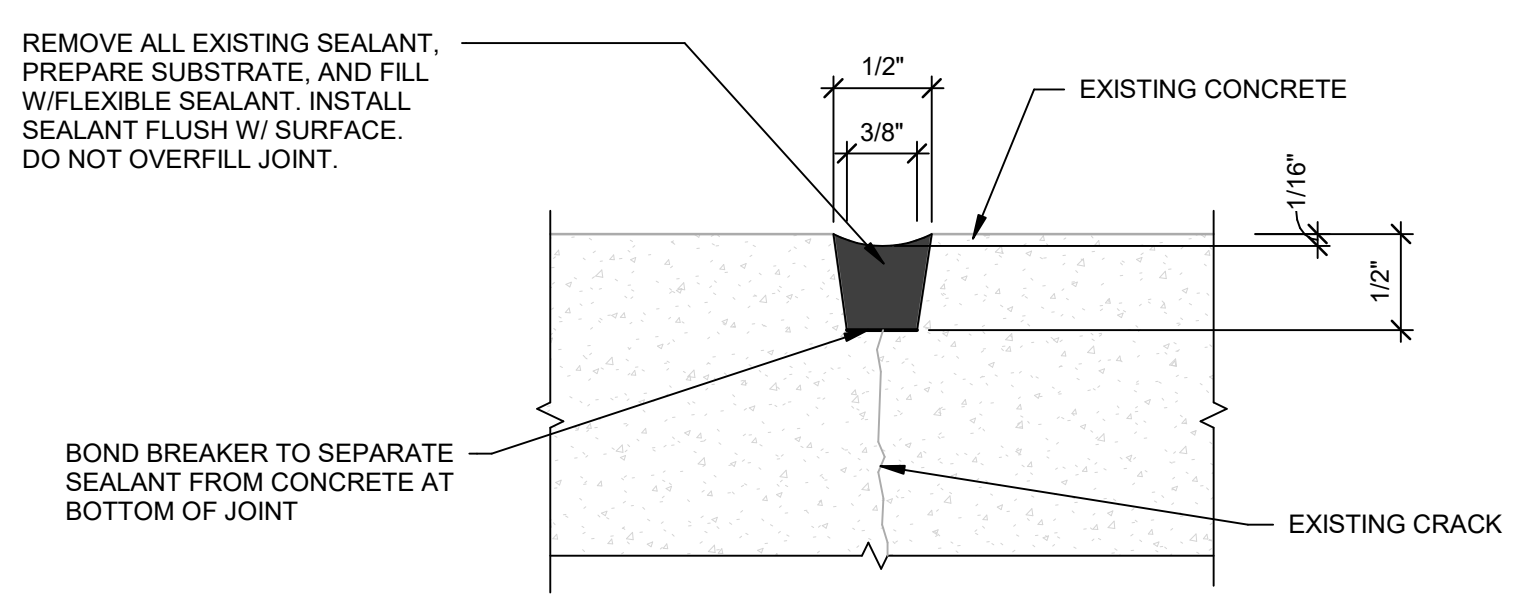
- NOTES:**
1. CONTRACTOR SHALL TEMPORARILY SHORE PLANKS PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. TEMPORARY SHORING SHALL REMAIN IN PLACE UNTIL CONCRETE REPAIRS HAVE FULLY CURED. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK.
 2. FLOOR REPAIR WORK, INCLUDING DEMOLITION, SHALL NOT BEGIN UNTIL UNDERSIDE REPAIRS HAVE BEEN COMPLETED AND HAVE FULLY CURED.
 3. CONTRACTOR SHALL NOTIFY ENGINEER IF ONE REINFORCING BAR HAS LOST MORE THAN 20% CROSS SECTION OR IF TWO ADJACENT BARS HAVE LOST MORE THAN 10% CROSS SECTION EACH.
 4. IF CONCRETE REMOVAL LIMITS EXCEED 1 1/2\"/>



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STRUCTURAL
No. 36481
REGISTERED
Professional Engineer
State of New Hampshire

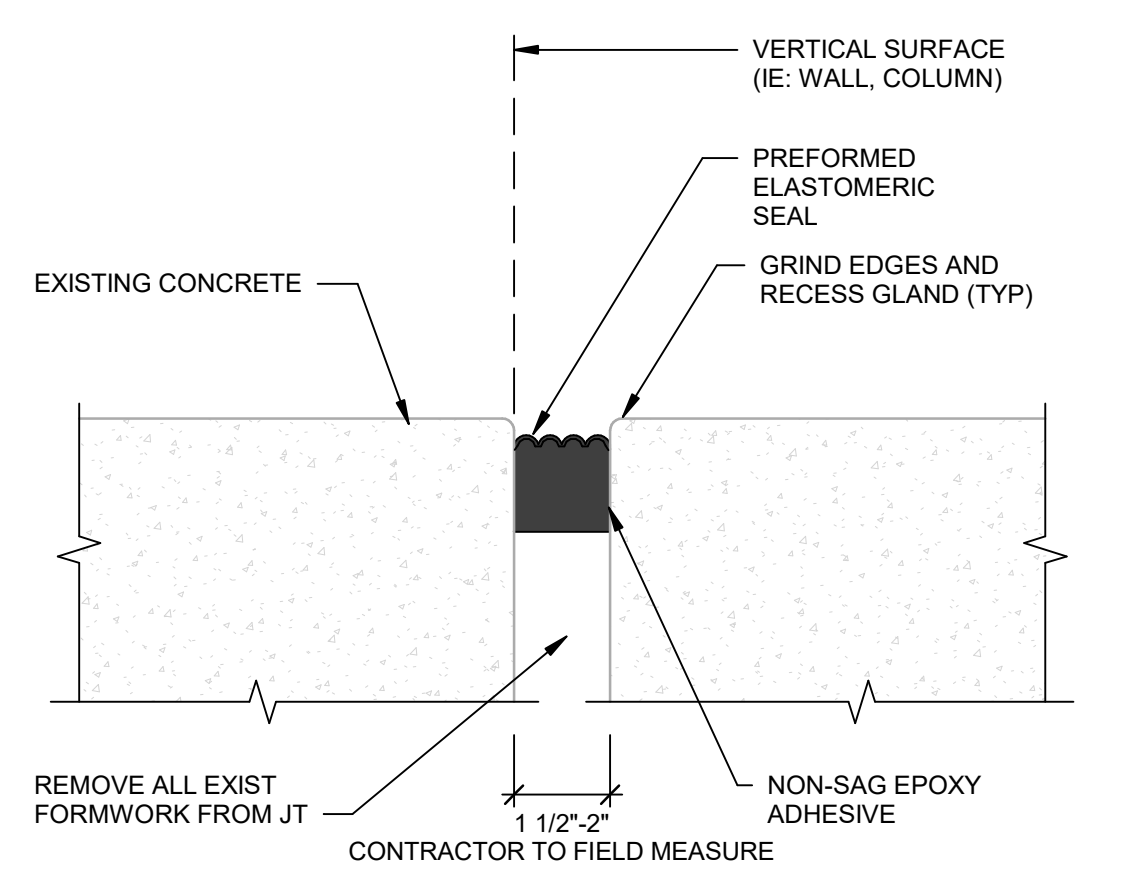


11.2 REPLACE / INSTALL CRACK SEALANT (CRACKS 0.030" OR GREATER)



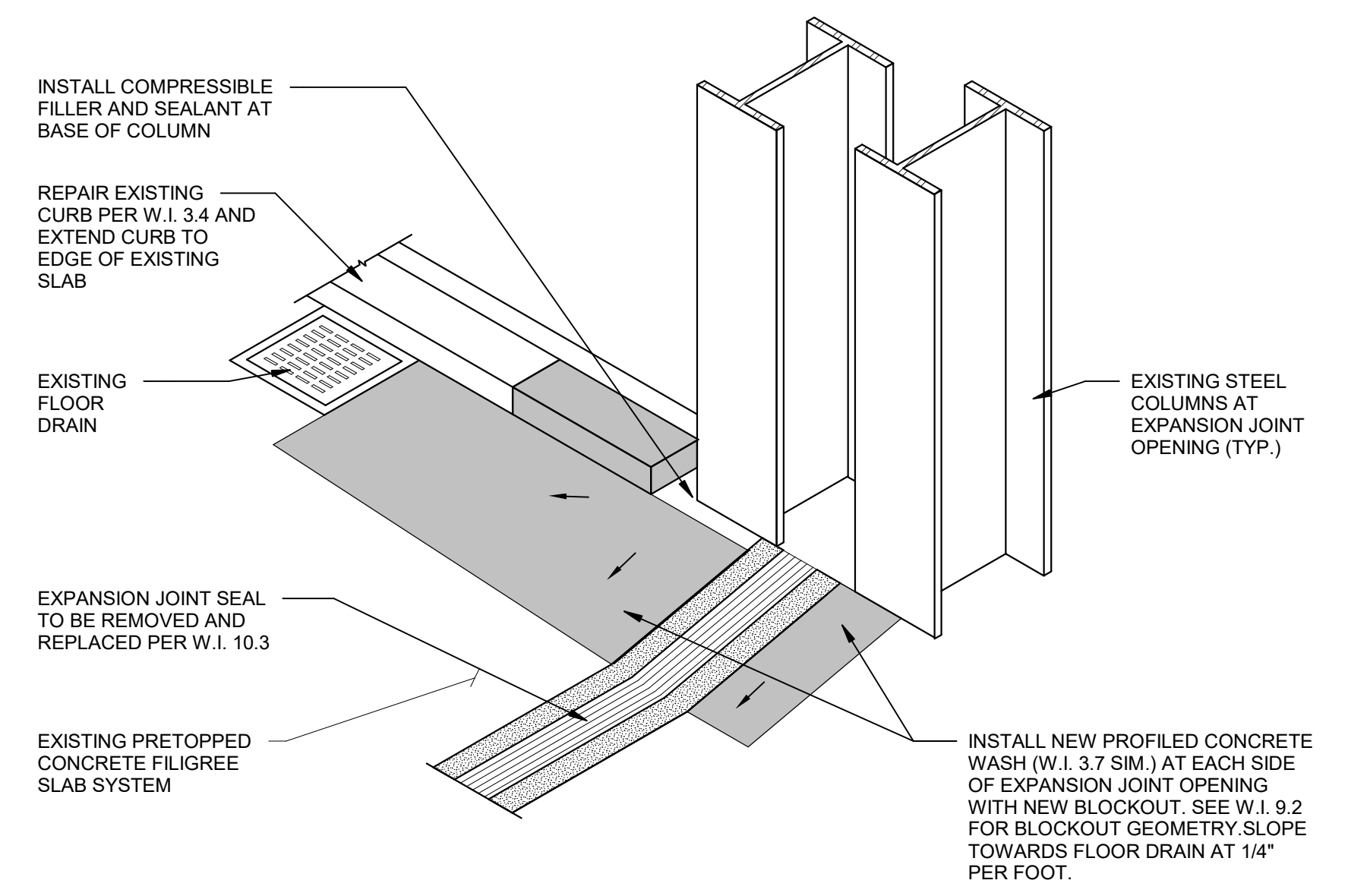
NOTES:
1. FOR 1/2" WIDE JOINT SEALANT REPLACEMENT AT PRE-TOPPED FILIGREE PLANKS SEE W.I. 11.5 SIM.

11.1 REMOVE AND REPLACE CONTROL JOINT SEALANT 1/2" WIDE

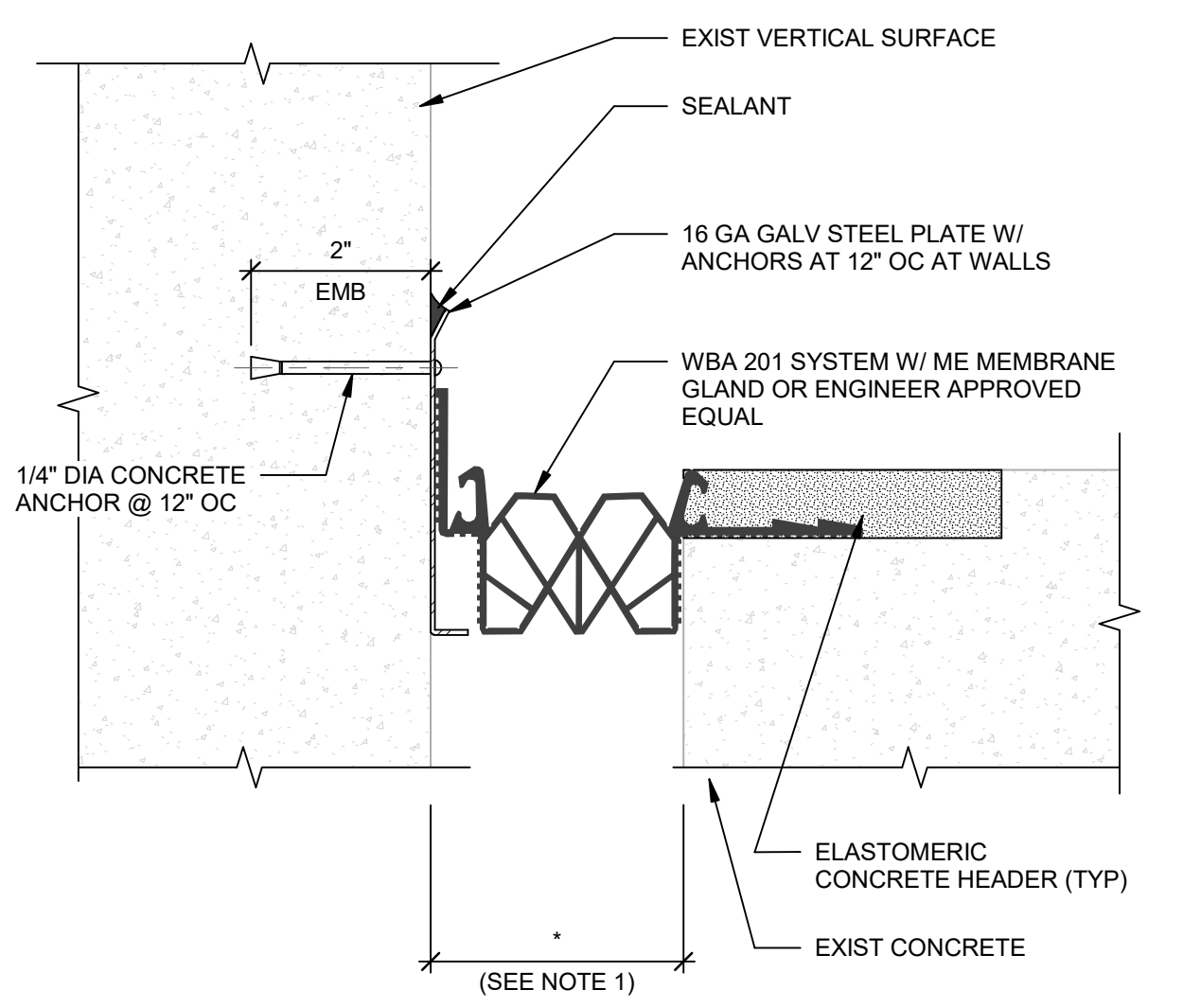


NOTES:
1. CONTRACTOR SHALL, WITH MANUFACTURER, VERIFY FIELD INSTALLATION WIDTH BASED ON TEMPERATURE CONDITIONS.
2. ALL INTERSECTIONS AND 90° CORNERS TO BE FIELD FABRICATED PER MANUFACTURER'S DETAILS AND WRITTEN INSTRUCTIONS.

10.5 EXPANSION JOINT- ADHERED

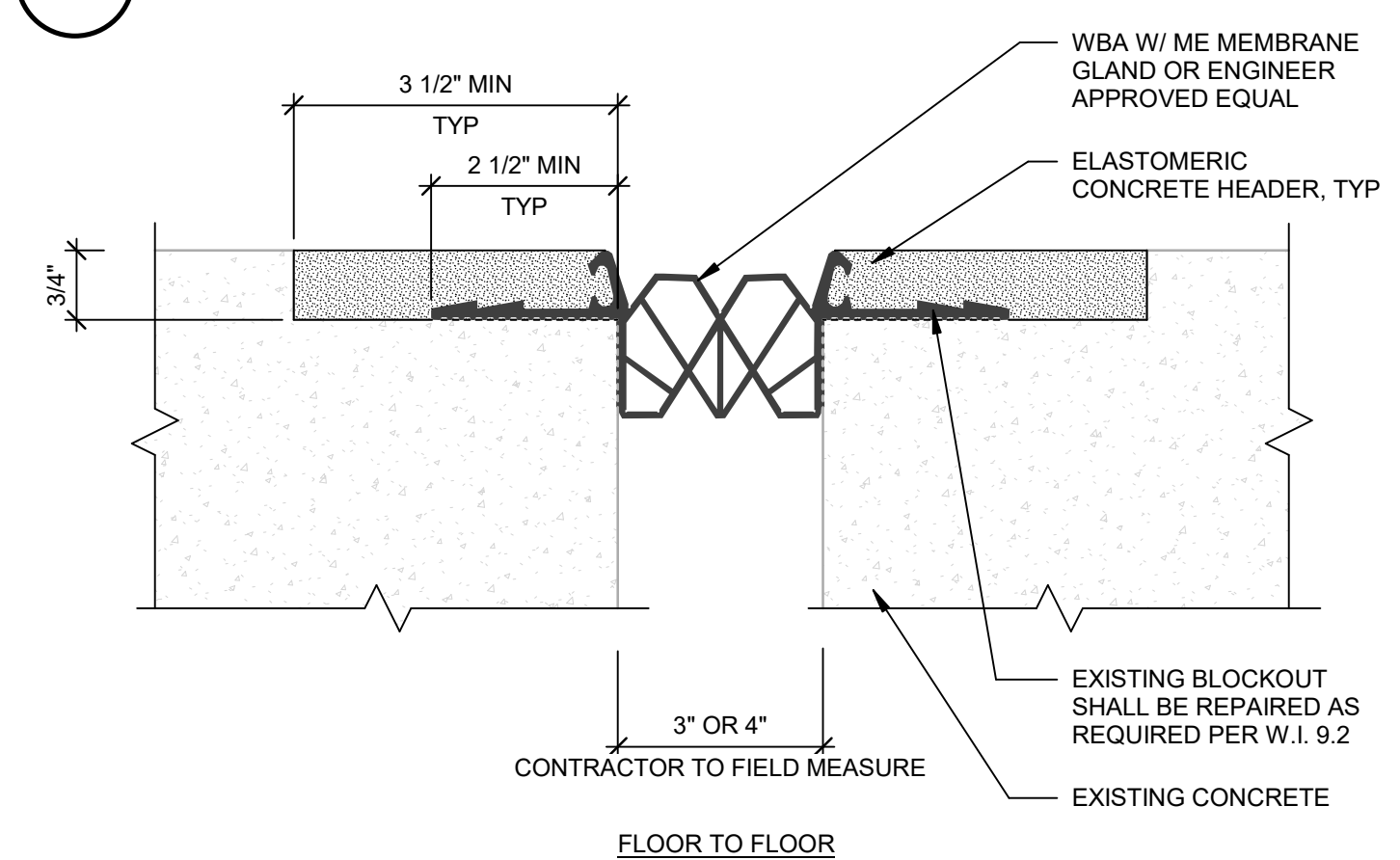


10.3.2 EXPANSION JOINT TERMINATION AT STEEL COLUMNS



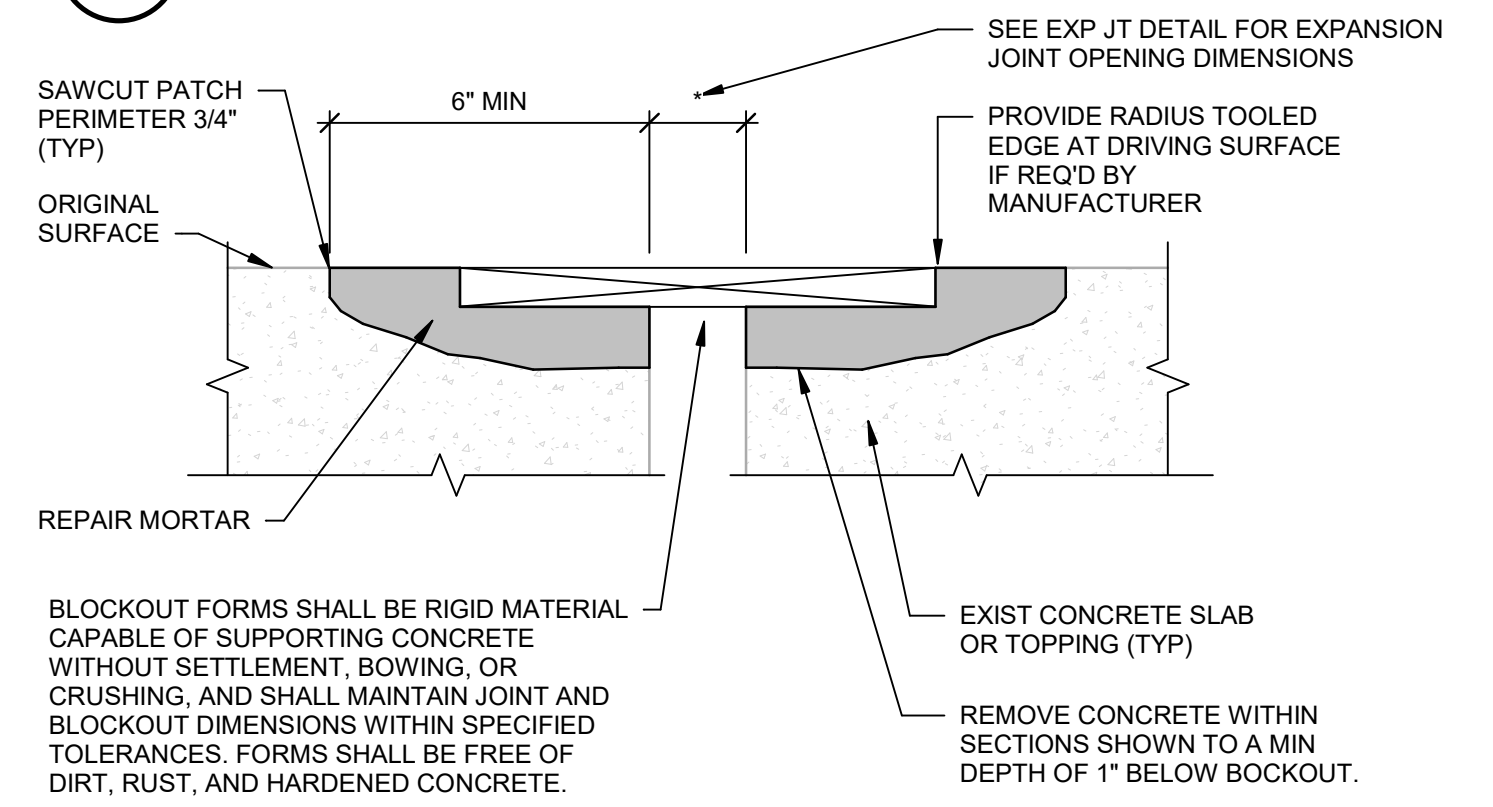
NOTES:
1. SEE DETAIL 10.3 FOR REMAINING INFORMATION.

10.3.1 EXPANSION JOINT- ELASTOMERIC CONCRETE EDGED



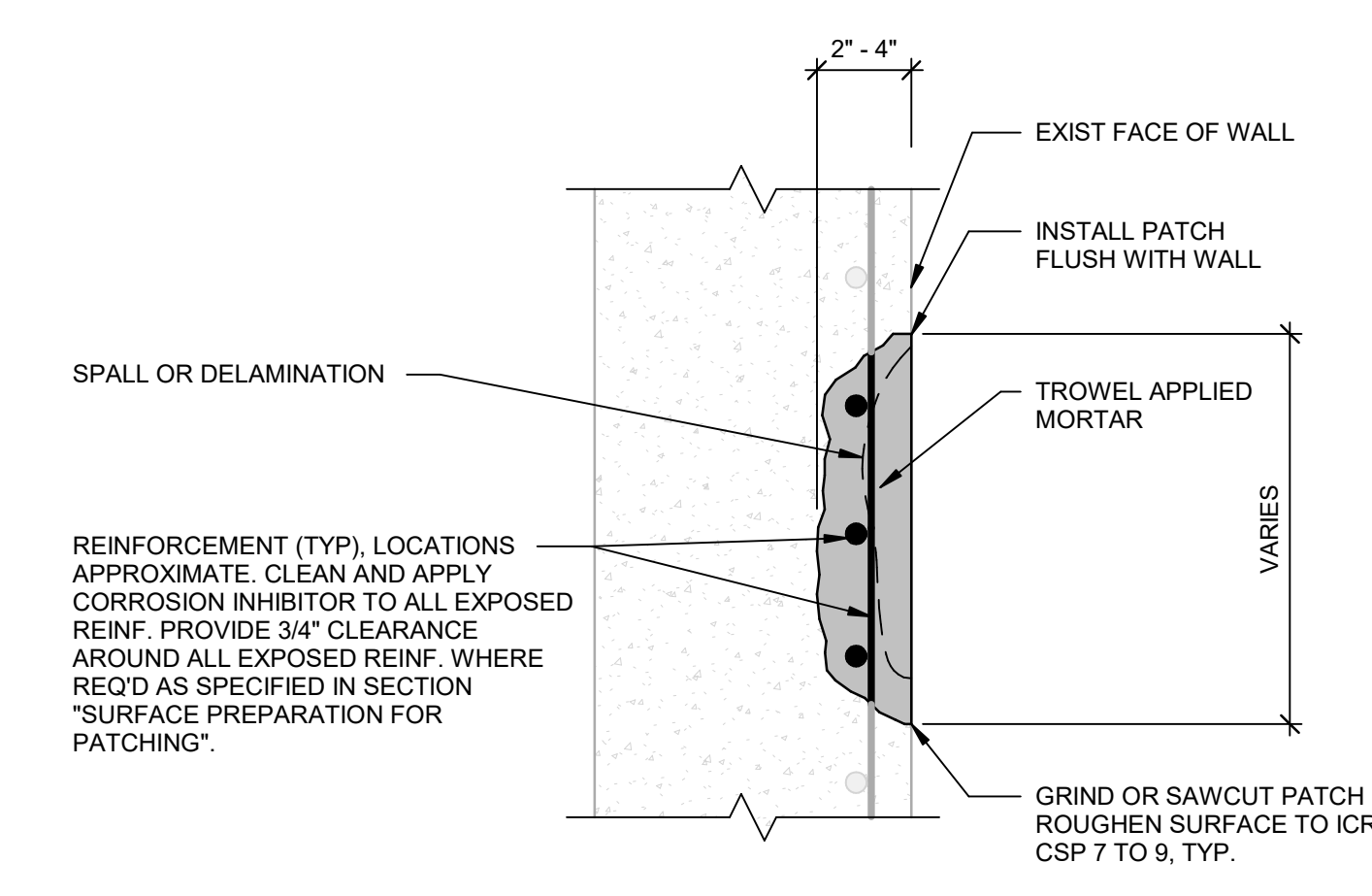
NOTES:
1. CONTRACTOR SHALL, WITH MANUFACTURER, VERIFY FIELD INSTALLATION WIDTH BASED ON TEMPERATURE CONDITIONS.
2. PROVIDE SHOP DRAWINGS SHOWING JOINT INTERSECTIONS AND WALL TERMINATIONS.

10.3 EXPANSION JOINT-ELASTOMERIC CONCRETE EDGED



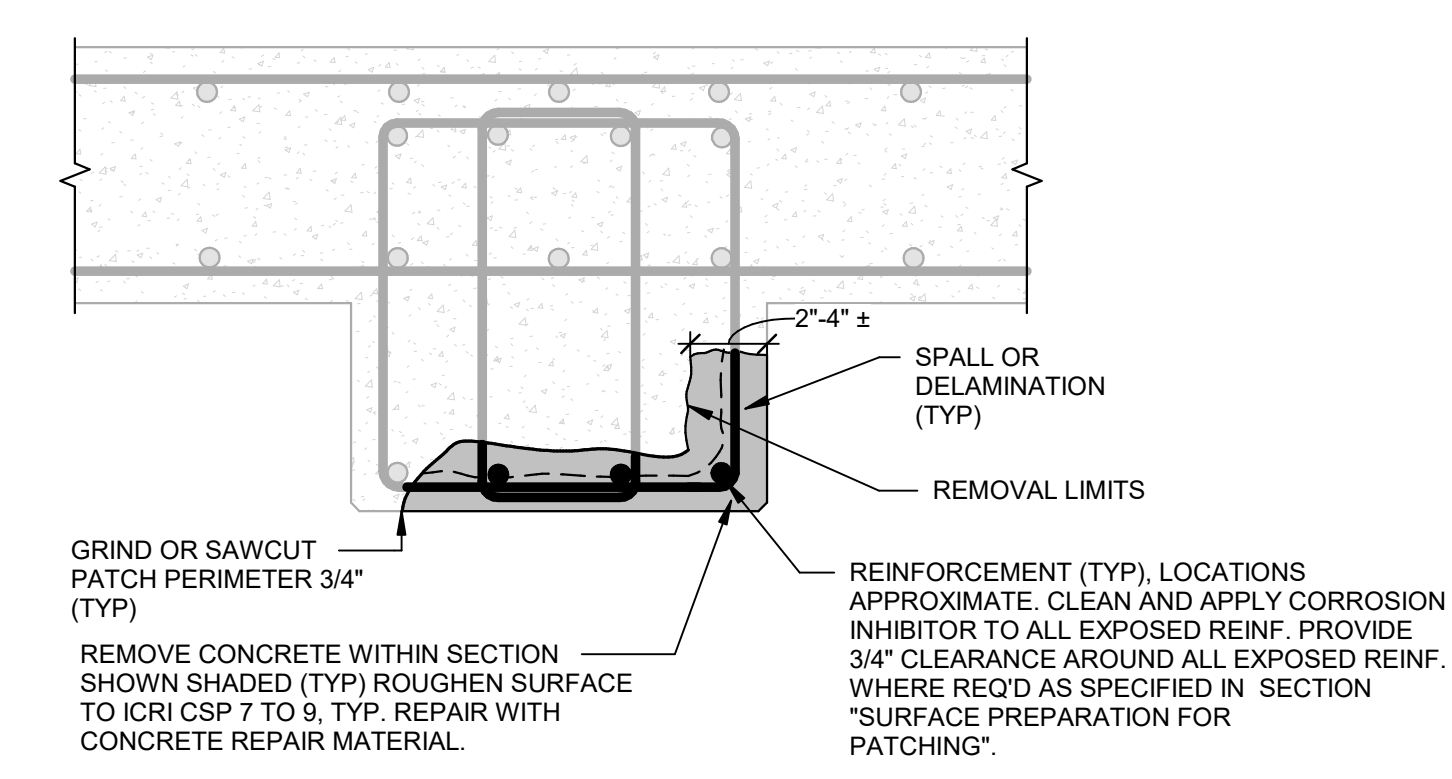
NOTES:
1. MAINTAIN EXPANSION JOINT OPENING CLEAR OF DEBRIS. VERTICAL SURFACE MUST BE PLUMB AND SHALL NOT VARY MORE THAN 1/8\"/>

9.2 EXPANSION JOINT PREPARATION-BLOCKOUT REPAIR



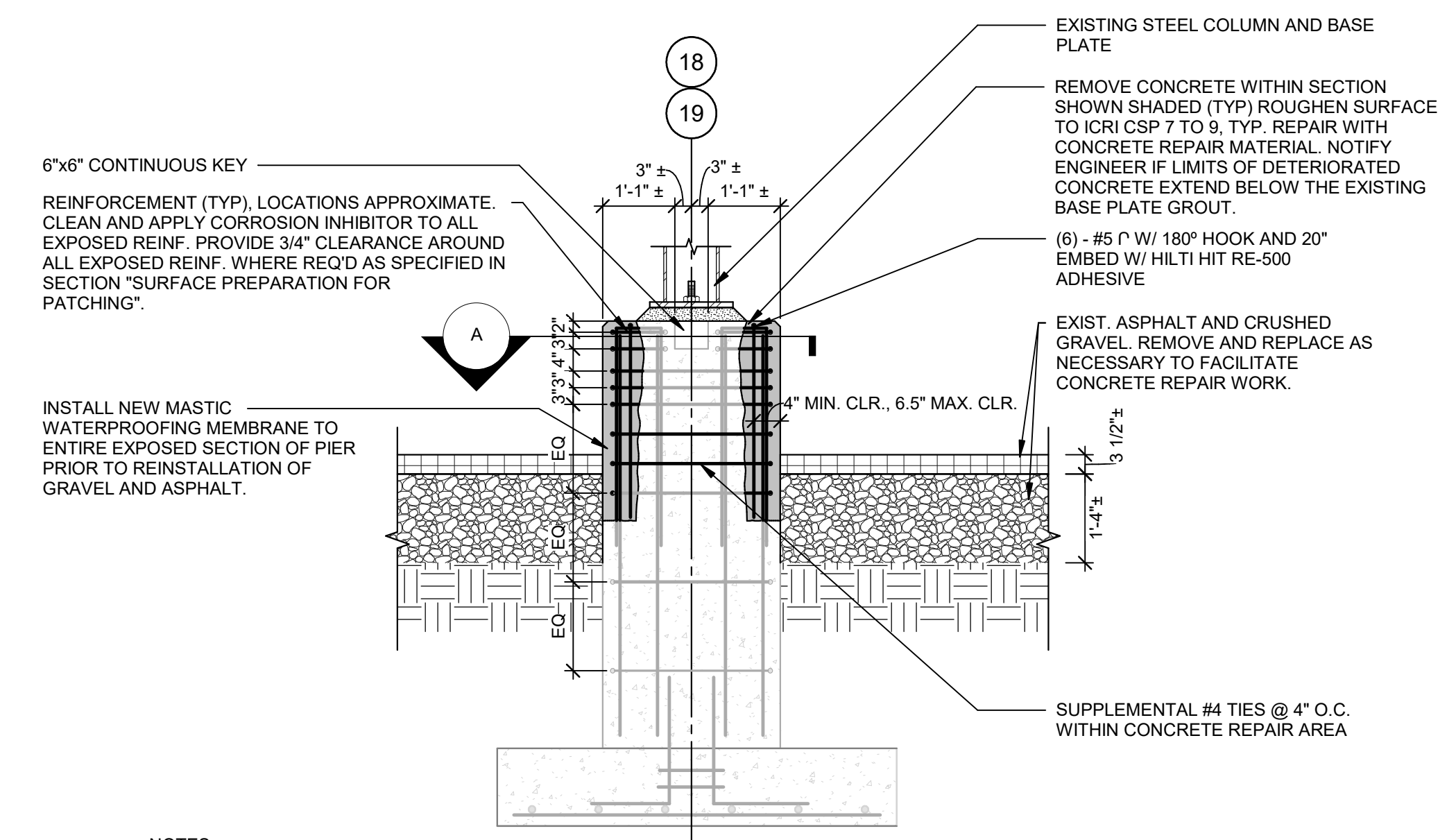
NOTES:
1. CONTRACTOR SHALL TEMPORARILY SHORE PLANKS AND BEAMS PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK.

7.1 WALL REPAIR-PARTIAL DEPTH/SHALLOW

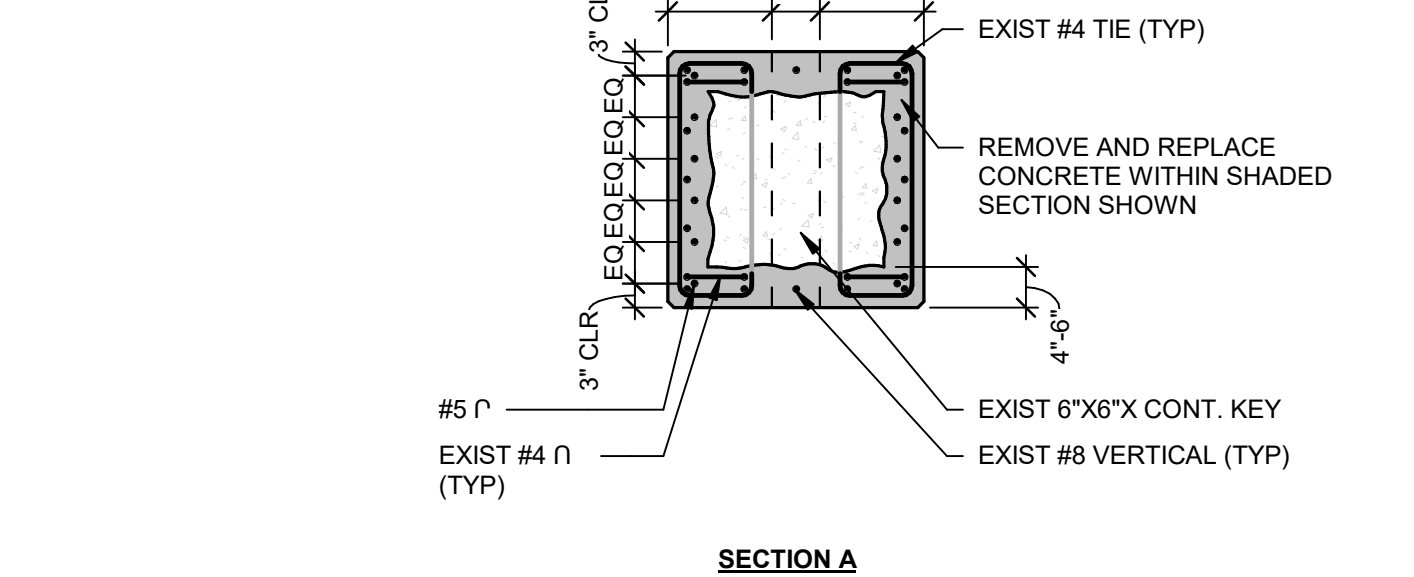


NOTES:
1. CONTRACTOR SHALL TEMPORARILY SHORE PLANKS AND BEAMS ON ALL LEVELS TO GRADE PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK.
2. COLUMN TIES WHICH HAVE LOST MORE THAN 15% OF ORIGINAL CROSS SECTIONAL AREA SHALL BE SUPPLEMENTED AS ENGINEER DIRECTS.
3. NUMBER AND LOCATION OF REINFORCEMENT SHOWN MAY DIFFER FROM ACTUAL FIELD CONDITIONS.
4. DO NOT CUT OR DAMAGE REINFORCING. NOTIFY ENGINEER IF EXISTING REINFORCING STEEL APPEARS TO HAVE OVER 20% SECTION LOSS OR TWO ADJACENT BARS HAVE 10% SECTION LOSS. ENGINEER SHALL DETERMINE DURING CONSTRUCTION IF REINFORCING REPAIRS ARE REQUIRED.

6.7 COLUMN REPAIR-PILASTER



NOTES:
1. CONTRACTOR SHALL TEMPORARILY SHORE PLANKS AND BEAMS ON ALL LEVELS TO GRADE PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK.
2. NUMBER AND LOCATION OF REINFORCEMENT SHOWN MAY DIFFER FROM ACTUAL FIELD CONDITIONS.
3. COLUMN TIES WHICH HAVE LOST MORE THAN 15% OF ORIGINAL CROSS SECTIONAL AREA SHALL BE SUPPLEMENTED AS ENGINEER DIRECTS.
4. DO NOT CUT OR DAMAGE REINFORCING. NOTIFY ENGINEER IF EXISTING REINFORCING STEEL APPEARS TO HAVE OVER 20% SECTION LOSS OR TWO ADJACENT BARS HAVE 10% SECTION LOSS. ENGINEER SHALL DETERMINE DURING CONSTRUCTION IF REINFORCING REPAIRS ARE REQUIRED.



6.6.1 COLUMN REPAIR - PIER BASE AT BRACED FRAME

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SHEET TITLE:
REPAIR DETAILS



Arthur G. Stading

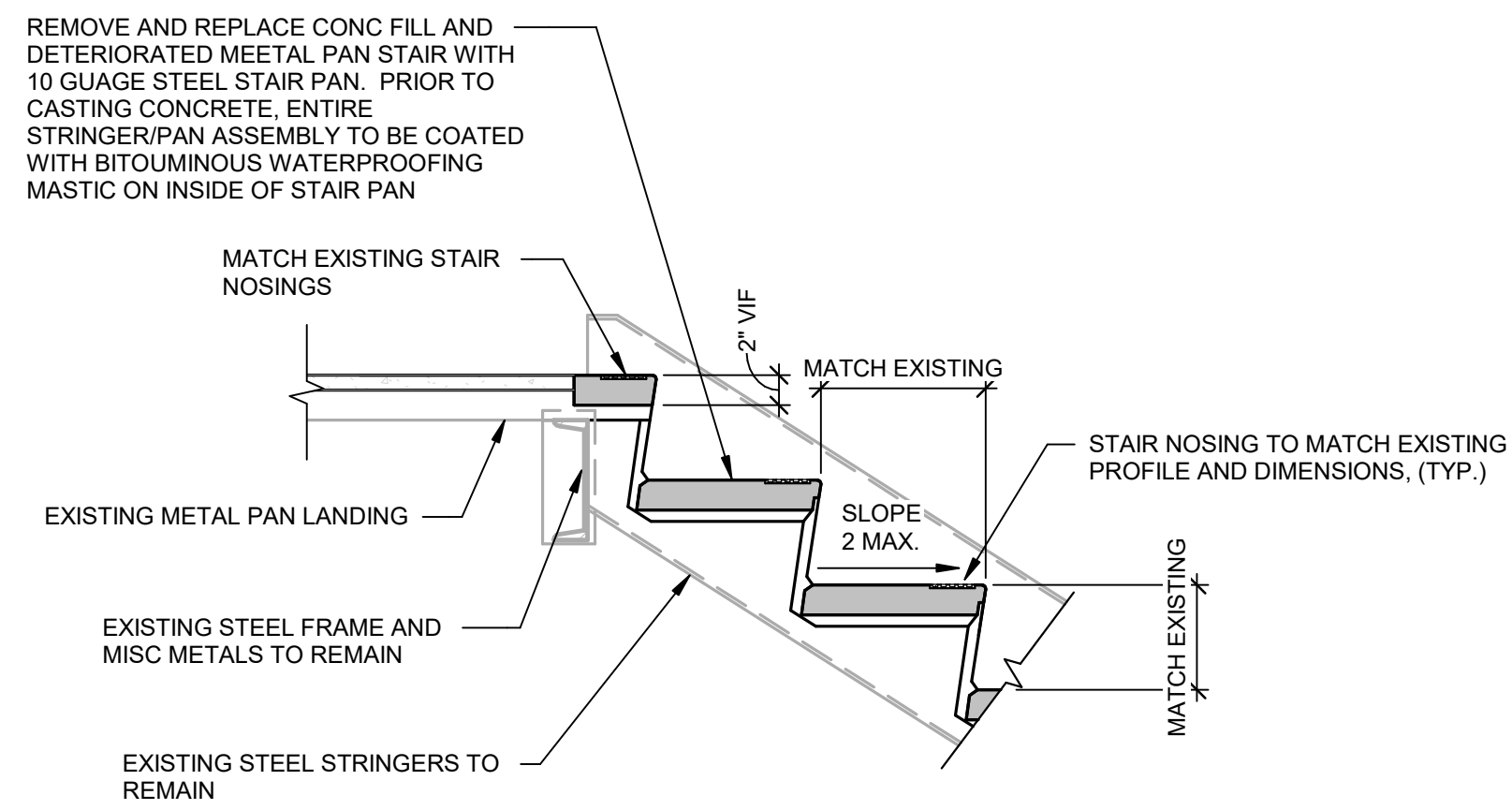
HANOVER STREET GARAGE RESTORATION

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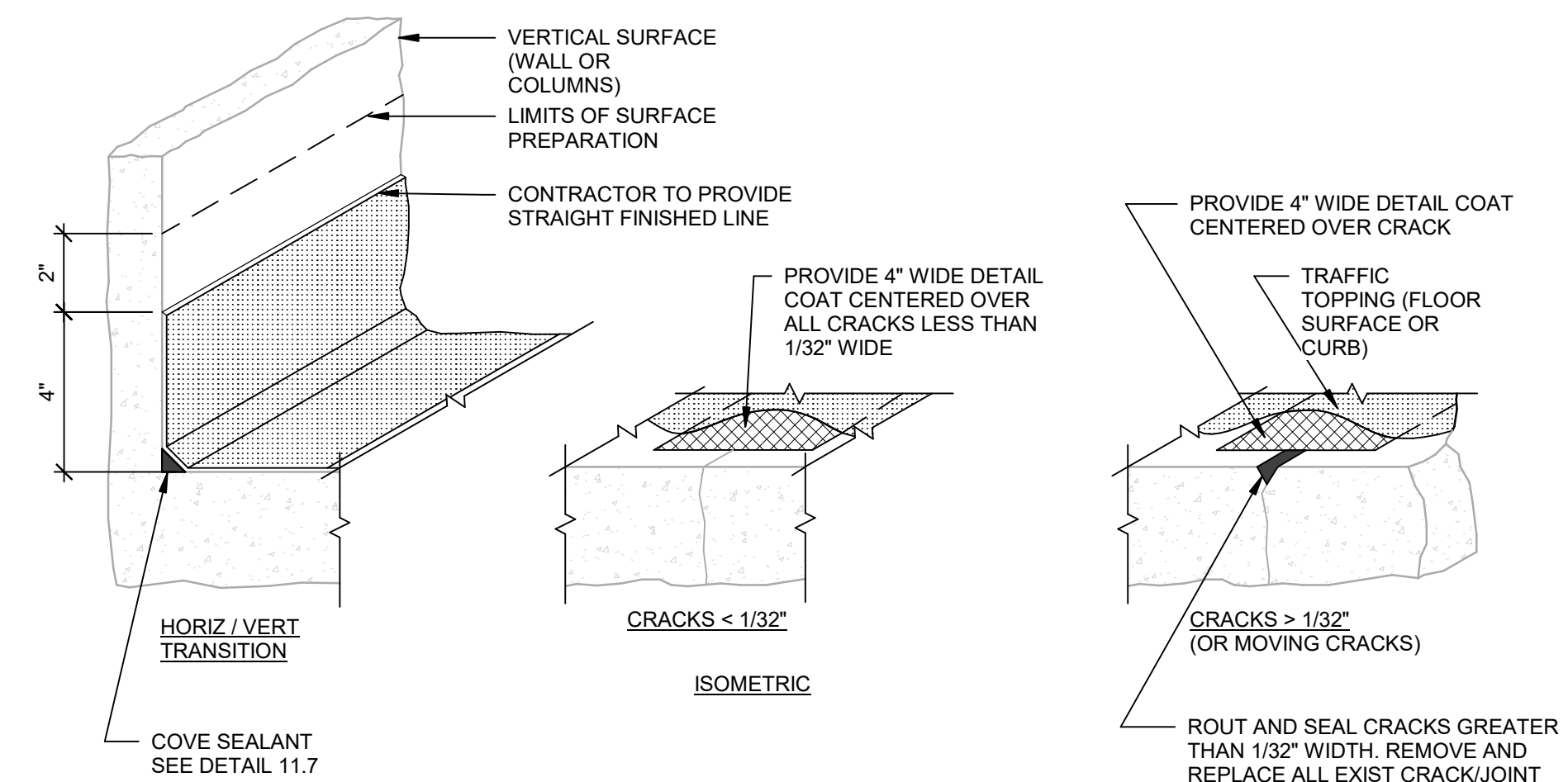
SHEET TITLE:
REPAIR DETAILS



- NOTES:**
1. VERIFY ALL DIMENSIONS IN FIELD PRIOR TO FABRICATION OF METAL STAIR PANS.
 2. ALL IN PLACE CONSTRUCTION AFFECTED BY STAIR PAN REPLACEMENT TO BE CLEANED AND PAINTED. COLOR AND FINISH TO MATCH EXISTING.
 3. FASTEN NEW STAIR PANS TO EXISTING STRUCTURAL STEEL PER MANUFACTURER'S REQUIREMENTS.

REPLACE STAIR RISER / PAN CONCRETE FILL

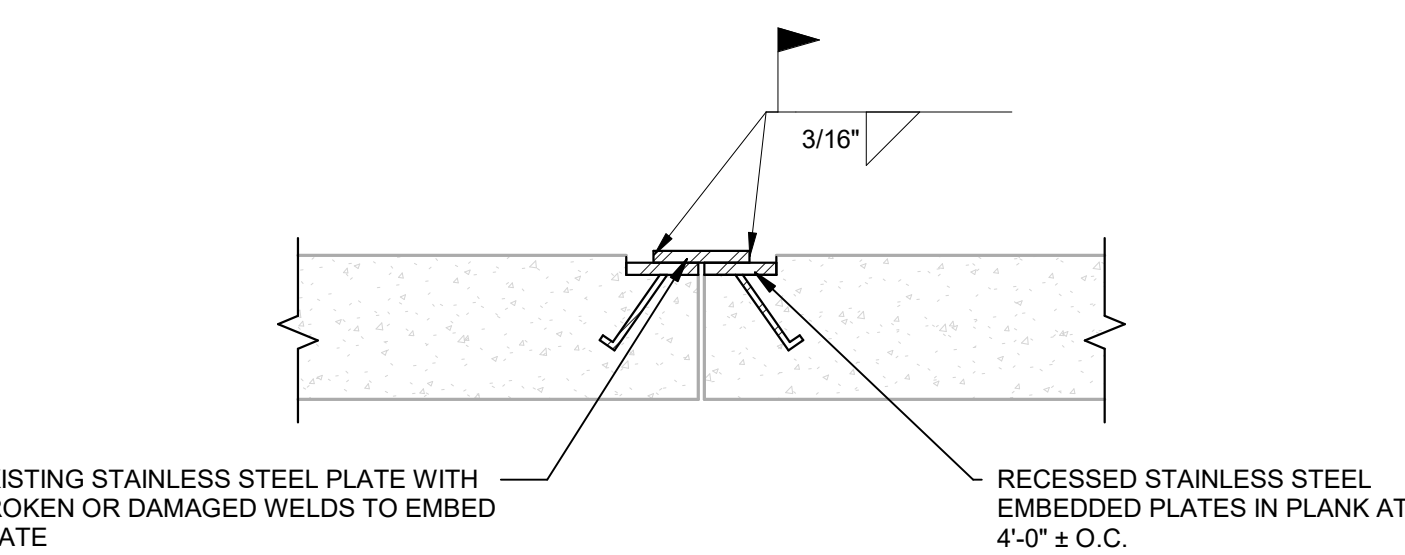
41.1



- NOTES:**
1. QUANTITIES BASED ON HORIZONTAL APPLICATION AREA. VERTICAL DETAILING, ADDITIONAL DETAIL COAT OVER CRACKS, ROUTING AND SEALING CRACKS, INSTALLATION OF COVE SEALANT, AND REMOVAL AND REPLACEMENT OF EXISTING SEALANTS ARE INCIDENTAL TO THIS WORK.

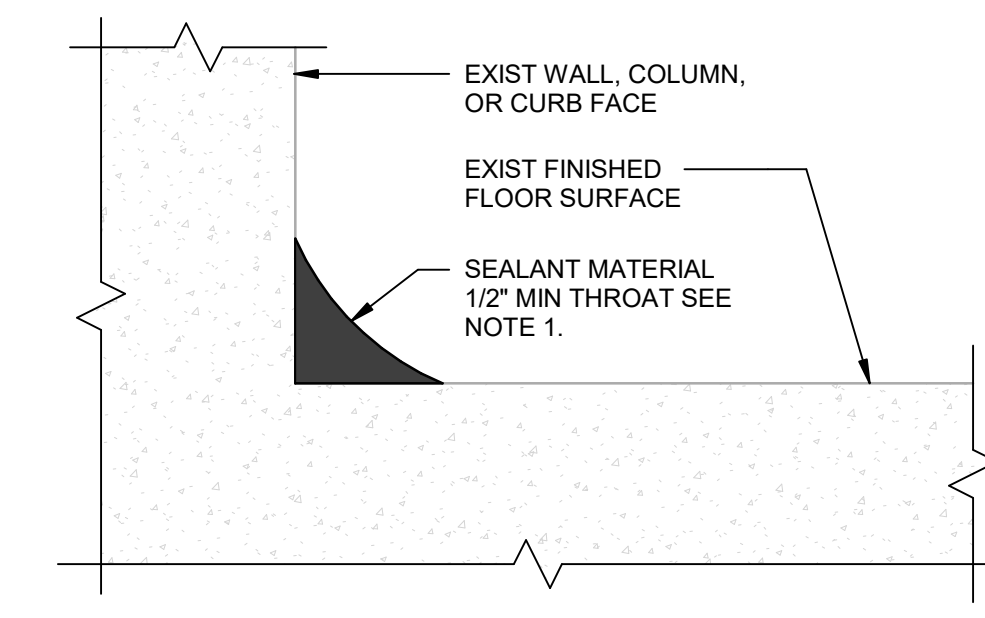
TRAFFIC TOPPING

16.1
16.3
16.4



RE-WELD SHEAR CONNECTOR

40.3

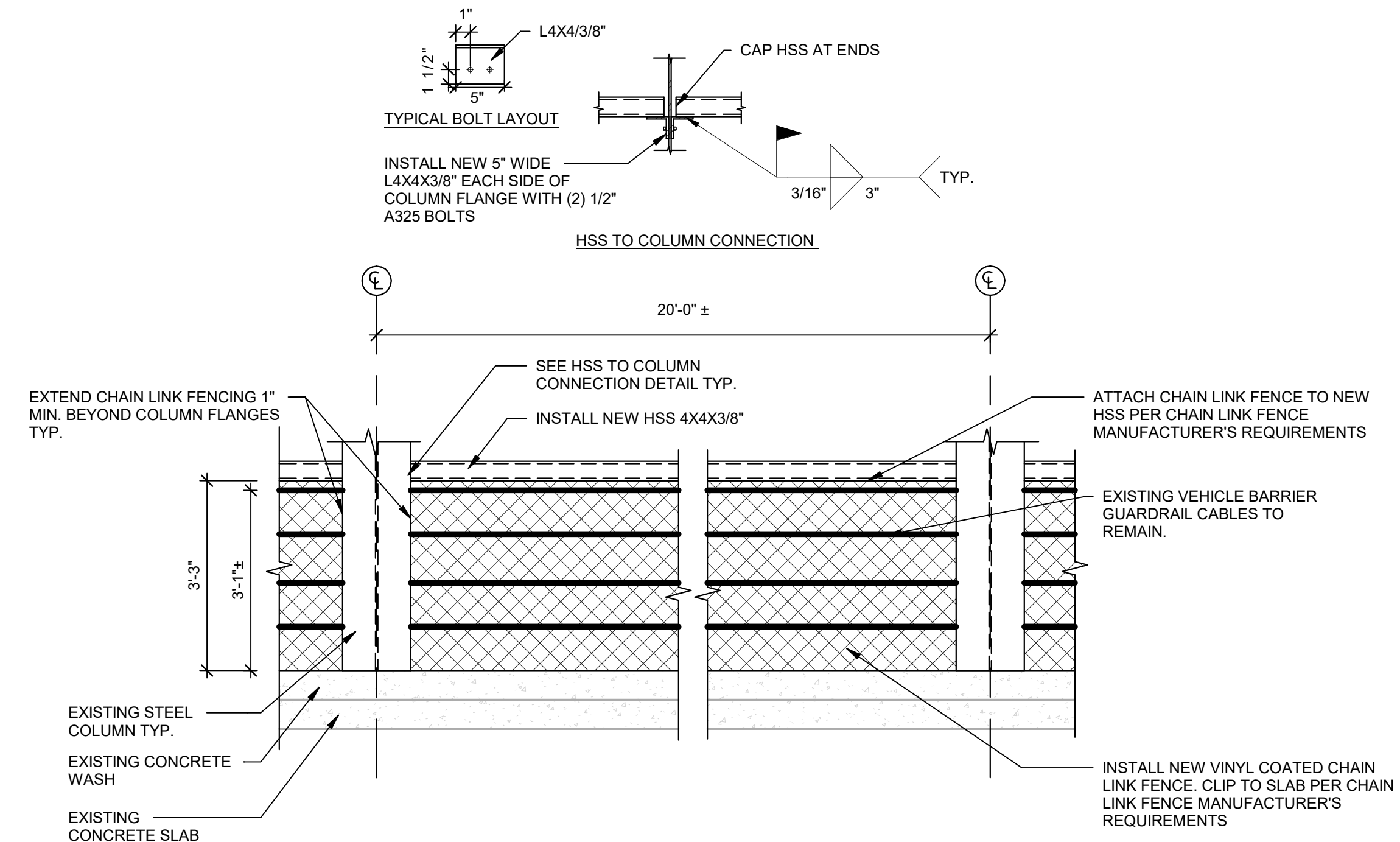


- NOTES:**
1. REMOVE EXISTING COVE SEALANT MATERIAL IF PRESENT. PREPARE SURFACE PER SPECIFICATIONS.

COVE SEALANT

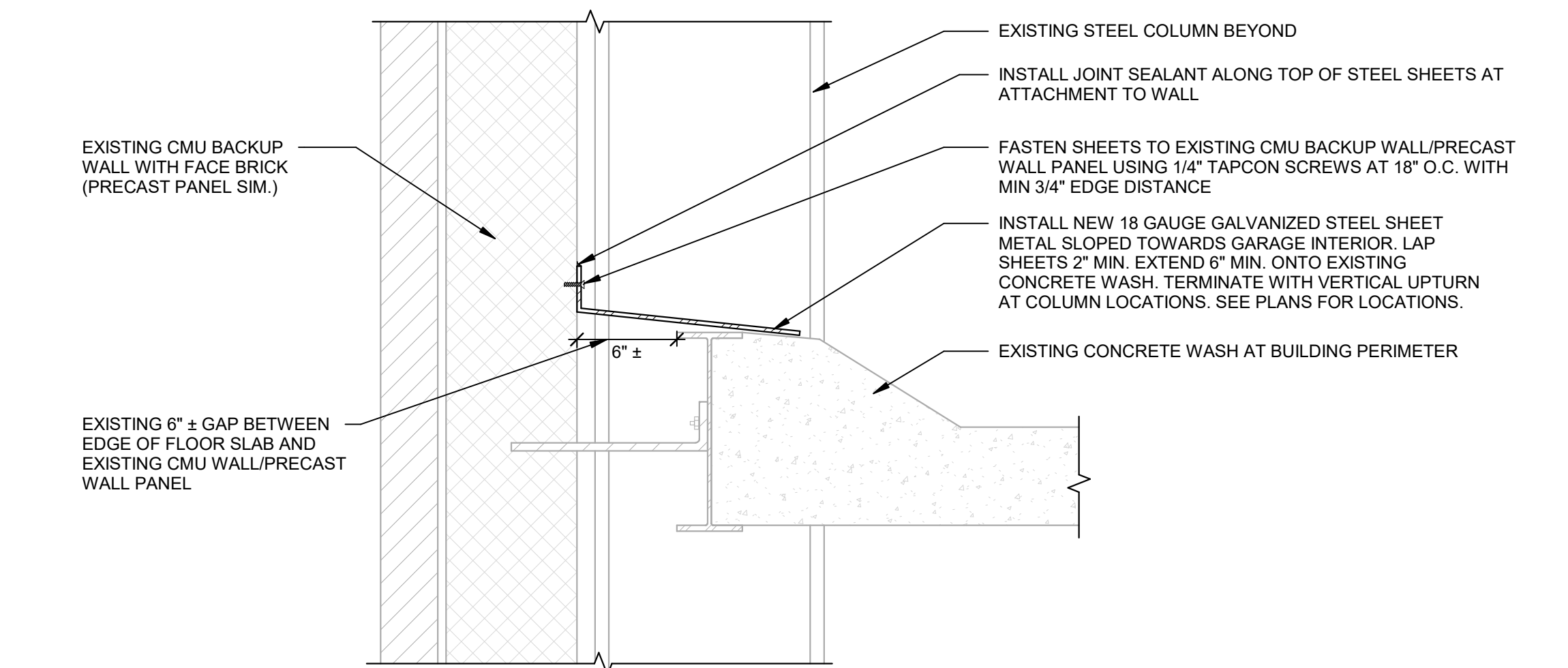
(INCIDENTAL TO W.I. SERIES 16.0)

11.7



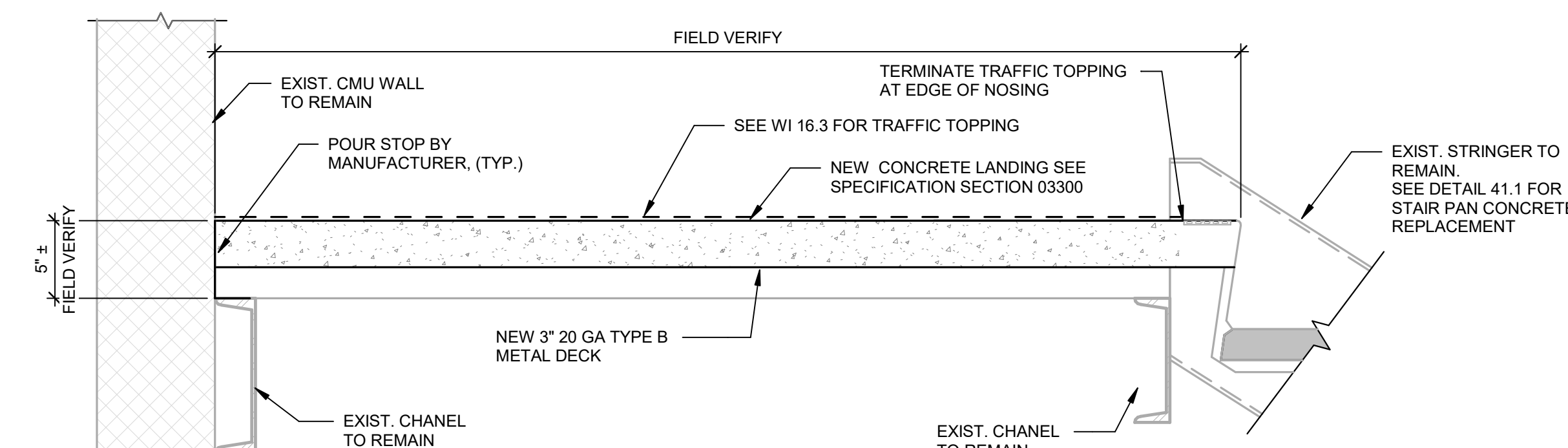
INSTALL CHAIN LINK FENCE AT VEHICLE BARRIER

43.3



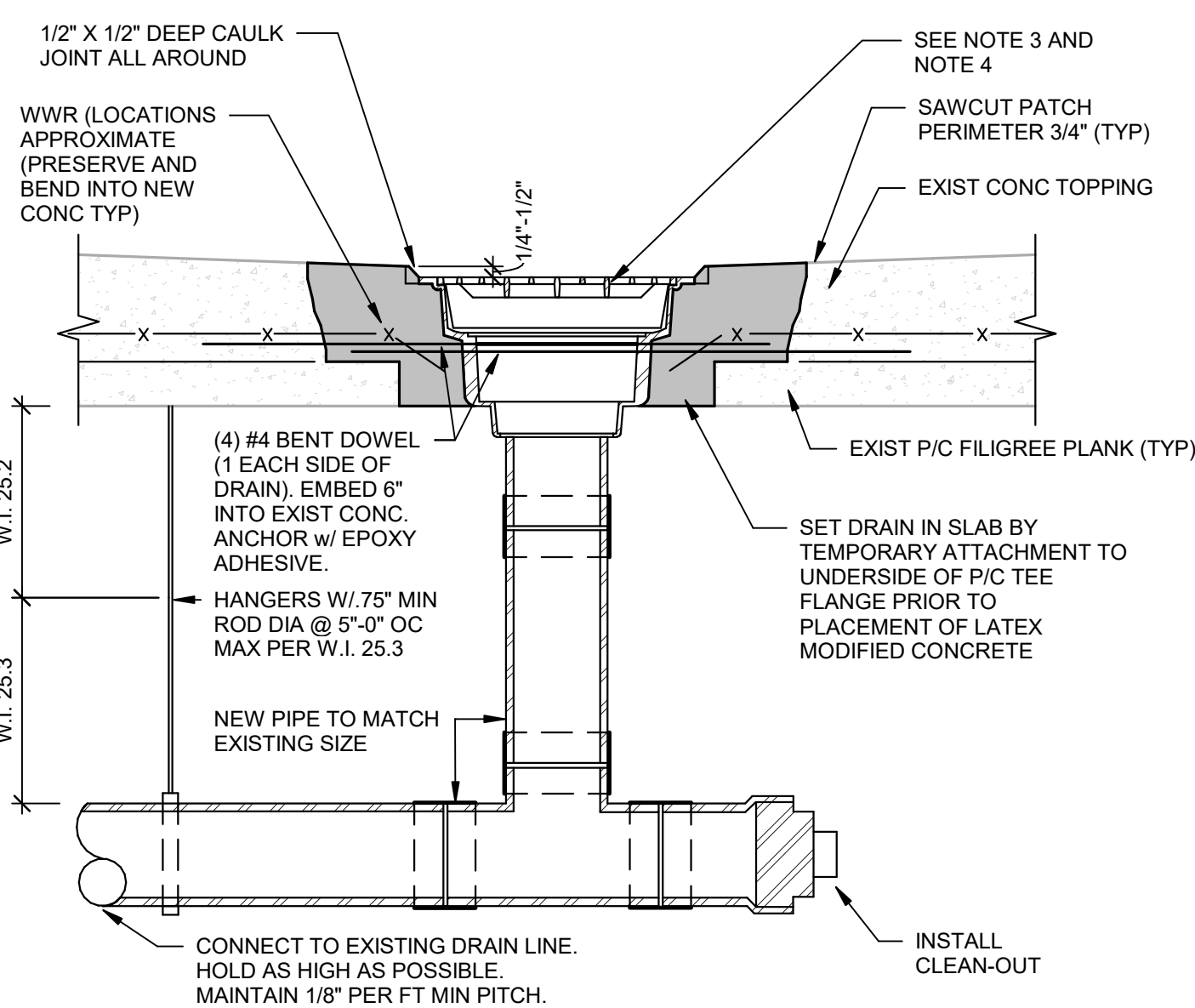
INSTALL STEEL PLATE AT BUILDING PERIMETER GAP

43.1



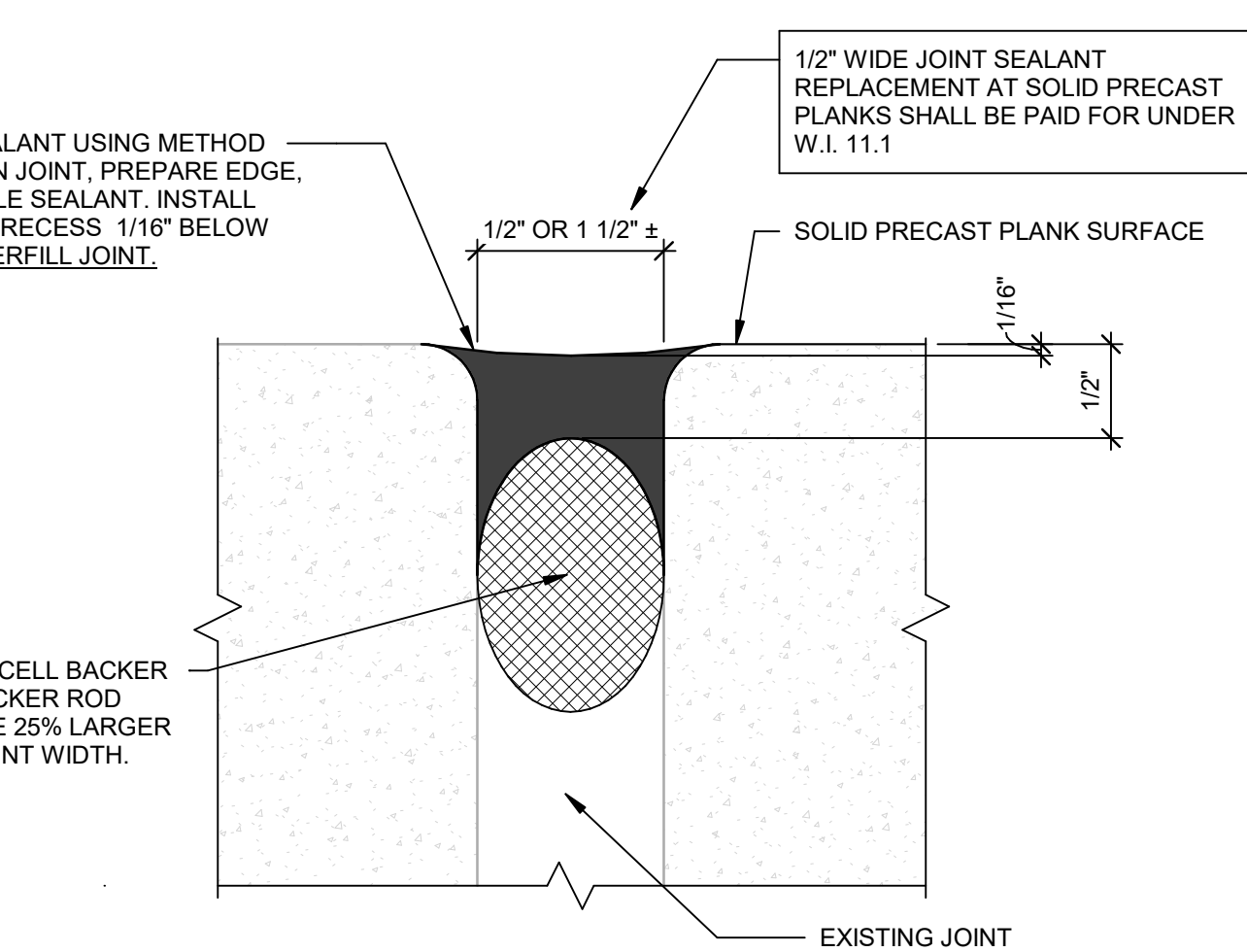
REPLACE LANDING/FILL

41.2



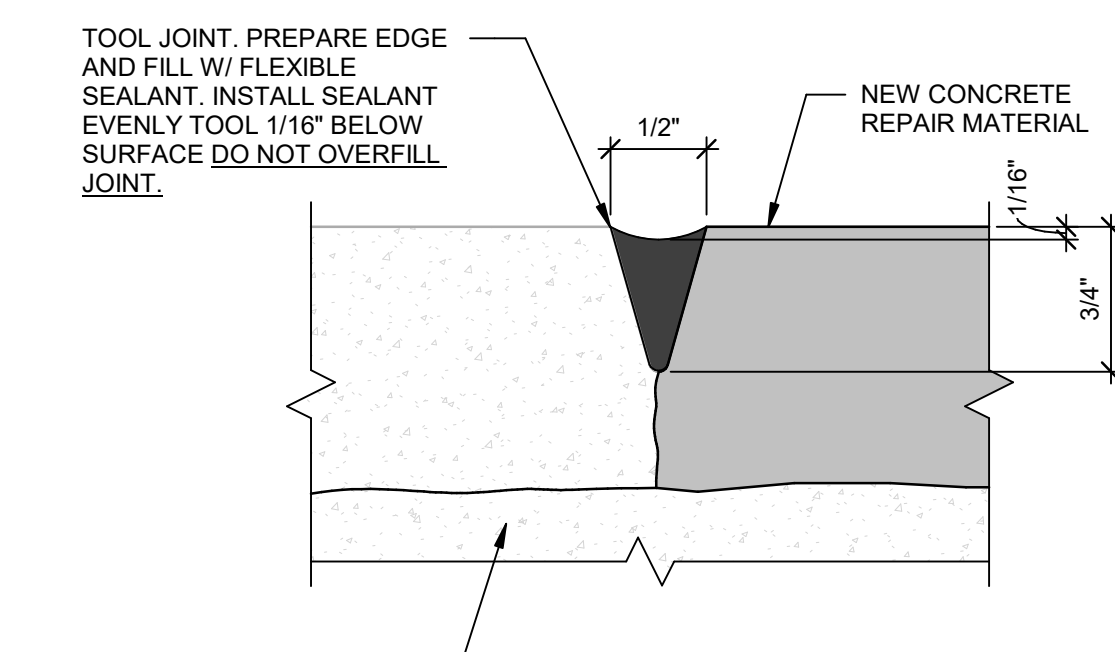
MECHANICAL-SUPPLEMENTAL/REPLACEMENT FLOOR DRAIN/PIPE & HANGERS

25.2
25.3



REMOVE AND REPLACE CONTROL JOINT SEALANT 1 1/2\"/>

11.5



TOOL & SEAL PATCH PERIMETERS AND CONTROL JOINTS

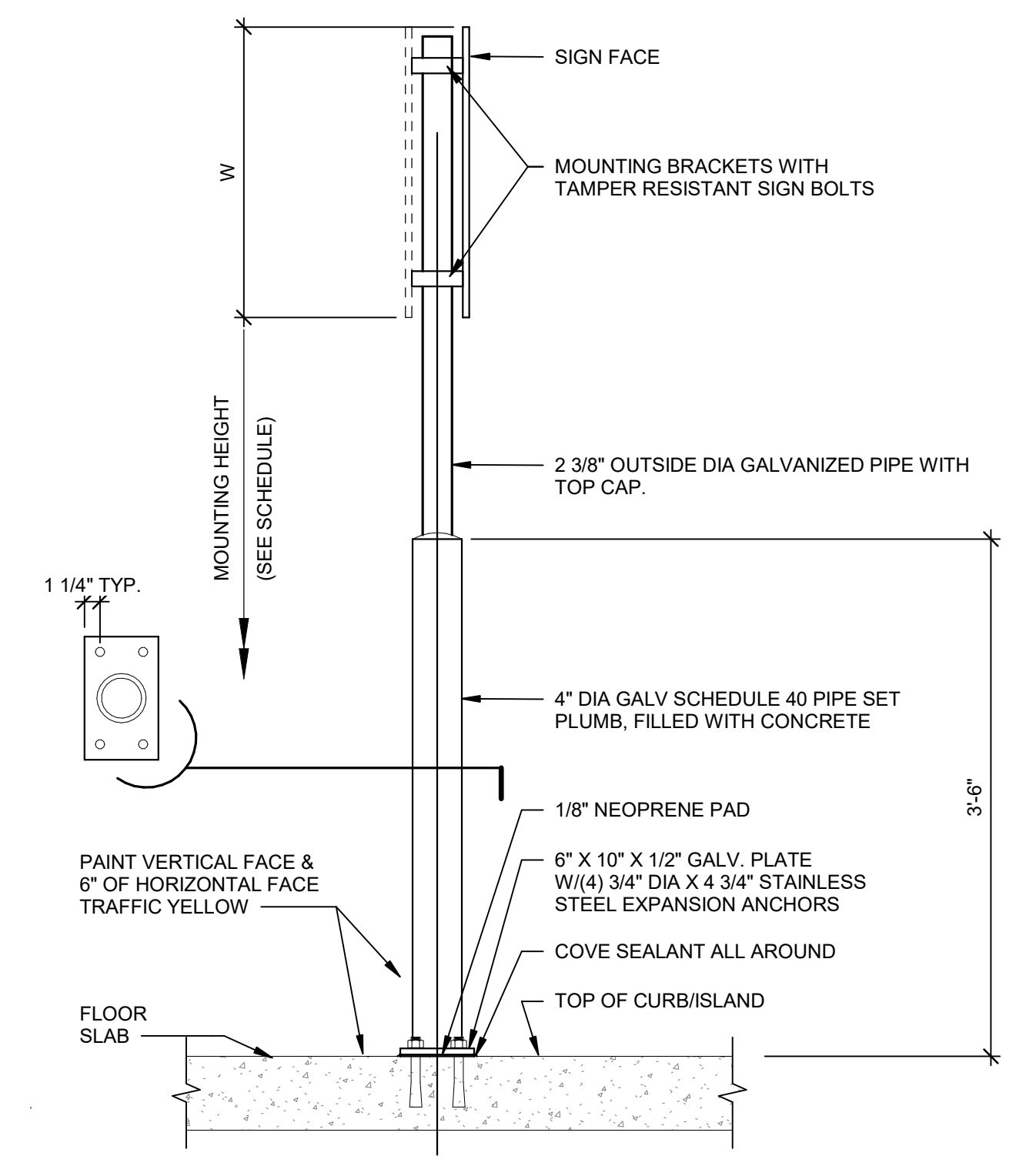
11.4

(INCIDENTAL TO W.I. SERIES 3.0)

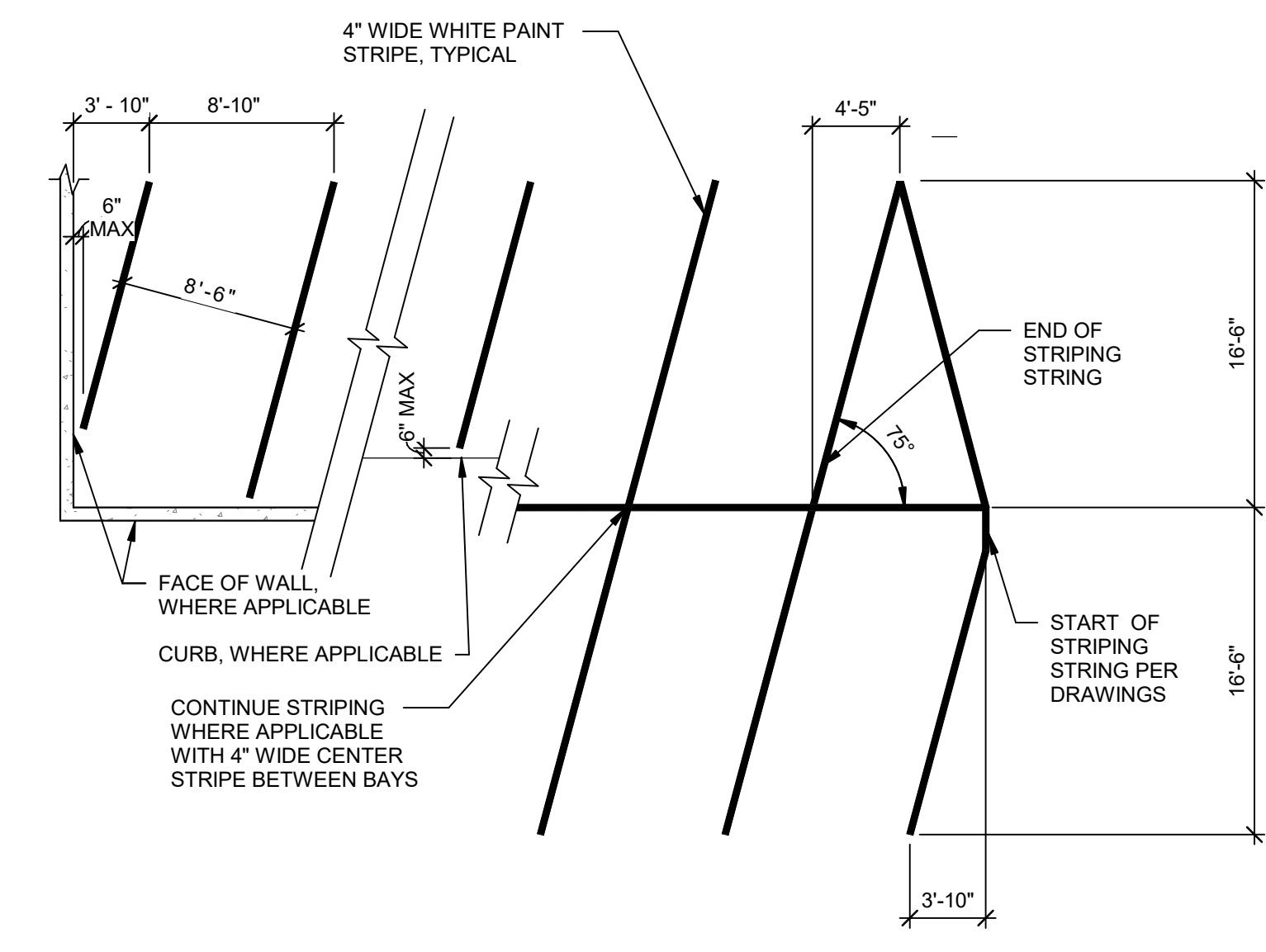
- NOTES:**
1. CONTRACTOR SHALL INSTALL TEMPORARY SHORING PRIOR TO DEMOLITION PER W.I. 1.3. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD.
 2. VERIFY ALL DIMENSIONS IN FIELD PRIOR TO INSTALLATION OF NEW METAL DECK AND CONCRETE FILL.
 3. FASTEN NEW METAL DECK TO EXISTING STRUCTURAL FRAMING PER MANUFACTURER'S REQUIREMENTS.

- NOTES:**
1. USE SERVICE WEIGHT CAST IRON PIPE. ALL MATERIALS SHALL BE IN ACCORDANCE WITH PREVAILING BUILDING ORDINANCES.
 2. INSTALL ADJUSTABLE HANGERS AS REQUIRED. VERIFY MINIMUM HEIGHT RESTRICTIONS OF PREVAILING BUILDING ORDINANCES PRIOR TO INSTALLATION.
 3. ALL DRAINS SHALL BE HOT DIP GALVANIZED.
 4. ORIGINAL GARAGE AREA 12\"/>

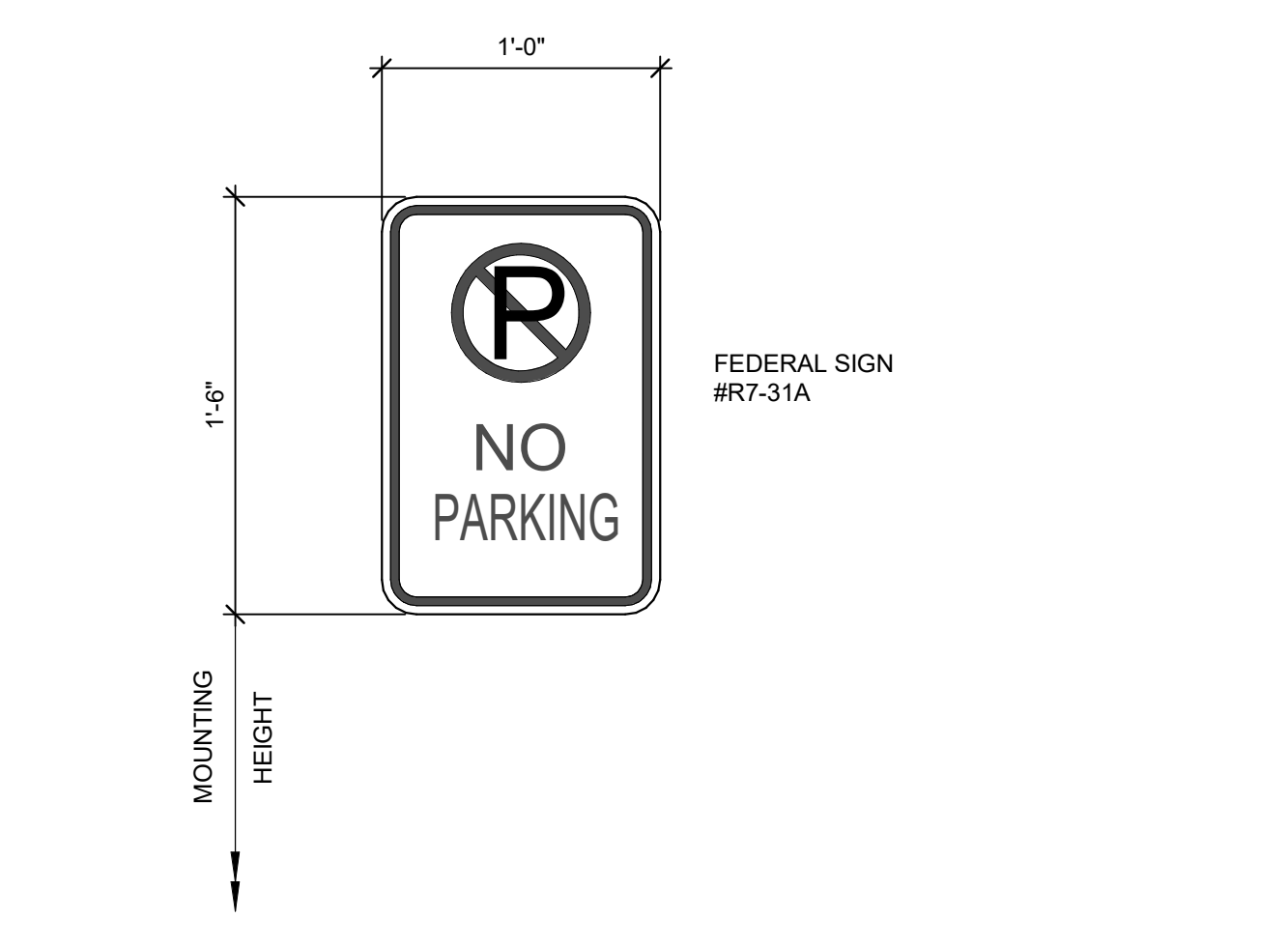
- NOTES:**
1. PLACE CONTROL JOINTS AS SHOWN ON PLANS.
 2. ADDITIONAL OVERLAY CONTROL JOINTS WILL BE REQUIRED OVER ALL ORIGINAL CONSTRUCTION JOINTS. CONTRACTOR SHALL LOCATE ALL SUCH JOINTS PRIOR TO OVERLAY INSTALLATION.
 3. CONTROL JOINT (C.J.) MUST BE TOoled IN PLASTIC CONCRETE AND DIMENSIONS AS DETAILED WILL BE MAINTAINED IN SET CONCRETE. SAWCUTTING OF CONTROL JOINT IN HARDENED CONCRETE WILL NOT BE PERMITTED.



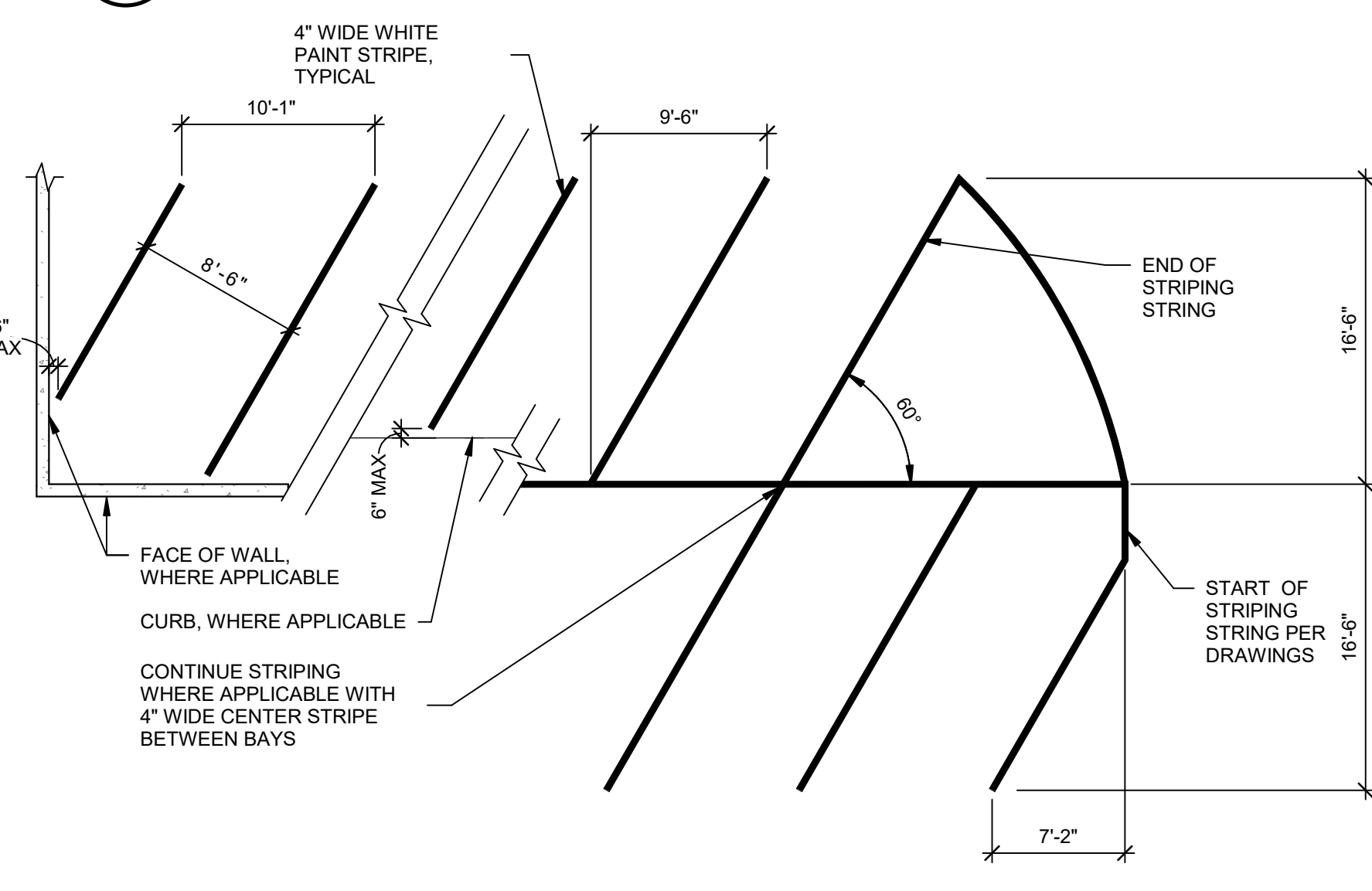
45.4 SIGN DETAIL MOUNT - PIPE



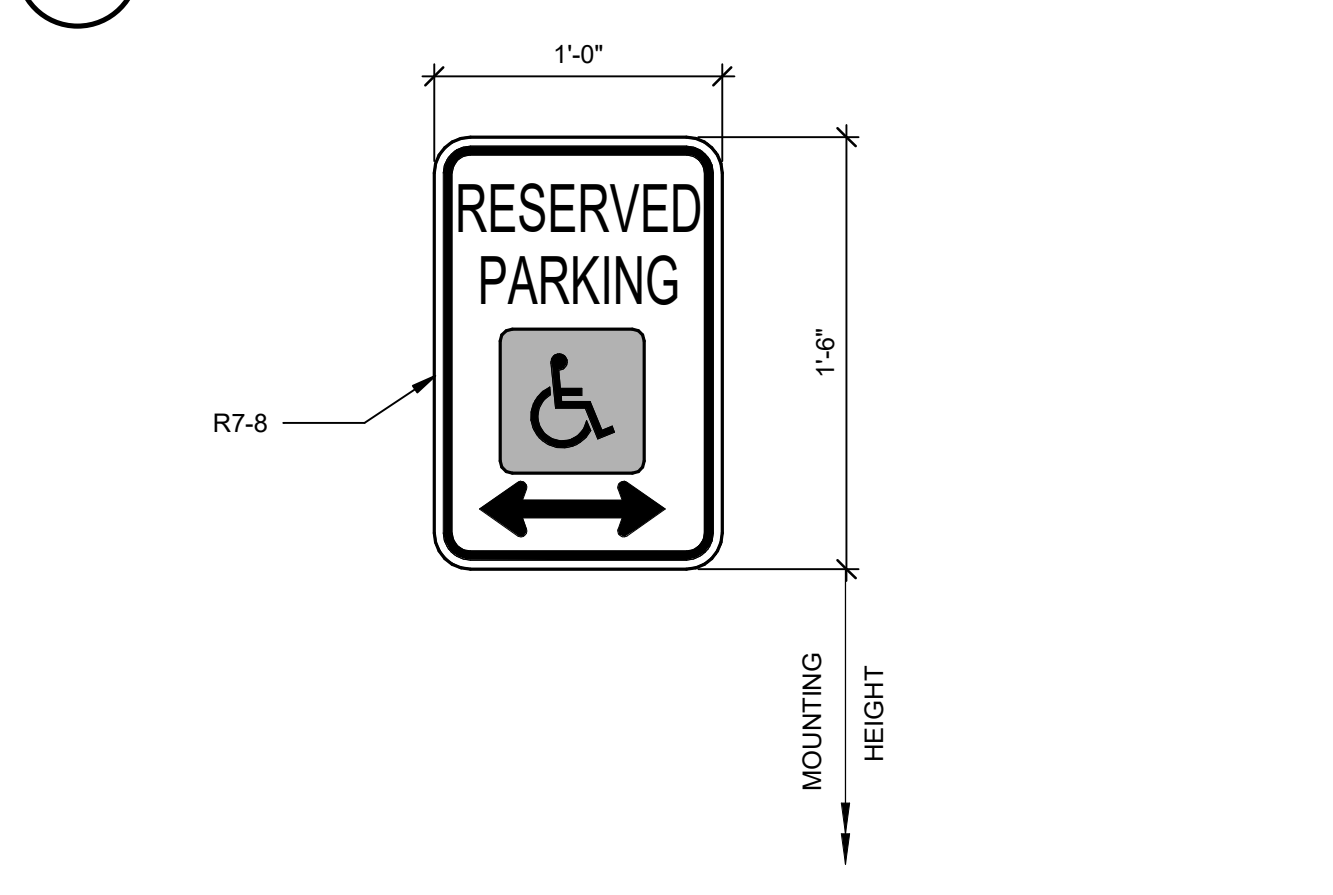
45.1.3 75° STRIPING DETAIL



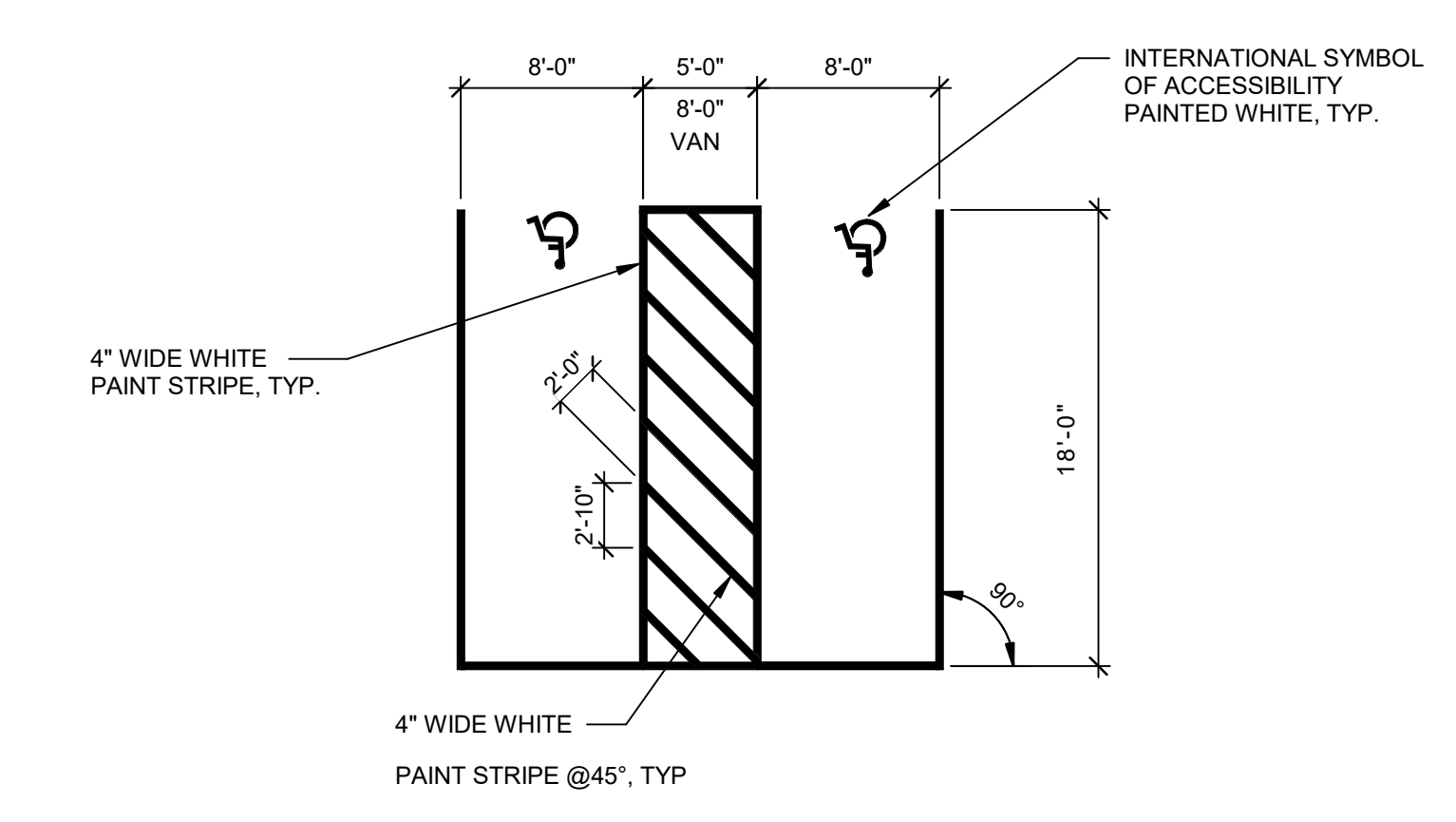
43.3.1 NO PARKING (FEDERAL)



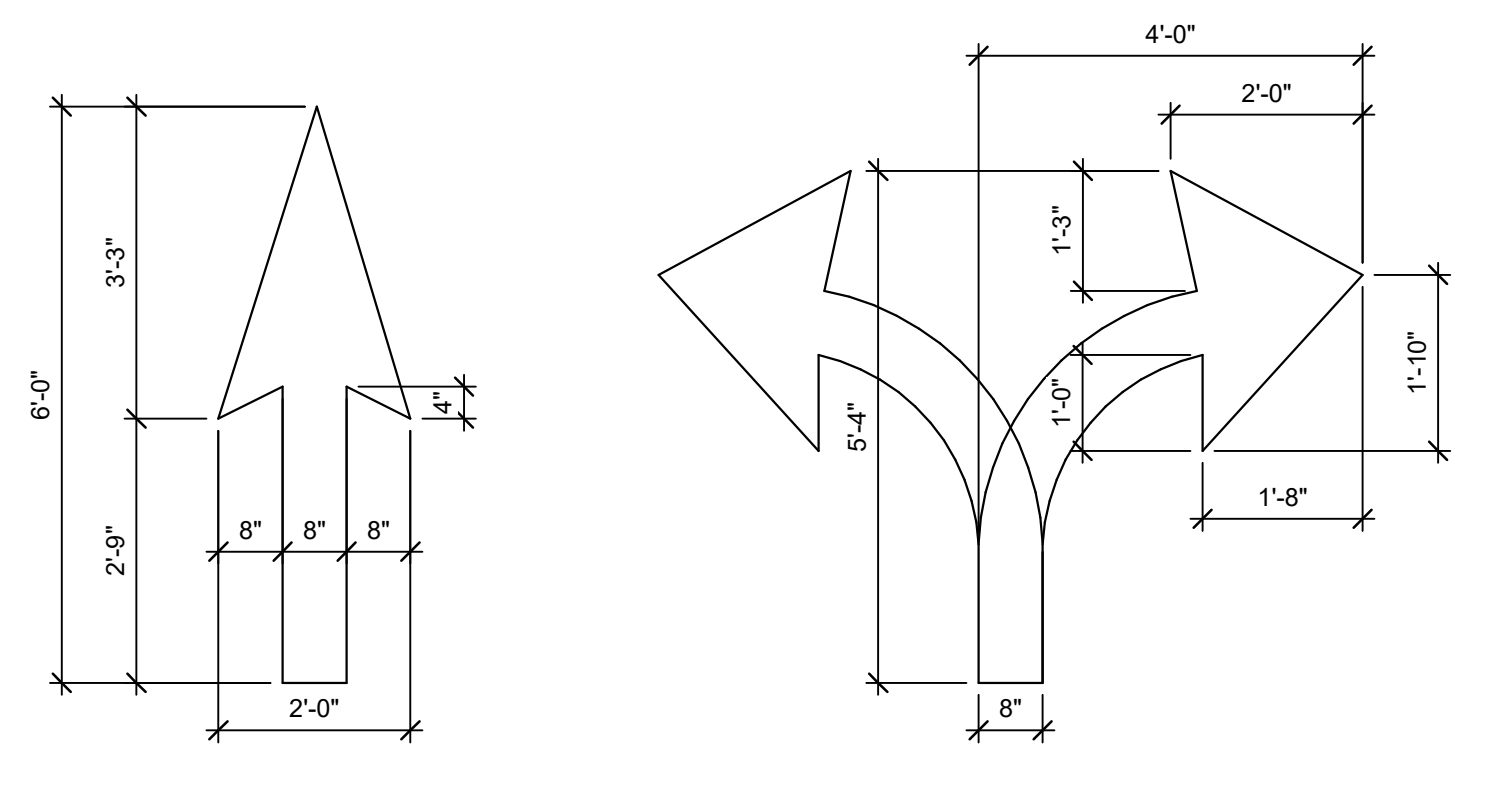
45.1.2 60° STRIPING DETAIL



45.3 RESERVED PARKING (FEDERAL)

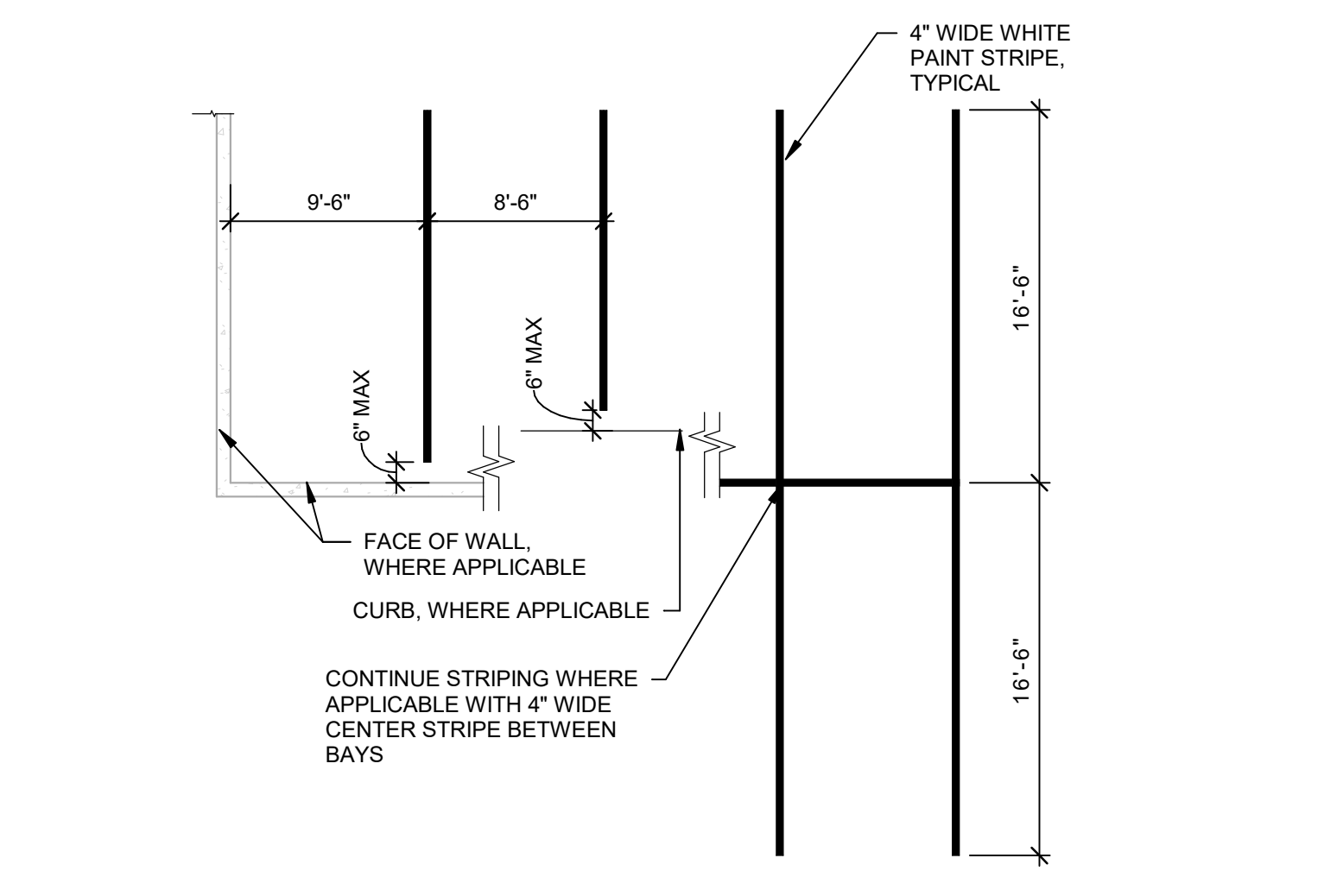


45.1.1 ACCESSIBLE STRIPING DETAIL



NOTE: PAINT ARROWS SOLID YELLOW, UNLESS NOTED OTHERWISE.

45.2 TYPICAL ARROW DETAIL



45.1 PAINT TRAFFIC MARKINGS



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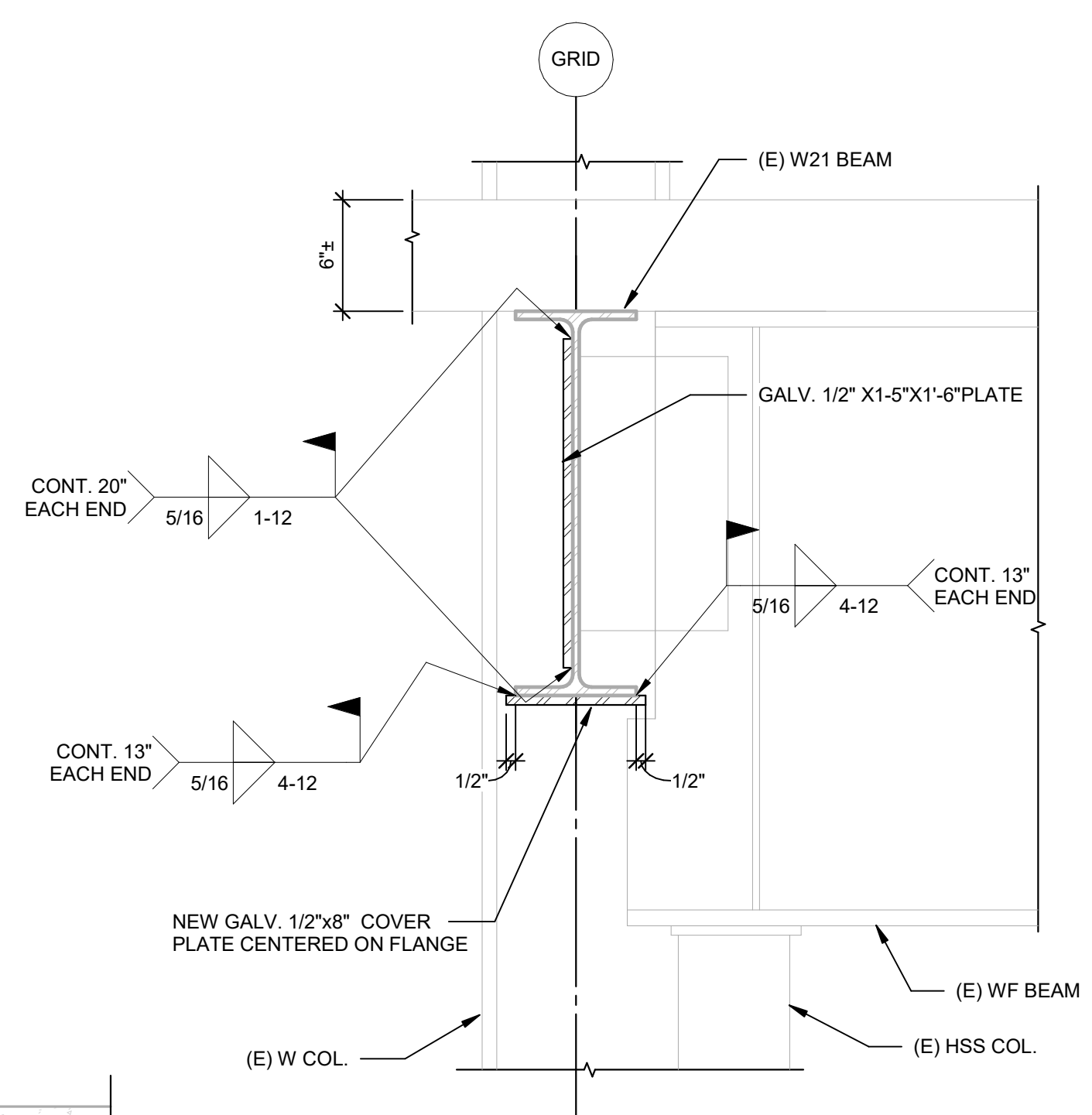
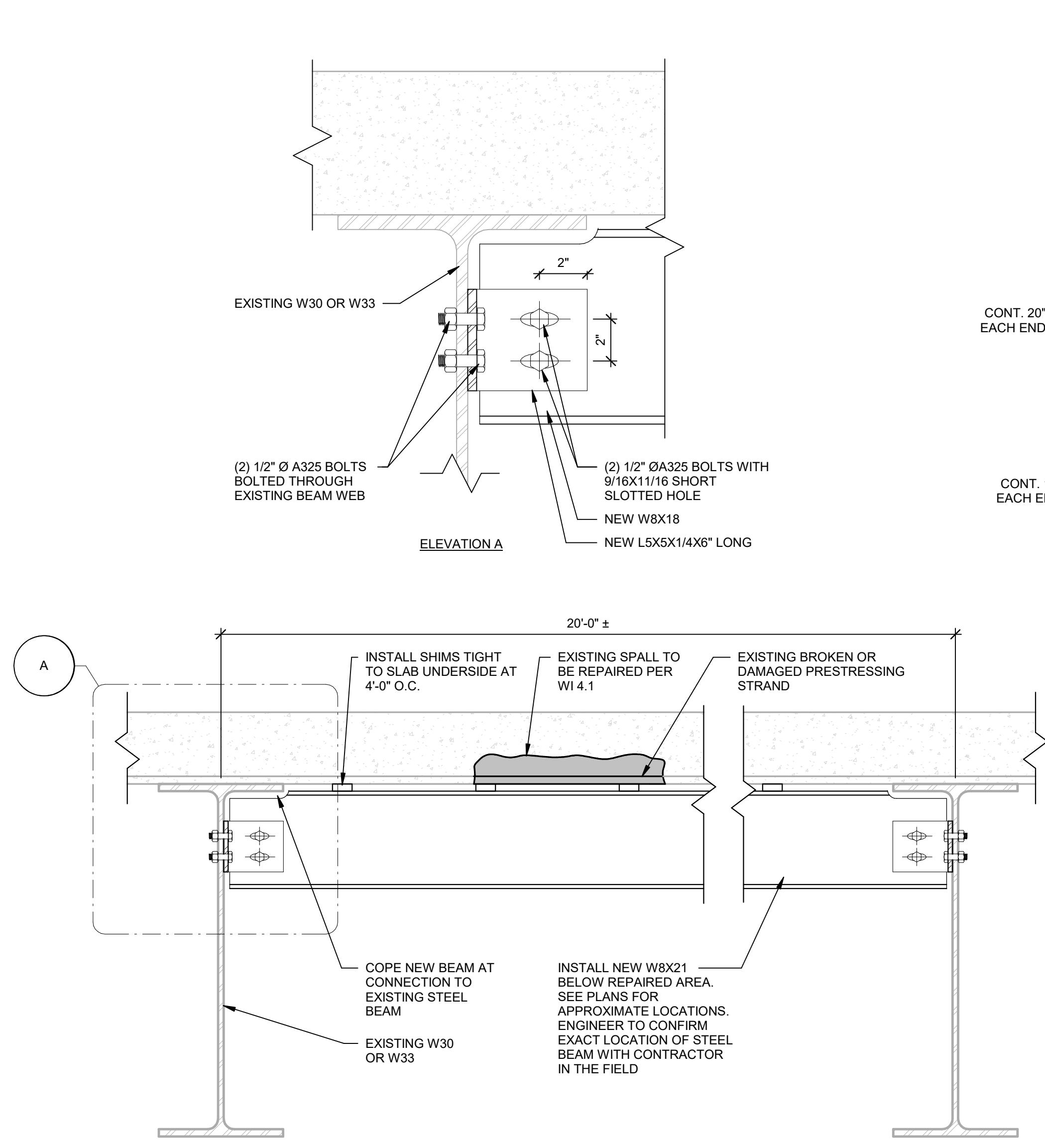
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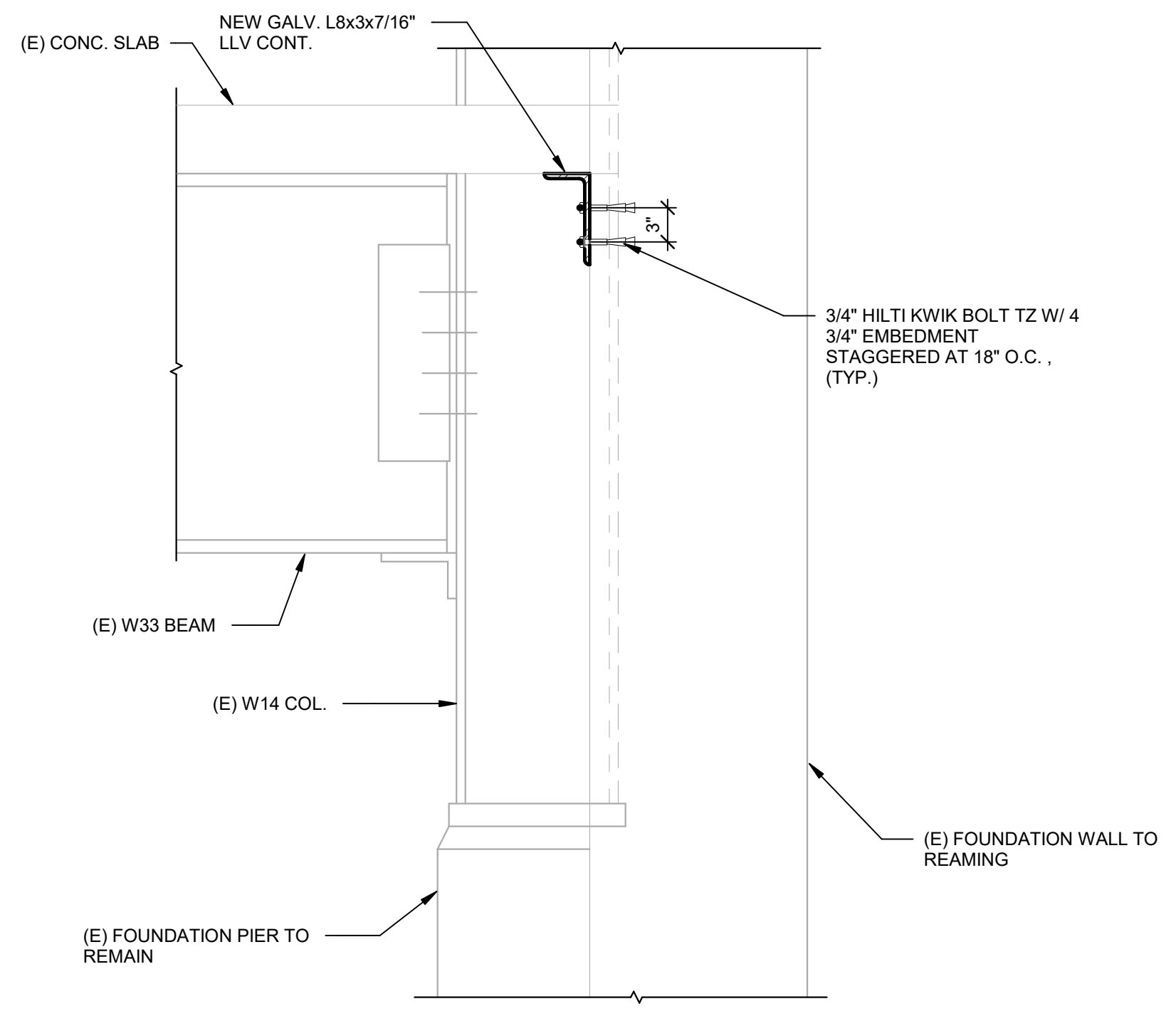
REPAIR DETAILS



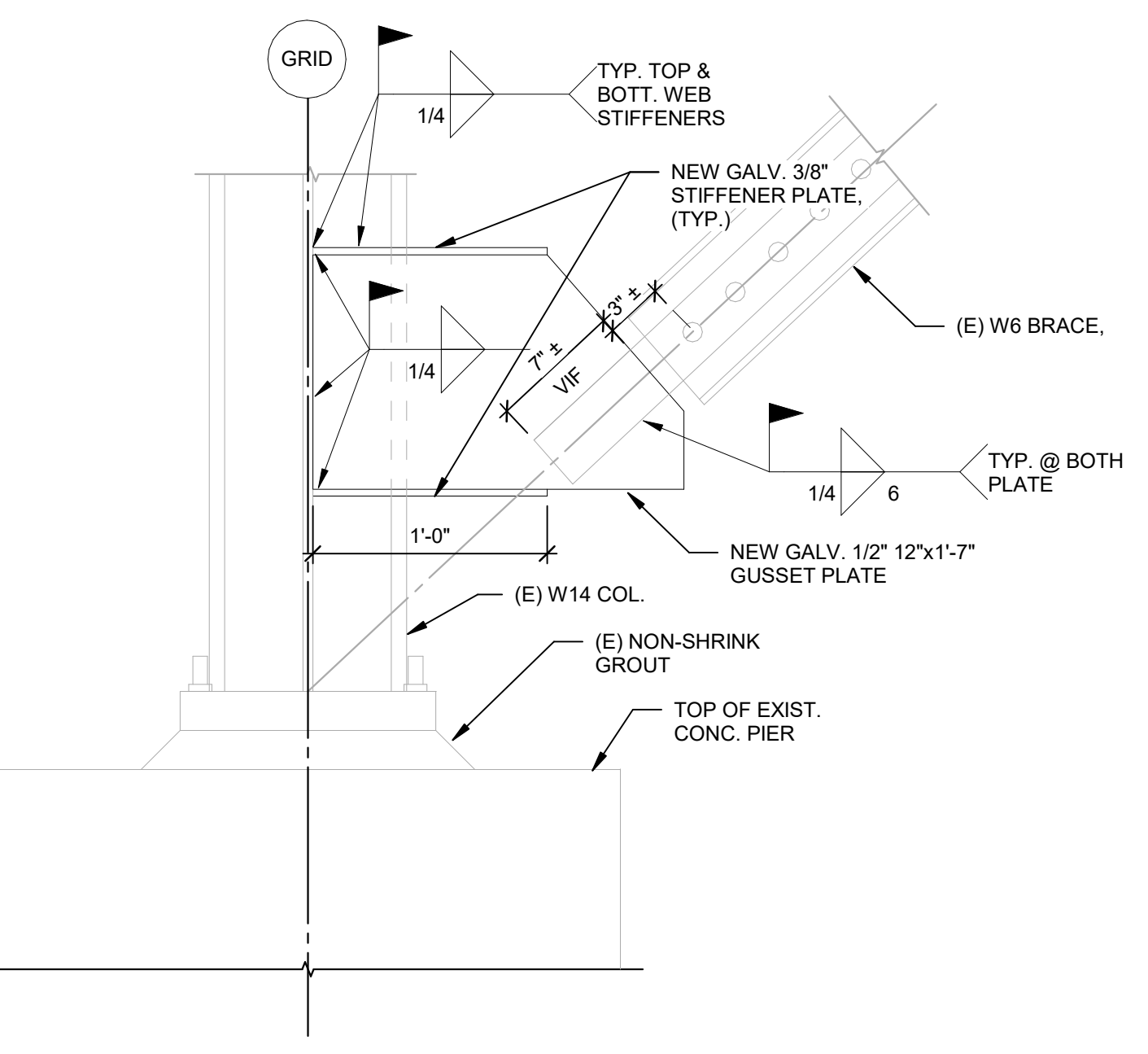
- NOTES:
- GC SHALL SHORE ALL STEEL MEMBERS PRIOR TO BEGINNING WORK. SHORING DESIGN IS THE SOLE RESPONSIBILITY OF THE GC. SHORING SHALL BE INSTALLED PRIOR TO BEGINNING OF ANY WORK.
 - CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO THE START OF WORK.
 - PREPARE STEEL SURFACE WITH POWERS TOOLS TO SP3. REMOVE ALL LOOSE RUST, LOOSE MILL SCALE, AND LOOSE PAINT.
 - HOLD POINT - GC SHALL NOTIFY ENGINEER WHEN STEEL SURFACE IS PREPARED TO SP3. ENGINEER TO OBSERVE FIELD CONDITIONS AND IDENTIFY IF EXISTING STEEL MEMBERS REQUIRES SUPPLEMENTAL REINFORCING.
 - INSTALL SUPPLEMENTAL REINFORCING PER DIRECTION OF ENGINEER.
 - GC MAY REMOVE SHORING AFTER INSTALLATION OF SUPPLEMENTAL REINFORCING.
 - PAINT STEEL PER PAINT PACKAGE.
 - REFER TO SPECIFICATION SECTION 004310 FOR ADDITIONAL INFORMATION.
 - DETAILS THIS SHEET ARE CONCEPTUAL ONLY AND ARE TO BE USED FOR BIDDING PURPOSES ONLY. EXISTING CONDITIONS WILL BE FIELD VERIFIED BY ENGINEER AFTER STEEL IS CLEANED FREE OF RUST. FINAL REPAIR DETAILS WILL BE DETERMINED AT THAT TIME.



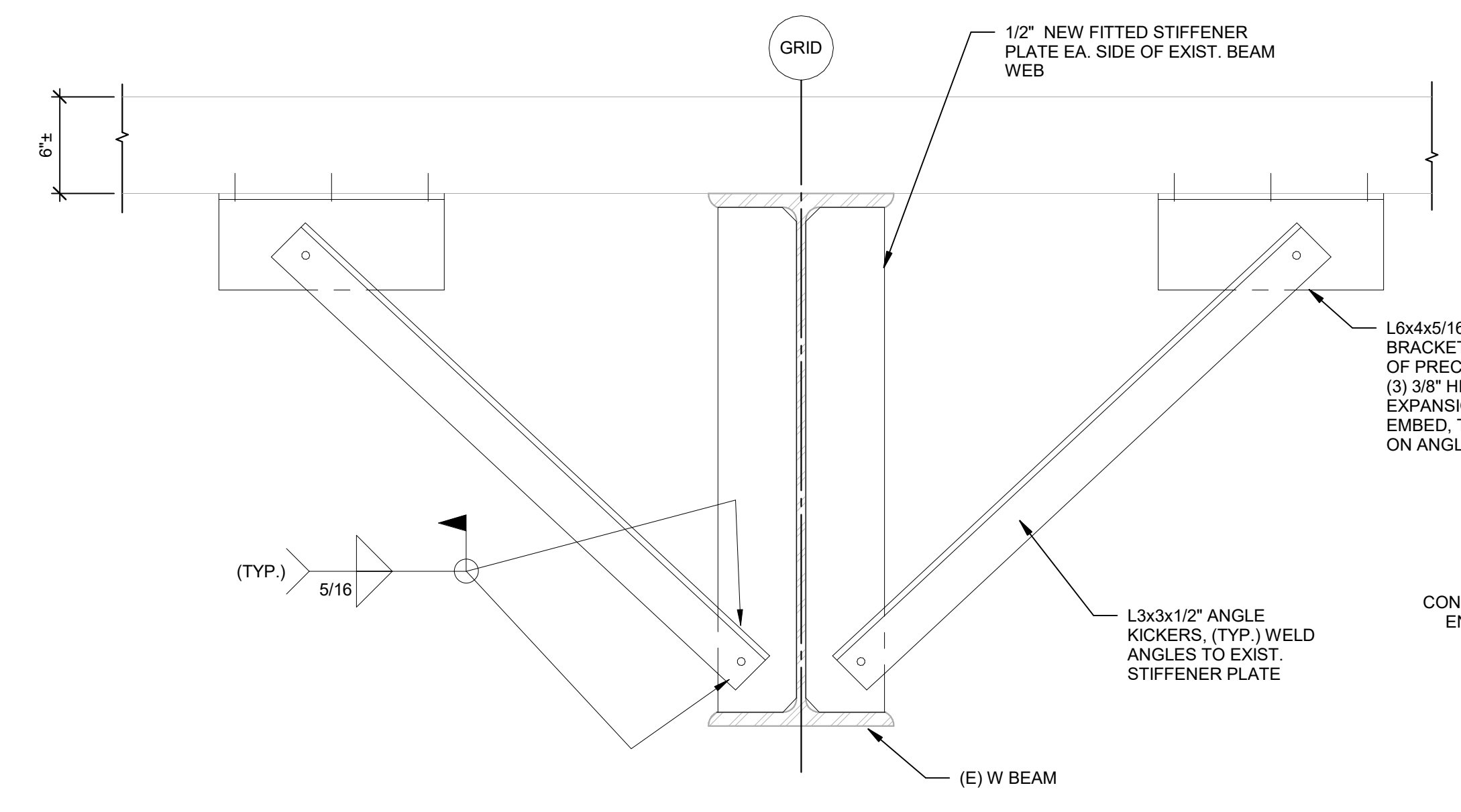
46.1F BEAM REPAIR - STIFFENER / BRACING



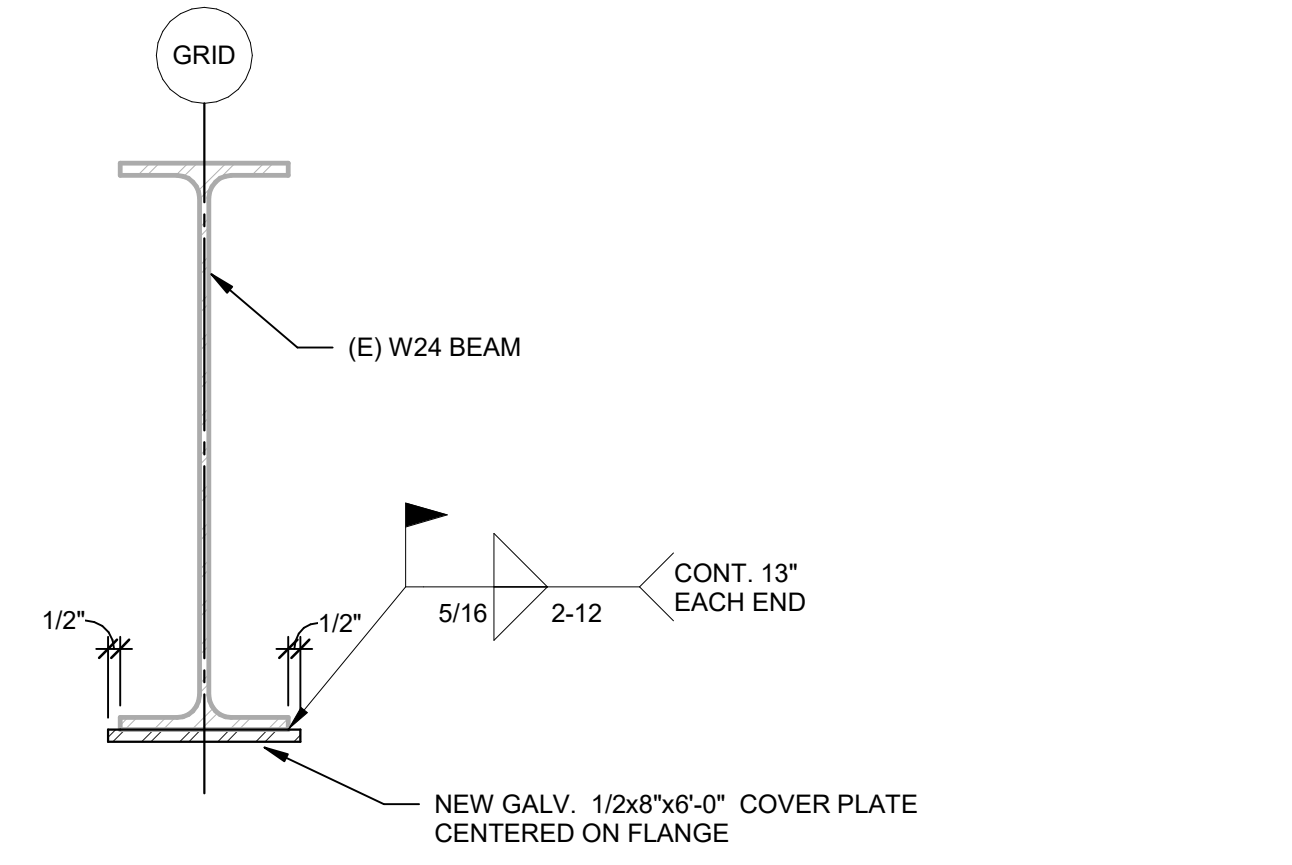
46.1E ANGLE SUPPORT



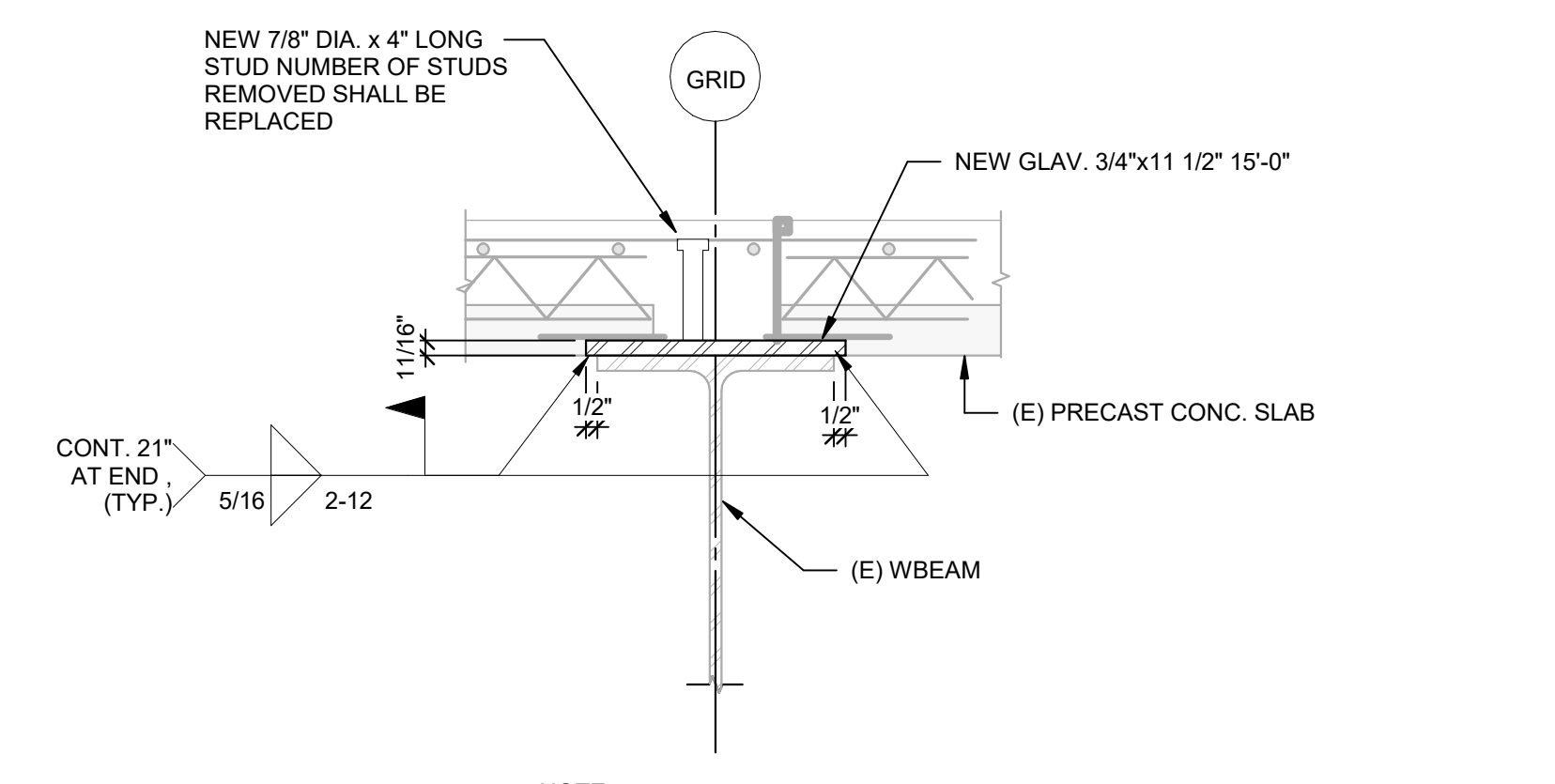
46.1D STIFFENER AND GUSSET PLATE REPLACEMENT



46.1C BEAM REPAIR



46.1B BOTTOM FLANGE REPAIR



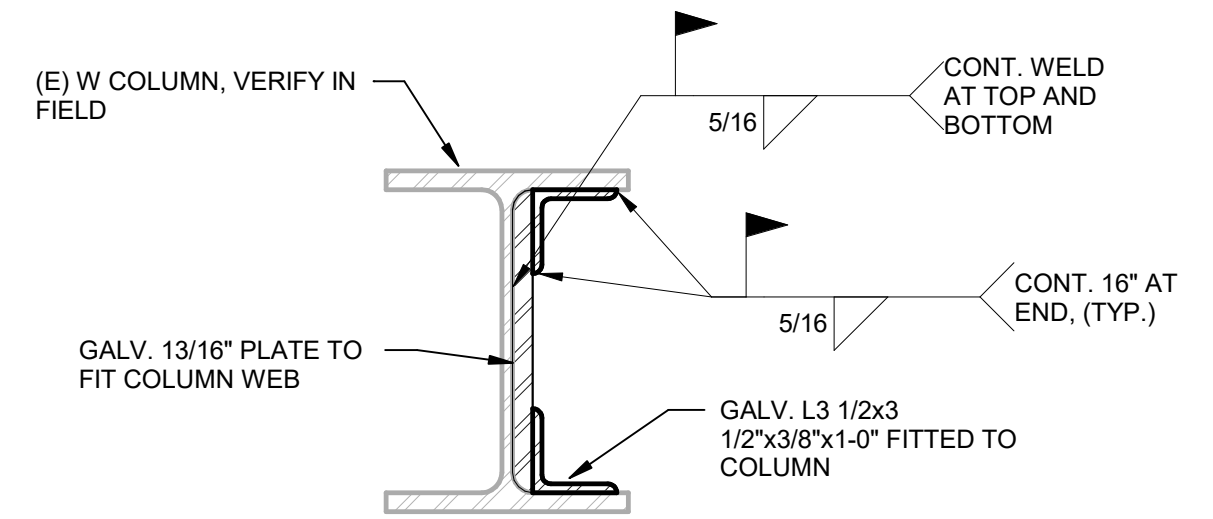
46.1A FLANGE REPAIR

NOTE:

- CONTRACTOR SHALL INSTALL TEMPORARY SHORING PRIOR TO DEMOLITION. SHORING OF THE STRUCTURE IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL SUBMIT STAMPED AND SEALED SHORING DRAWINGS AND CALCULATIONS BY REGISTERED ENGINEER IN THE STATE OF NEW HAMPSHIRE TO E.O.R. FOR RECORD PRIOR TO START OF WORK.

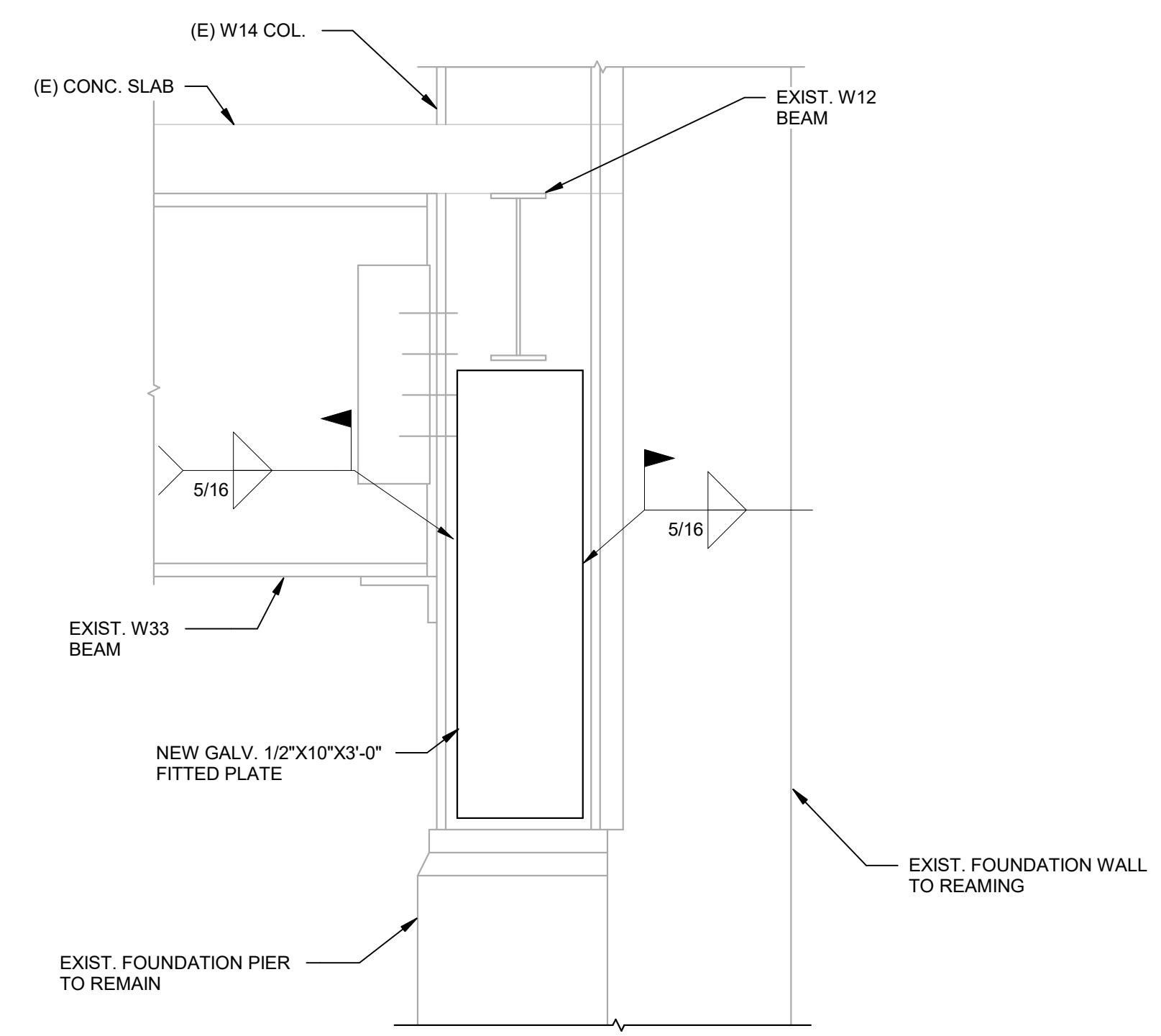
SUPPLEMENTAL STEEL BEAM INSTALLATION AT DAMAGED PRECAST PLANK

46.2

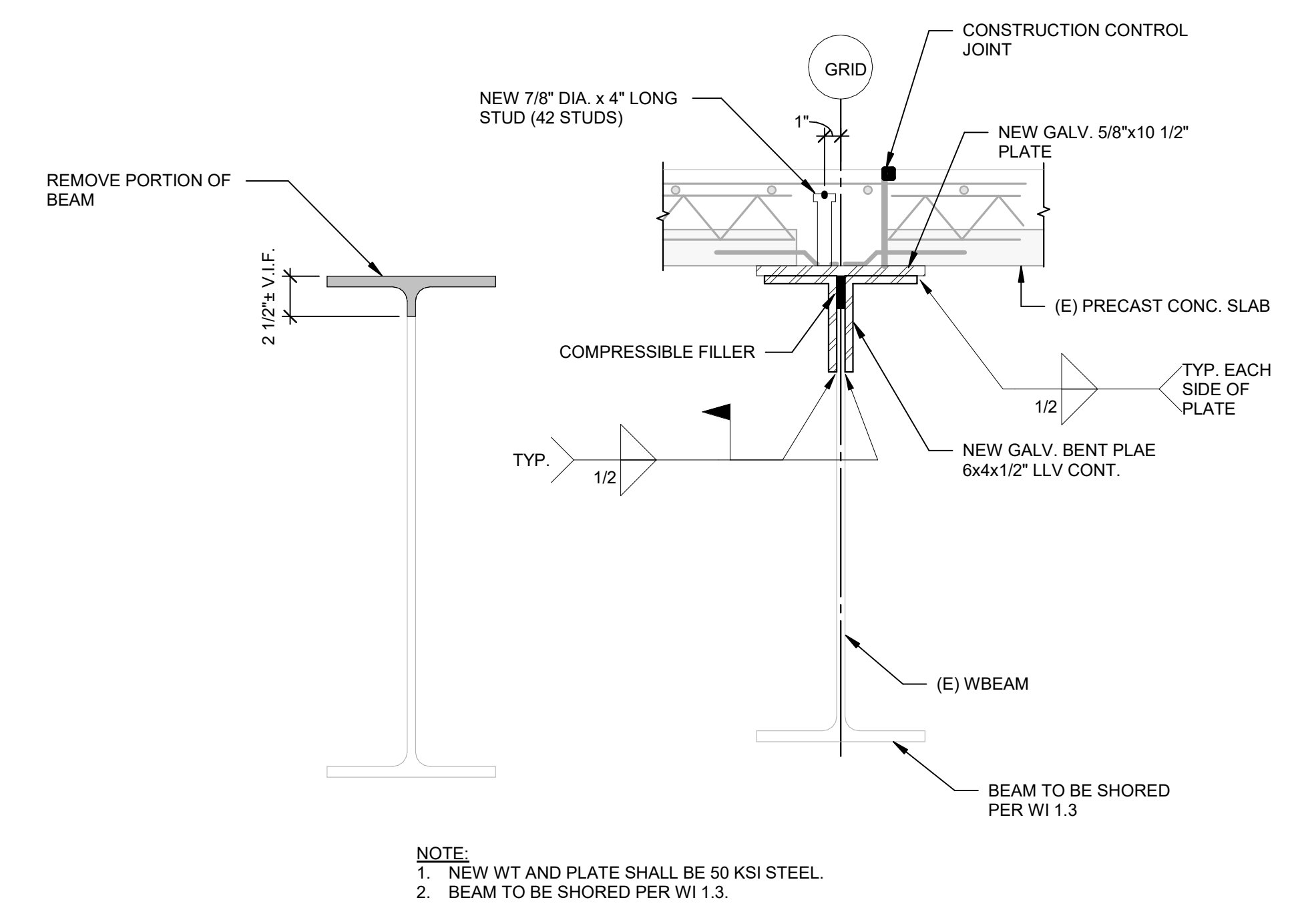


46.1H COLUMN FLANGE REPAIR

46.1H



46.1G COLUMN REPAIR



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SHEET TITLE: **REPAIR DETAILS**



Arthur G. Stading

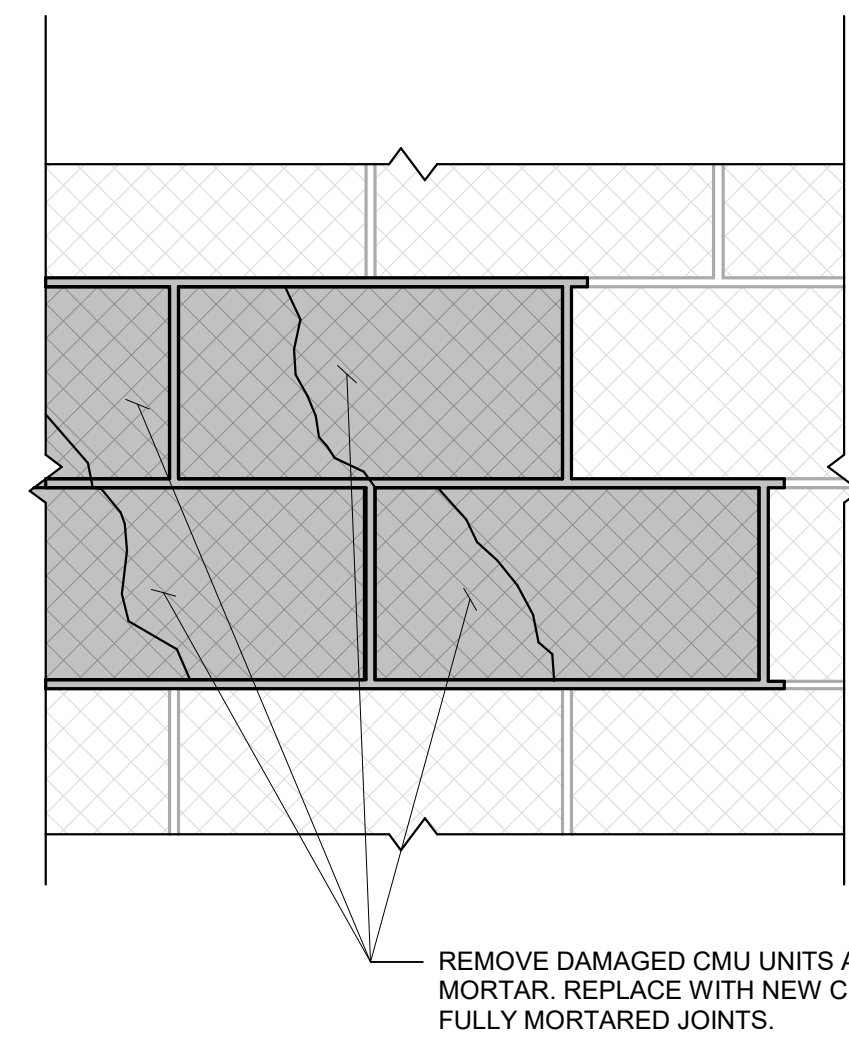
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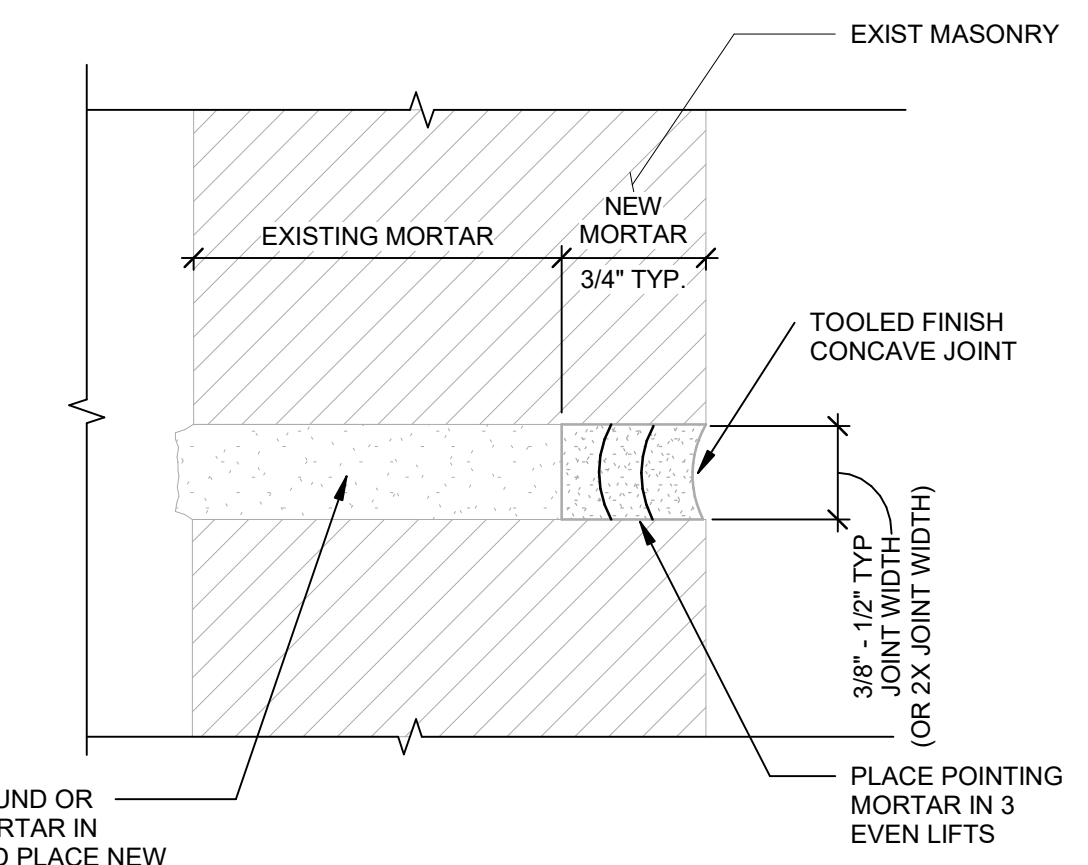
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SHEET TITLE:
REPAIR DETAILS



- NOTES:**
- CONTRACTOR SHALL SUPPORT REMAINING AREAS OF CMU DURING REPAIRS.
 - PAY UNIT INCLUDES PROVIDING CORNER UNITS, BOND BEAM UNITS, ETC. AS REQUIRED TO REPAIR ALL DAMAGED CMU.
 - INCIDENTAL TO THIS WORK IS PROVIDING NEW TRUSS OR LADDER TYPE REINFORCEMENT TO MATCH EXISTING. LAP MINIMUM ONE BLOCK LENGTH.

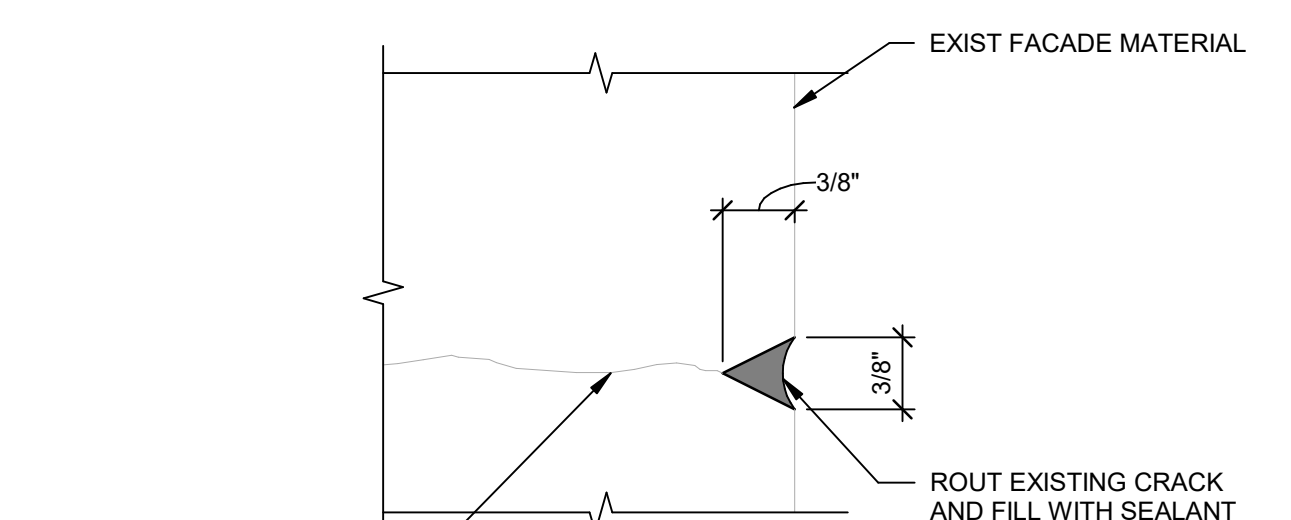
80.3 REMOVE AND REPLACE CONCRETE MASONRY UNIT



- REMOVE ALL UNSOUND OR DETERIORATED MORTAR IN HATCHED AREA AND PLACE NEW MORTAR, AS REQ'D. (INCIDENTAL TO EXIST.)**

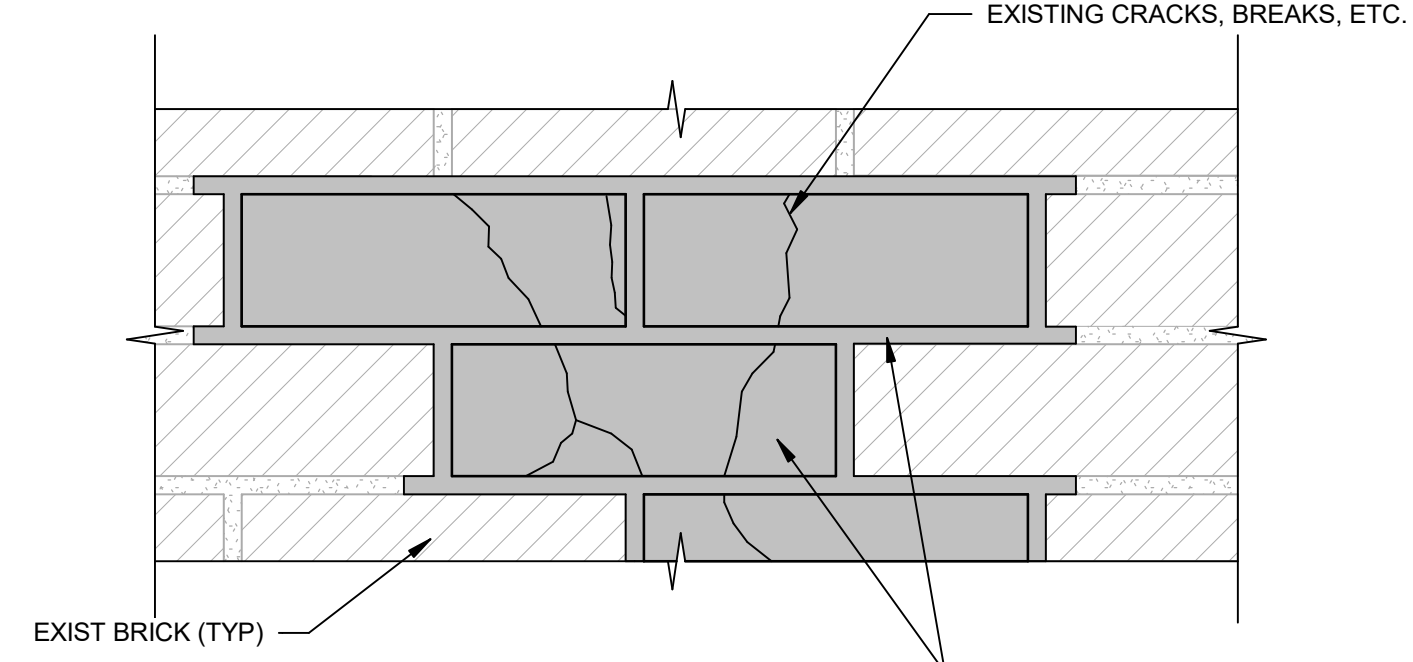
- NOTES:**
- REMOVE LOOSE MORTAR MATERIAL FROM JOINT.
 - FILL AND VOIDS IN JOINT BEYOND POINTING WORK DEPTH.
 - GRIND BRICK SURFACES CLEAN OF EXISTING MORTAR. DO NOT DAMAGE ADJACENT MASONRY.
 - INSTALL MORTAR IN THREE LIFTS, COMPACTING EACH LIFT.
 - TOOL MORTAR JOINT CONCAVE.

76.3 TUCKPOINTING



- NOTES:**
- PRIME JOINT AS REQUIRED BY SEALANT MANUFACTURER.
 - INSTALL SEALANT EVENLY AND TOOL JOINT CONCAVE. **WET TOOLING WILL NOT BE ALLOWED.**
 - SUBMIT COLOR SAMPLES TO OWNER FOR APPROVAL PRIOR TO START OF WORK.

76.1 ROUT AND SEAL FAÇADE CRACK

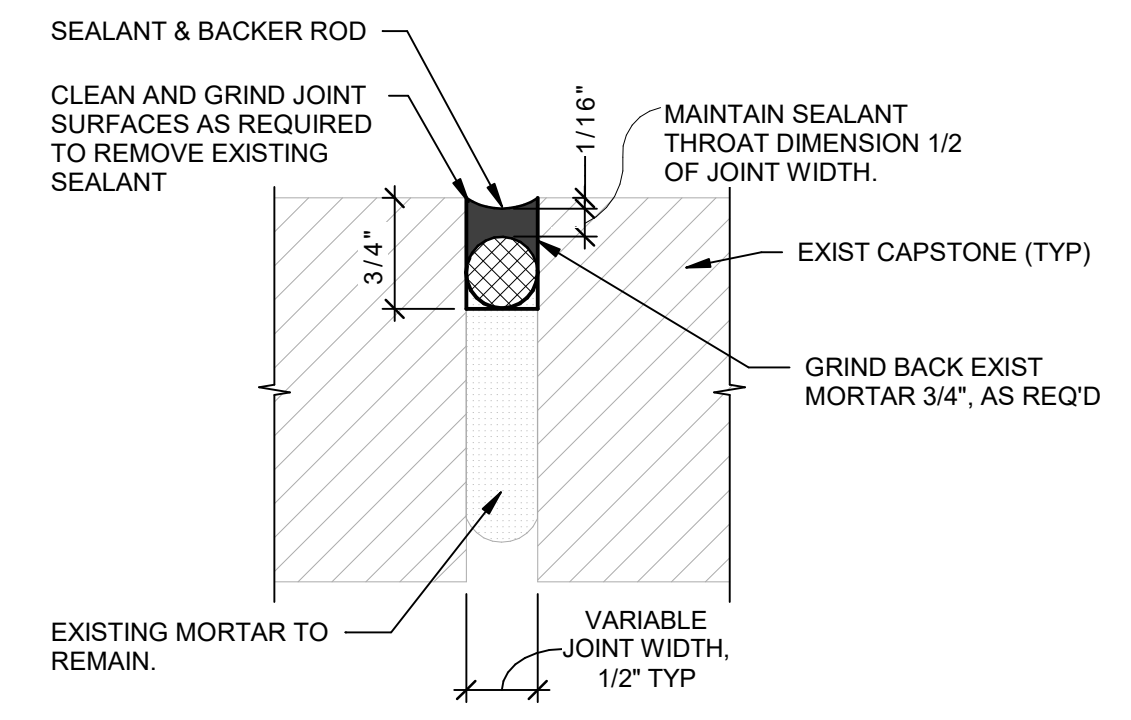


- REMOVE DAMAGED BRICK UNITS AND MORTAR. REPLACE WITH NEW BRICK. PLACE MORTAR FULL DEPTH OF UNIT.**

ELEVATION

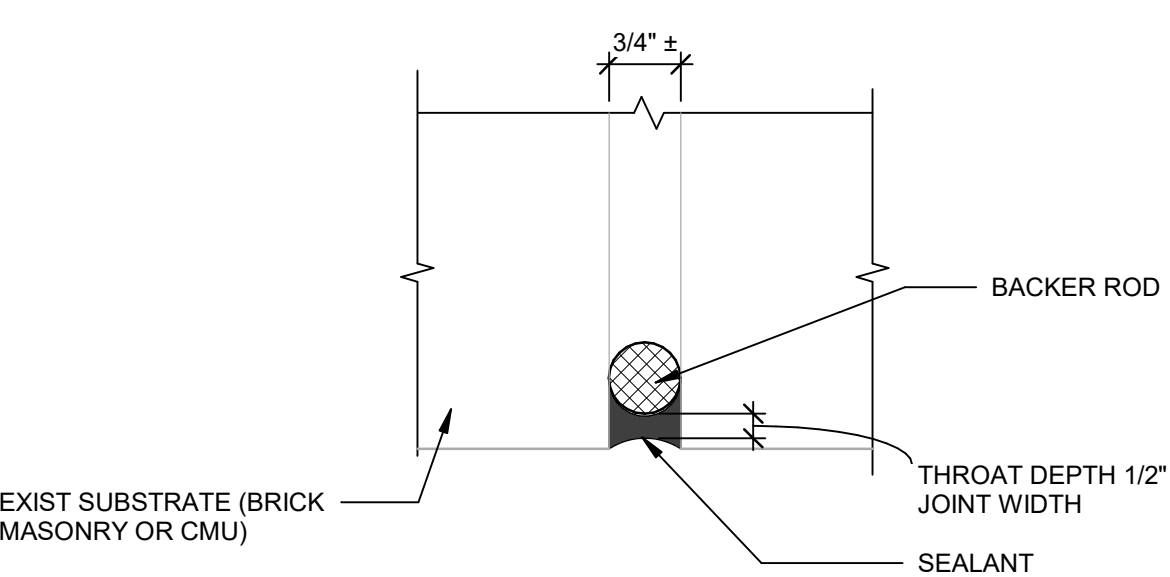
- NOTES:**
- MORTAR JOINT REPLACEMENT AT BRICK MASONRY UNIT REPLACEMENT LOCATIONS IS INCIDENTAL. NO EXTRA PAYMENT UNDER TUCKPOINTING WORK ITEM.
 - DO NOT DAMAGE ADJACENT BRICK BY OVERCUTTING.
 - EXISTING BRICK TO MATCH EXISTING BRICK AND SAMPLE TO BE SUBMITTED AND APPROVED BY ENGINEER AND/OR OWNER PRIOR TO START OF WORK.

80.1 REMOVE AND REPLACE FACE BRICK



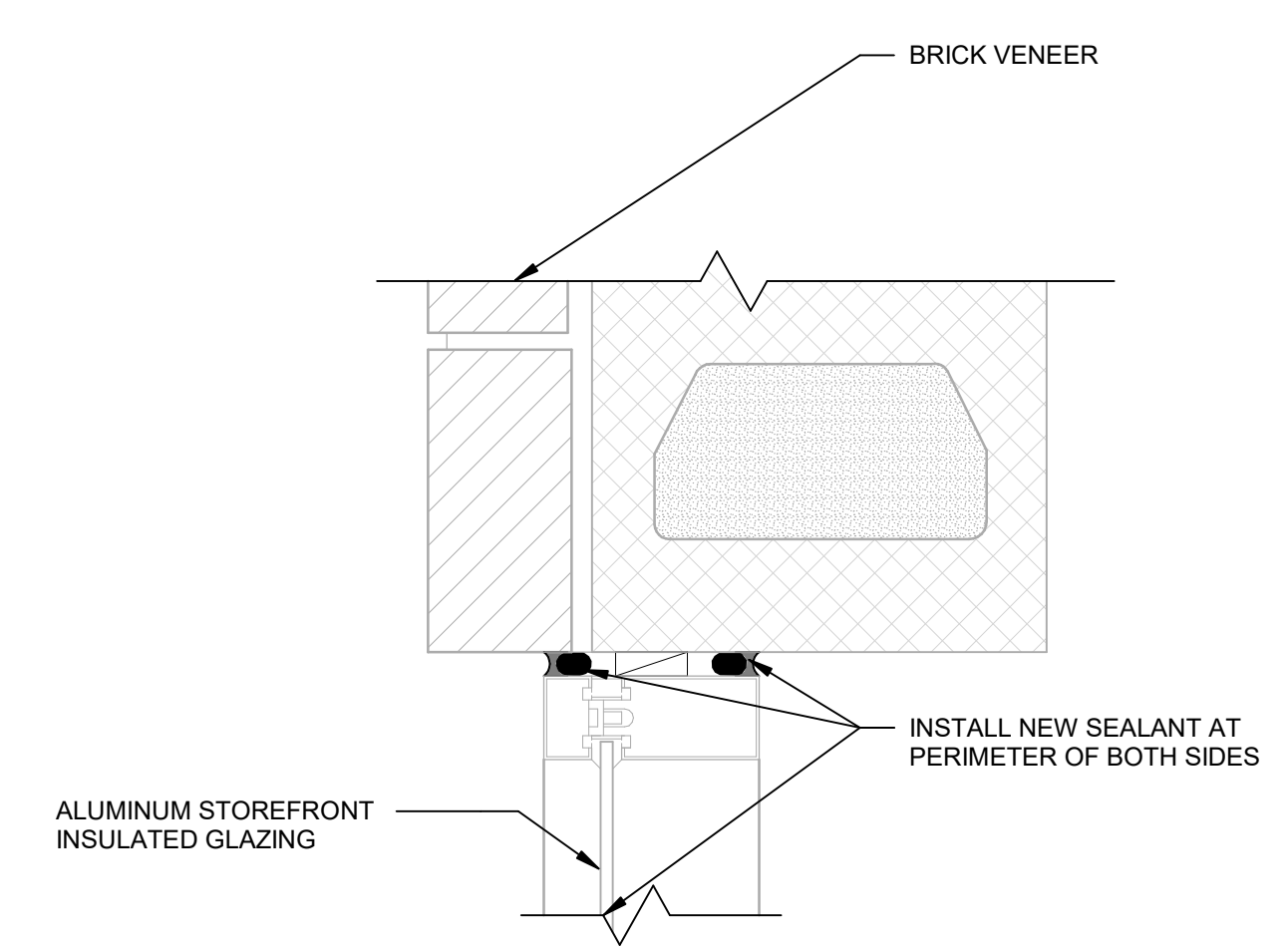
- NOTES:**
- REMOVE ALL EXISTING SEALANT AND BACKER MATERIAL (IF PRESENT) AT EACH CAPSTONE JOINT.
 - PRIME JOINT AS REQUIRED BY SEALANT MANUFACTURER.
 - INSTALL SEALANT AND TOOL JOINT CONCAVE 1/16" BELOW EXISTING SURFACE.
 - DO NOT SEAL CAPSTONE BED JOINT. **WET TOOLING WILL NOT BE ALLOWED.**
 - SUBMIT COLOR SAMPLES FOR APPROVAL PRIOR TO START OF WORK.

74.7 CAPSTONE JOINT REPAIR

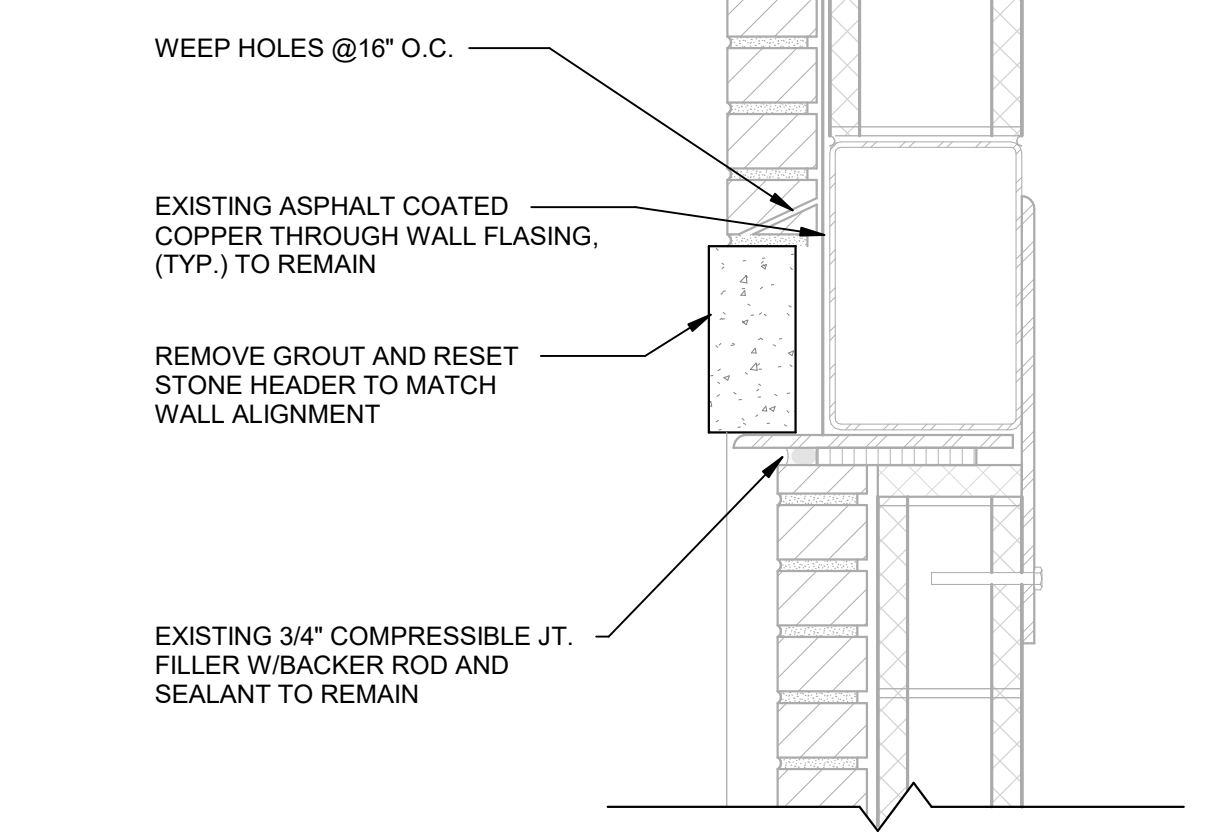


- NOTES:**
- REMOVE ALL EXISTING SEALANT AND BACKER ROD AS REQUIRED TO EXPOSE BASE FAÇADE. WIDEN JOINT AS REQUIRED TO MAINTAIN SEALANT WIDTH.
 - CLEAN ALL DUST AND DEBRIS FROM JOINT. MAKE SURE JOINT IS CLEAN AND DRY.
 - PREPARE AND PRIME SEALANT CAVITY ACCORDING TO SEALANT MANUFACTURER'S RECOMMENDATIONS.

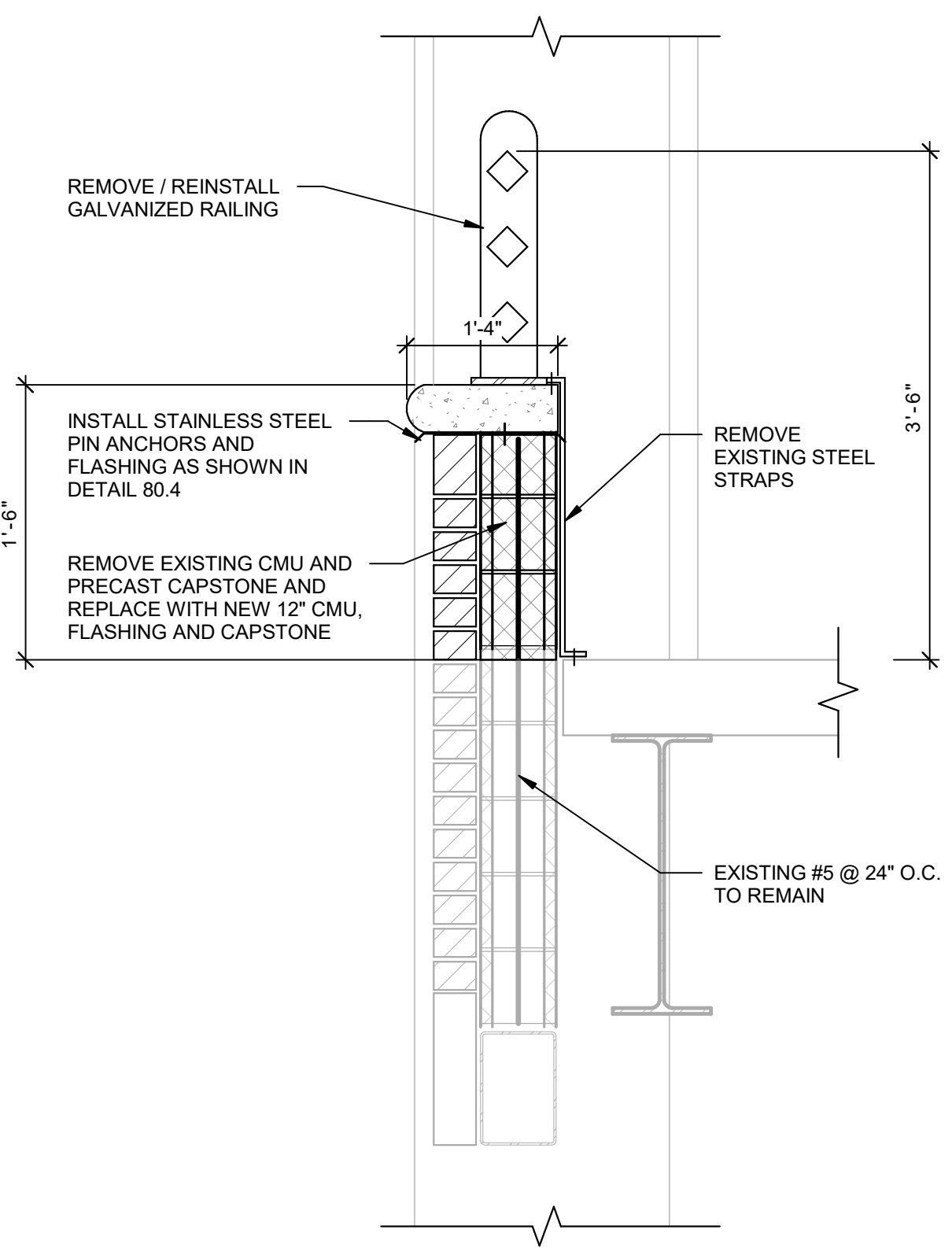
74.1 ISOLATION JOINT SEALANT REPAIR



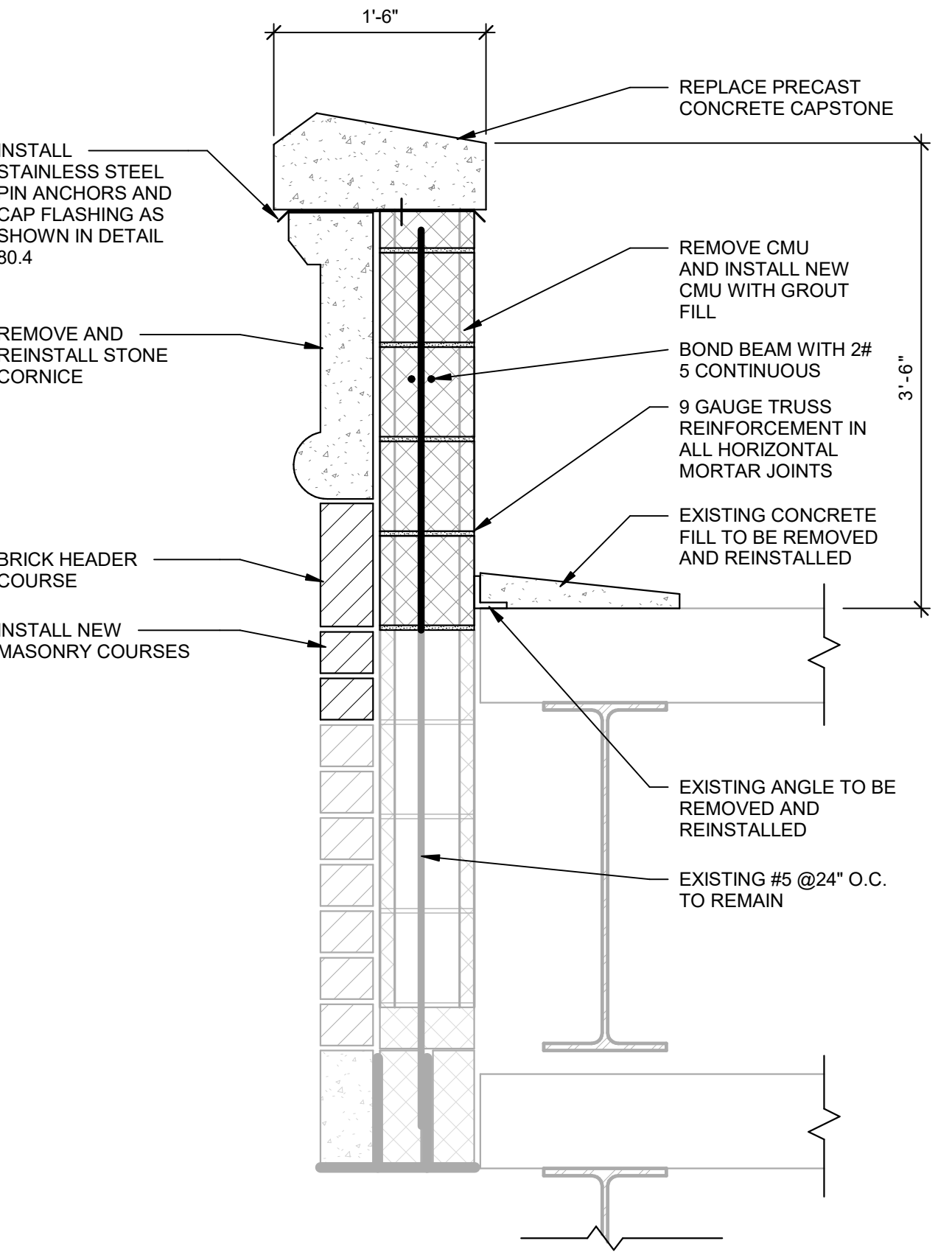
95.1 REPLACE WINDOW FRAME/FACADE PERIMETER JOINT



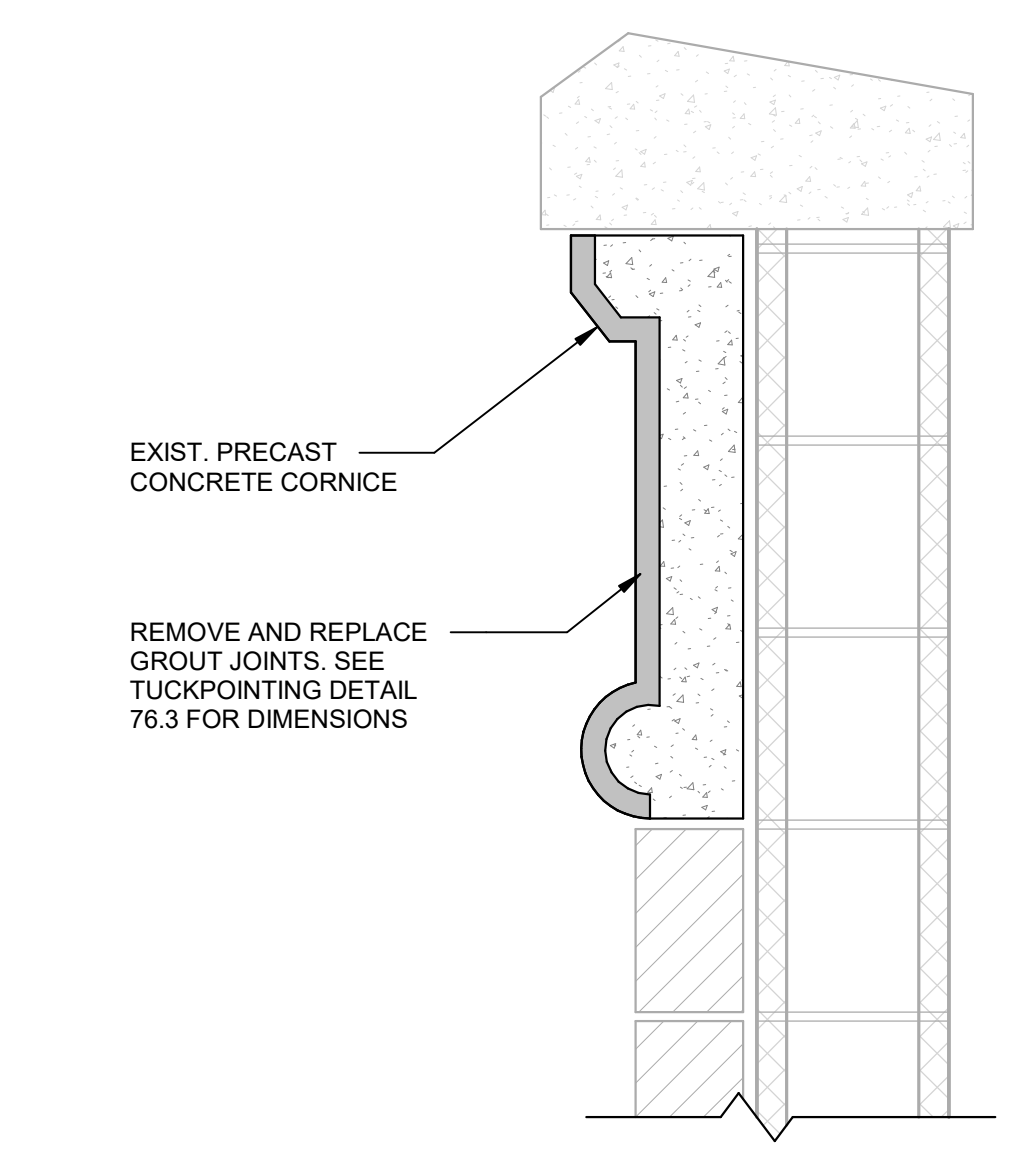
80.7 RESET STONE UNIT



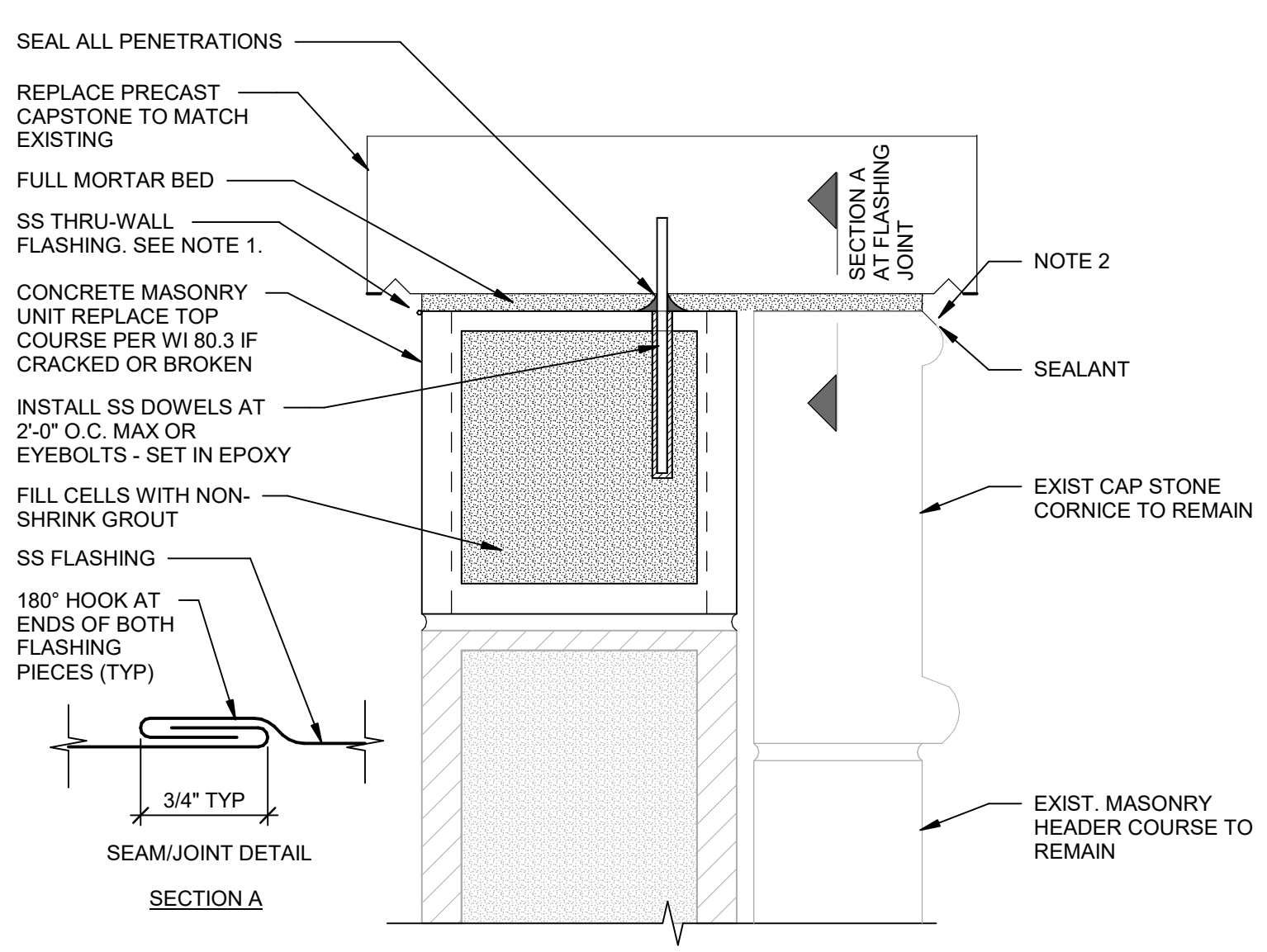
80.6 REMOVE/REPLACE PARAPET WALL W/TOP RAIL



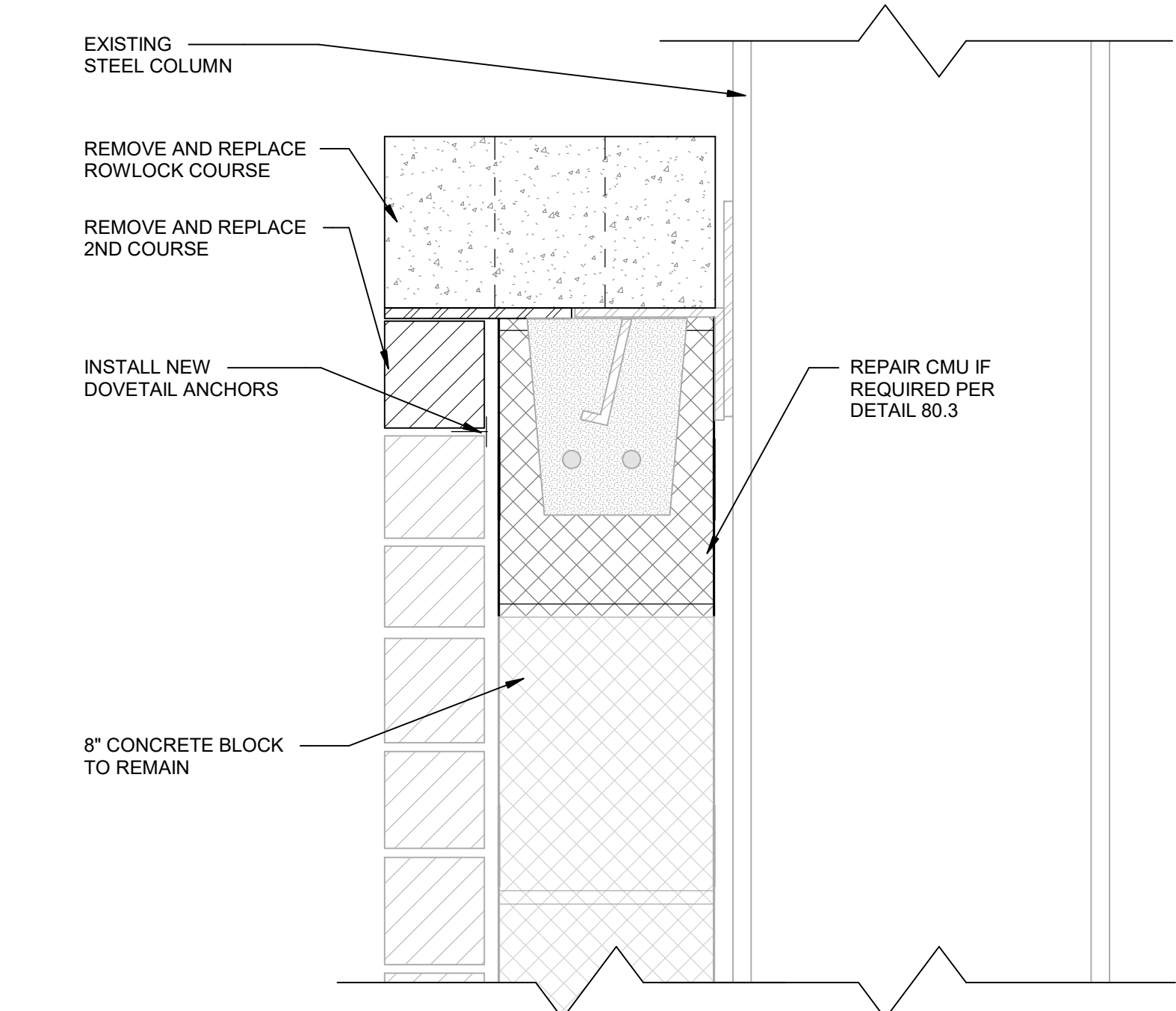
80.5 REMOVE/REPLACE PARAPET WALL



76.4 GROUT JOINT REPAIR - CORNICE



80.4 REMOVE AND REPLACE CAPSTONE/FLASHING



80.2 REMOVE AND REPLACE ROWLOCK CAP

- NOTES:**
- CONTRACTOR SHALL REMOVE ROWLOCK COURSE AND SECOND COURSE OF BRICK AND CLEAN TOP OF MASONRY WALL.
 - CONTRACTOR TO INSERT GALVANIZED DOVETAIL ANCHOR INTO MASONRY JOINT AND ATTACH TO CMU BACKUP AT 24" O.C.
 - INSTALL BRICK COURSING CONSISTENT WITH EXISTING BRICK PATTERN. TOOL JOINTS TO MATCH EXISTING.
 - APPLY SEALER TO ROWLOCK COURSING FOLLOWING MORTAR CURE.

- NOTES:**
- FLASHING SECTIONS SHALL BE ASSEMBLED WITH WATERTIGHT SEAMS TO FORM AN IMPERVIOUS BARRIER THAT ALLOWS WATER TO WEEP FROM FLASHING SURFACE.
 - BEND 1/2" EDGE PORTION OF FLASHING DOWNWARD TO FORM DRIP EDGE. DRIP EDGE TO BE HEMMED.
 - CUT AND FIELD BEND FLASHING TO ACCOMMODATE PARAPET WIDTH VARIATIONS.
 - IN THE CASE THAT FOR ANY REASON SMOOTH (NON-INTERLOCKING) SS FLASHING IS USED, CONTRACTOR MUST PROVIDE (2) SS 3/8" DIA PINS PER CAPSTONE TO LATERALLY SECURE STONES. PROVIDE APPROVED MASTIC/SEALANT WHERE PINS PENETRATE FLASHING.



Arthur G. Stading

HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET
PORTSMOUTH, NH 03801

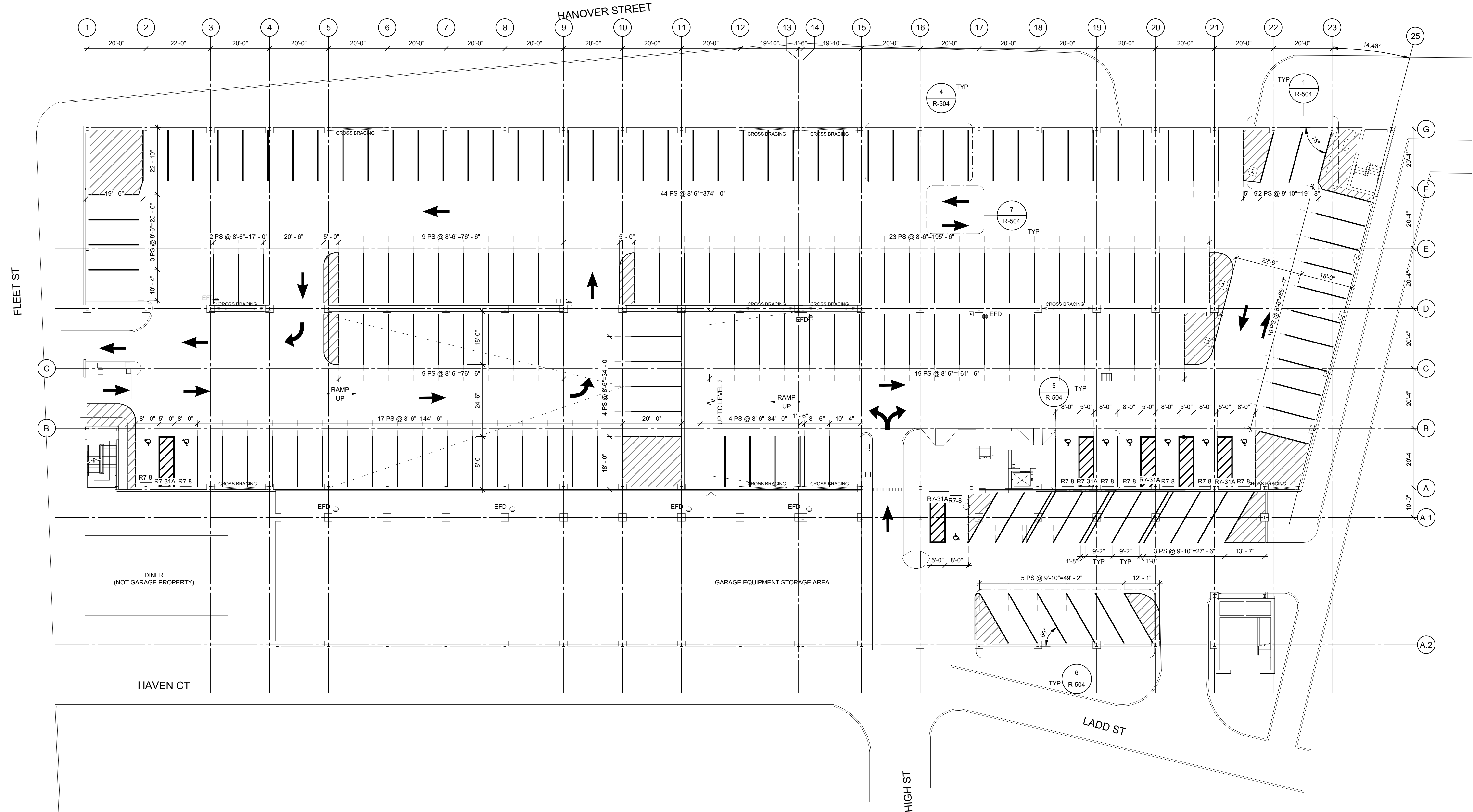
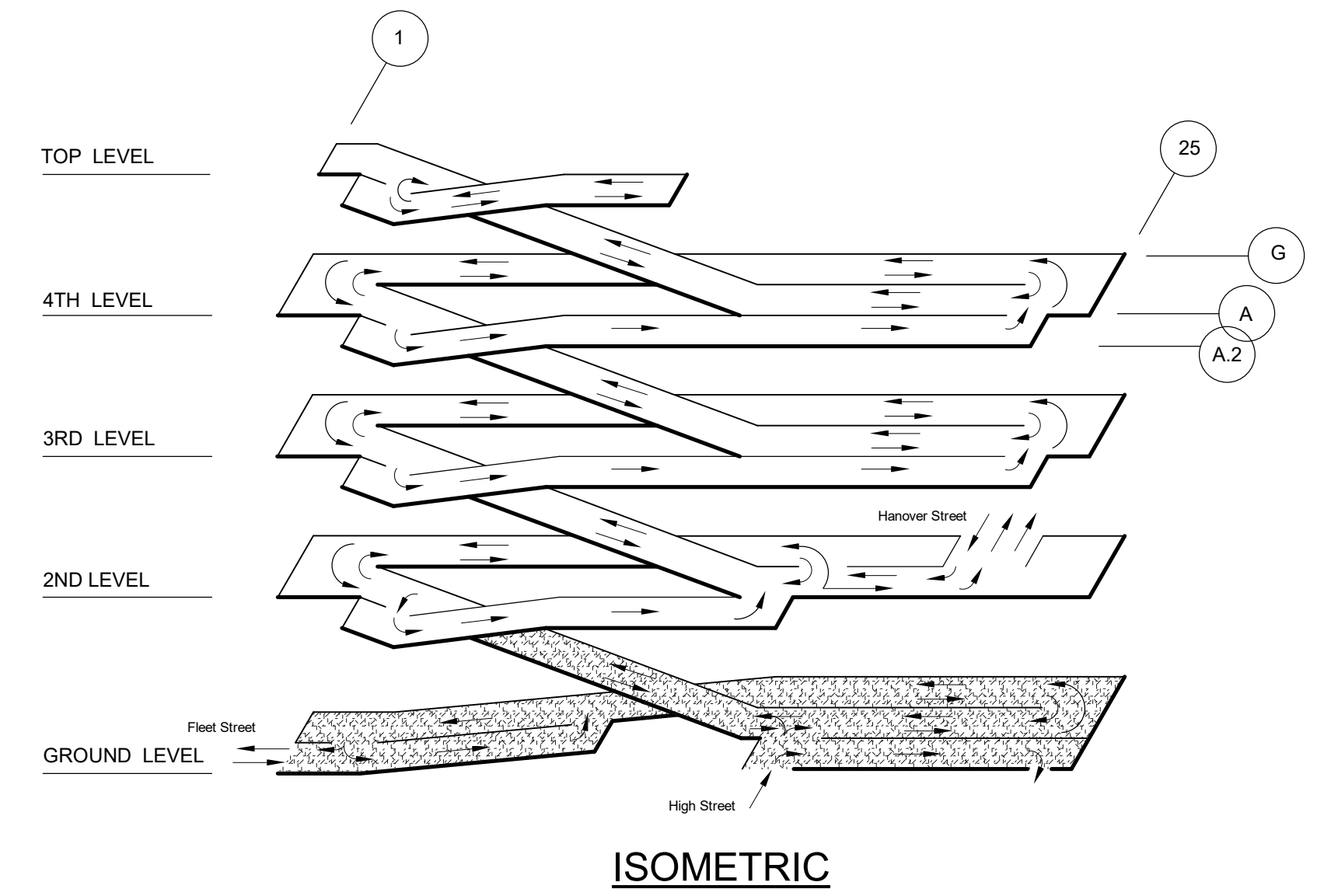
MARK	DATE	DESCRIPTION
REVISIONS		
ISSUE: BIDDING AND CONSTRUCTION		
ISSUE DATE: 1/25/2022		
PROJECT NO: 16-003129.00		
DRAWN BY: JM		
CHECKED BY: MAZ		

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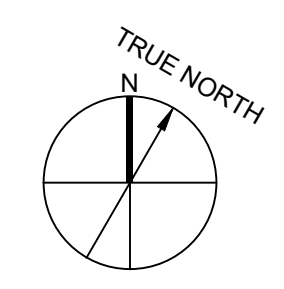
SHEET TITLE:
GROUND LEVEL STRIPING PLAN

LEVEL	STANDARD STALL	ADA STALL	ADA VAN STALL	60° STALL	75° STALL	TOTAL
GROUND TIER	148	9	0	14	2	173
SECOND TIER	138	5	0	31	0	174
THIRD TIER	175	4	0	56	2	237
FOURTH TIER	179	0	0	57	2	238
FIFTH TIER	71	0	0	0	0	71
Grand total	711	18	0	158	6	893

MARK	TEXT/MESSAGE	SIGN DETAIL	MOUNTING DETAIL	MOUNTING HEIGHT	REMARKS
R7-8	ACCESSIBLE	45.3 / R-504	45.4 / R-504	5'-0"	
R7-31A	NO PARKING	45.3.1 / R-504	45.4 / R-504	5'-0"	



1 GROUND LEVEL
1/16" = 1'-0"





Arthur G. Stacks

HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET
PORTSMOUTH, NH 03801

MARK	DATE	DESCRIPTION
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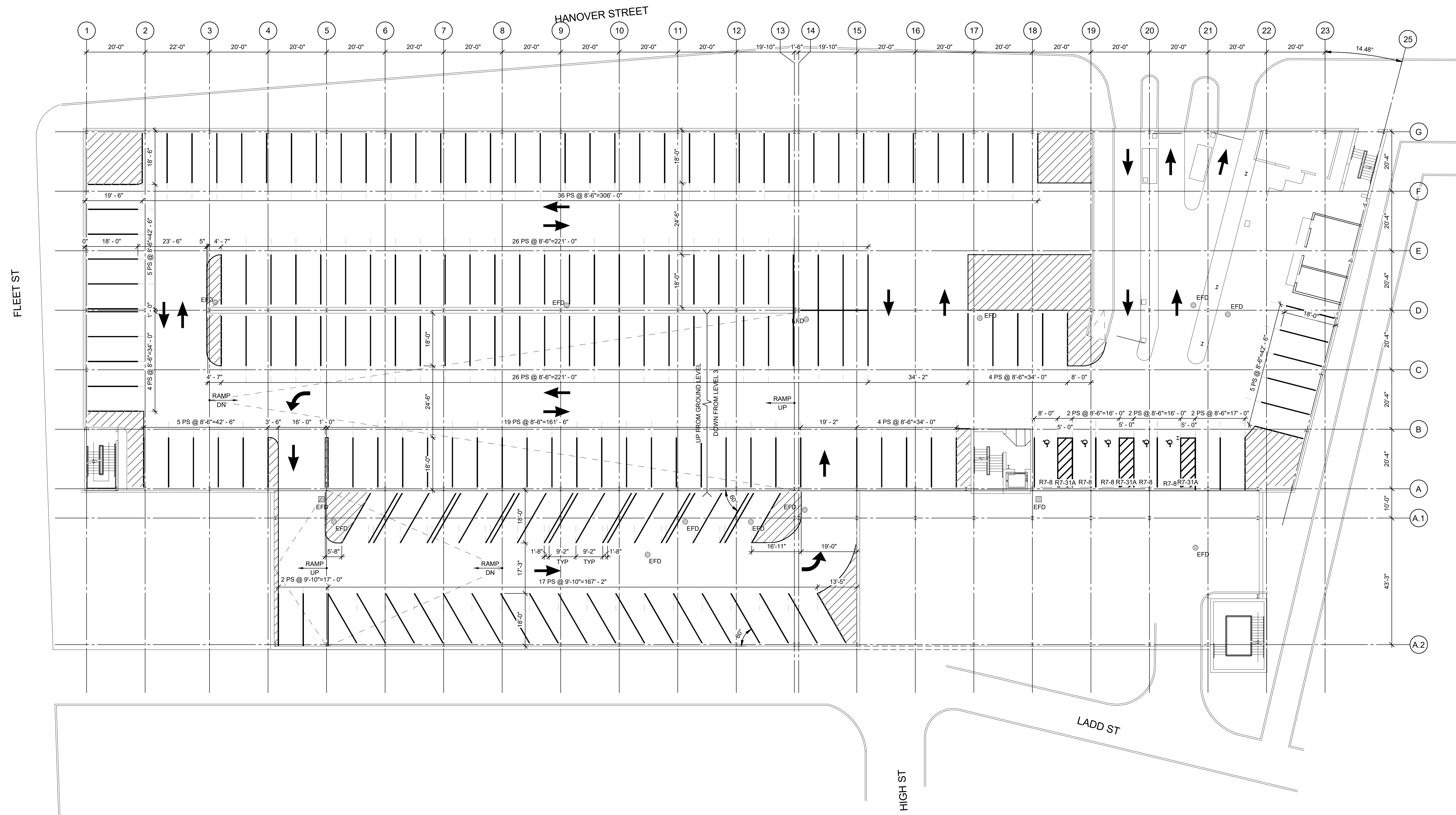
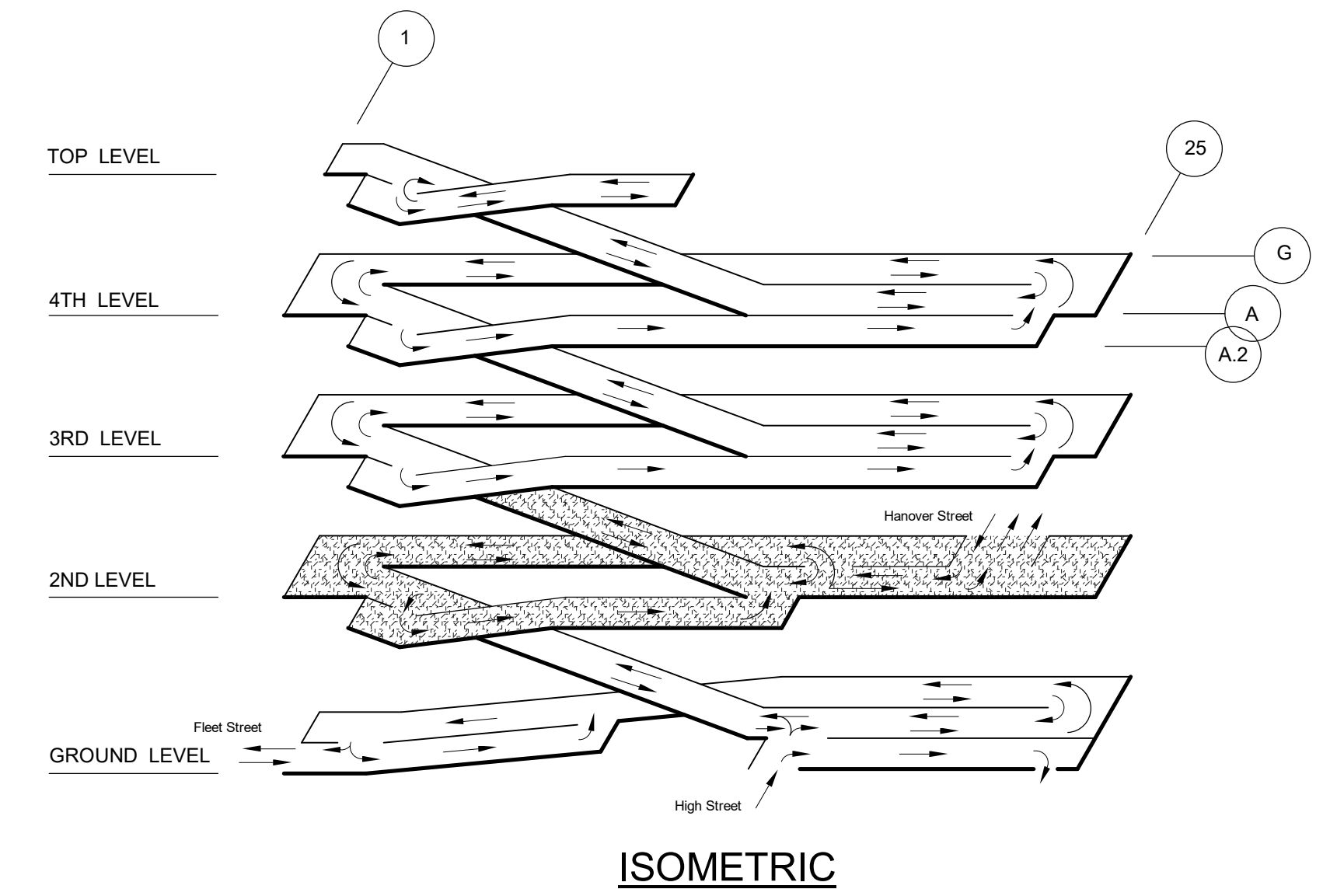
SHEET TITLE:
SECOND LEVEL STRIPING PLAN

RG-102

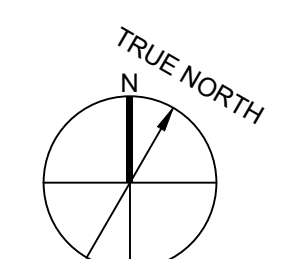
LEVEL	STANDARD STALL	ADA STALL	ADA VAN STALL	60° STALL	75° STALL	TOTAL
GROUND TIER	148	9	0	14	2	173
SECOND TIER	138	5	0	31	0	174
THIRD TIER	175	4	0	56	2	237
FOURTH TIER	179	0	0	57	2	238
FIFTH TIER	71	0	0	0	0	71
Grand total	711	18	0	158	6	893

SEE R-101 FOR STRIPING TYPICAL CALLOUT.

MARK	TEXT/MESSAGE	SIGN DETAIL	MOUNTING DETAIL	MOUNTING HEIGHT	REMARKS
R7-8	ACCESSIBLE	45.3 / R-504	45.4 / R-504	5'-0"	
R7-31A	NO PARKING	45.3.1 / R-504	45.4 / R-504	5'-0"	



1 SECOND LEVEL
1/16" = 1'-0"





Arthur G. Stadio

HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET,
PORTSMOUTH, NH 03801

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PROJECT NO: 16-003129.00		
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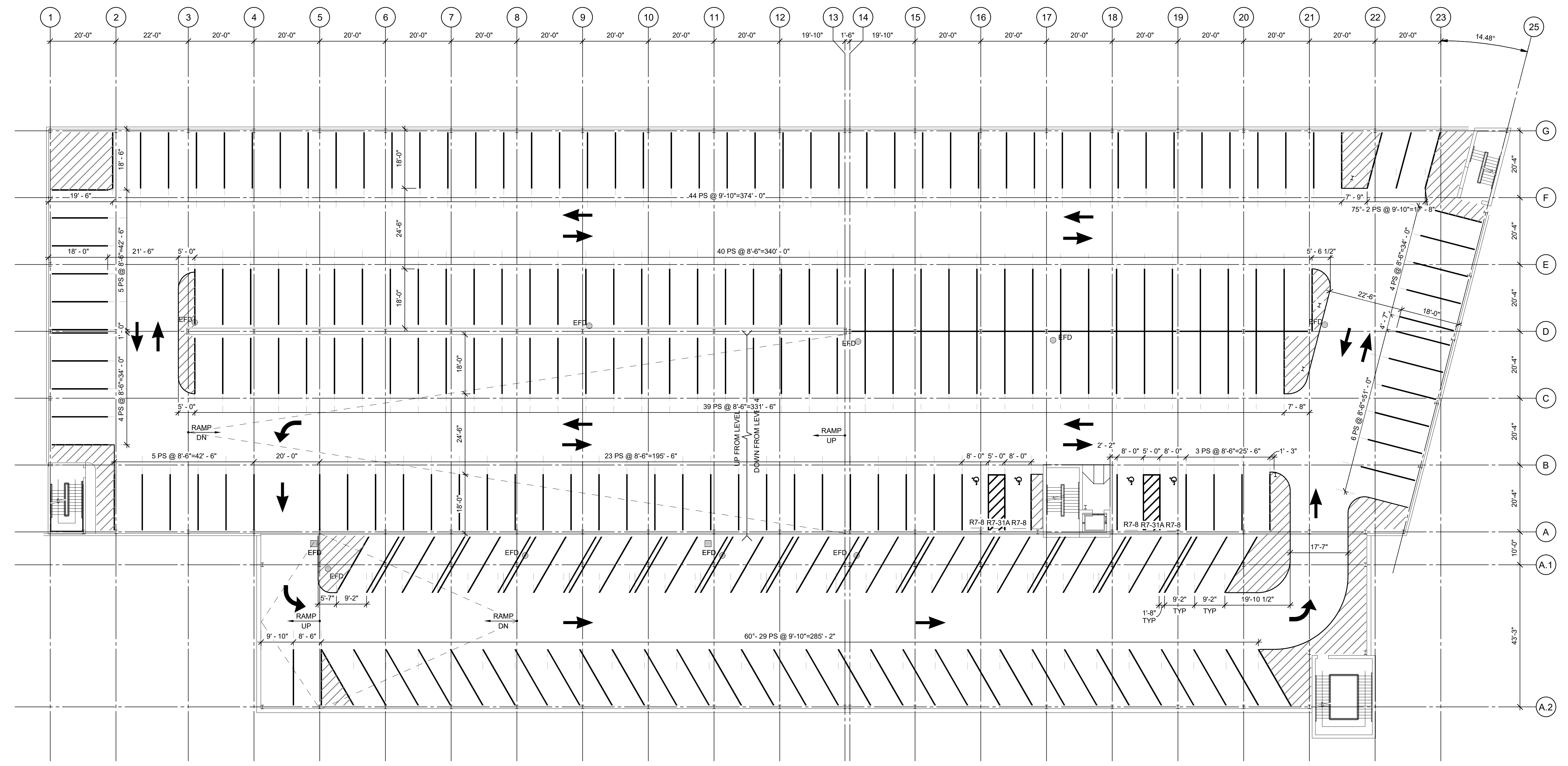
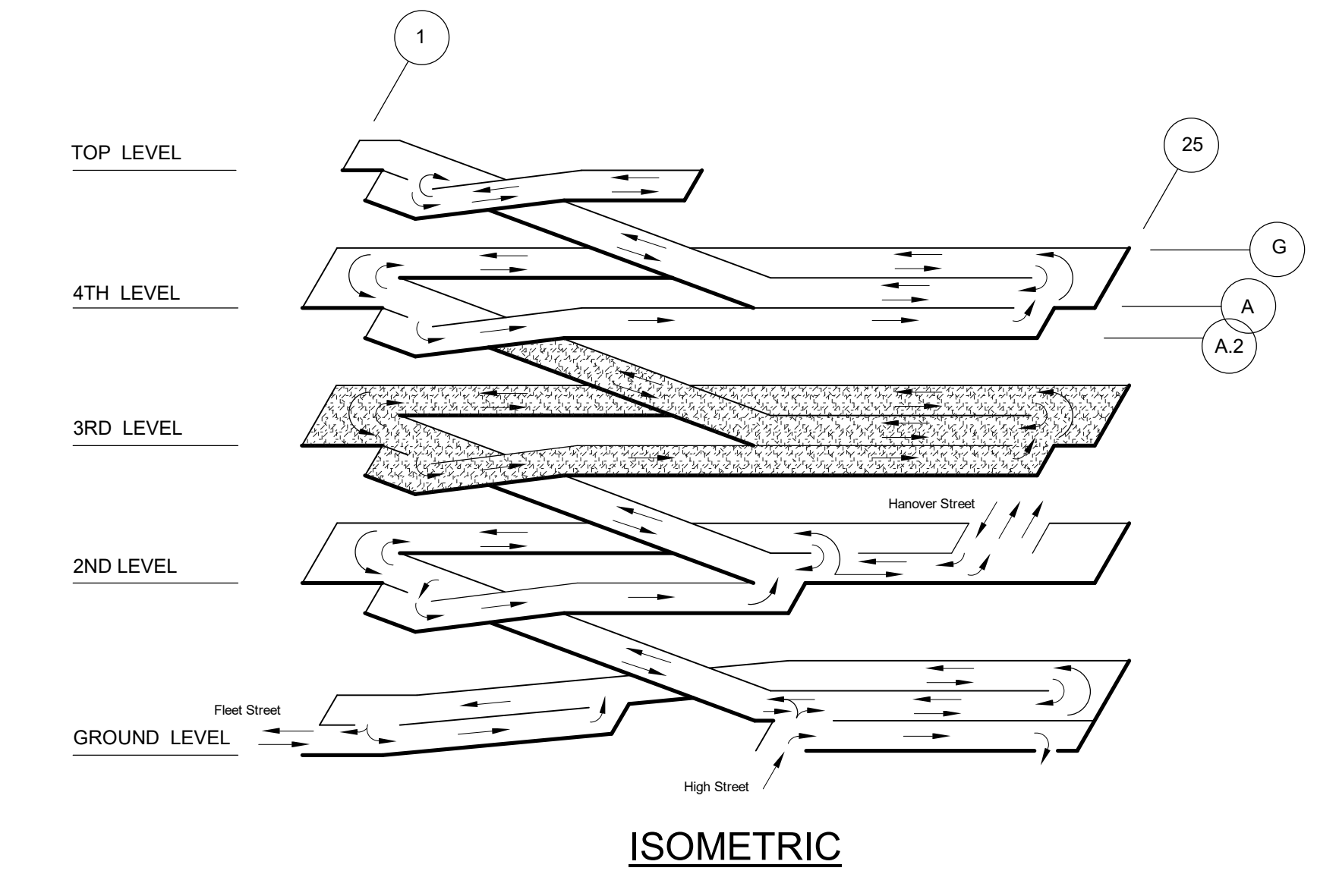
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SHEET TITLE:
THIRD LEVEL STRIPING PLAN

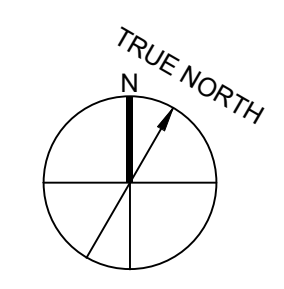
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GROUND TIER	148	9	0	14	2	173
SECOND TIER	138	5	0	31	0	174
THIRD TIER	175	4	0	56	2	237
FOURTH TIER	179	0	0	57	2	238
FIFTH TIER	71	0	0	0	0	71
Grand total	711	18	0	158	6	893

SEE R-101 FOR STRIPING TYPICAL CALLOUT.

MARK	TEXT/MESSAGE	SIGN DETAIL	MOUNTING DETAIL	MOUNTING HEIGHT	REMARKS
R7-8	ACCESSIBLE	45.3 / R-504	45.4 / R-504	5'-0"	
R7-31A	NO PARKING	45.3.1 / R-504	45.4 / R-504	5'-0"	



1 THIRD LEVEL
1/16" = 1'-0"





Arthur G. Stading

HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET
PORTSMOUTH, NH 03801

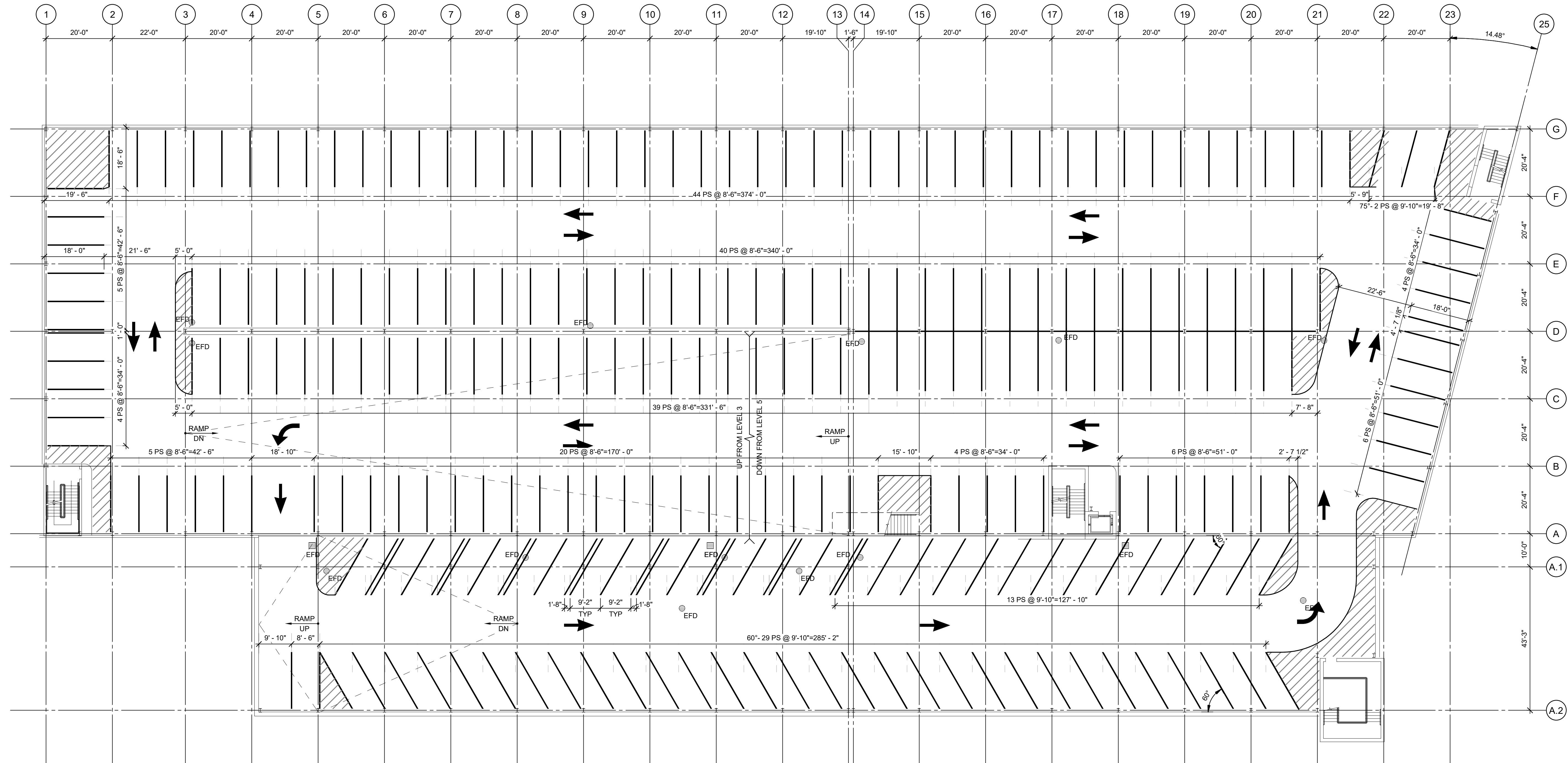
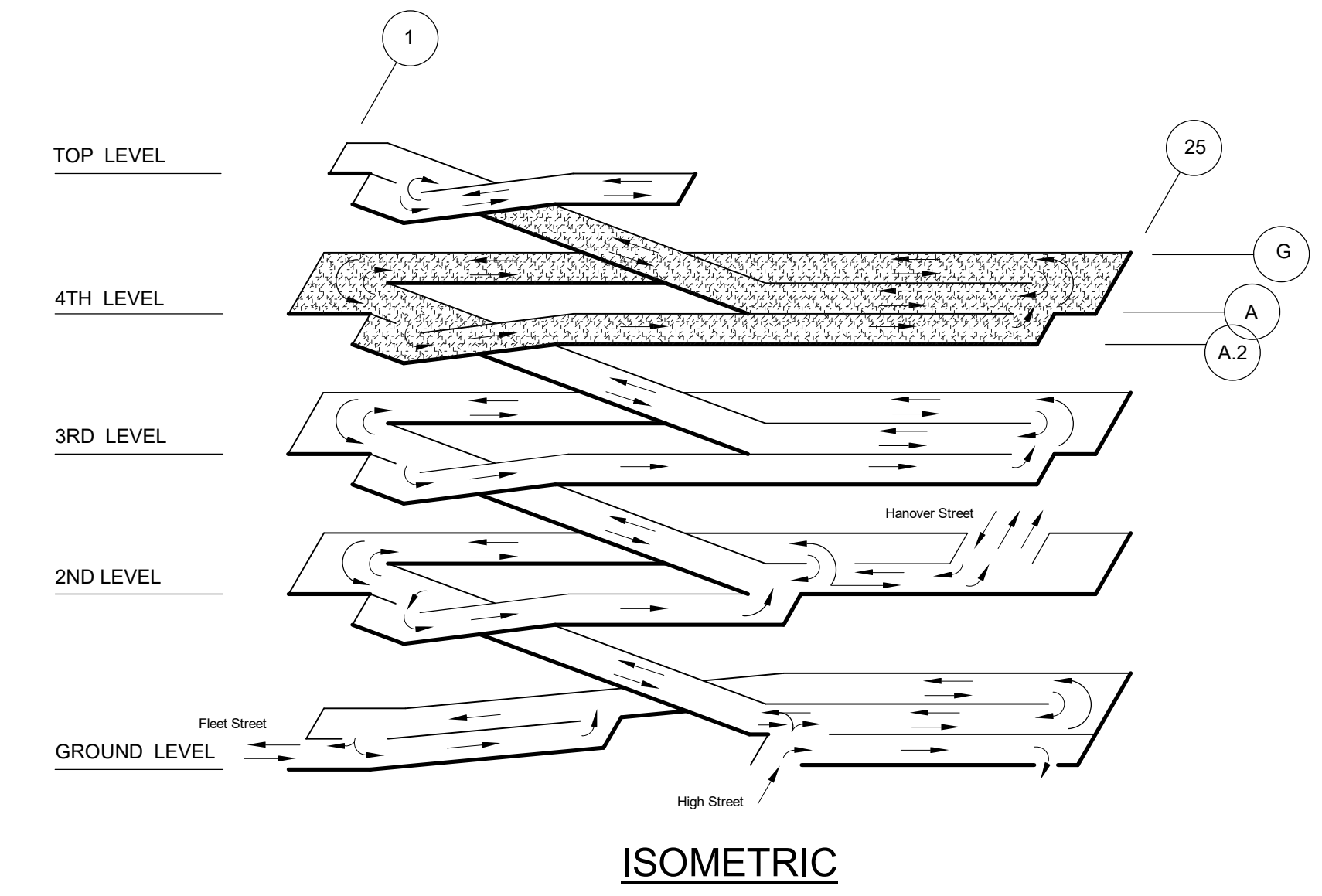
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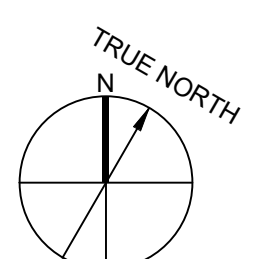
SHEET TITLE:
FOURTH LEVEL STRIPING PLAN

CAR COUNT SCHEDULE						
LEVEL	STANDARD STALL	ADA STALL	ADA VAN STALL	60° STALL	75° STALL	TOTAL
GROUND TIER	148	9	0	14	2	173
SECOND TIER	138	5	0	31	0	174
THIRD TIER	175	4	0	36	2	237
FOURTH TIER	179	0	0	57	2	238
FIFTH TIER	71	0	0	0	0	71
Grand total	711	18	0	158	6	893

SEE R-101 FOR STRIPING TYPICAL CALLOUT.

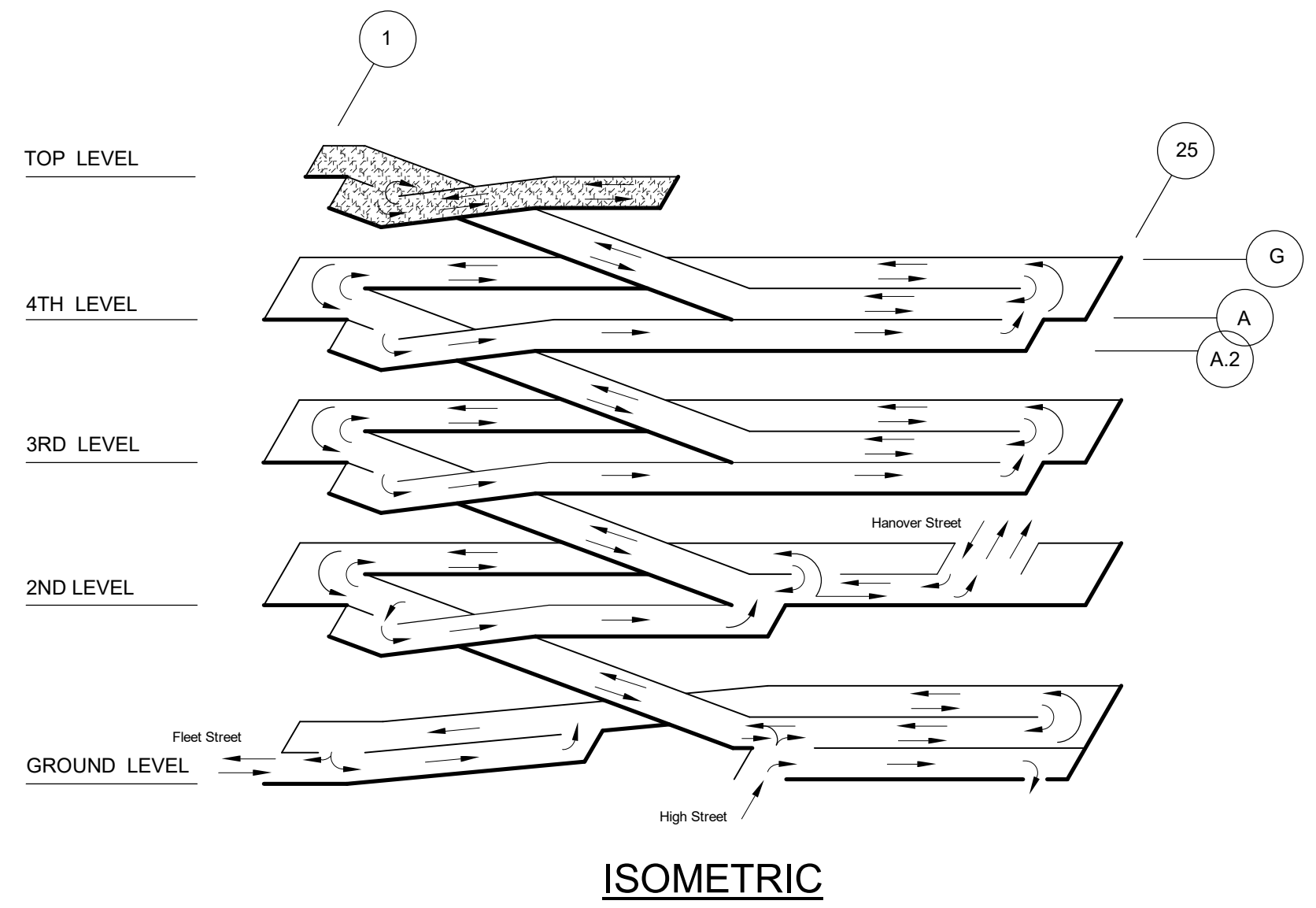


1 FOURTH LEVEL
1/16" = 1'-0"

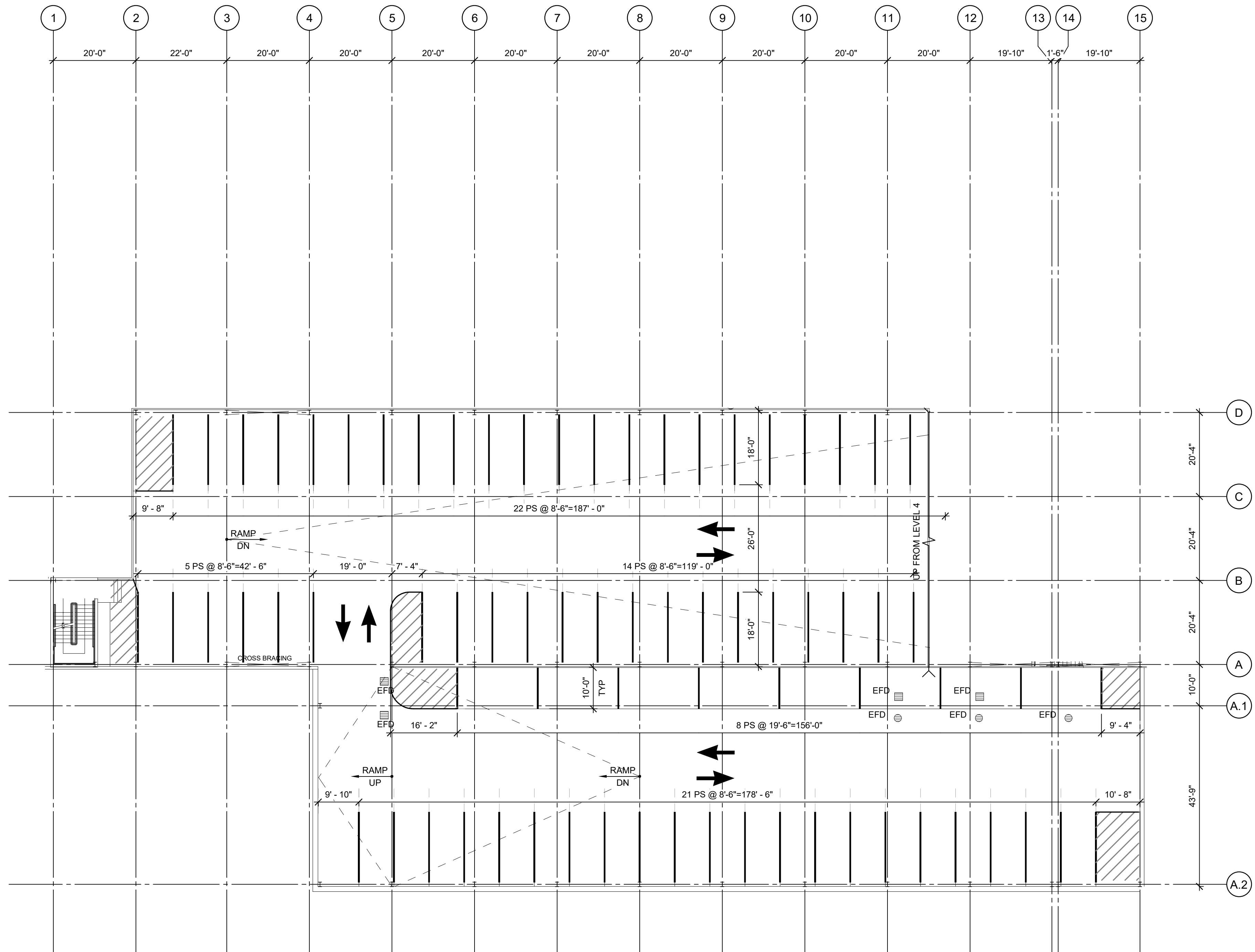


CAR COUNT SCHEDULE						
LEVEL	STANDARD STALL	ADA STALL	ADA VAN STALL	60° STALL	75° STALL	TOTAL
GROUND TIER	148	9	0	14	2	173
SECOND TIER	138	5	0	31	0	174
THIRD TIER	175	4	0	56	2	237
FOURTH TIER	179	0	0	57	2	238
FIFTH TIER	71	0	0	0	0	71
Grand total	711	18	0	158	6	893

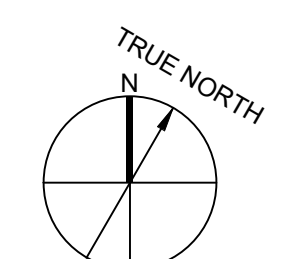
SEE R-101 FOR STRIPING TYPICAL CALLOUT.



20 Park Plaza, Suite 1202
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1 FIFTH LEVEL
1/16" = 1'-0"



HANOVER STREET GARAGE RESTORATION

34 HANOVER STREET
PORTSMOUTH, NH 03801

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SHEET TITLE:
FIFTH LEVEL STRIPING PLAN